

**ITEM 4 POST EXHIBITION - WEST DAPTO CENTRES MASTER PLAN**

The West Dapto Vision 2018 identifies the need to plan for three Town Centres and five Village Centres within the West Dapto Urban Release Area (WDURA).

The Master Planning Project has been led by Council to guide the delivery of some of these centres. The project is funded by the NSW Department of Planning, Housing and Infrastructure from a grant issued under the Regional Housing Strategic Planning Fund.

The centres which are the focus of this project are -

- Future Fowlers Village Centre, Cleveland Road, Stage 3 WDURA.
- Future Marshall Mount Town Centre, Corner Marshall Mount and Yallah Roads, Stage 5 WDURA.

This report outlines the nature of issues raised in public submissions following exhibition of a draft West Dapto Centres Urban Design report from 5 February to 4 March 2024. It is recommended that Council adopt the final West Dapto Centres Urban Design report and commence the necessary review of the Wollongong Development Control Plan and Local Environmental Plan to ensure implementation of the intended outcome for the centres.

**RECOMMENDATION**

- 1 The West Dapto Centres Urban Design Report (Attachment 2) be adopted.
- 2 The draft Wollongong Development Control Plan, 2009 - Chapter D16 – West Dapto Release Area (Attachment 3) be exhibited for a minimum 28 days.
- 3 Council staff commence further detailed review of Wollongong Development Control Plan 2009 and Wollongong Local Environmental Plan, 2009 to ensure relevant environmental planning instrument provisions are consistent with the intended outcomes of the Centres Master Plans.

**REPORT AUTHORISATIONS**

Report of: Chris Stewart, Manager City Strategy

Authorised by: Linda Davis, Director Planning + Environment - Future City + Neighbourhoods

**ATTACHMENTS**

- 1 Summary of Submissions
- 2 West Dapto Centres Urban Design Report
- 3 Draft Amendments to Chapter D16 - West Dapto Release Area of Wollongong DCP 2009

**BACKGROUND**

West Dapto Urban Release Area (WDURA) will be serviced by eight centres over time. Three new towns centres known as Bong Bong, Darkes and Marshall Mount and five new village centres referred to as Wongawilli, Jersey Farm, Fowlers, Huntley and Avondale.

The three Town Centre Principles of the West Dapto Vision, 2018, which are also embedded in Section 11, Chapter D16 of Wollongong Development Control Plan (DCP) 2009, relate to -

- Hierarchy.
- Movement Sensitive.
- Diversity and Identity.

To ensure the West Dapto Vision Centre Principles are met, the planning undertaken for our future centres needs to be holistic and not on an individual land holding basis.

Council staff identified an opportunity to lead the master planning of two centres as a pilot through the (then) NSW Department of Planning and Environment (DPE) Regional Housing Strategic Planning Fund Program (RHSPF). Staff lodged an application for grant funding under the RHSPF in September 2022. DPE notified Council of grant funding approval in December 2022.

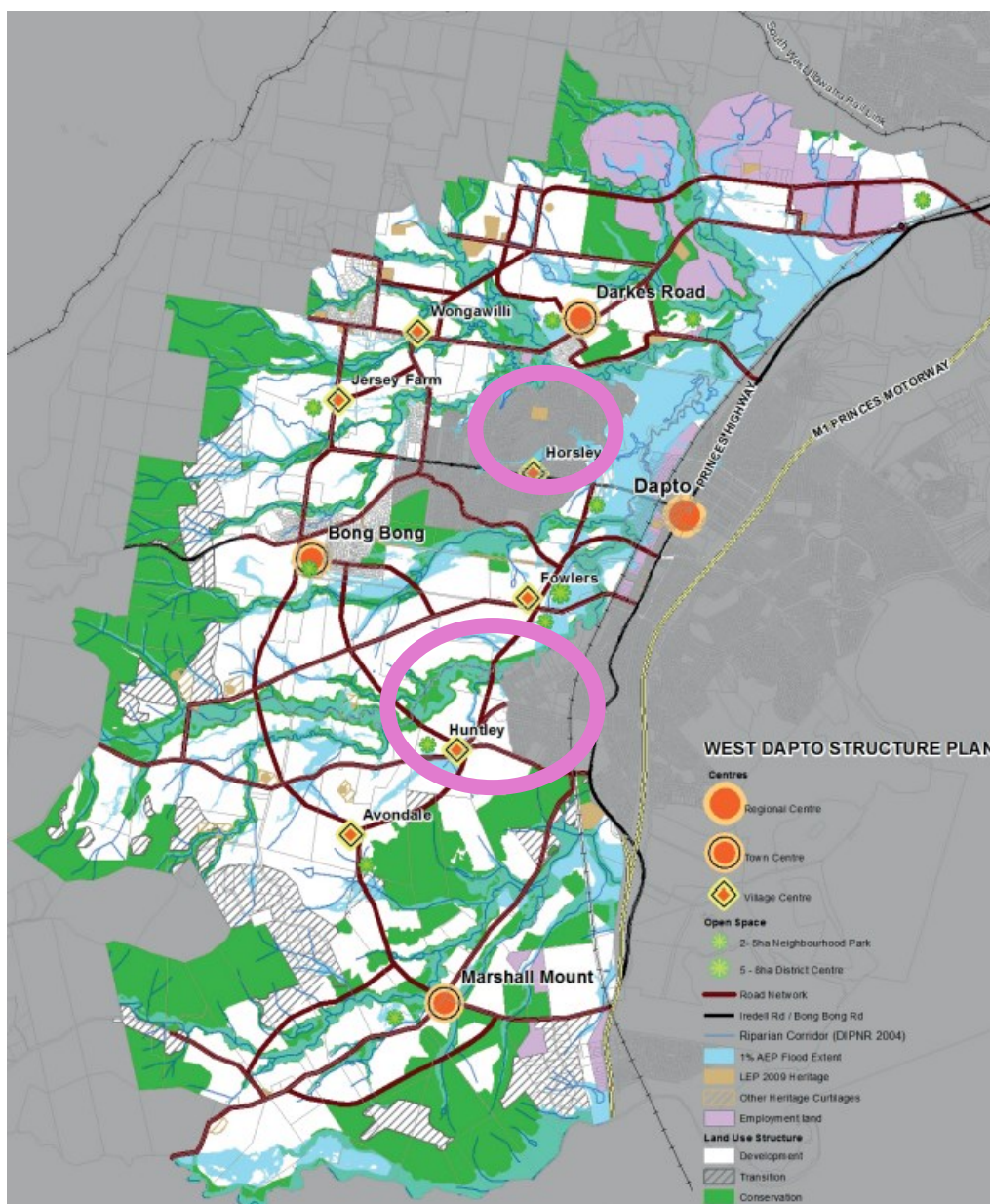
On 20 January 2023, Council entered into a funding agreement with DPE allocating \$250,000 toward the project. Council has contributed in-kind staff time across several teams towards the project.

In April 2023, Council staff engaged Urbis Consulting to provide urban design and stakeholder engagement services to assist with the project.

The project has three key aims -

- 1 Implement the West Dapto Vision for centres and represent, through master planning, the collective vision of West Dapto stakeholders for the future -
  - a Fowlers Village Centre.
  - b Marshall Mount Town Centre sites.
- 2 Enable the implementation of the master plans via amendment to Chapter D16, West Dapto Release Area, of Wollongong DCP 2009, including incorporation of the master plans into relevant Neighbourhood Plans. This step of the process will be subject to a separate Council report.
- 3 Demonstrate a best practice approach to delivering Centre master plans which will inform Master Planning and Neighbourhood planning requirements for future remaining centres, and an appropriate update to Chapter D16 West Dapto Release Area of Wollongong DCP 2009.

The West Dapto Vision, 2018 Structure Plan is shown below with the two pilot centre locations circled (in pink).



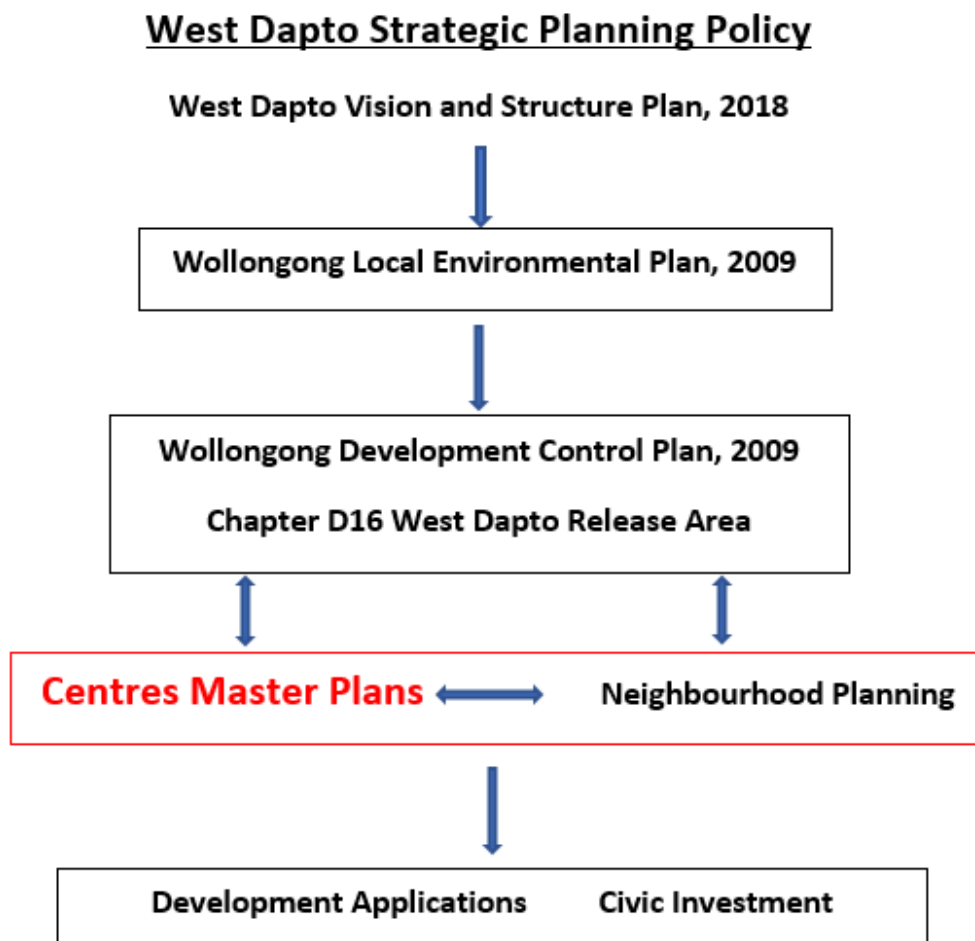


On 11 December 2023 Council adopted staff recommendations to progress a draft Centres Master Plan package to public exhibition for a minimum of 28 days. The draft Centres Master Plan package (Urban Design Report) public exhibition was held between 5 February 2024 and 4 March 2024. Further detail regarding the exhibition is provided in the proposal and consultation sections of this report.

## PROPOSAL

The flow chart below illustrates how Council staff propose the Centres Master Planning project fits within the West Dapto Strategic Planning Context.

### West Dapto Strategic Planning Context and Guiding Documents: How does this project fit?



The final proposed Centres Master Plan package consists of the Urban Design Report (Attachment 2) and the draft Amendments to Chapter D16 – West Dapto Release Area of Wollongong Development Control Plan 2009 (Attachment 3).

The Urban Design Report at Attachment 2 has been guided by existing technical information available to Council and the project consultant Urbis. This has included understanding economic feasibility, flooding, traffic impacts / management and analysis, riparian corridors, vegetation conservation, heritage and infrastructure requirements, including social infrastructure. The Urban Design Report has also been finalised with consideration given to issues raised in submissions received during public exhibition of the draft.

Council staff acknowledge that final development outcomes for the Centres will be guided by further technical studies and analysis at subsequent Neighbourhood Plan and development application stages where relevant.

## Summary exhibition issues and resulting proposed changes to Master Plans at Attachment 2 following exhibition.

A detailed staff response to exhibition submissions is provided at Attachment 1. A summary of major themes raised in submissions for Marshall Mount is provided in the table below. One submission was received in relation to Fowlers Village Centre. That submission suggested that infrastructure for centres should be build first before populations increase.

A summary of master plan outcomes for Marshall Mount and Fowlers Village are provided following the submission summary table. A revised West Dapto Centres post exhibition Urban Design Report is provided at Attachment 2.

Exhibition Theme Summary	Staff Summary Response	Final Master Plan Package Change
<b>Open Space</b> Some opposition to “proposed open space buffer” bordering Duck Creek at Marshall Mount. Concern that the buffer would result in underutilisation of commercial land.	<p>Council information indicates that the proposed open space buffer on the eastern side of the E1 zone is affected by the 1% Annual Exceedance Probability flood and contains areas of existing endangered ecological communities (EEC).</p> <p>The Master Plan provides a conceptual layout. Refinements would be given merit consideration only where they are supported by detailed assessments as part of future Development Applications. Any refinement would still need to demonstrate consistency with the overall guiding principles in the Master Plan objectives.</p>	No change proposed.
<b>Riparian Corridor, Biodiversity Values and Natural Areas</b> Concern regarding basis of Core Riparian Zone identification and high value biodiversity land shown on the draft master plan. Concern that using the MU1, mixed use zoned land for buffer is a duplication of adjoining C3, environmental management zoned land.	<p>The master plans are based on baseline constraints information available to Council staff and project consultant.</p> <p>The Master Plan provides a conceptual layout. Refinements would be given merit consideration only where they are supported by detailed assessments as part of future Development Applications. Any refinement would still need to demonstrate consistency with the overall guiding principles in the Master Plan objectives.</p>	No change proposed.
<b>Commercial Feasibility</b> Concern regarding the feasibility testing of the proposed master plans and misalignment with the centres hierarchy identified in Council’s strategic documents.	<p>Preparation of the Master Plan has considered a range of land uses, built forms and outcomes, balancing commercial outcomes, planning controls (e.g. height, FSR), other outcomes along with site constraints.</p> <p>Alternative outcomes may be considered noting they are required to be consistent with the overall guiding principles in the Master Plan.</p>	Clarifications made regarding the guiding nature of the master plans

Exhibition Theme Summary	Staff Summary Response	Final Master Plan Package Change
<p><b>Planning Controls – Local Environmental Plan and Development Control Plan</b></p> <p>Comments ranged from individual planning proposal considerations to broader master plan relevant comments. In relation to the master plans concern centred around the need to ensure the master plans utilise the LEP governed permissible outcomes, including height and FSR for example.</p>	<p>Review of LEP and DCP provisions will be undertaken by staff as part of a separate, subsequent process to ensure master plan proposed outcomes can be fully realised.</p> <p>It is also important to note that the Master Plans provide a conceptual layout. Refinements would be given merit consideration only where supported by detailed assessments as part of future Development Applications.</p> <p>Council staff are also aware of the SEPP Housing 2021 affordable housing bonus provisions. Any subsequent review of LEP provisions following adoption of the master plan package would need to be undertaken with consideration made to those affordable housing bonuses.</p> <p>Any refinement would still need to demonstrate consistency with the overall guiding principles in the Master Plan objectives.</p>	<p>Changes and recommendations proposed to more closely align with LEP objectives.</p>
<p><b>Traffic and Transport</b></p> <p>Concern regarding road widths and developable area reductions have been raised along with specific intersection treatments and wider M1 network interactions.</p>	<p>The proposed master plans are consistent with Council's road design standards as set out in Chapter B2 of Wollongong DCP, 2009. Council staff will continue to consider intersection and pedestrian crossing treatments as part of ongoing detailed design stages. Council staff will continue to work closely with Transport for NSW counterparts regarding the connection of the West Dapto Urban Release Area, including its centres, and the wider regional road network, including M1.</p>	<p>Clarification regarding the need for final intersection and pedestrian treatment arrangements to be subject to detailed traffic analysis as a project level.</p>
<p><b>Master Plan proposed land use locations</b></p> <p>Some concern regarding specific uses such as childcare and car parking locations. General concern that the Master Plan should not limit land uses governed by the LEP.</p>	<p>The Master Plans illustrate the preferred town centre outcome and need for childcare facilities.</p> <p>The preferred land use outcome is dependent upon developers progressing proposals of this nature. Alternative outcomes may be considered against the overall guiding principles in the Master Plan.</p>	<p>No change proposed.</p>



Exhibition Theme Summary	Staff Summary Response	Final Master Plan Package Change
<p><b>Development of the Master Plan and Supporting Studies</b></p> <p>Several individual specific comments and general observations ranging from the need to advance technical studies to support the Master Plan proposed outcomes, the need to master plan other West Dapto Centres, the need to deliver essential infrastructure first to reference to the usefulness of specific current DA information.</p>	<p>As noted previously The Master Plans do not propose to rezone land. The Master Plan will not predetermine the outcome of development assessments that would still be required through future Development Applications. Rather it provides a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.</p> <p>Any refinement would still need to demonstrate consistency with the overall guiding principles in the Master Plan objectives.</p>	<p>No change proposed.</p>
<p><b>Stormwater and Flooding</b></p> <p>Suggestion that a proposed basin at Marshall Mount should not duplicate the West Dapto Development Contributions Plan proposed enhanced storage area and should not necessarily create heritage impacts. In addition, there was some concern raised regarding flood analysis that informed the master plans.</p>	<p>The proposed basin is indicative only. The need is based on servicing peak flows from the Town Centre in addition to the broader function of the enhanced storage area.</p> <p>The master plans are based on baseline constraints information available to Council staff and project consultant.</p>	<p>Clarification noting that basin/s shown are indicative only and the exact size and location would be subject to project specific analysis. This would include detailed impact assessment.</p>
<p><b>Community Facilities</b></p> <p>Concern regarding the proposed community facilities at Marshall Mount ranging from the proposed location through to the proposed scale.</p>	<p>Council staff note the draft West Dapto Development Contributions Plan post exhibition Council report (18 March 2024) recommended change in size of CF05 facility from the publicly exhibited subdistrict scale to local sized facility.</p> <p>Council staff support continued planning for community facilities consistent with the adopted West Dapto Social Infrastructure Needs Assessment, 2023 and Council endorsed draft Development Contributions Plan, 2024.</p>	<p>Recommended change in size of Marshall Mount Community Facility (CF05) from the publicly exhibited subdistrict scale to local sized facility.</p>
<p><b>Shared Paths</b></p> <p>Suggestions regarding specific location of the proposed shared path in relation to Duck Ck and suggestion that paths be indicative. Also concern that duplicated pedestrian bridge provision will be expensive.</p>	<p>The Master Plan's proposed shared pathways provide an indication of the movement and access corridors and infrastructure required through the site. The shared paths are a conceptual layout that can be refined based on detailed assessments and design as part of future Development Applications.</p> <p>Any refinement would still need to demonstrate consistency with the overall guiding principles in the Master Plan objectives.</p>	<p>No changes proposed</p>

Exhibition Theme Summary	Staff Summary Response	Final Master Plan Package Change
<p><b>Aboriginal Heritage</b></p> <p>The need for detailed understanding of Aboriginal heritage and potential impacts prior to development applications within the centres. This includes a desire to minimise impacts on heritage in the vicinity of the Fig Tree, proposed community facility and Western Ring Road at Marshall Mount.</p>	<p>Council staff agree that further Aboriginal Heritage impact investigations and engagement with Aboriginal stakeholders would be required as specific projects are planned further.</p> <p>The Master Plan will not predetermine the outcome of environmental assessments that would still be required through future Development Applications. Rather it provides a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.</p>	<p>The Master five guiding principles maintains the principle Celebrate Country. In addition to the final Master Plan, Council staff consider there is merit in considering how the Aboriginal Heritage values can be celebrated in any future community facility at Marshall Mount.</p>
<p><b>Underutilisation of MU1 and E1 zones</b></p> <p>A few submissions raised concern regarding the underutilisation of the commercial MU1, Mixed Use and E1, Local Centre zones. Specific references were made to open space (buffer open space) and at grade car parking.</p>	<p>It is important to note that the Master Plans provide a conceptual layout. Refinements would be given merit consideration only where supported by detailed assessments as part of future Development Applications.</p> <p>Any refinement would still need to demonstrate consistency with the overall guiding principles in the Master Plan objectives.</p>	<p>No changes proposed</p>
<p><b>Processes</b></p> <p>Concern raised regarding how the two centres were selected and also seeking more clarity on associated Neighbourhood / Concept DA process in relation to the NPs and further clarification of how the project is meeting its grant funded objectives.</p>	<p>The two centres were selected following consultation with the West Dapto Steering Committee. One village and one town were targeted as pilots. The two selected centres are yet to be Neighbourhood Planned. The other two Towns in West Dapto both fall within the footprint of Neighbourhood Planned sites. In the case of Marshall Mount the town is subject to fragmented ownership when compared to the single owner Towns within West Dapto.</p> <p>Council staff are required to provide ongoing reporting to the grant fund administrator, NSW Department of Housing and Infrastructure, regarding the project progress against grant funding objectives.</p>	<p>No changes to Master Plans proposed. Attachment 3 of this Council report includes proposed draft DCP amendments adding further clarity of how the master plans relate to Neighbourhood Planning / concept DAs.</p>
<p><b>Various individual matters</b></p>	<p>Council staff have provided a response to individual raised matters in Attachment 1.</p>	<p>No changes proposed.</p>

## Proposed Final Master Plan Outcomes

Both Marshall Mount and Fowlers Master Plans include -

- A Vision Statement and Concept Master Plans.
- Guiding Principles.
- Structure Plan and Development Summary.
- Layered Strategies -
  - Natural Assets.
  - Access and Movement.
  - Centre Activation.
  - Housing Mix.
  - Built Form.

Five guiding principles have been identified for the Marshall Mount Town and Fowlers Village Centres which bring together the planning context, place analysis and development opportunities to underpin the master plans –

- 1 Celebrate Country.
- 2 A Connected and Accessible Centre.
- 3 An Activated Place and Viable Centre.
- 4 Deliver Varied Housing Offer Close to Amenities.
- 5 Contextual Built Form Response.

Council staff have considered potential housing outcomes for each centre to address housing supply and choice needs of the community. Where housing is proposed within each Centre there would be a need to ensure appropriate Local Environmental Plan (LEP) and Development Control Plan (DCP) provisions support that housing outcome. Any controls and standards considerations would be part of a separate report to Council as acknowledged in the recommendation section of this report.

Since the time of exhibition there has been an increased focus on affordable housing delivery. This is apparent from the NSW Government's recently announced bonuses for affordable housing via State Environmental Planning Policy (Housing) 2021. The housing opportunities within the two master planned centres and subsequent review of LEP provisions should be undertaken with consideration given to the SEPP Housing bonuses.

### Marshall Mount

The Vision for Marshall Mount Town Centre is -

*"A compact mixed retail and community precinct with housing choice, conveniently located, and accessible by active and public transport. It is characterised by a vibrant main street that balances pedestrian and vehicular movements, and local places for people that celebrate views to the scenic Illawarra Escarpment and the existing Moreton Bay Fig Tree."*

The final Master Plan is expected to yield -

- 8,100 m<sup>2</sup> of retail space, including supermarket, convenience retail and hospitality retail. This is an increase from the 7,604 m<sup>2</sup> presented in the exhibition draft Master Plan. The increase is due to the reduction in community facility size as described below.
- 2,500+ m<sup>2</sup> of community facilities space. This is reduced from the draft Master Plan 3,090 m<sup>2</sup> to reflect a "local" scale consistent with the ongoing review of the draft West Dapto Development Contribution Plan, 2024
- 380m of active street frontage. Remains unchanged from draft Master Plan.



- Potential for 120 new childcare spaces (subject to developer interest) and potential for 343 dwellings. Prior to the Centre Master Planning project previous planning exercises for Stage 5 of WDURA have not assumed residential outcomes within the Town Centre (dwellings would be subject to supporting LEP planning controls). Remains unchanged from draft Master Plan.

A post exhibition Illustrative Master Plan image is provided below (Figure 1). Refer to Attachment 2 for full detail of the proposed Master Plan, including proposed Town Centre Structure Plan.



Figure 1 - Illustrative Master Plan image for Marshall Mount Town Centre

## Fowlers Village

The Vision for Fowlers Village is -

*“A compact and local scale retail precinct, conveniently located for daily needs and accessible via public and active transport. Characterised by a Main Street which is aligned to capture views to Mount Keira and Mount Kembla, it celebrates place and balances pedestrian and vehicle movement.”*

The final Master Plan is expected to yield -

- 2,785 m<sup>2</sup> of retail space, including supermarket, convenience retail and hospitality retail. No change from draft Master Plan.
- 90m of active street frontage. No change from draft Master Plan.
- Potential for 60 new childcare spaces (subject to developer interest) and 29 dwellings. Previous planning exercises for Stage 3 of WDURA have not assumed residential outcomes within the Centre (dwellings would be subject to supporting LEP planning controls). No change from draft Master Plan.

A post exhibition Illustrative Master Plan image is provided below (Figure 2). Refer to Attachment 2 for full detail of the proposed Master Plan, including proposed Village Centre Structure Plan.





Figure 2 - Illustrative Master Plan image for Fowlers Village

## Further Analysis

The post exhibition Master Plans illustrate and guide the intended outcome for the centres. It is important to note that the Master Plans would not replace the need for a Neighbourhood Plan (or Concept Development Application) and associated Neighbourhood Plan level analysis. More analysis would be required prior to Development Application stage. Council is yet to adopt a Neighbourhood Plan relevant to each of the two centres. However, Council staff are currently assessing a draft proposed Neighbourhood Plan for “East Cleveland”. The Fowlers village is located within the proposed East Cleveland Neighbourhood Plan site area. If the recommendations of this report are adopted by Council, the Master Plan should be incorporated into any adopted Neighbourhood Plan for the Cleveland East precinct.

Some of the residential outcomes proposed in both Marshall Mount Town Centre and Fowlers Village Centre would rely on subsequent amendment to Wollongong LEP. Council staff propose a two-phase process. The recommendations of this Council report are Phase 1. The Second Phase is expected to commence later in 2024, when a detailed review of LEP standards and DCP controls will be undertaken.

## Proposed amendments to Wollongong DCP 2009, Chapter D16 West Dapto Release Area to implement the Master Plans.

As mentioned above, a second phase to the project is proposed to deal with any enabling LEP amendments and draft DCP controls. That second phase will be pursued later in 2024, subject to endorsement of this report. Detailed implementation of the second phase would be subject to separate Council reporting.

## CONSULTATION AND COMMUNICATION

Council staff undertook engagement with relevant stakeholders prior to and during public exhibition of the draft Centres Master Planning package.

## Pre-Exhibition

Two (2) stakeholder engagement workshops were undertaken by Council staff in partnership with project consultant Urbis. The stakeholder engagement involved landowners and / or landowner representatives where that stakeholder has direct interest in the two Centres. For Marshall Mount Town Centre that area is the Town Core Defined Neighbourhood. For Fowlers Village Centre that area is the recently adopted Planning Proposal and March 2024 urban zoning for Cleveland. Council's Property Team has also been included in this stakeholder engagement process for Fowlers Village as a landowner in accordance with defined probity arrangements.

Staff received representations from several landowner and/or landowner representatives regarding issues specific to their site prior to finalising the draft Master Plan package for exhibition and again during exhibition. Staff reviewed the issues raised in detail along with all submissions following the public exhibition process. All stakeholder workshop participants were informed of the Council report being available on-line ahead of the 11 December 2023 Council meeting.

Council staff also undertook ongoing engagement with the local Aboriginal community, consistent with Council's Aboriginal Engagement Framework. This engagement is explained in further detail below.

Several Council teams also participated in the two stakeholder workshops and have provided input to the draft Centres Master Plan package including -

- Urban Release – City Strategy Division.
- Land Use Planning – City Strategy Division.
- Environmental Planning – City Strategy Division.
- Infrastructure Strategy + Planning and Project Delivery Division.
- Library and Community Services Division.
- Recreation Services - Property + Recreation Division.

## Public Exhibition

The public exhibition was undertaken from 5 February to 4 March 2024 following completion of the summer school holiday period, consistent with Council's Community Participation Plan 2023. Council staff received seven detailed submissions. This included submissions from developers, landowners and other interested parties.

The major themes of issues raised in submissions is summarised in the proposal section of this report and in more detail at Attachment 1.

## Aboriginal Community Engagement

Council staff have undertaken ongoing engagement with the local Aboriginal community, consistent with Council's Aboriginal Engagement Framework. Early engagement, prior to Council's previous December 2023 report, was undertaken as an introduction to the project and initial discussion on the two Centre sites. Further engagement was undertaken during the exhibition period. The project team, working with Council's Community Engagement Team, has undertaken ongoing engagement which has occurred via emails, phone calls and specific meetings with traditional custodian groups, knowledge holders within the community, Illawarra Local Aboriginal Land Council and Council's Aboriginal Reference Group. Ongoing discussion themes included -

- Opportunity for Aboriginal community employment within the future centres,
- The need for detailed understanding of Aboriginal heritage and potential impacts prior to development applications within the centres. This includes a desire to minimise impacts on heritage in the vicinity of the Fig Tree at Marshall Mount and in the area of the proposed community facility and Western Ring Road. Council staff agree that further Aboriginal Heritage impact investigations would be required as specific projects are planned further.

The Master Plan will not predetermine the outcome of environmental assessments that would still be required through future Development Applications. Rather it provides a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.



- Celebration and respect for country with a focus on the broader landscape, creeks, and specific trees such as the Fig Tree identified in the Marshall Mount Town Centres.
- In addition, an opportunity to consider naming of the proposed Public Plaza at Marshall Mount and proposed Main Street at Fowlers Village after an Aboriginal Elder past should be considered.

## PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2032 Goal 1 We Value and protect our environment. It specifically delivers on the following -

Community Strategic Plan 2032	Delivery Program 2022-2026
Strategy	Service
1.6 West Dapto urban growth is effectively managed with facilities and spaces to support the future community	Land Use Planning

## SUSTAINABILITY IMPLICATIONS

The Centres Master Planning Project is an opportunity to ensure a holistic approach to guide planning of the Town and Village centres in the WDURA. Guiding Principles proposed within the indicative Master Plan package at Attachment 2 promote a sustainable outcome for each centre.

## RISK MANAGEMENT

Without a centres master plan approach there is risk that each centre of WDURA will be planned in an ad hoc fashion and in response to individual Development Application processes. Many centres and their surrounds are made up of fragmented land ownership which is the case at Marshall Mount Town Centre. Without a coordinated master plan approach the ability to deliver on the three West Dapto Town Centre Principles of Hierarchy, Movement Sensitive, and Diversity and Identity would be restricted. Each proposed Master Plan at Attachment 2 includes a specific identity vision and promotes an outcome to meet the social needs of the community served by those centres.

Without a clear direction for the West Dapto Centres there is limited information to inform investment certainty.

## FINANCIAL IMPLICATIONS

The West Dapto Centres Master Plan Project is a grant funded project under Regional Housing Strategic Planning Fund administered by the NSW Department of Planning, Housing and Infrastructure.

The West Dapto Centres Master Plan project could have an impact on investment certainty. Stakeholder engagement has been an important step in the development of the Master Plans to ensure staff have heard from stakeholders and understand all implications, including financial.

The Centres Master Plan package at Attachment 2 was prepared by a project team which includes Urbis professional staff where the feasibility of the future centre was an important informing consideration.

## CONCLUSION

Clear guidance on the desired outcomes and expectation for West Dapto Centres is required to meet the needs of the growing and future community while also providing investment certainty for landowners.

This report proposes adoption of the West Dapto Centres Master Plan to allow the desired outcomes for Marshall Mount Town and Fowlers Village Centres at West Dapto opportunity to be realised via embedding them within the West Dapto Release Area Chapter D16 of Wollongong Development Control Plan, 2009.

## Attachment 1

### Response to Submissions West Dapto Centres Master Plan

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
<b>Theme: Open Space</b>		
<p>Oppose the creation of the “open space buffer” bordering the Duck Creek Tributary to the East of the Master Plan. This is an unnecessary sterilisation of land not in keeping with the planning objectives of the E1 Commercial zone. This should be removed from the Master Plan.</p> <p>The master plan should not pre-determine the outcome of environmental assessments. It appears to be based on the principle of utilising the arbitrary flood zone for passive open space. If this is adopted in a DCP then it sets an unfounded precedent for all of the duck creek corridor.</p>	Submitter	<p>It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications. The current open space buffer on the eastern side of the E1 zone is currently affected by 1% AEP flood extents and also contains areas of existing endangered ecological communities. The function of the flood planning area is to identify areas which would be subject to flood related development controls as outlined in Councils DCP Chapter E13.</p>
<p>The passive open space area that adjoins the Duck Creek riparian corridor removes the opportunity for this land to be developed for mixed uses as per the land use table for the MU1 zone. The Master Plan proposes to effectively expand the Duck Creek corridor into the Marshall Mount Town Centre land for reasons that do not appear to be based on environmental investigations.</p>	Submitter	<p>It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.</p>
<b>Theme: Riparian Corridor, Biodiversity Values and Natural Areas</b>		
<p>Council has not undertaken a riparian corridor assessment to validate the CRZ extent shown on the Master Plan. The CRZ boundary should be labelled as indicative and subject to detailed assessment.</p>	Submitter	<p>It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.</p>
<p>It appears that the Master Plan has not appreciated the scale and function of the Duck Creek riparian corridor and that the corridor captures all the flood affected land and riparian land. Shared pathway and additional passive open space can be catered for in the C3 zoned land and does not need to be provided in the MU1 zoned land.</p>	Submitter	<p>It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.</p>

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
The passive open space corridor along the tributary (east of the Town Centre) is similarly not entirely of high biodiversity value and the flood extent in this area needs to be validated.		
Theme: Commercial Feasibility		
How has the commercial feasibility been addressed in these documents or assessed as part of the process to arrive at the “Master Plan”.	Stantec	<p>Council’s Consultant Team included an Economic Specialist who provided oversight into the high level feasibility of the proposed Master Plans.</p> <p>The final outcome for the village and town centres are consistent with the minimum function and scale of the commercial centres as outlined in Wollongong Development Control Plan Chapter B4 Development in Centres and Peripheral Sales Precincts.</p> <p>The final outcomes for the centres are generally feasible with the minimum requirements in the Wollongong Retail and Business Strategy 2023. Council staff note the Master Plan reflects the ultimate development outcome for the life of West Dapto while the <i>Wollongong Retail and Business Centres Strategy 2023</i> identifies requirements up to 2041.</p>
The commercial/retail floor areas do not align with the adopted Centres hierarchy and identified needs within Council’s strategic documents. The Master Plans require amendment to address this.	Stantec	<p>Council staff note the Master Plan reflects the ultimate development outcome for the life of West Dapto which would be developed well beyond 2041 while the <i>Wollongong Retail and Business Centres Strategy 2023</i> identifies requirements only up to 2041.</p>
Theme: Planning Controls – Local Environment Plan and Development Control Plan		
Request that Council review and consider amending the West Dapto LEP 2009 to allow the construction of access roads, utilities, buffer water quality/quantity and APZ areas in C2 and C3 zones as a permitted use where they are required to provide roads and services access to residential/environmental living zones and access to adjoining lots.	Submitter	<p>This is out of scope of this project and generally not supported by Council staff. Any such LEP amendment would be subject to a specific Planning Proposal merit assessment.</p>
Building heights within the town centre and surrounding residential zones should be consistent with the LEP.	Submitter	<p>Preparation of the Master Plan has considered a range of land uses, built forms and outcomes, balancing commercial outcomes, planning controls (eg, height, FSR) and other outcomes along with site constraints.</p>



Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
Council should be mindful of the objectives and accompanying development standards of the MU1 and E1 Zones as they are not achieved in the Marshall Mount Master Plan.		<p>The Master Plan provides a conceptual layout that can be refined based on detailed assessments as part of future Development Applications. Alternative outcomes may be considered noting they are required to be consistent with the overall principles in the Master Plan, ie flooding, open space and public domain, environmental management, access and movement, built form (building setbacks), active frontage, solar access and overshadowing.</p> <p>The Master Plan has considered a range of commercial outcomes and balances built form with the opportunity to provide additional housing.</p>
It should be permitted taller - to 6 storeys at least and up to a 10 minute walk away, to seize the opportunity for dense housing.	Submitter	The Urban Design Reports (Part A and Part B) considered a range of housing densities and typologies to inform preparation of the Master Plans. The exhibited Master Plan aims to balance commercial outcomes, housing density and built form outcomes within an urban release context. Additional building height outcomes would be subject to a separate Planning Proposal merit assessment. Council staff would still expect the Master Plan overall principles and outcomes to be achieved.
We request amendment to LEP Section 4.1 and 4.1 A to help bring forward the development of the proposed centre.	Submitter	Amendment of clause 4.1 or 4.1A of the Wollongong LEP is subject to separate process via a planning proposal. This is not included in the scope of this project.
<p>It is premature for Council to adopt any Master Plan until it is consistent with the 'higher order' zoning and land use controls within the applicable LEP for the land. LEP amendment should ensure no further ambiguity with landholders. A second phase of the project detailing LEP amendments is objected to.</p> <p>A holistic investigation of zoning / LEP and development control amendment for all centres in West Dapto is required and aligns with Council's own approach in the West Dapto Vision.</p>	Stantec	<p>In 2023 Council adopted the West Dapto Social Infrastructure Needs Assessment and Wollongong Retail and Business Centres Strategy. Both policy documents promote Council lead master planning of centres at West Dapto.</p> <p>Staff, through the West Dapto Steering Committee, identified Marshall Mount town and Fowlers village as the focus of the Centres Master Plan Project.</p> <p>Council led master plans of the two centres are also seen as a pilot to guide master planning of all future centres at West Dapto.</p> <p>Council staff are aware amendment of planning provisions is required to fully realise the outcomes of the Master Plan. The post exhibition Council report associated with this attachment details this process.</p>

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
The State Environmental Planning Policy (Housing) 2021 and specifically the inclusion of a height bonus for delivery of social housing is to be incorporated.	Stantec	Council staff are aware of the SEPP Housing 2021 affordable housing bonus provisions. Any subsequent review of LEP provisions following adoption of the master plan package would need to be undertaken with consideration made to those affordable housing bonuses.
<b>Theme: Traffic and Transport</b>		
Council should carefully consider the width of proposed roads, to support the economic and efficient use of development space for an already compact and constrained Town Centre zone. The proposed road widths consume 27.5% of the developable land (Urbis Design report Part B Table 8) which appears excessive.	Landowner	Road widths have been applied in accordance with Wollongong Development Control Plan Chapter B2. These road widths are consistently being adopted throughout the release area.
There are many intersections proposed along Marshall Mount Road and the Western Ring Road. These intersections all occur within short distance from one another. The spacing and location of intersections needs careful consideration, including any turning restrictions which could limit the convenience of the car parks. The location of the crossing points and intersections could result in cars queuing and raise safety questions.	Submitter	Council staff agree that final spacing and location of intersections requires careful consideration and therefore the final intersection arrangements and layout of the road network is subject to future detailed transport analysis and road design.
Street trees appear to cut into the MU1 and E1 Zones and the road corridor/width and should not be unnecessarily increased as it further reduces the Town Centre Development Opportunities.	Submitter	The road reserve for Marshall Mount Road (south of the intersection of Yallah Road) is a standard cross section width in accordance with the Wollongong Development Control Plan Chapter B2. This width includes provision for street tree planting and would be a required outcome with or without the master plan.
Marshall Mount Road creates a physical and psychological barrier for accessing the community facilities from the rest of the town centre. The safety of a pedestrian crossing / public plaza across a sub-arterial road (as nominated in the Master Plan) is highly questionable.	Submitter	The Master Plan aims to balance a range of competing outcomes. Mainstreet has multiple crossing points e.g. The Fig Tree link will connect pedestrians east and west of Marshall Mount Road and will provide crossing treatment enabling safe movement of pedestrians across both sites. The Master Plan vision for Marshall Mount acknowledges that the centre will be characterised by a vibrant main street that balances pedestrian and vehicular movements.

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
Round about along Marshall Mount Road – approx. 150m north of the intersection of Yallah Road and Marshall Mount Road. The concept DA Traffic Impact Assessment has not identified the need for a roundabout at this location for the 2036 ultimate scenario along Marshall Mount Road.	Submitter	Staff note the concept DA is subject to ongoing assessment.  Staff note a 4-way intersection at this location requires the provision of a roundabout in accordance with Wollongong Development Control Plan Chapter B2. All intersection arrangements would also be subject to final design.
<b>Theme: Shared Paths</b>		
The shared path should be located along the perimeter of the riparian corridor. Location of off-road shared paths should also be indicative as they are not validated by detailed design planning.	Submitter	Shared pathways shown in the Master Plan provide indication of the movement and access corridors and infrastructure required through the site. It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications and relevant detailed designs.
The proposed pedestrian bridge will be expensive to build and maintain. There is already a shared path proposed along Marshall Mount Road under CP 2020 avoiding the need to duplicate a bridge over Duck Creek.	Submitter	Council staff note the West Dapto Development Contributions Plan 2020 (and draft 2024) include provision for a shared use bridge (PB5) in the wider vicinity of Marshall Mount Town Centre. Such a bridge is required to provide connection and enable movement through neighbourhoods surrounding the centre. This bridge is required in accordance with the West Dapto Development Contributions Plan.
<b>Theme: Stormwater and Flooding</b>		
The local basin should not duplicate the function of basin SM06.	Submitter	The proposed basin is located outside of the mainstream flooding extent of Duck Creek and outside of the footprint of basin SM06. The function of the proposed basin is to control the peak flows from the Town Centre to existing conditions and does not duplicate the function of SM06. The purpose of SM06 is to reduce flooding impacts to downstream areas, to offset flood impacts associated with floodplain filling and proposed roughening of riparian corridors downstream.
Council has not undertaken flood modelling to inform the Master Plan (the CRZ boundary should be labelled as indicative subject to detailed assessment).	Submitter	It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
<b>Theme: Community Facilities</b>		
A neighbourhood multi-purpose community centre of 1500m2 GFA (CF05) on the southern side of Marshall Mount Road which lies just west of the Town Centre does not appear to be considered.	Submitter	A neighbourhood scale community facility in this location represents previous policy position. Community facilities across the WDURA have undergone contemporary review as part of the West Dapto Social Infrastructure Needs Assessment (SINA 2023). The SINA was publicly exhibited, and recommendations endorsed by Council late 2023. The draft West Dapto Development Contributions Plan 2024 incorporates the recommendations of the SINA (2023) and exhibited these for public comment. Council staff note the draft West Dapto Development Contributions Plan post exhibition Council report (18 March 2024) recommended change to this facility to local scale facility.
Confirmation that stage 5 community facility changes (to local scale) will flow through to the Future West Dapto Centres project, as well as WSP's Site Feasibility project.	Submitter	Council staff note the draft West Dapto Development Contributions Plan post exhibition Council report (18 March 2024) recommended change in size of CF05 facility from the publicly exhibited subdistrict scale to local sized facility. Council endorsed this staff response noting this position is subject to review by IPART, advice by the Minister and adoption by Council. The Master Plan for Marshall Mount Town Centre will be updated to reflect the latest endorsed position noting the exact size and location will be subject to future reporting and assessment.
The RE1 zone to the west of the Town Centre which is identified in CP 2020 (specifically OS23) for acquisition should be considered as a location for community facilities to avoid duplicating land to be acquired by Council.	Submitter	The RE1 zoned land known as OS23 in accordance with the West Dapto Development Contributions Plan 2020 (and draft 2024 Plan) will provide land for active recreation and open space purposes. Additional land is required for community facilities.
Sub-district scale community facilities at Marshall Mount appears to be an overprovision based on Council's SINA. Stage 4 of WDURA is covered by existing facilities in Dapto, Horsley, Wongawilli and planned facilities in Bong Bong Town Centre and the projected population of Stage 4 is not reliant upon facilities in Stage 5.  Council should explore the spare capacity at Dapto Ribbonwood Centre in Area 9 to reduce the additional community space needed in Marshall Mount. Council should explore the option of providing local centres utilising North Marshall Mount Progress Hall and the already planned CF05.	Submitter	Council staff note the draft West Dapto Development Contributions Plan post exhibition Council report (18 March 2024) recommended a change in size of CF05 facility from the publicly exhibited sub-district scale to a local sized facility. Council endorsed this staff response noting this position is subject to review by IPART, direction from the Minister Planning and Public Spaces and adoption by Council. The Master Plan for Marshall Mount Town Centre is proposed to be updated in this report to reflect the latest endorsed position noting the exact size and location will be subject to future reporting and assessment.  Council staff will continue dialogue with surrounding local government areas. Staff note previous feedback has been provided through exhibition of the draft 2024 West

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
<p>This master plan will have significant implications for council in terms of land acquisition and construction costs. How will council fund the acquisition of land and construct these public facilities.</p> <p>Consultation with Shellharbour Council to ascertain whether there is an opportunity for this and other shared facilities in Calderwood.</p>		<p>Dapto Development Contributions Plan CF05 facility and Calderwood. Staff understand the size of the planned Calderwood multi - purpose community facility is based on the expected population of Calderwood. Staff understand library facilities for the expected population of Calderwood will be provided offsite via an extension and refurbishment of the Albion Park Library. Council staff consider the spatial proximity to the planned multi -purpose community facility within Calderwood may be relevant however note the capacity of this site will not cater for the needs of WDURA residents.</p> <p>Land acquisition for community facilities will be funded through West Dapto Development Contributions. The costs associated with design and construction present challenge for Council. Council staff anticipate grant or other funding sources will be required to complete construction.</p>
Theme: Master Plan - Proposed Locations		
The location of the proposed childcare centre within the E1 commercial zone further reduces the dwelling/commercial density necessary for the economic viability of the town centre.	Submitter	The Master Plan illustrates the potential preferred town centre outcome and need for childcare facilities. Alternative outcomes may be considered noting they are required to be consistent with the overall guiding principles in the Master Plan.
The Master Plan proposes a surface car parking physically separating a future building from the remainder of the Town Centre.	Submitter	Noted. Multiple access points are incorporated in the Master Plan enabling pedestrians access eg, Fig Tree Link.
The surface car park in the E1 zone is proposed to be flanked by residential buildings all orientated to overlook the surface car park. This is likely to produce poor amenity for apartments looking over a car parking area with potential lighting and acoustic issues.	Submitter	The Master Plan has considered a range of competing and inter-related planning provisions to provide a conceptual Master Plan layout. Final DA level designs would need to consider amenity outcomes further.
The master plan should not operate to limit land uses which are ultimately governed by the Wollongong LEP 2009 and other statutory instruments	Submitter	It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.



Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
<b>Theme: Development of the Master Plan and supporting studies</b>		
Council should consider advancing technical studies in support of this urban design concept, and should not be limited to existing LEP controls, if further and better information provides for the better economic viability and development outcomes of the Town Core precinct.	Submitter	It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.
We oppose any further reduction in the developable areas (i.e., commercial or residential) given the vast amounts of undevelopable C2 and C3 conservation areas conceived in the LEP disproportionately affecting properties.	Submitter	Noted. The Master Plan does not propose to rezone land. The Master Plan will not predetermine the outcome of environmental assessments required through future Development Applications. Rather it provides a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.
The Master Plan should be subject to more detailed investigations and the assessment submitted with DA 2023/481 will be useful for council to inform the Master Plan and the future DCP controls.	Submitter	Noted. Staff note benefit in reviewing and considering all information available, whilst noting this development application is subject to current ongoing merit assessment.  The flood information used to inform the draft Marshall Mount Master Plan is based on Council's adopted 2019 Duck Creek Flood Study.
The Master Plan documents require amended to reflect the West Dapto Social Infrastructure Needs Assessment (SINA 2023) and updated forthcoming West Dapto Development Contributions Plan infrastructure needs.	Stantec	The Master Plan will respond to the latest position of the Council in response to the SINA 2023 and the draft West Dapto Development Contributions Plan 2024. The Master Plan will be updated to reflect the recommended local scale community facility for CF05 moving forward.
We need more diverse housing options that is liveable with high nearby amenity	Submitter	Noted. Council staff note the proposed Master Plans will provide for a range of housing types, additional to surrounding residentially zoned land. A range of medium density housing options is expected to be provided.
Need to ensure the MU1 and E1 zones are not limited to surface car parks, community uses with a large land take or single storey buildings (potential for dwelling densities).	Submitter	The Master Plan illustrates the potential preferred town centre outcome. Alternative outcomes may be considered noting they are required to be consistent with the overall guiding principles in the Master Plan, for example: flooding, open space and public domain, environmental management, access and movement, built form (building setbacks), active frontage, solar access and overshadowing.

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
<b>Theme: Underutilisation of the MU1 and E1 Zone</b>		
Urbis suggest that 51% of the Town Centre is not developable for housing. This does not appear to be due to site constraints, but simply that the Master Plan only proposes buildings on 49% of the Town Centre area zoned MU1 and E1.	Submitter	Council staff working with Urbis has reviewed the final developable footprint. Non-developable land is considered to be land identified as open space/ constrained land, land set aside for a western promenade and land required for road reserves.
The low utilisation of the available site area of the Marshall Mount Town Centre raises a question as to whether the zone objectives are being curtailed by the Master Plan. Passive Open Space Area could represent over 1ha of the Town Centre Land	Submitter	Council staff confirm that constrained land, some of which has been identified for use as passive open space, represents just over 1ha in the Master Plan. This is an existing constraint of the future centre context.
The Master Plan includes one or two storey buildings and surface car parking is proposed. This is a lost opportunity for housing located in the town centre. Significant areas of car parking.	Submitter	The Master Plan has considered a range or competing and inter-related planning provisions to provide a conceptual Master Plan layout. It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.
The line of sight line to the fig tree is not considered a key design principle and should be reviewed to allow more flexibility to achieve higher order urban outcomes such as density per the existing DCP controls.	Submitter	Staff note the Fig Tree, views to / from, and the resultant amenity from the Fig Tree formulate a key component of the Marshall Mount Town Centre Master Plan. The Master Plan enables flexibility in other areas across the site.
The single storey building form along Marshall Mount Road is also inconsistent with Section 11.1 – Town Centre development controls in Chapter D16 of the DCP. In particular, the DCP is seeking to create a strong urban form with street walls of 2-3 storeys.	Submitter	The Master Plan as exhibited proposed 1 x one storey building along the southern extent of Marshall Mount Town Centre, on the western side of Marshall Mount Road. This built form results from balance of a range of competing planning considerations and site constraints. It is not expected that the Master Plan will predetermine the outcome of the environmental assessments, but rather provide a conceptual layout that can be refined based on detailed assessments as part of future Development Applications.
<b>Theme: Processes</b>		
How are the aims of the grant funding (as applied for by Council) achieved and how are they articulated in the documents?	Stantec	In 2023 Council adopted the West Dapto Social Infrastructure Needs Assessment and Wollongong Retail and Business Centres Strategy. Both policy documents promote Council lead master planning of centres at West Dapto.

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
What was the process of selection for the Marshall Mount and Fowlers Centres for the Project (ie over other centres in the WDURA)?		<p>Staff, through the West Dapto Steering Committee, identified Marshall Mount town and Fowlers village as the focus of the Centres Master Plan Project.</p> <p>Council led master plans of the two centres are also seen as a pilot to guide master planning of all future centres at West Dapto.</p> <p>Council staff selected 1 village, Fowlers, and 1 town, Marshall Mount. The centres were selected for a number of reasons, including -</p> <p><b>Marshall Mount</b></p> <p>The Marshall Mount Town Centre was selected as it has the most fragmented ownership of the three town centres within West Dapto and would benefit from one party, Council, leading the Master Plan process. Marshall Mount is the only town centre that is yet to have a Neighbourhood Plan adopted into Chapter D16 of the Wollongong Development Control Plan. The other two town centres do have neighbourhood plans. Those other two towns can have development applications progressed now while Marshall Mount cannot without relying on Concept DA provision. As with the other two towns Marshall Mount is zoned for urban development. This master plan process provides an opportunity for the third town centre of West Dapto to be a step closer to development applications consistent with the status of planning for the other two towns.</p> <p><b>Fowlers</b></p> <p>At the time of selecting the appropriate village for the Master Planning process Fowlers Village was subject to a Planning Proposal, referred to as Cleveland, which was post public exhibition and proposed to zone the majority of Stage 3 of West Dapto, including the Fowlers Village, for urban development. Therefore, it was timely to look at this specific village. Council's property team is responsible for the land as landowner and were invited to participate in the process along with adjoining landowners from the proposed Cleveland proposed rezoning land. The location of Fowlers village near Dapto High School and accessibility from the recently delivered Karrara Bridge are seen as other positive reasons to have focused on this village as a pilot.</p>



Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
The two exhibited Master Plans should be deferred until the process of incorporation of Master Plans into the Neighbourhood Plan or Concept DA process is thoroughly analysed. This should involve prior and extensive consultation with the development industry, including our client, and should address the costs associated with preparation of the Master Plans.	Stantec	Council staff note any future adoption of the Master plan into the Wollongong DCP provides a contemporary policy position and inform the planning process. The Report to Council associated with this project details the proposed pathway to explore amendment of planning provisions.
Individual Matters		
Where are the detailed plans for the Darkes Road/ West Dapto and Bong Bong Centre  Shopping centre on West Dapto Road with grocery store, petrol station, chemist, and other essential stores.	Submitter	Darkes Road and Bong Bong Town Centres have existing neighbourhood plans in place and adopted into the Wollongong Development Control Plan (specifically chapter D16 West Dapto Release Area). Council staff note adoption of these neighbourhood plans enable Development Application assessed in accordance with the Wollongong Local Environment Plan 2009.
Important to have a footpath on Darkes Road to make it safe for pedestrians and a footpath connecting Darkes Road to Horsley and Wongawilli.	Individual	Although outside the scope of this project, Council staff confirm the concept road design completed for Darkes Road includes shared pathway along the southern side of Darkes Road.
A new school for the Kembla Grange/Wongawilli area is requested.	Submitter	Council staff will continue to liaise with the Schools Infrastructure NSW regarding school infrastructure.
Just start. Build the infrastructure first.	Submitter	Council staff note infrastructure delivery is programmed through Council's annual Infrastructure Delivery Program. Development of the commercial buildings in West Dapto centres will ultimately rely on private investment.
Extra on ramp and off ramp for freeway south from Kanahooka Road	Individual	Transport for NSW are currently undertaking investigation works into for south facing ramp/s on M1 motorway near Dapto. Transport for NSW recently consulted with the public for their initial views in relation to this. Council staff will continue to liaise with Transport for NSW regarding planning work and impacts on the local road network.
Build the commercial and the infrastructure before you allow more houses. The roads can't take it.	Individual	Noted. Council staff note infrastructure delivery is programmed through Council's annual Infrastructure Delivery Program.

Marshall Mount Town Centre		
Concerns Raised	Submitter	Council Staff Response
Open space adjacent to Sanctuary Views Estate – please maintain areas of trees and keeping as many existing trees as possible - a nature area behind sanctuary views estate with playground, walking paths and trees - walking paths provided for recreation.	Individual	Staff note this submission relates to a local open space item relating to a different project. The local open space item subject to this submission is an area of land planned as local passive open space.
Update mapping and legend where they are inconsistent.	Individual	Noted. Reviewed and updated.

Fowlers Village Centre		
Concerns Raised	Submitter	Council Staff Response
Build the infrastructure first. Town centres with growth space allocated before populations becomes out of control.	Individual	<p>Delivery of infrastructure such as roads and open space is programmed through Council's Annual Infrastructure Delivery Program. The West Dapto Development Contributions Plan provides indicative timing for infrastructure delivery.</p> <p>The Fowlers Village Master Plan contributes to delivery of the Cleveland east Neighbourhood Plan. A Neighbourhood Plan is required to be adopted into the Wollongong DCP to enable development to occur. The Centre is zoned E1 Local Centre which will ensure final land use outcomes are consistent with the local centre objectives.</p>



# WEST DAPTO TOWN CENTRES

## URBAN DESIGN REPORT

### PART B - INDICATIVE MASTER PLAN

PREPARED FOR  
**WOLLONGONG CITY COUNCIL**  
07 JUN 2024  
FINAL

**URBIS**



# 05 INDICATIVE MASTER PLANS

## CONTENTS - PART B

<b>05 INDICATIVE MASTER PLAN</b>	<b>B2</b>
MARSHALL MOUNT TOWN CENTRE	B4
FOWLERS VILLAGE CENTRE	B40
<b>APPENDIX</b>	<b>B68</b>

This chapter sets out the indicative Master Plans and supporting layered strategies for the two centres respectively being:

- Marshall Mount Town Centre; and
- Fowlers Village Centre.

Each centre provides the following:

- Vision Statement and Concept Master Plan;
- Guiding Principles;
- Structure Plan & Development Summary; and
- Layered Strategies:
  - Natural Assets
  - Access and Movement
  - Centre Activation
  - Housing Mix
  - Built Form.





## INDICATIVE MASTER PLAN

# MARSHALL MOUNT TOWN CENTRE

## VISION

The vision for Marshall Mount Town Centre is:

**“A compact mixed retail and community precinct with housing choice, conveniently located and accessible by active and public transport. It is characterised by a vibrant main street that balances pedestrian and vehicular movements, and local places for people that celebrate views to the scenic Illawarra Escarpment and connection to place through local landscape features including the existing Moreton Bay Fig Tree.”**

To enable this vision, the Town Centre will:

- **Celebrate the local and scenic landscape characters:** Establish visual connections to the existing Moreton Bay Fig Tree along the Fig Tree Link and a distant view to Illawarra Escarpment including Mount Kembla from the Western Promenade.
- **Encourage active and healthy lifestyles:** Provide seamless pedestrian and cyclist connections into the Town Centre from the adjacent roadways, open space and key destinations.
- **Provide day-to-day convenience:** Concentrate the supermarket and convenience retail along the Main Street and Fig Tree Link with visibility and access from Yallah Road.
- **Become the community heart:** Deliver community facilities including a multi-purpose hall and a potential library within the Town Centre Core precinct co-located with the outdoor public plaza fronting the Moreton Bay Fig Tree.
- **Provide a mix of housing choice close to the centre:** Medium density housing typologies of shop top housing and residential flat buildings providing housing choice with convenience.

The concept master plan for Marshall Mount Town Centre is illustrated opposite.





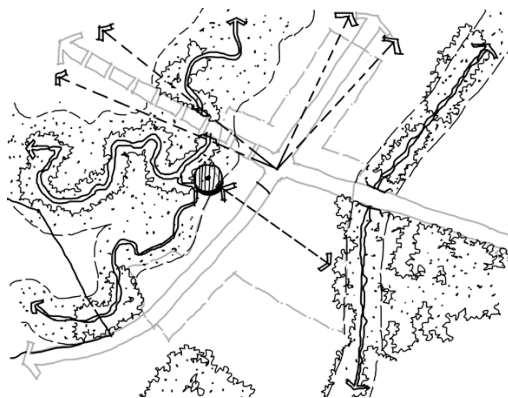
Figure 39 Marshall Mount Town Centre - Concept Master Plan

## MARSHALL MOUNT TOWN CENTRE

### GUIDING PRINCIPLES

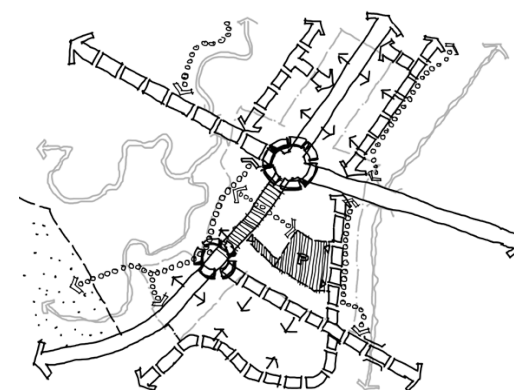
Five guiding principles were identified for the Marshall Mount Town Centre site which brings together the planning context, place analysis and development opportunities to underpin the master plan.

### 01 CELEBRATE COUNTRY



1. Protect Duck Creek and its tributaries, riparian corridors and high biodiversity value areas.
2. Establish a view corridor to the local landscape feature of the Moreton Bay Fig Tree and Illawarra Escarpment.
3. Orientate development to the north-west to celebrate panoramic views across the scenic Illawarra Escarpment and the undulating hills landscape character.
4. Protect the 1% AEP flood zone by locating all development and structures outside of this zone.

### 02 A CONNECTED AND ACCESSIBLE CENTRE

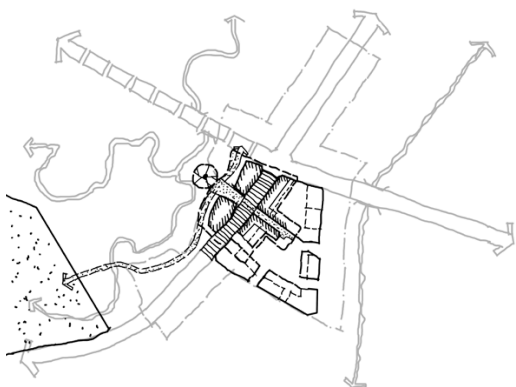


1. Design Marshall Mount Road (south) to function as a Main Street to balancing place vibrancy, pedestrian movement and vehicle access.
2. Locate signalised intersections with safe pedestrian crossings at both ends of the Main Street.
3. Provide a signalised pedestrian crossing at the Main Street & Fig Tree Link to balance efficient pedestrian and vehicular movements.\*
4. Locate bus stops between the signalised intersections to provide direct access to the Town Centre Core area.
5. Establish perimeter roads and shared paths along the eastern and western natural area edges as public domain and Asset Protection Zones (APZ).
6. Create a secondary access route to the east to improve connectivity to the adjoining neighbourhoods and the Bypass Road to the east.
7. Locate vehicle and service access off the perimeter roads and laneways, away from the Main Street.
8. Consolidate the parking within the Town Centre Core area within at-grade parking behind the buildings and basement parking within the E1 zone.

*\*Note: Final intersection arrangement is subject to future transport analysis.*

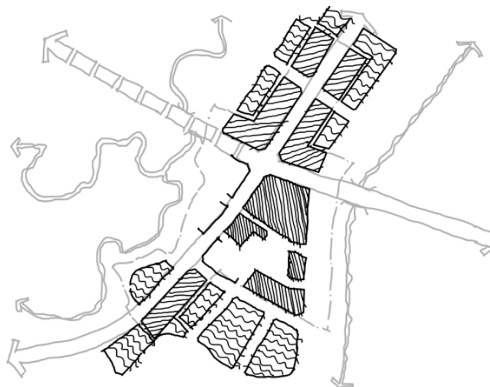


### 03 AN ACTIVATED PLACE AND VIABLE CENTRE



1. Create a walkable and compact centre by consolidating the ground level primary retail and community activations along the Main Street and Fig Tree Link.
2. Establish secondary retail and community activity at the Yallah Road primary intersection and along the Western Promenade.
3. Promote cross-usage between residential, retail, and community within the Town Centre Core area to increase dwell-time and promote passive surveillance.
4. Deliver a varied retail offer including supermarket, convenience retail and hospitality retail.

### 04 DELIVER VARIED HOUSING OFFER CLOSE TO AMENITIES



1. Provide a mix of housing up to 4\* storey residential apartments within the Town Centre.
2. Locate the apartment typology along the primary streets and along the eastern boundary with outlook towards the natural amenity.

*\*Note: Total max includes ground floor retail. It is recommended Council consider a total max 4-6 storey subject to a Local Environmental Plan review process.*

### 05 CONTEXTUAL BUILT FORM RESPONSE

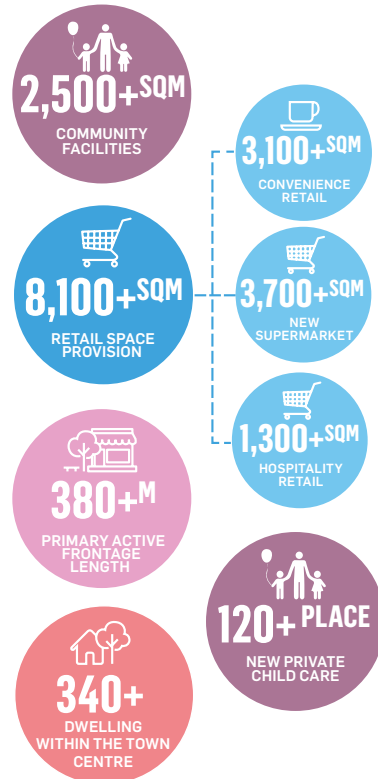


1. Establish a two-storey street wall with upper-level setback along Marshall Mount Road and Yallah Road to provide human-scale street frontage.
2. Locate key marker buildings at the primary intersection of Yallah Road and Marshall Mount Road.
3. Provide varied height and built form breaks to ensure views to key landscape features are maintained.
4. Provide for built form separation / setback between apartment typologies and adjoining medium-density housing within R3 zone through rear laneway and / or roads.

## MARSHALL MOUNT TOWN CENTRE

### STRUCTURE PLAN

The Structure Plan summarises the key elements of the Indicative Master Plan including natural assets and open space, access and movement, public domain and place activation and building uses.



#### LEGEND

Marshall Mount Town Centre

#### NATURAL ASSETS & OPEN SPACE

- Duck Creek and Tributary
- Core Riparian Zone
- 2m Contour Lines
- Existing Moreton Bay Fig Tree\*
- High Biodiversity Value Area
- Potential Indicative Stormwater Basin
- Western Promenade
- Passive Open Space
- Recreational Open Space (Zoned RE1)
- C2/C3 Zone Boundary
- Visual Connections

#### ACCESS & MOVEMENT\*\*

- Sub-arterial Road (Type 2) - 22.9m
- Sub-arterial Road (Type 2A) - 22.9m
- Major Collector Road (Type 3) - 21.9m
- Main Street - 22.9m
- Local Road (Type 5) - 18.8m

\*Note:

- Aboriginal Cultural Heritage Assessment Report would be required and should inform final project outcomes for any project in the vicinity of the Moreton Bay Fig Tree, including but not limited to, future community facilities, stormwater basin, shared paths and road.

- Access Road (Type 6) - 17.1m
- Perimeter Road - 14.5m
- Laneway (Type 8) - 8m
- Indicative Access Point
- Indicative Access to Basement Parking
- Proposed Bus Stop
- Bus Route (Southern Loop)
- At-Grade Parking
- Ground Level Podium Parking
- Indicative Loading Area
- Off-road Shared Path
- Road-side Shared Path
- Pedestrian Link
- Signalised Primary Intersection
- Signalised Secondary Intersection
- Left in Left Out with Right Turn
- Left in Left Out
- Roundabout
- Indicative Signalised Pedestrian Crossing Location

#### OPEN SPACE, PUBLIC DOMAIN AND PLACE ACTIVATION

- Fig Tree Link
- Hardscape Open Space
- Ground Level Communal Open Space
- Podium Rooftop Communal Open Space
- Outdoor Space for Private Child Care
- Primary Retail Active Frontage
- Secondary Retail Active Frontage
- Primary Community Active Frontage
- Secondary Community Active Frontage
- Key Building Markers

#### BUILDING USES

- Mixed Retail (Convenience) / Supermarket and Residential
- Mixed Community and Retail (Convenience)
- Mixed Retail (Stand-alone / Hospitality) and Residential
- Retail (Stand-alone / Hospitality)
- Private Child Care
- Residential Flat Building

\*\*Note:

- Final intersection arrangement is subject to future transport analysis.
- Final layout of road network is subject to design which may differ from that shown in the proposed structure plan.

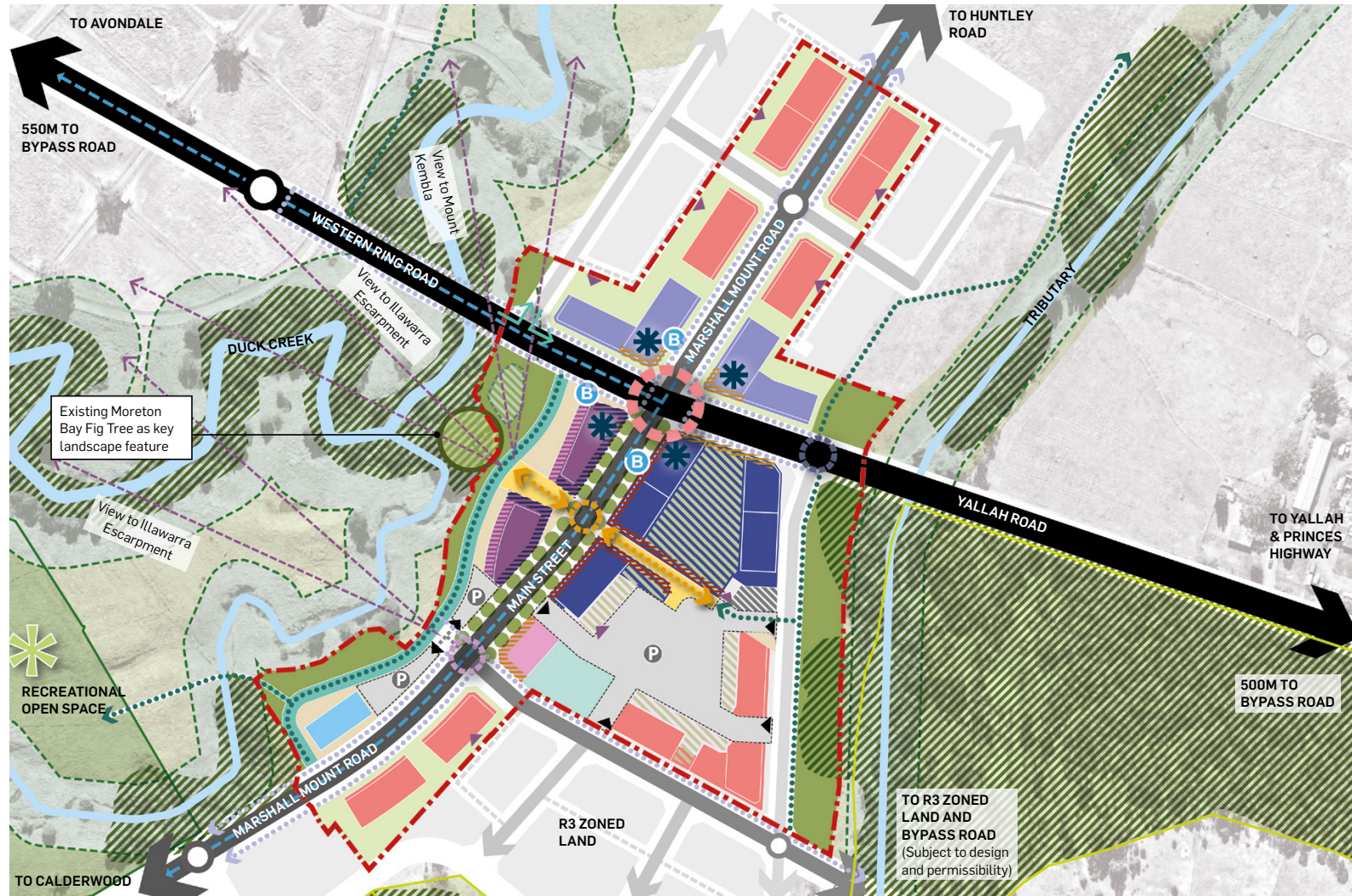


Figure 40 Marshall Mount Town Centre - Structure Plan



## MARSHALL MOUNT TOWN CENTRE

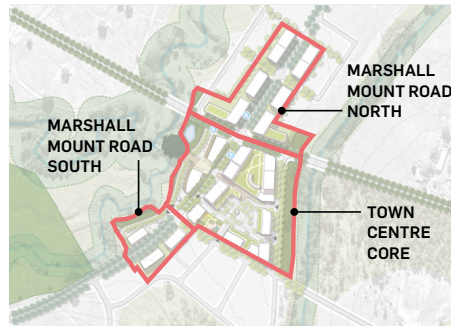


Figure 41 Key Precincts

### KEY PRECINCTS

Marshall Mount Town Centre is characterised by three sub-precincts being:

- **Marshall Mount Road North** – Medium density residential precinct with secondary ground-level retail activation at the primary intersection of Yallah Road and Marshall Mount Road. This provides a transition to the R3 zone to the north of the Town Centre.
- **Town Centre Core** – A compact, mixed retail, community and residential precinct with pedestrian-friendly environment, promoting ground level activation along Main Street, Fig Tree Link and the Western Promenade as the core activation zone of the Town Centre.
- **Marshall Mount Road South** – Medium density residential and stand-alone hospitality retail uses as a transition to the R3 zone to the south.

### DEVELOPMENT SUMMARY

The following table and diagram provides the proposed land use breakdown of Marshall Mount Town Centre.

Table 7 Marshall Mount Town Centre - Land Use Breakdown

LAND USE BREAKDOWN		%
Total Site Area (sqm)	82,466	100.0%
Developable Area (sqm)	47,247	57.3%
TC1 - Retail (Convenience & Supermarket) / Residential	8,169	9.9%
TC2 - Retail (Convenience & Supermarket) / Residential	3,488	4.2%
TC3 - Private Child Care	1,954	2.4%
TC4 - Residential	8,288	10.1%
M1 - Mixed Use - Community Facility	4,713	5.7%
M2 - Mixed Use - Retail	1,838	2.2%
M3 - Mixed Use - Residential	3,945	4.7%
M4 - Retail (standalone / hospitality) / Residential	2,940	3.6%
M5 - Residential	1,549	1.9%
M6 - Residential	2,145	2.6%
M7 - Residential	2,589	3.1%
M8 - Residential	2,112	2.6%
M9 - Retail (standalone / hospitality) / Residential	3,517	4.3%
Non-Developable Area (sqm)	35,219	42.7%
Open Space / Constrained Land (sqm)	10,327	12.5%
Western Promenade	2,366	2.9%
Road Reserve (sqm)	22,526	27.3%

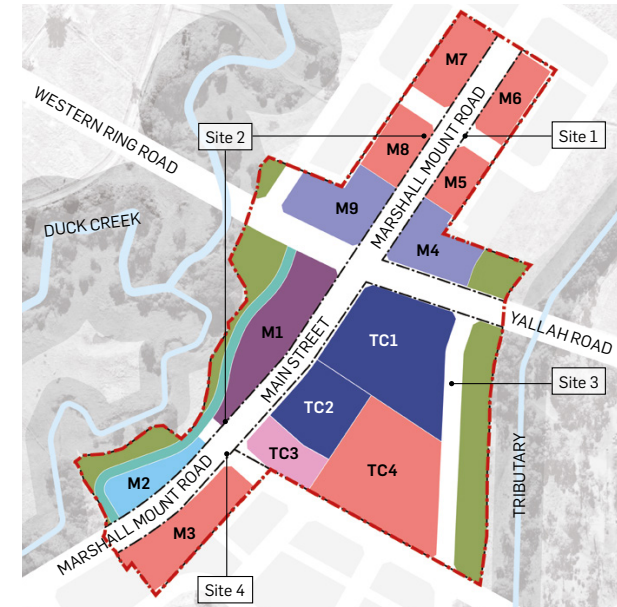


Figure 42 Marshall Mount Town Centre - Land Use Plan

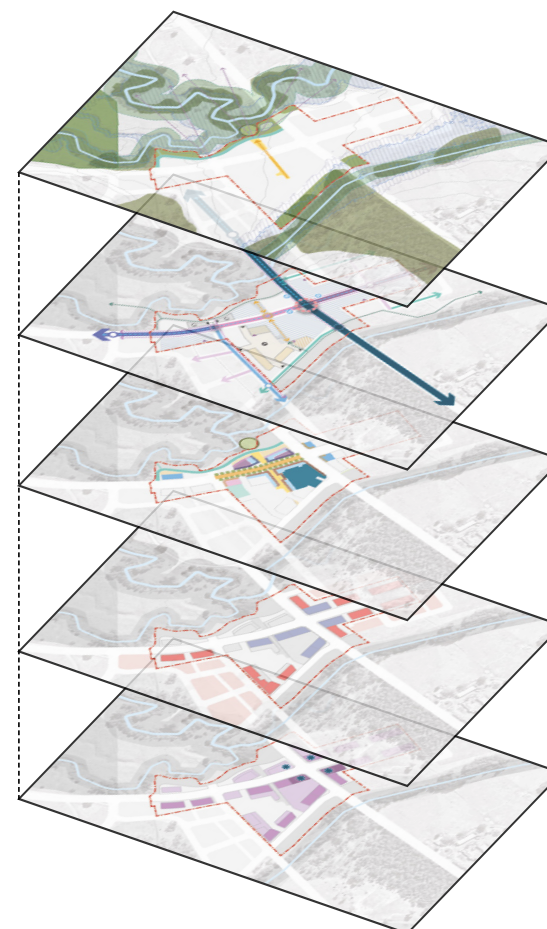
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### LEGEND

<span style="border: 1px dashed red; padding: 2px;"> </span>	Marshall Mount Town Centre	<span style="background-color: #4a69bd; color: white; padding: 2px;"> </span>	Retail (Stand-alone / Hospitality) / Residential
<span style="border: 1px dashed black; padding: 2px;"> </span>	Sub-sites	<span style="background-color: #00a0e3; color: white; padding: 2px;"> </span>	Retail (Stand-alone / Hospitality)
<span style="background-color: #003366; color: white; padding: 2px;"> </span>	Retail (Convenience & Supermarket) / Residential	<span style="background-color: #ff6666; color: white; padding: 2px;"> </span>	Residential
<span style="background-color: #993366; color: white; padding: 2px;"> </span>	Community / Retail (Convenience)	<span style="background-color: #ff99cc; color: white; padding: 2px;"> </span>	Private Child Care

## LAYERED STRATEGIES

Five layered strategies demonstrate the detailed design outcomes of a new walkable and viable Town Centre in alignment with the guiding principles.





## MARSHALL MOUNT TOWN CENTRE















### STRATEGY 1 NATURAL ASSETS

### PRINCIPLE 1 CELEBRATE COUNTRY




Celebrates the unique scenic landscape setting of the Site including protecting the existing Moreton Bay Fig Tree, Duck Creek and its tributaries, riparian corridors, and the high biodiversity value areas.

The key outcomes include:

	<b>Marshall Mount Town Centre</b>
<b>NATURAL ASSETS</b>	
	<b>Duck Creek and Tributary</b> Protect the alignment of the watercourses of Duck Creek and its tributaries along the western and eastern boundary.
	<b>Indicative Core Riparian Zone</b> Exclude the core riparian zones for Duck Creek and its tributaries from the development footprint.
	<b>2m Contour Lines</b>
	<b>High Biodiversity Value Area</b> Preserve the high biodiversity areas including the vegetation cluster along the eastern boundary.
	<b>Indicative 1% AEP Flood Level</b> The identified 1% AEP area contributes to passive open space along the eastern boundary.
	<b>Flood Planning Zone</b> The development within flood planning area is subject to flood related development control plan.

	<b>Passive Open Space</b> Proposed passive open space along the western and eastern edges to protect Duck Creek and its tributaries, their core riparian zones, high biodiversity value areas and land below the 1% AEP flood level.
	<b>Existing Moreton Bay Fig Tree</b> Retain the existing Moreton Bay Fig Tree as a local landscape feature.
	<b>Local View Corridor</b> Establish local view corridor to Moreton Bay Fig Tree along the Fig Tree Link.
	<b>Visual Connections</b> Establish visual connections to Illawarra Escarpment from the Western Promenade and Fig Tree Link.
	<b>Potential Indicative Stormwater Basin</b> Potential location for stormwater basin within the low elevation within Site - subject to further technical analysis in the detailed design stage.

#### OPEN SPACE

	<b>Western Promenade</b> Proposed shared pedestrian / cycleway along the Town Centre western edge.
	<b>Fig Tree Link</b> Proposed east-west pedestrian link align with view corridor to the Moreton Bay Fig Tree.
	<b>Recreational Open Space (Zoned RE1)</b> Proposed recreational open space immediately to the south of the Town Centre.

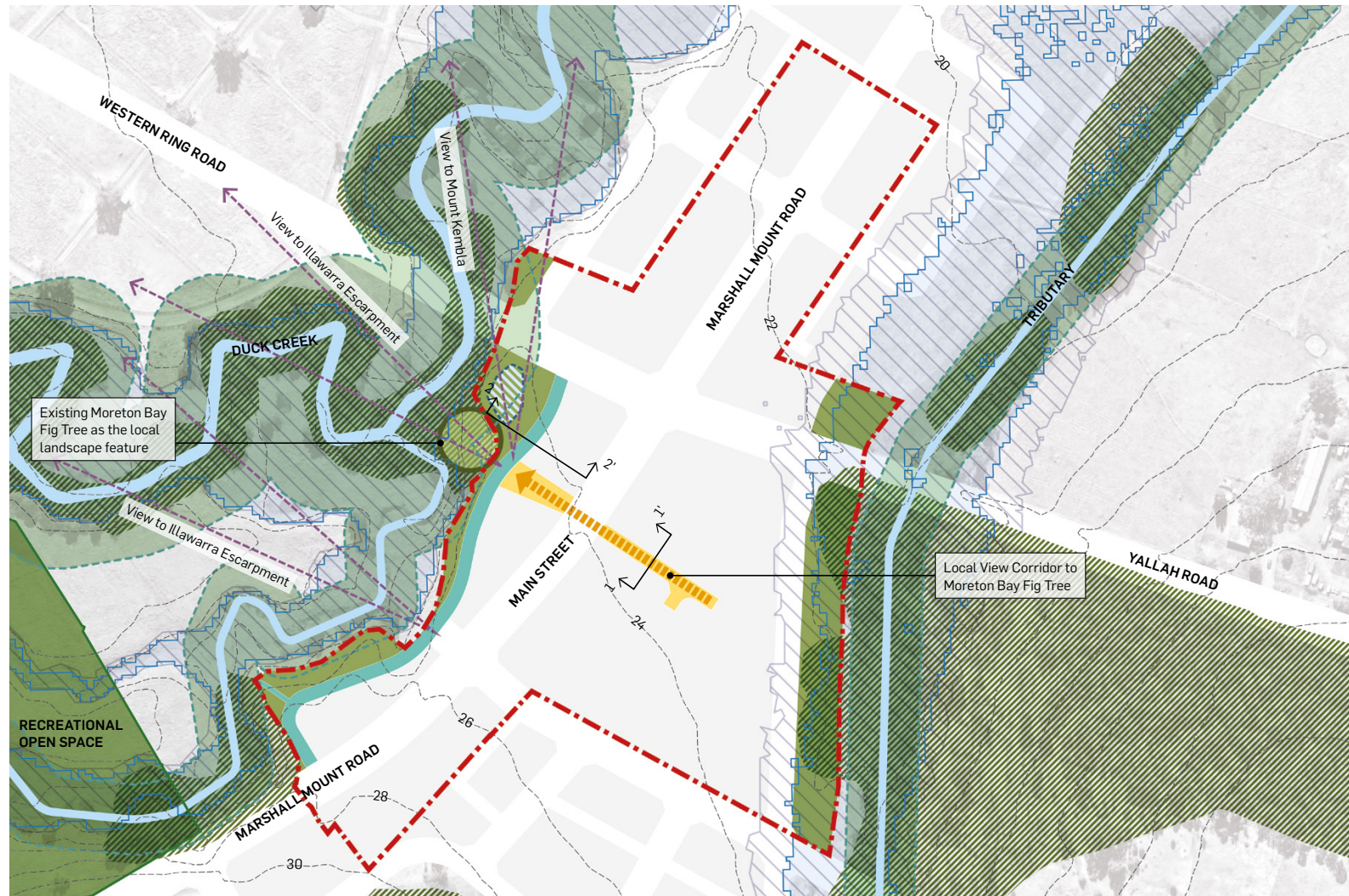


Figure 43 Marshall Mount Town Centre - Natural Assets Strategy

## MARSHALL MOUNT TOWN CENTRE



### FIG TREE LINK

The Fig Tree Link provides a local view corridor to the existing landscape feature of the Moreton Bay Fig Tree to the west of the Town Centre. Designed as a pedestrian laneway, it extends an active uses from the Main Street connecting to the consolidated at-grade parking space. It is characterised by:

- North-west to south-east alignment extending from the natural landscape through to the carpark;
- Potential for signalised pedestrian crossing at the intersection with the Main Street - subject to future transport analysis;
- Ground-level retail frontage to the east of main street and community frontage to the west; and
- Public plaza on the western end of the Pedestrian Link fronting Moreton Bay Fig Tree.

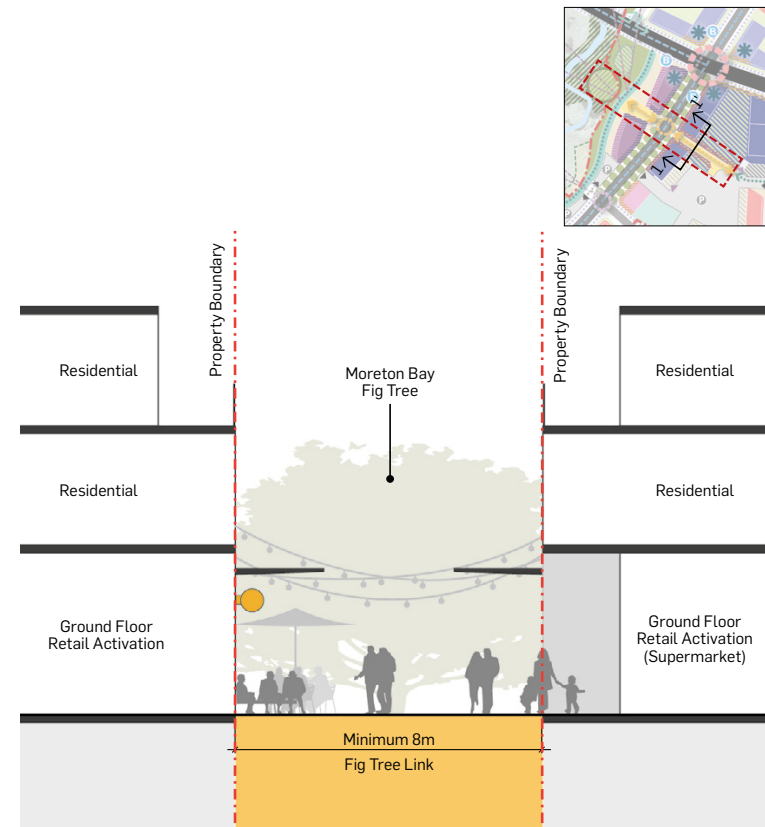


Figure 44 Indicative Section 1-1' Fig Tree Link



## FIG TREE LINK - VISUAL CONNECTION

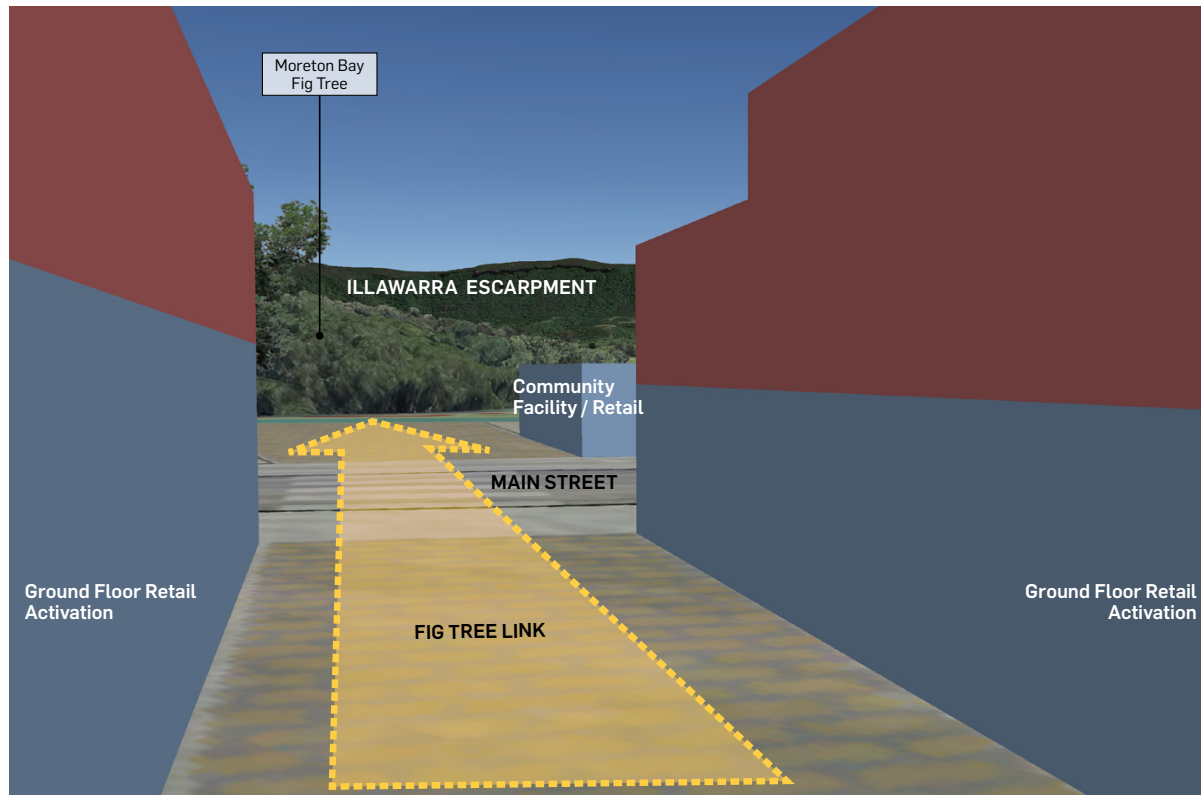


Figure 45 Visual Connection - Fig Tree Link

## MARSHALL MOUNT TOWN CENTRE



### WESTERN PROMENADE

The Western Promenade runs along the western edge of the Town Centre, providing a place to appreciate the scenic landscape character of the Illawarra Escarpment and providing pedestrian and cycleway connection to the proposed recreational open space to the southwest of the Town Centre. It is characterised by:

- A min. 4m wide shared path that allows for bushfire emergency access; and
- The northern section adjoining the community facilities and public plaza, provides the opportunity for place activation.

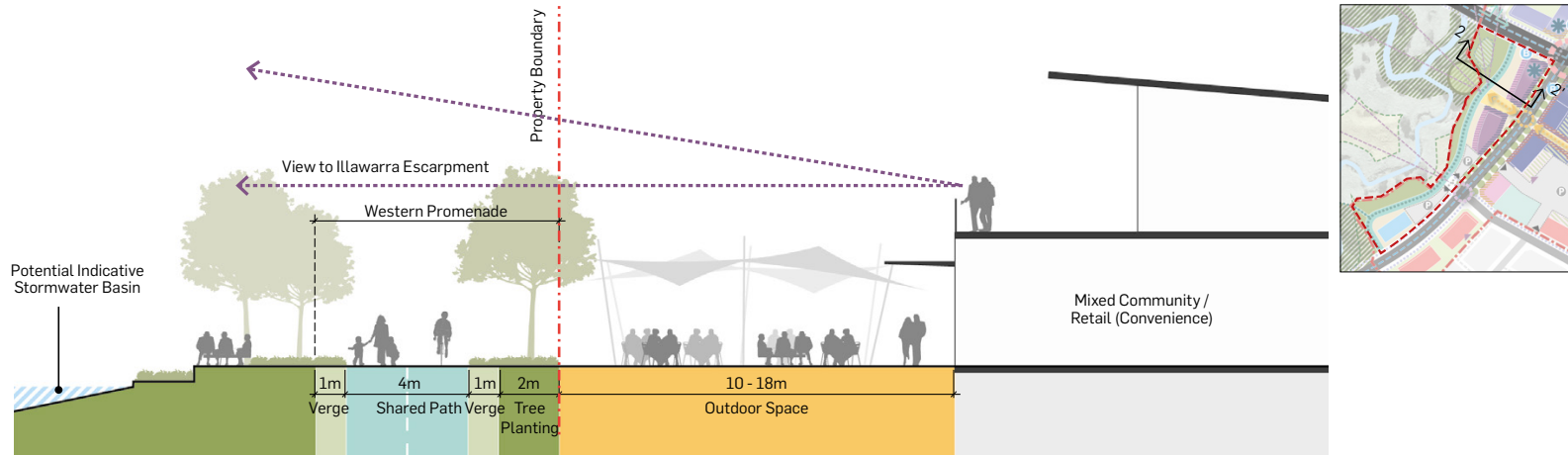
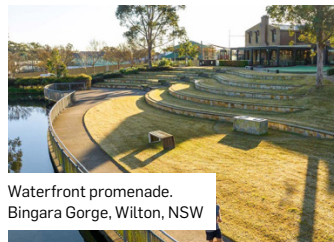


Figure 46 Indicative Section 2-2' - Western Promenade



## WESTERN PROMENADE - VISUAL CONNECTIONS

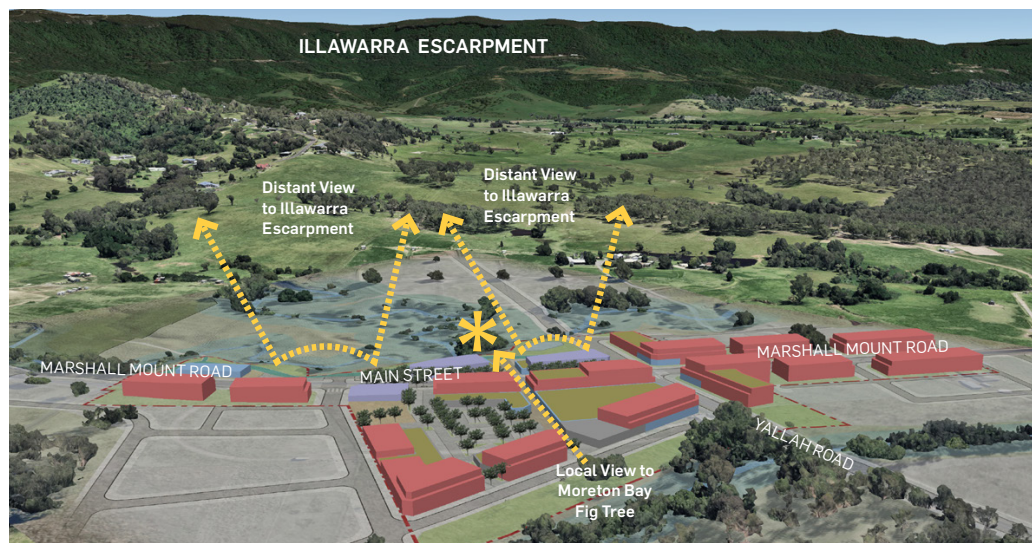


Figure 47 Visual Connections - Local and Distant Views

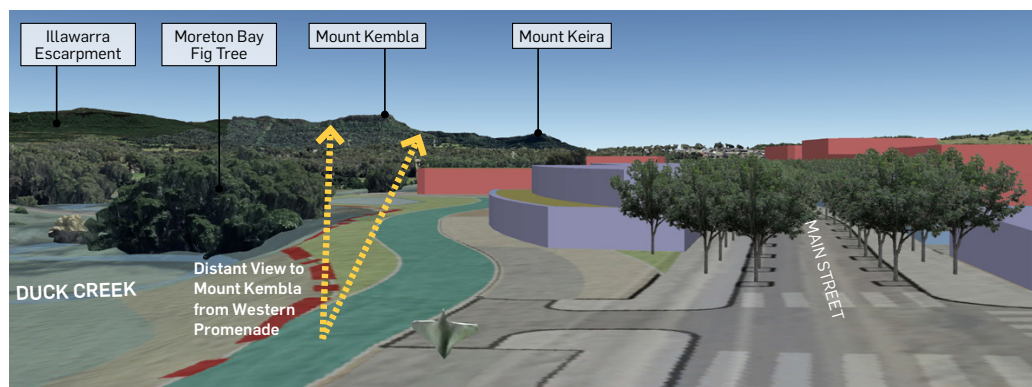


Figure 48 Visual Connections - Western Promenade

## MARSHALL MOUNT TOWN CENTRE



### STRATEGY 2 ACCESS AND MOVEMENT

Create convenient, safe and prioritised active and public transport connections to and from the Town Centre.

The key outcomes include:



**Marshall Mount Town Centre**

#### ROAD NETWORK AND INTERSECTIONS\*



**Sub-arterial Road (Type 2) - 22.9m**

Four lanes primary east to west access route with bus service that connecting the Town Centre to Princes Highway and key centres within West Dapto.



**Sub-arterial Road with Bus Service and Parking (Type 2A) - 22.9m**

Two lanes secondary north-south access routes south of Town Centre Core Precinct providing connection to Calderwood.



**Major Collector Road with Bus Service and Parking (Type 3) - 21.9m**

Two lanes secondary north-south access routes along Marshall Mount Road north of Yallah Road providing connection from Town Centre to Huntley Road.



**Main Street - 22.9m**

Two lanes main street with wider footpaths, activated frontage and on-street parking on both sides, and a slower traffic speed environment.



**Local Road with Parking (Type 5) - 18.8m**

Two lanes local access routes providing connection to the adjoining neighbourhood east of Duck Creek tributary.



**Access Street (Type 6) - 17.1m**

Minor local access route with residential on both sides.



**Perimeter Street - Bushfire Compliant - 14.5m**

Two lanes local access routes along the open space corridors to the east and northwest that are bushfire compliant.



**Laneway (Type 8) - 8m**

Vehicular access to the rear of properties.



**Signalised Primary Intersection**



**Signalised Secondary Intersection**



**Left in Left Out with Right Turn**



**Left in Left Out**



**Roundabout**

#### ACTIVE AND PUBLIC TRANSPORT



**Proposed Bus Stop**



**Bus Route (Southern Loop)**

## PRINCIPLE 2

### A CONNECTED AND ACCESSIBLE CENTRE



**Off-road Shared Path**

Establish shared pedestrian / cycle paths along the natural area edges along east and west.



**Road-side Shared Path**

Establish shared pedestrian / cycle paths within the road reserve.



**Pedestrian Link**

Create an east-west pedestrian link between both sides of the Main Street.



**Signalised Pedestrian Crossing**

Final pedestrian crossing measures are subject to future transport analysis.

#### PARKING AND ACCESS



**Indicative Access Point**



**Indicative Access to Basement Parking**



**Consolidated Town Centre Core At-grade Parking**

Consolidated at-grade parking within the Town Centre Core behind the buildings.



**At Grade Parking**



**Ground Level Podium Parking**



**Indicative Loading Area**



**Indicative Basement Parking**

\*Note:

- Final intersection arrangement is subject to future transport analysis.
- Final layout of road network is subject to design which may differ from that shown in the proposed structure plan.

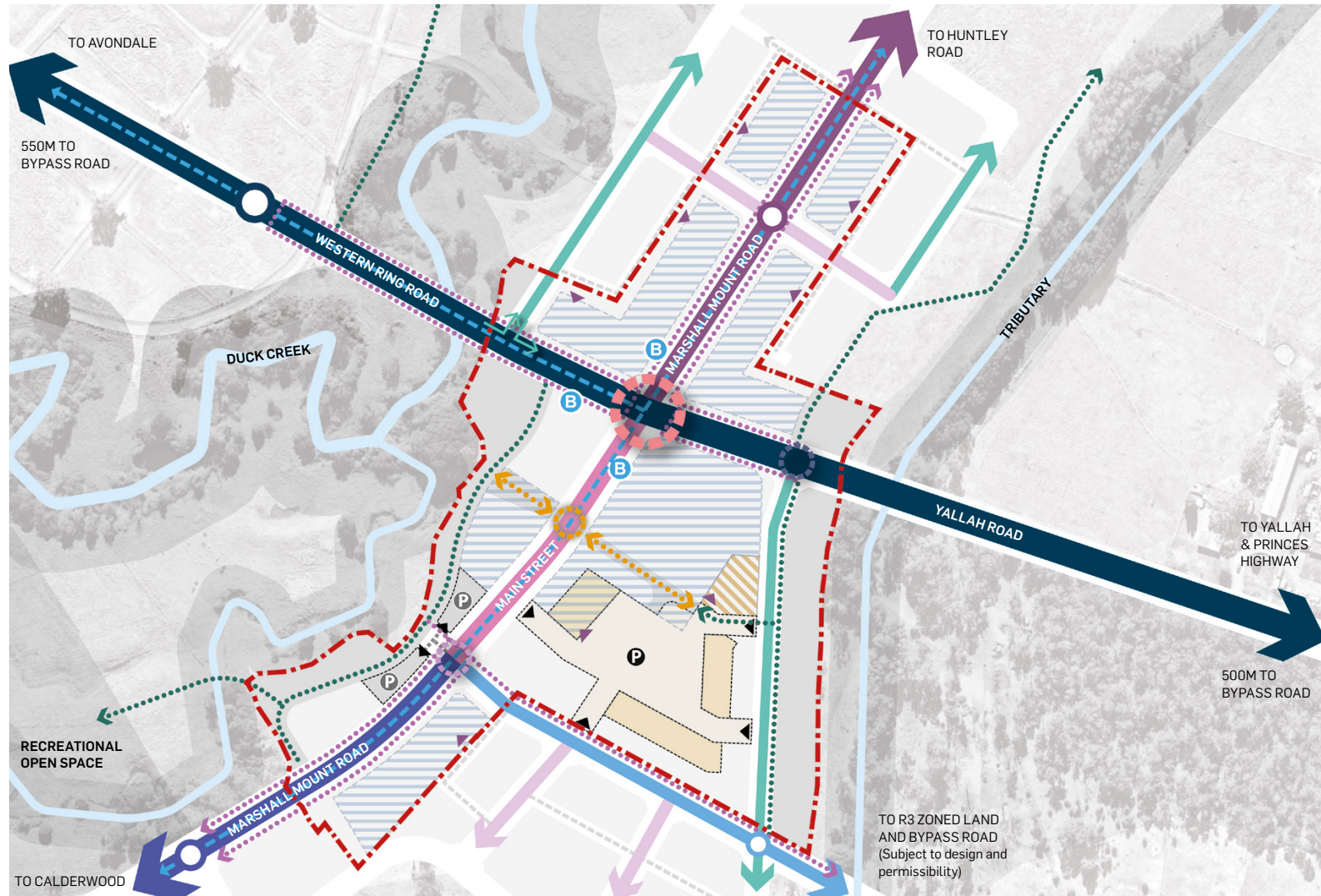


Figure 49 Marshall Mount Town Centre - Access and Movement Strategy



## MARSHALL MOUNT TOWN CENTRE



### TYPICAL STREET SECTION

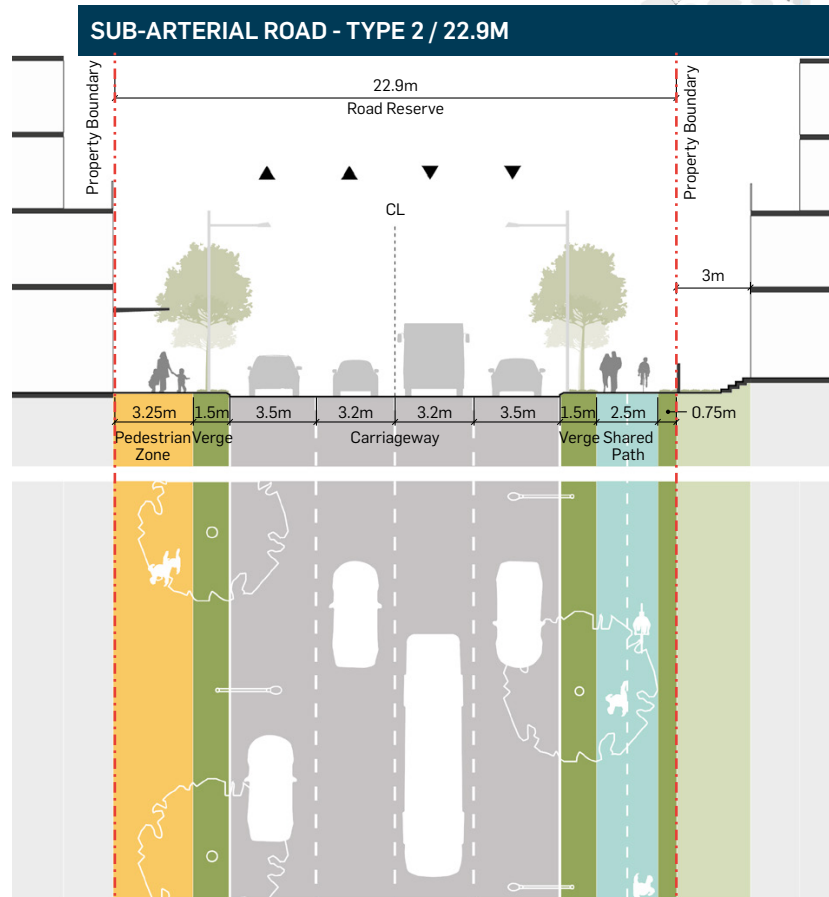


Figure 51 Indicative Street Section A-A' - Sub-Arterial Road - Type 2

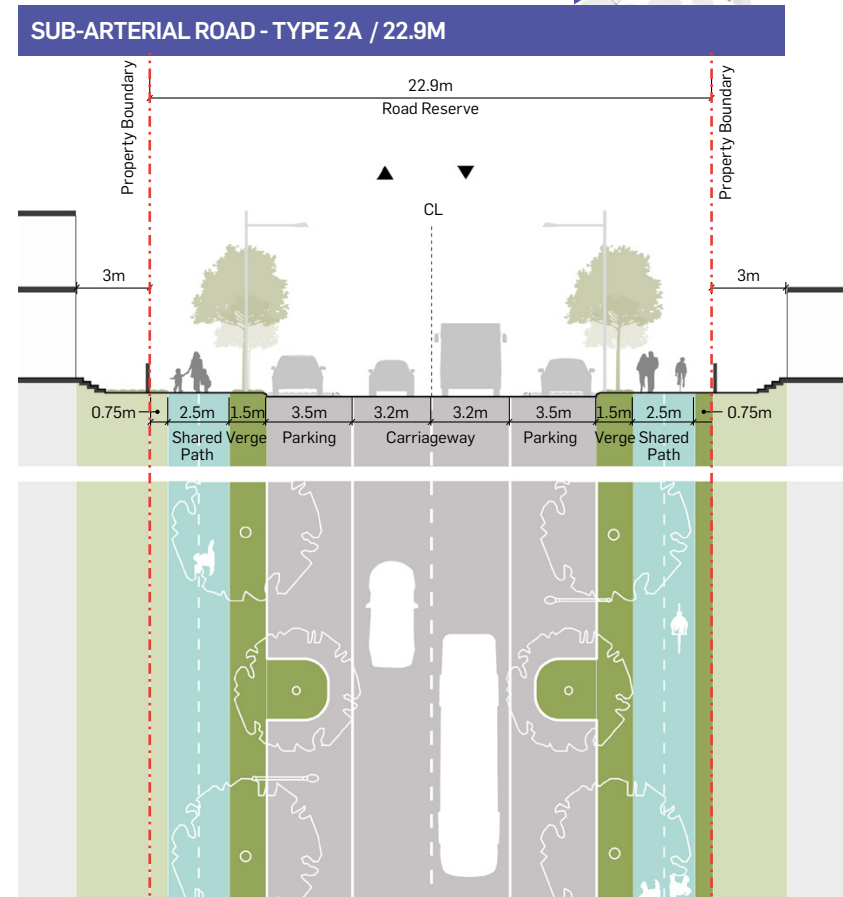


Figure 50 Indicative Street Section B-B' - Sub-Arterial Road - Type 2A



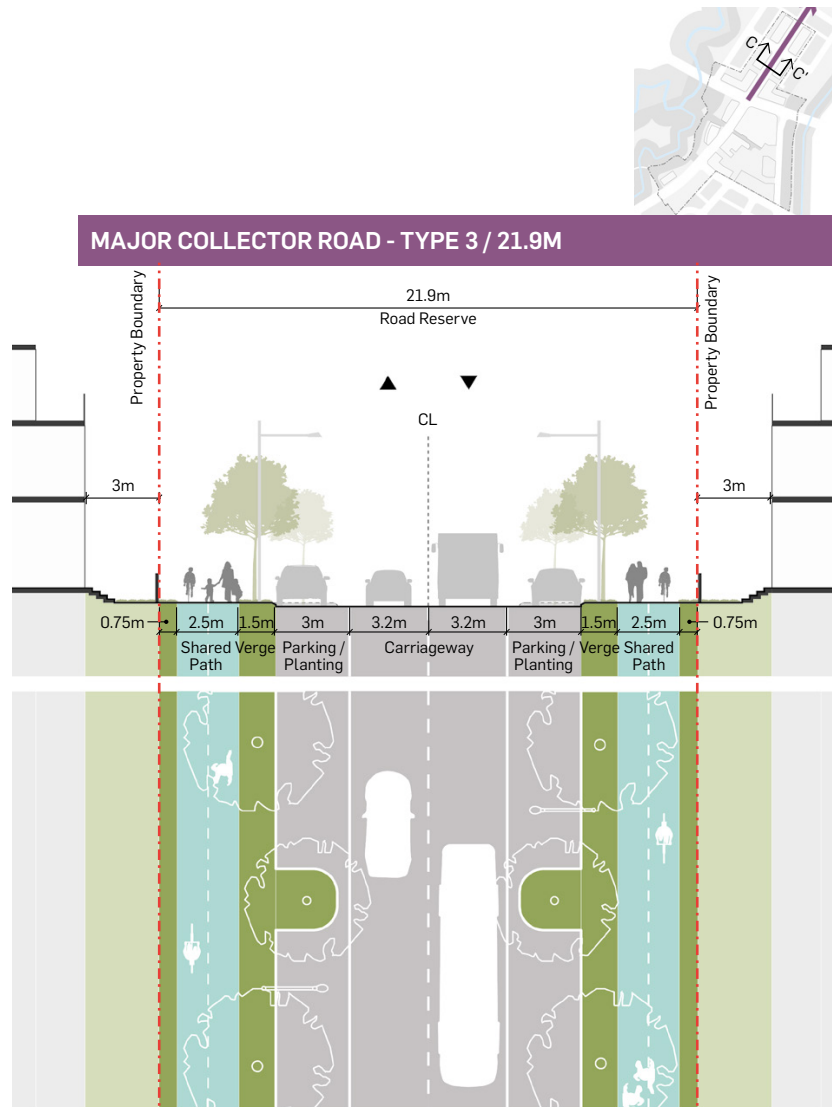


Figure 52 Indicative Street Section C-C' - Major Collector Road - Type 3

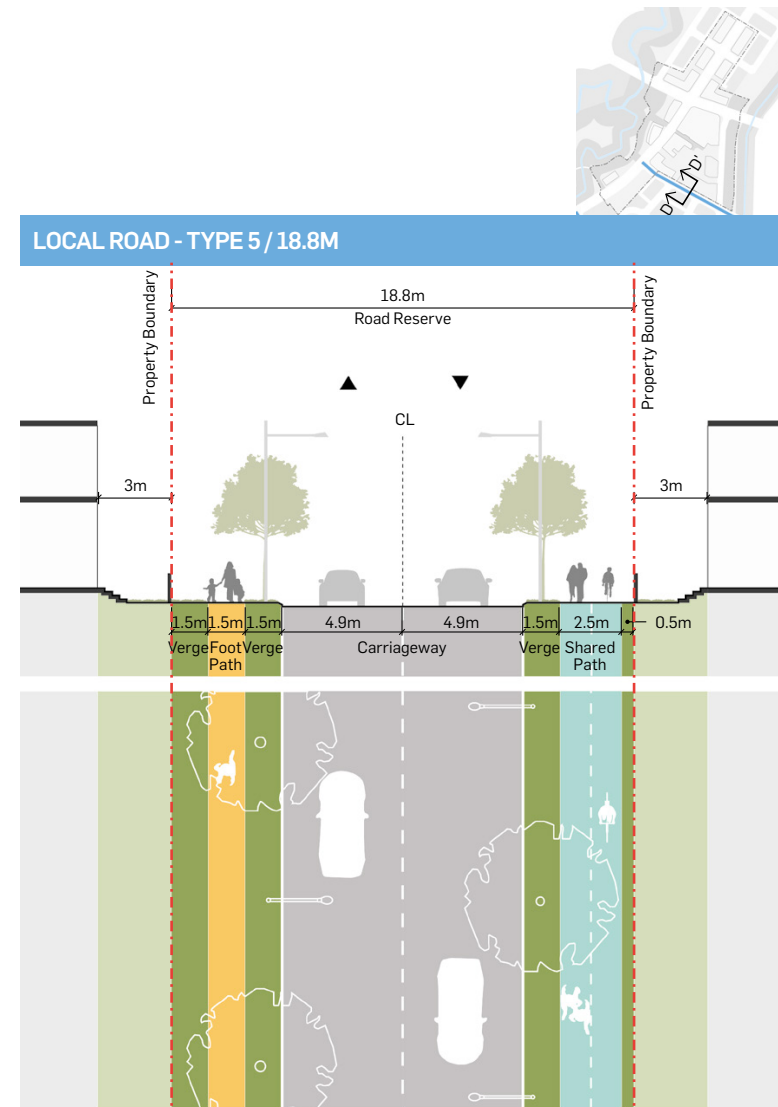


Figure 53 Indicative Street Section D-D' - Local Road - Type 5

## MARSHALL MOUNT TOWN CENTRE



### TYPICAL STREET SECTION

#### ACCESS STREET - TYPE 6 / 17.1M

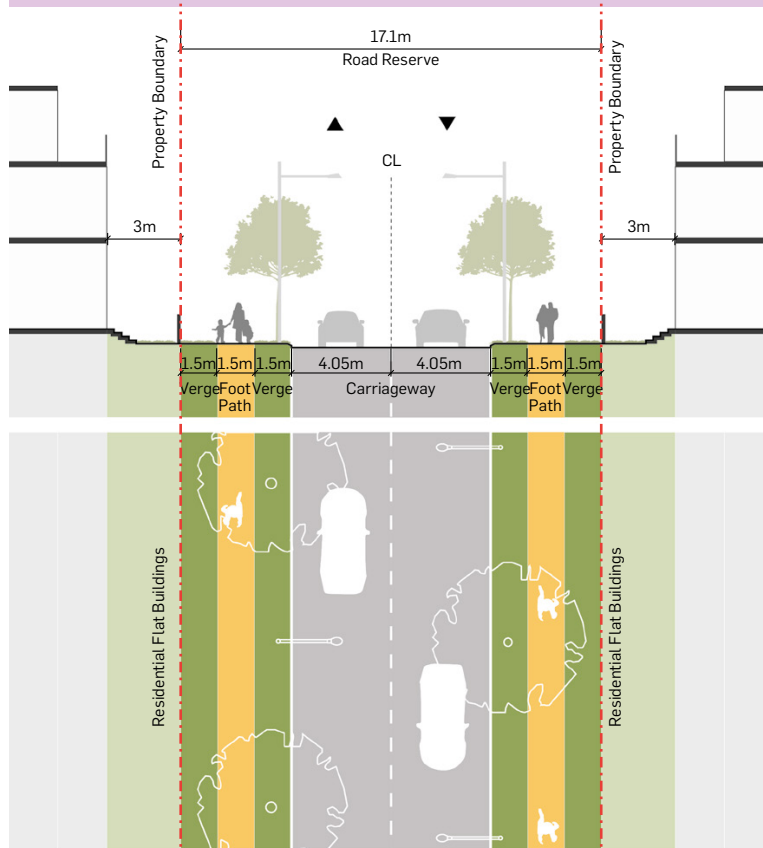


Figure 55 Indicative Street Section E-E' - Access Street - Type 6

#### ACCESS PLACE - BUSHFIRE COMPLIANT / 14.5M

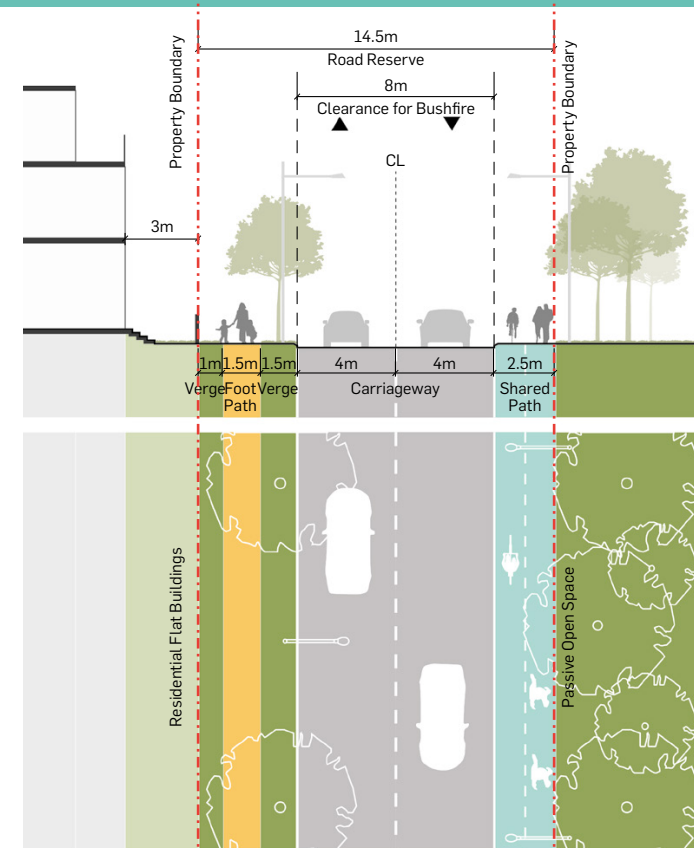


Figure 54 Indicative Street Section F-F' - Access Place - Bushfire Compliant

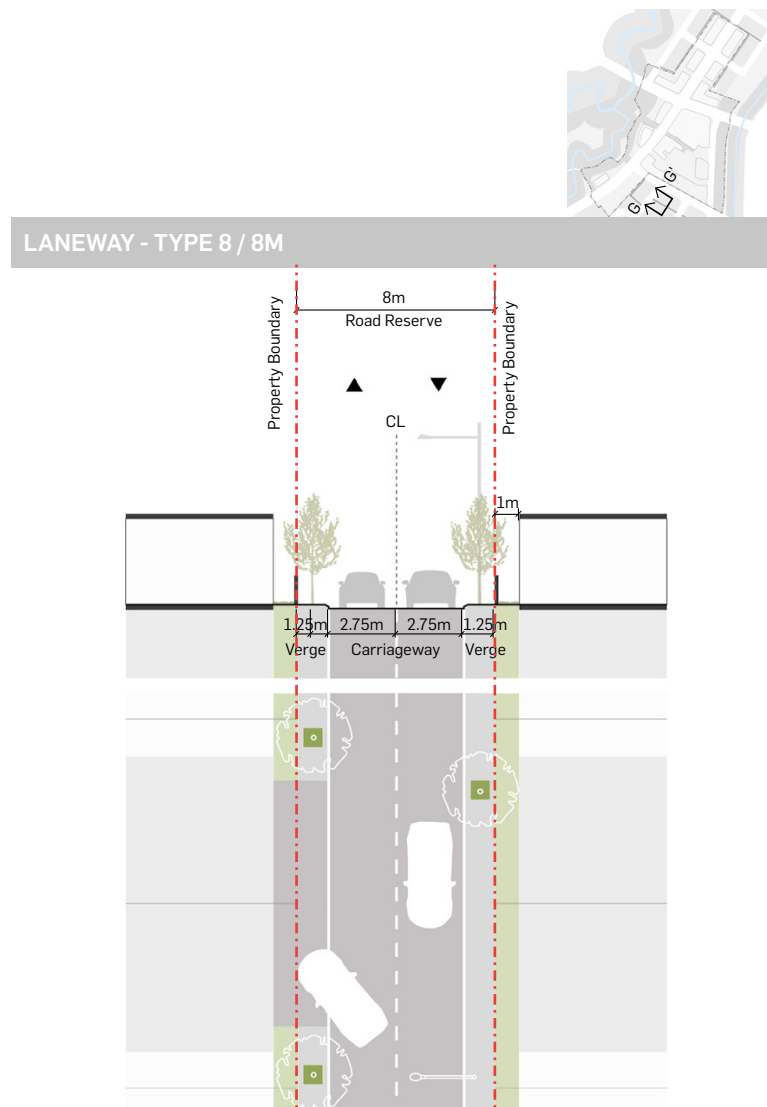


Figure 56 Indicative Street Section G-G' - Laneway - Type 8

## MARSHALL MOUNT TOWN CENTRE



### MAIN STREET CHARACTERISTICS



Main Street with ground floor retail activation and wide footpath. Rouse Hill, NSW



Main Street with ground floor retail and upper level residential. Polaris Town Centre, VIC

The Main Street is the primary activation zone within Marshall Mount Town Centre and characterised by the following:

- Ground floor retail activation with upper level residential apartment on the eastern side;
- Ground level community activation on the western side;
- Wider footpath fronting the retail to allow for outdoor dining;
- Tree canopy cover;
- Adopted similar carriageway configuration with Type 2A Major Collector Road type.
- Slower traffic speed environment;
- On-street parking for convenience and act as buffer to the traffic movement; and
- Potential signalised pedestrian crossing with the Fig Tree Link - subject to future transport analysis.

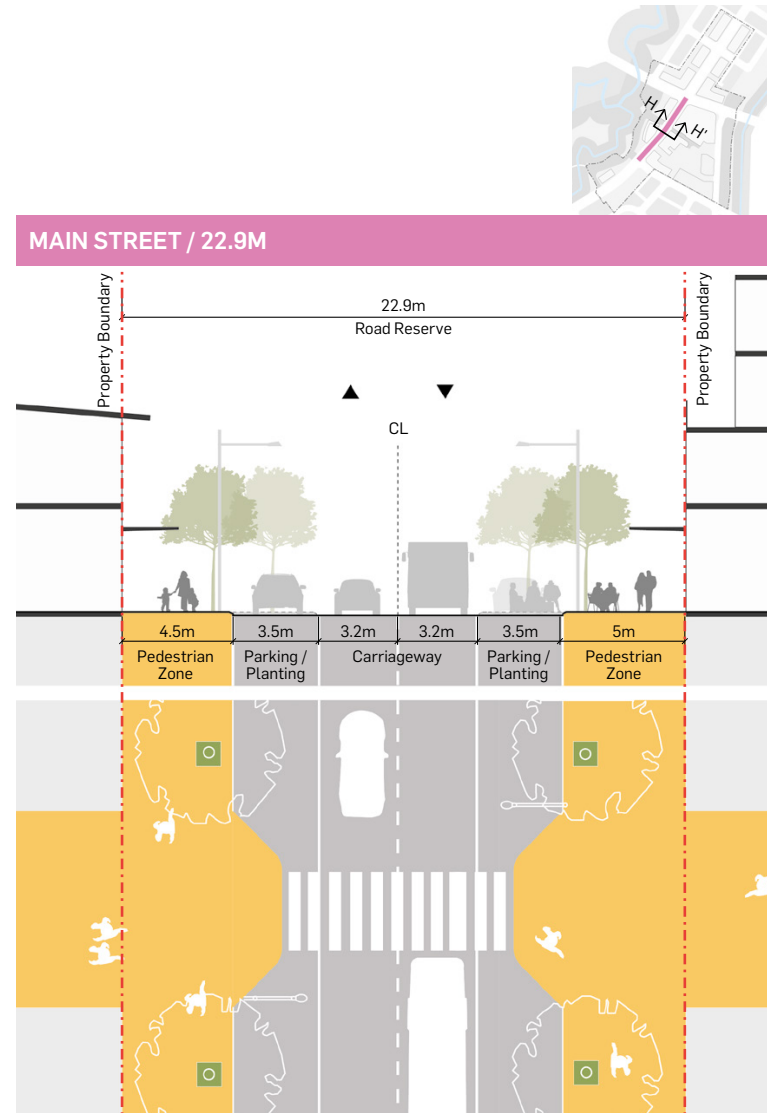


Figure 57 Indicative Street Section H-H' - Main Street





## MOVEMENT AND PLACE CLASSIFICATIONS

The Transport for NSW's Design of Roads and Streets document provide guidelines and classification of the four road and street environment identified in the Movement and Place framework. This includes:

- Main Road;
- Main Street;
- Local Street; and
- Civic Space.






The guidelines provide further sub-classifications for each environment based on its function and design parameters.

The following diagram identifies the adopted road and street environment sub-classification within Marshall Mount Town Centre.

### LEGEND

 Marshall Mount Town Centre

### MOVEMENT AND PLACE CLASSIFICATION

-  Main Street - Transit Boulevard
-  Main Street - High Activity High Street
-  Main Street - Connector Avenue
-  Local Street - Yield Street
-  Local Street - Residential Lane

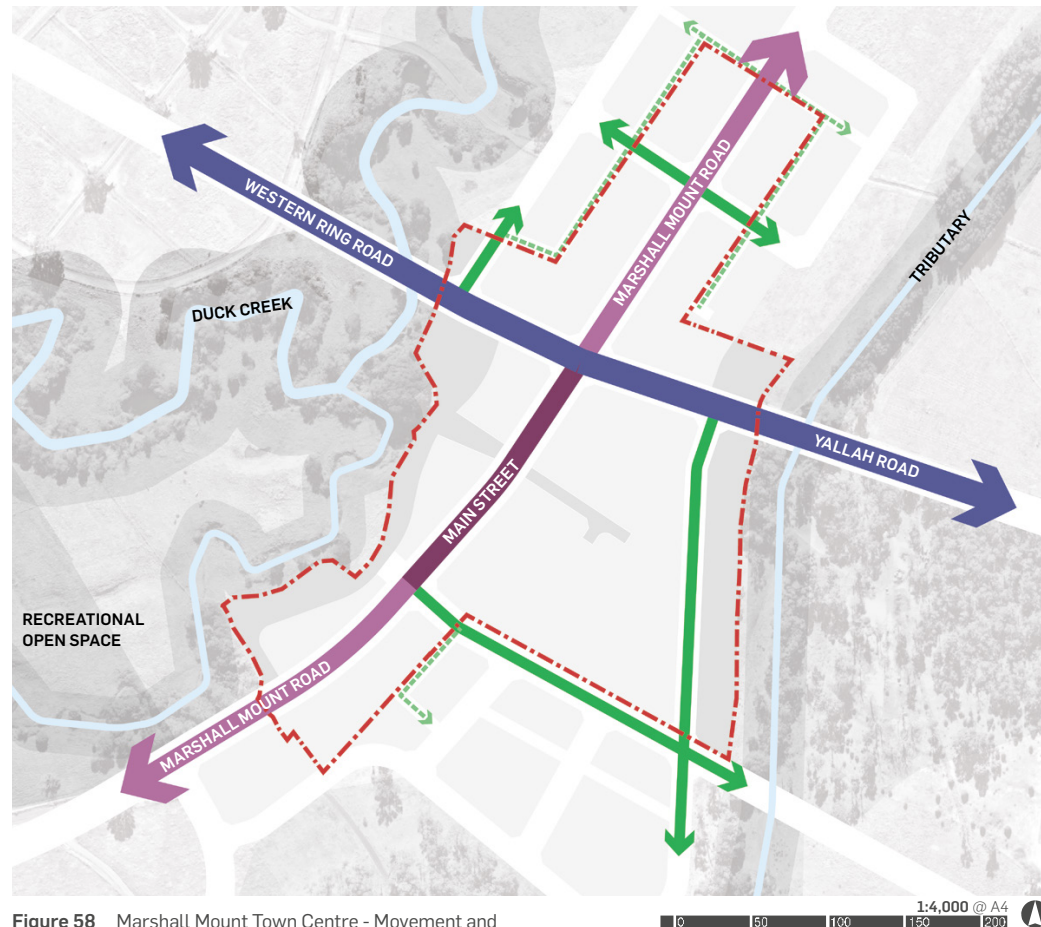


Figure 58 Marshall Mount Town Centre - Movement and Place Classifications

## MARSHALL MOUNT TOWN CENTRE



### INDICATIVE PARKING STRATEGY

The parking strategy for Marshall Mount Town Centre includes:

- Proposed a consolidated at-grade and basement parking provision within the Town Centre Core precinct with additional accessible parking close to community facility site and kerbside parking along Main Street and Local Street.
- Each residential lot to provide its own parking provision in basement with access from the rear laneway.

The following diagram identify the indicative parking provision.

#### LEGEND

- Marshall Mount Town Centre
- Town Centre Core

#### PARKING AND ACCESS

- Indicative Access Points
- Indicative Access to Basement Parking
- Indicative Consolidated Town Centre Core At-grade Parking
- Indicative At-grade Parking
- Indicative Ground Level Podium Parking
- Indicative Basement Parking
- Indicative Kerbside Parking
- Indicative Loading Area

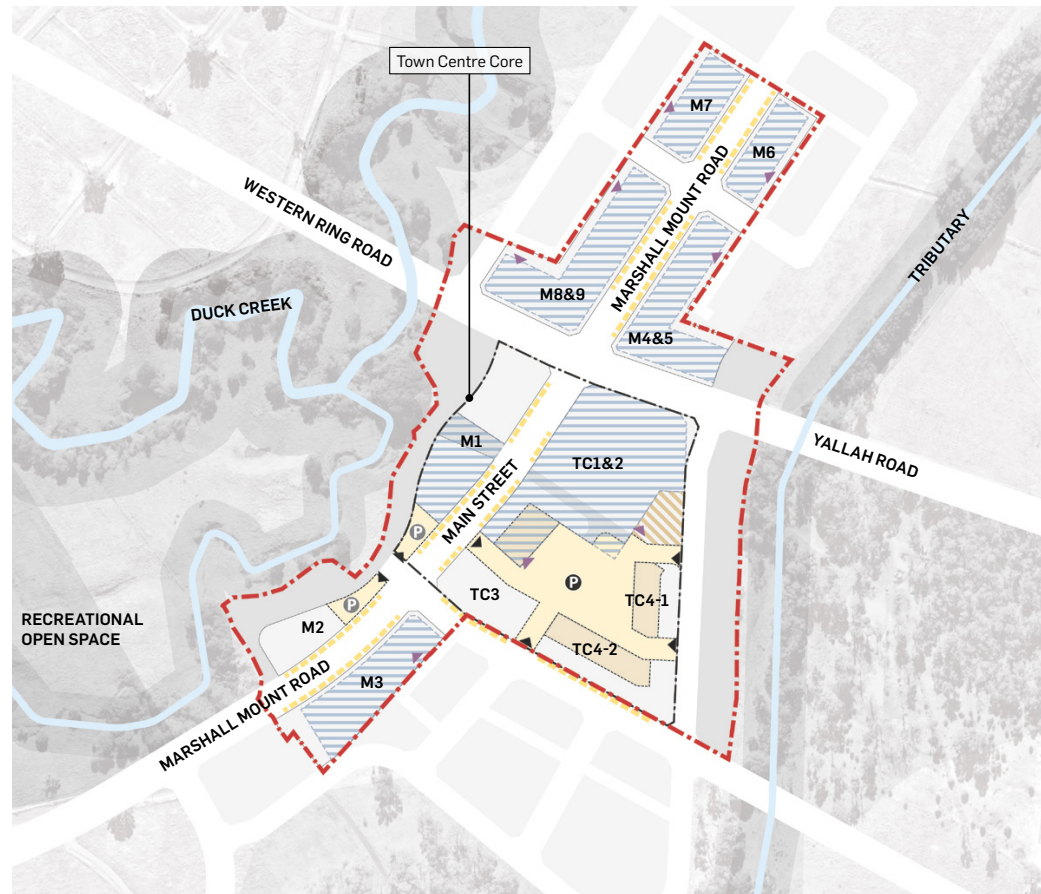


Figure 59 Marshall Mount Town Centre - Indicative Parking Strategy

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## MARSHALL MOUNT TOWN CENTRE



### STRATEGY 3 CENTRE ACTIVATION

Create a walkable and compact Town Centre that is accessible to the community and benefiting from the exposure to passing trade along Yallah Road and Marshall Mount Road.

The key centre activation outcomes for Marshall Mount Town Centre include:

	<b>Marshall Mount Town Centre</b>
<b>GROUND LEVEL USES</b>	
In order to create a walkable and viable Town Centre, the retail and community uses are to be concentrated along Main Street, Fig Tree Link and the primary intersection of Marshall Mount Road and Yallah Road.	
	<b>Retail (Supermarket)</b> Proposed full-size supermarket within the Town Centre Core precinct with access from the Fig Tree Link.
	<b>Retail (Convenience)</b> Proposed convenience retail including specialty retail, pharmacy, shops sleeveing the Retail (Supermarket) along the Main Street and Fig Tree Link.
	<b>Retail (Stand-alone / Hospitality)</b> Proposed standalone / hospitality retail including pub, tavern, restaurant, brewery.

	<b>Community Facility</b> <ul style="list-style-type: none"> <li>Proposed community facilities include a multi-purpose community centre and a library.</li> <li>Locate community facilities along the west side of Main Street and open the interface towards Duck Creek to the west.</li> </ul>
	<b>Private Child Care</b> Locate the child care at the southeast corner of Main Street.
<b>PLACE ACTIVATION</b>	
	<b>Main Street Activation</b>
	<b>Existing Moreton Bay Fig Tree</b>
	<b>Fig Tree Link</b>
	<b>Western Promenade</b>
	<b>Public Area within Core Activation Zone</b>

### PRINCIPLE 3 AN ACTIVATED PLACE AND VIALE CENTRE

	<b>Private Child Care Outdoor Space</b>
<b>ACTIVE STREET FRONTAGE</b>	
	<b>Primary Retail Active Frontage</b> Ground-level retail uses with frequent entrances and predominantly glazed frontage, located along the Main Street and Fig Tree Link (eastern side).
	<b>Secondary Retail Active Frontage</b> Ground-level retail uses with predominant glazed frontage and allow for some blank walls and less frequent entrances, located at the primary intersection of Marshall Mount Road and Western Ring Road and south of Yallah Road.
	<b>Primary Community Active Frontage</b> Ground level community facility uses with main entrances and predominantly glazed frontage, located along the Main Street.
	<b>Secondary Community Active Frontage</b> Ground-level community facility uses with predominant glazed frontage and allow for some blank walls and less frequent entrances, located along the Western Promenade and Fig Tree Link (western side).



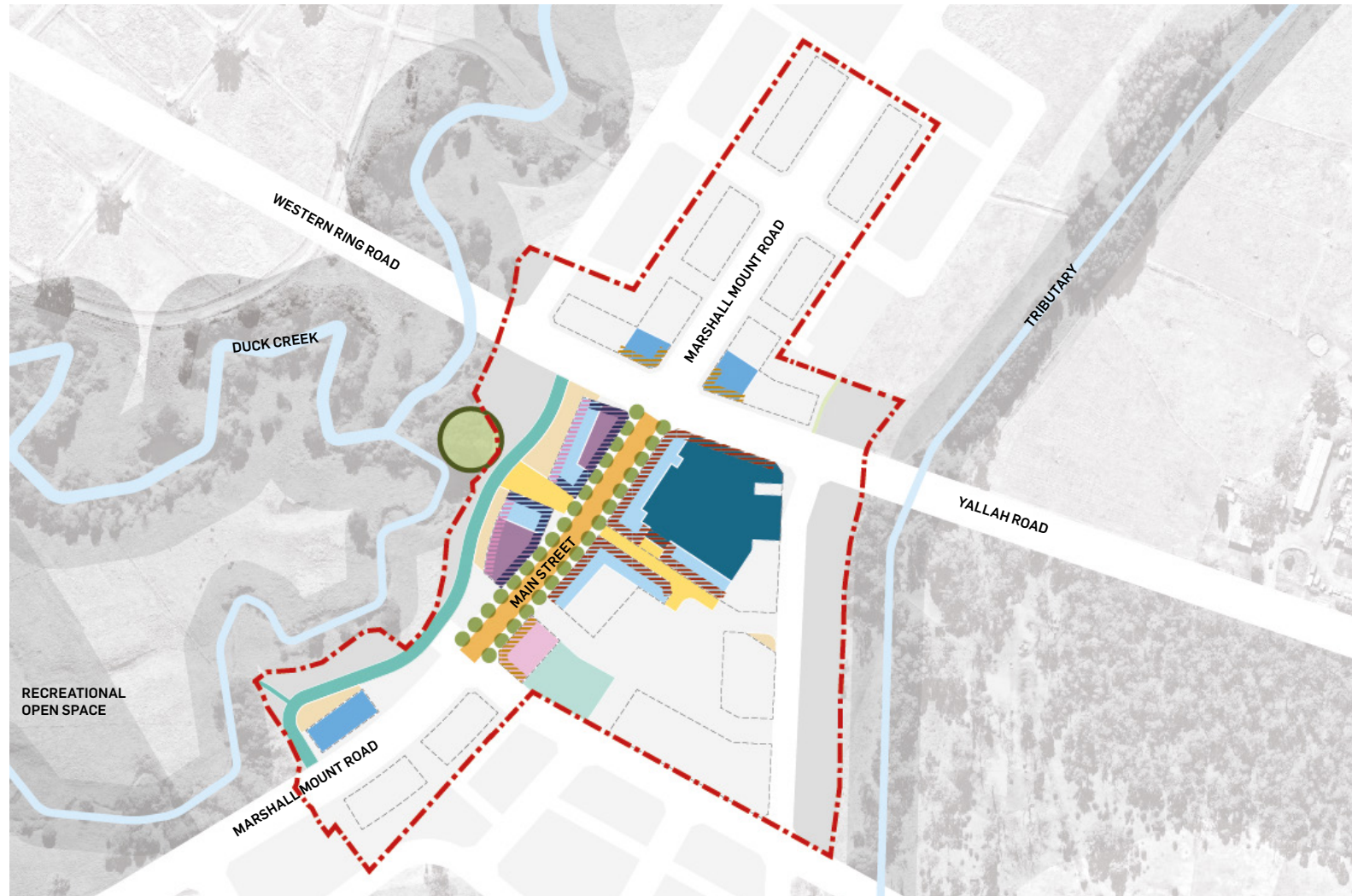


Figure 60 Marshall Mount Town Centre - Centre Activation Strategy

## MARSHALL MOUNT TOWN CENTRE



### RETAIL AND COMMUNITY SPACE PROVISION

Marshall Mount Town Centre provides retail and community uses comprised of supermarket, convenience retail, standalone / hospitality retail, multi-purpose community hall and potential private child care facility.

The master plan indicates the Centre can accommodate approximately 8,100 sqm of retail GFA (including a supermarket), 104% more than the retail demand forecast of 3,999 sqm, catering for the retail demand beyond 2041.

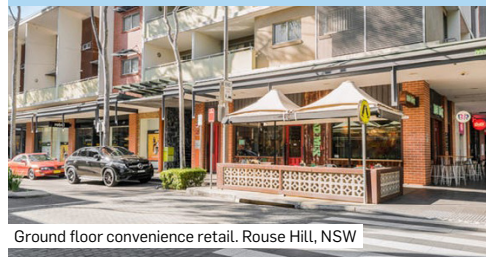
#### RETAIL - SUPERMARKET



#### COMMUNITY - MULTI PURPOSE HALL



#### RETAIL - CONVENIENCE



#### PRIVATE CHILDCARE



#### RETAIL - STAND ALONE / HOSPITALITY



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## MARSHALL MOUNT TOWN CENTRE



### STRATEGY 4 HOUSING MIX

Creating a diverse community with housing mix offer close to the Town Centre to support the centre's viability.



Marshall Mount Town Centre

#### HOUSING TYPOLOGY (WITHIN TOWN CENTRE)



##### Shoptop Housing

Proposed shop top housing located above the ground level uses (supermarket / retail / private child care) and the primary intersection corner buildings.



##### Residential Flat Buildings

Proposed medium rise apartments predominantly located along Marshall Mount Road, Yallah Road and the southeast corner of the Town Centre Core.

#### HOUSING TYPOLOGY (ADJACENT TOWN CENTRE)



##### Medium Density Housing

Potential mix of medium housing density housing typologies including apartments, attached homes and semi-detached homes within the surrounding R3 zone.

#### SHOP TOP HOUSING



Up to 6 storeys shoptop housing.  
Rouse Hill, NSW

#### RESIDENTIAL FLAT BUILDINGS



3-4 storeys residential flat buildings with facade articulation.  
Harold Park, NSW

#### PRINCIPLE 4

DELIVER VARIED HOUSING OFFER  
CLOSE TO AMENITIES

#### MEDIUM DENSITY HOUSING (ADJACENT TO TOWN CENTRE)



Thornton Park, Penrith NSW



Medium density housing typologies - attached homes, semi detached, dual occupancy.



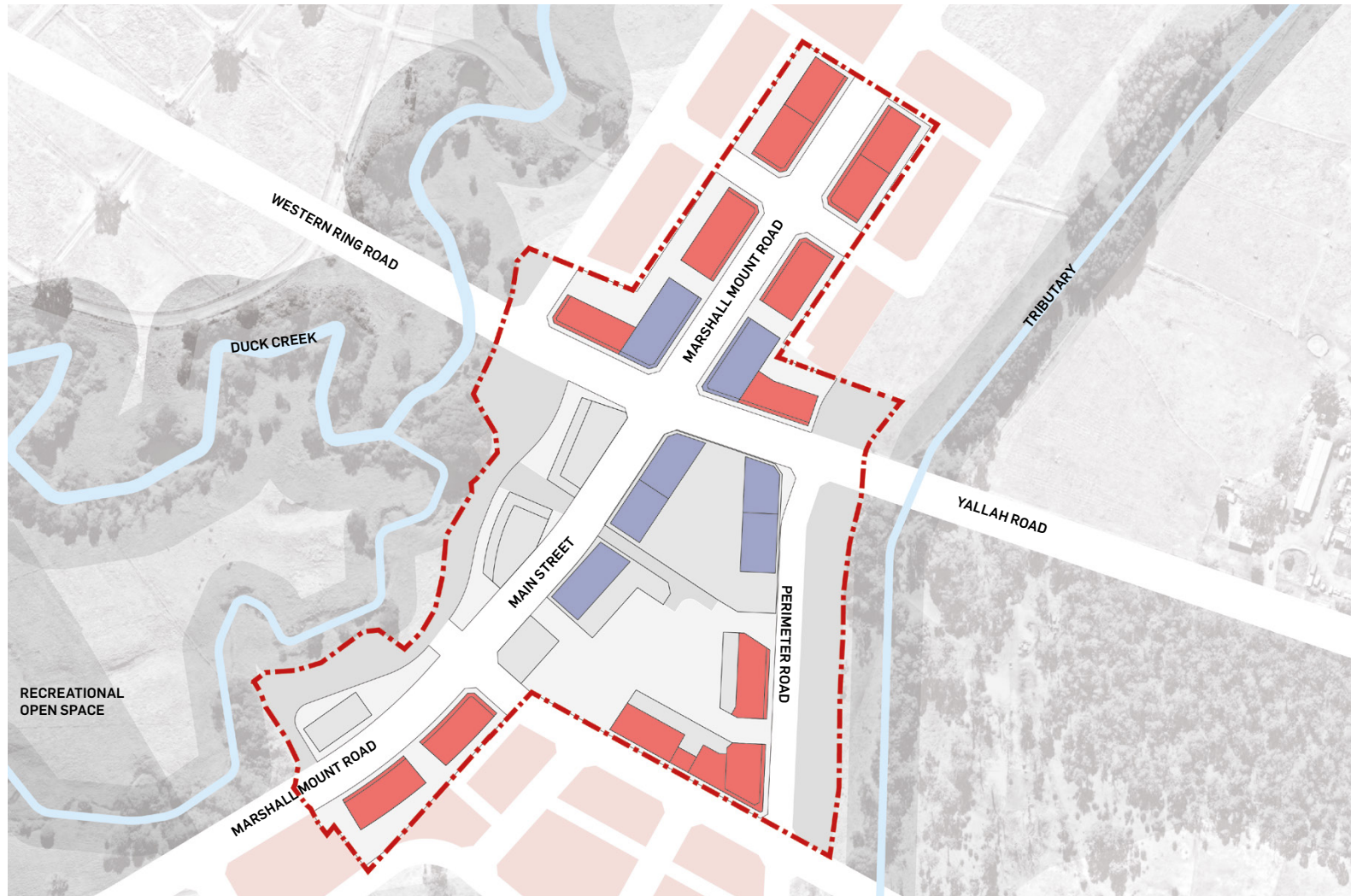


Figure 61 Marshall Mount Town Centre - Housing Mix Strategy

## MARSHALL MOUNT TOWN CENTRE



### STRATEGY 5 BUILT FORM

The Town Centre built form strategy is designed to provide a varied height outcomes\* that celebrates and responds to the surrounding natural assets, amenities and context.

The built form configuration within Marshall Mount Town Centres is based on the following principles:

- **Reinforce View Corridors** - Reinforce view corridors to the Illawarra Escarpment along Yallah Road and new east-west Local Road, and to the existing Moreton Bay Fig Tree along the Fig Tree Link.
- **Varied Building Height and Transition** - Locate the highest building within the Town Centre at the primary intersection and transition the heights towards the open space along the western and eastern edge and towards the R3 zone to the north and south.
- **Key Marker Buildings** - The corner buildings at the primary intersection are the key marker buildings that provide high-quality building design and articulation addressing the corner location.
- **Residential Building Orientation** - Optimise residential building north-south orientation for the taller elements and east-west orientation for lower height elements to maximise solar access to units and communal open space.
- **Varied Communal Open Spaces** - Facilitate rooftop communal open spaces in varied heights including ground level, podium rooftop level and north-facing tower rooftop level.



#### Marshall Mount Town Centre

##### BUILDING HEIGHT

###### 1-2 Storey Building

- Proposed 1-2 storey buildings at the following locations:
- Single storey buildings at the north side of Marshall Mouth Road at the southwest corner of the town centre.
  - 2-storey buildings along the west side of the Main Street.

###### 3\* Storey Building

Proposed 3-storey buildings along Marshall Mount Road, and the perimeter road on the east.

*NOTE: There may be merit in exploring an increase in LEP permitted building heights to enable development up to 5 storeys along Marshall Mount Road (noting the exception for 1-2 storeys above) and along the perimeter road overlooking natural areas on the east edge. This may be considered via a Council led process or in response to an application made to Council, whilst ensuring an amendment is generally otherwise consistent with the master plan and guiding principles. Any LEP review process should also consider the NSW Government's recently announced bonuses for affordable housing via State Environmental Planning Policy (Housing) 2021.*

###### 4\* Storey Building

Proposed 4-storey buildings at the primary intersection corner buildings as the tallest element within the Town Centre.

*NOTE: It is recommended that Council, via the same LEP review process mentioned above, consider potential for additional heights up to 6-storey buildings at these primary intersection corner building locations (noting the exception for 1-2 storeys above).*



##### Key Building Markers

Architectural response for key buildings to mark the town centre at the junction of Yallah Road and Marshall Mount Road. To provide high-quality building design and articulation addressing the corner location.



##### Indicative Building Height (Storey)\*

### PRINCIPLE 5

#### CONTEXTUAL BUILT FORM RESPONSE

##### KEY MARKER BUILDINGS



High quality architectural design and building articulation addressing corner location. North Village, Kellyville, NSW

##### 1-2 STOREY BUILDING



One - two storey community buildings along Main Street. SOHO Village, Point Cook, VIC

##### 3-4 STOREY BUILDING



3-4 storey residential buildings with two storey streetwall / articulation. Harold Park, NSW

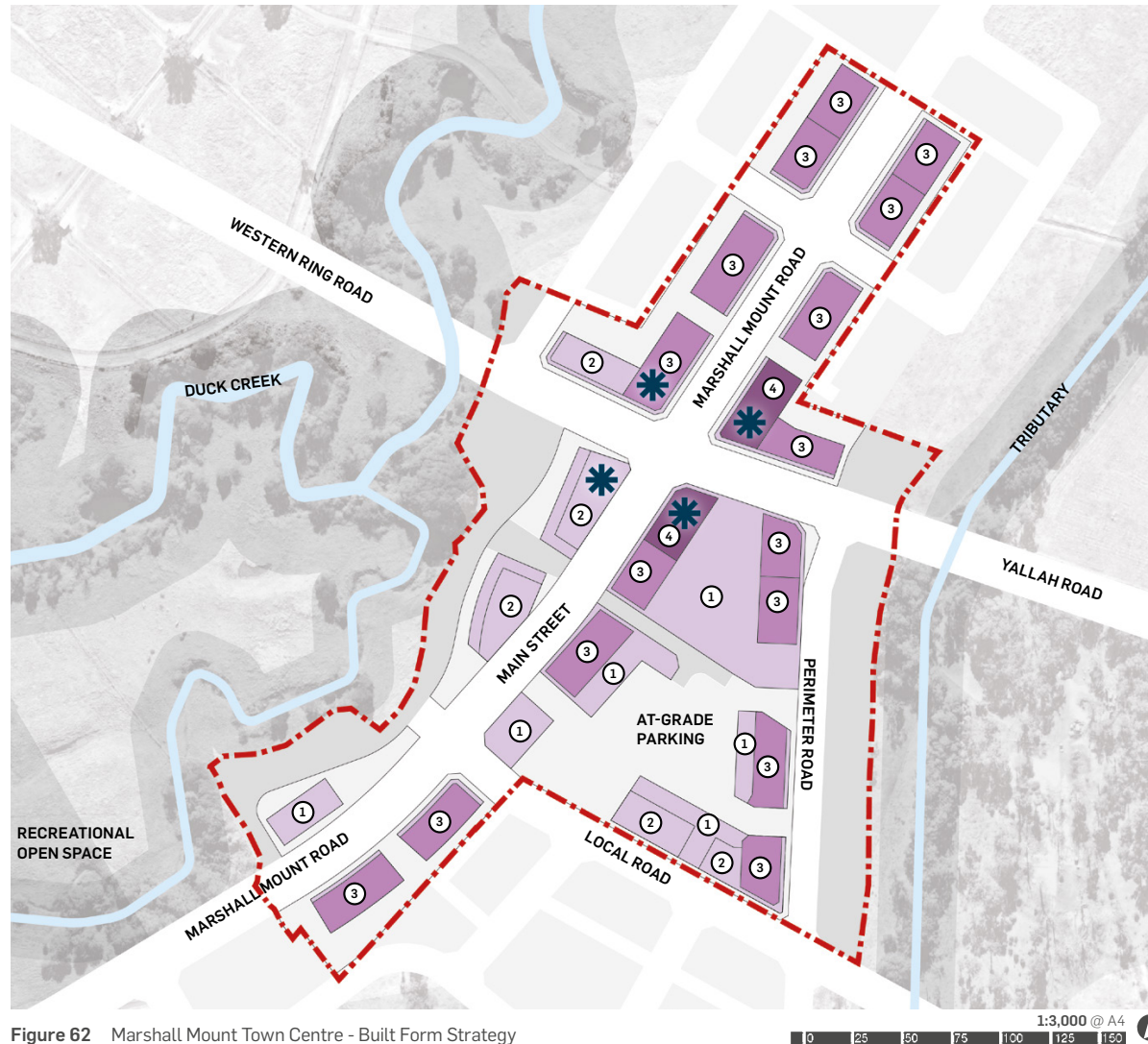


Figure 62 Marshall Mount Town Centre - Built Form Strategy

*\*NOTE: It is recommended that Council consider opportunities for additional height beyond the existing LEP controls to facilitate varied building height outcomes and facilitate achievable GFA closer to Existing LEP FSR provisions. Opportunities for additional height should be considered in the context of the NSW Governments recently announced bonuses for affordable housing via State Environmental Planning Policy (Housing) 2021.*

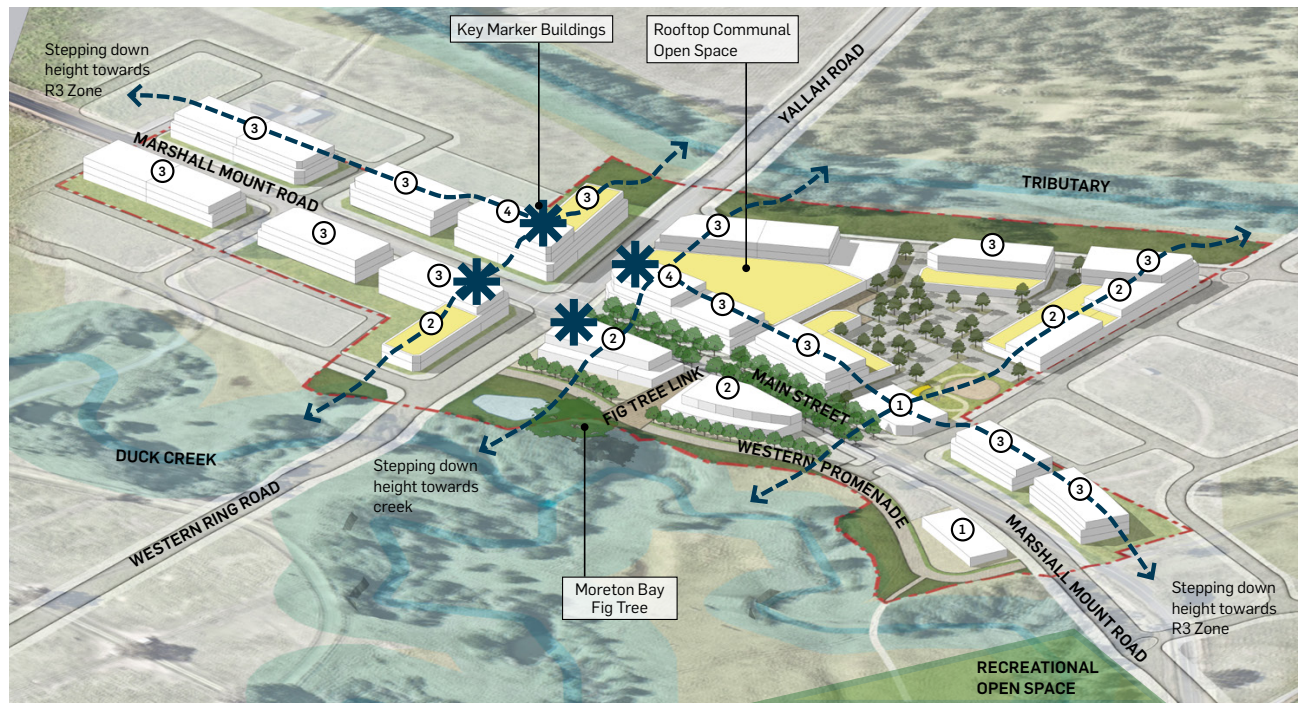


## MARSHALL MOUNT TOWN CENTRE



### INDICATIVE BUILT FORM MASSING

The following diagram provides the indicative built form strategy for Marshall Mount Town Centre.



*\*NOTE: It is recommended that Council consider opportunities for additional height beyond the existing LEP controls to facilitate varied building height outcomes and facilitate achievable GFA closer to Existing LEP FSR provisions. Opportunities for additional height should be considered in the context of the NSW Governments recently announced bonuses for affordable housing via State Environmental Planning Policy (Housing) 2021.*

Figure 63 Marshall Mount Town Centre - Indicative Built Form Massing





## INDICATIVE BUILDING SETBACKS

The following diagram provides the indicative building setbacks within the Marshall Mount Town Centre.



**Marshall Mount Town Centre**

### BUILDING SETBACKS



#### 0m Ground Floor Street Setback

Interface with retail and community facilities with up to two storey streetwall on all streets.



#### 3m Ground Floor Street Setback

Interface with buildings with ground level residential with up to two storey street wall on all streets.



#### 2m Upper Level Street Setback

2m upper level setbacks above the two storey streetwall on all streets.



#### 4m Ground Floor Setback (Western Promenade)

Interface with buildings fronting the Western Promenade with up to two storey.



#### 6m Upper Level Setback (Western Promenade)

Additional 6m setback for buildings fronting the Western Promenade

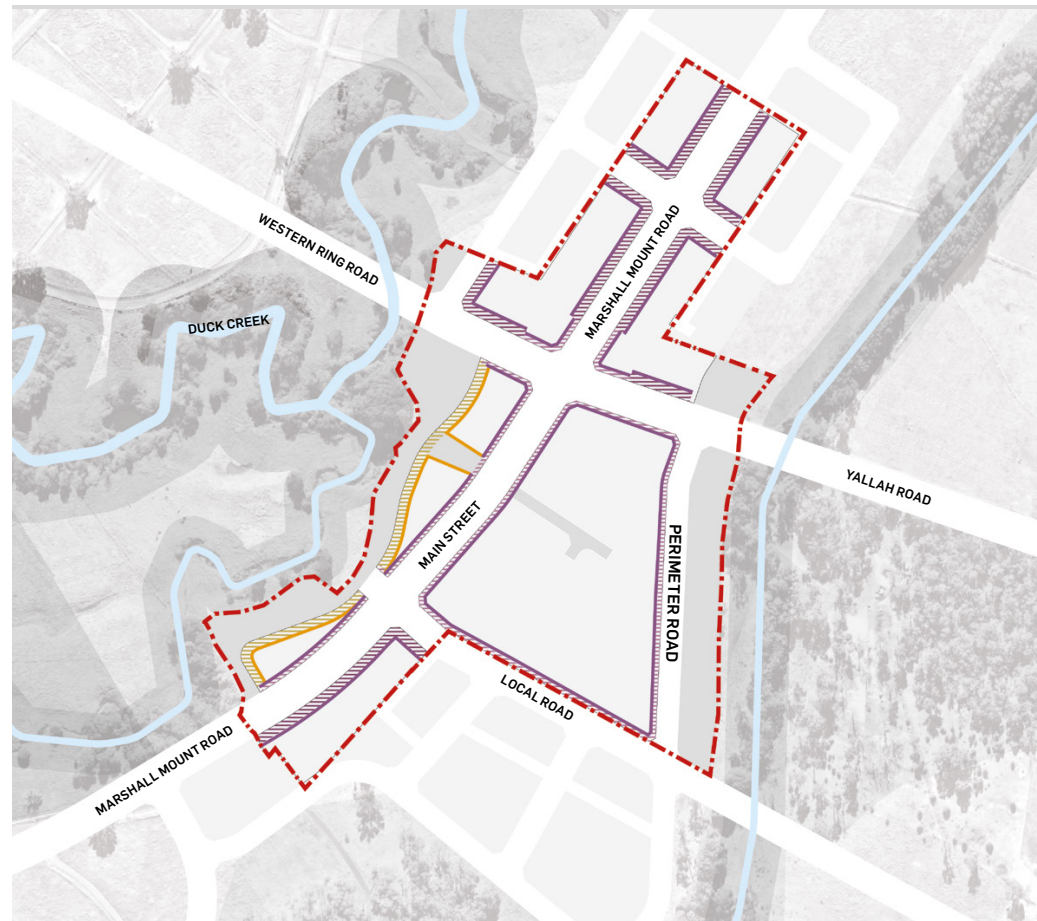


Figure 64 Marshall Mount Town Centre - Indicative Building Setbacks

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0 50 100 150 200

Prepared by Urbis for Wollongong City Council B37

## MARSHALL MOUNT TOWN CENTRE

# INDICATIVE YIELD BREAKDOWN - MARSHALL MOUNT TOWN CENTRE

The following table and diagram provides the proposed land use and yield breakdown of Marshall Mount Town Centre.

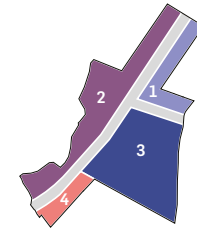


Table 8 Marshall Mount Town Centre - Yield Breakdown

LAND USE AND SITE AREA					GROSS FLOOR AREA										
LOT NO.	LAND USE	LAND USE AREA (SQM)	LAND USE AREA (HA)	LAND USE AREA (%)	RESI GFA (SQM)	COMMUNITY GFA (SQM)	CHILD CARE GFA (SQM)	RETAIL - SUPERMARKET GFA (SQM)	RETAIL - CONVENIENCE GFA (SQM)	RETAIL - F&B GFA (SQM)	TOTAL GFA (SQM)	FSR (m:1)	DW. NO.	DW. NO. (%)	DW/HA
SITE 1															
M4	Retail (Stand-alone / Hospitality) / Residential	2,940	0.29	3.6%	4,050					452	4,502	1.53	43	12.5%	146.2
M5	Residential	1,549	0.15	1.9%	2,075						2,075	1.34	22	6.4%	142.1
M6	Residential	2,145	0.21	2.6%	3,145						2,075	1.47	34	9.9%	158.5
OS	Open Space	1,220	0.12	1.5%											
	Open Space / Constrained Land	1,220	0.12												
	Road Reserve	895	0.09	1.1%											
SITE 1 TOTAL		8,749	0.87	10.6%	9,270					452	9,722	1.47	99	28.9%	149.2
SITE 2															
M1	Community / Retail (Convenience)	4,713	0.47	5.7%		2,506			1,016		3,522	0.75			
M2	Retail (Stand-alone / Hospitality)	1,838	0.18	2.2%						544	544	0.30			
M7	Residential	2,589	0.26	3.1%	3,144						3,144	1.21	34	9.9%	131.3
M8	Residential	2,112	0.21	2.6%	2,363						2,363	1.12	25	7.3%	118.4
M9	Retail (Stand-alone / Hospitality) / Residential	3,517	0.35	4.3%	3,281					336	3,617	1.03	35	10.2%	99.5
OS	Open Space	6,474	0.65	7.9%											
	Western Promenade	2,366	0.24												
	Open Space / Constrained Land	4,108	0.41												
	Road Reserve	3,581	0.36	4.3%											
SITE 2 TOTAL		24,824	2.48	30.1%	8,788	2,506			1,016	880	13,190	0.53	94	27.4%	37.9

*\*NOTE: It is recommended that Council consider opportunities for additional height beyond the existing LEP controls to facilitate varied building height outcomes and facilitate achievable GFA closer to Existing LEP FSR provisions. Opportunities for additional height should be considered in the context of the NSW Governments recently announced bonuses for affordable housing via State Environmental Planning Policy (Housing) 2021. The yield and parking outcomes would be expected to change if height standards are increased.*

LAND USE AND SITE AREA					GROSS FLOOR AREA										
LOT NO.	LAND USE	LAND USE AREA (SQM)	LAND USE AREA (HA)	LAND USE AREA (%)	RESI GFA (SQM)	COMMUNITY GFA (SQM)	CHILD CARE GFA (SQM)	RETAIL - SUPERMARKET GFA (SQM)	RETAIL - CONVENIENCE GFA (SQM)	RETAIL - F&B GFA (SQM)	TOTAL GFA (SQM)	FSR (N1)	DW. NO.	DW. NO. (%)	DW./HA
<b>SITE 3</b>															
TC1	Retail (Convenience & Supermarket) / Residential	8,169	0.82	9.9%	4,641			3,698	1,257		9,596	1.17	50	14.6%	61.2
TC2	Retail (Convenience) / Supermarket / Residential	3,488	0.35	4.2%	1,407				886		2,293	0.66	15	4.4%	43.0
TC3	Private Child Care	1,954	0.19	2.4%			566				566	0.29			
TC4	Residential	8,288	0.83	10.1%	4,272						4,272	0.52	45	13.1%	54.3
OS	Open Space	4,999	0.50	6.1%											
	Open Space / Constrained Land	4,999	0.50												
	Road Reserve	3,624	0.36	4.3%											
<b>SITE 3 TOTAL</b>		<b>30,522</b>	<b>3.05</b>	<b>37.0%</b>	<b>10,321</b>		<b>566</b>	<b>3,698</b>	<b>2,142</b>		<b>16,727</b>	<b>0.55</b>	<b>110</b>	<b>32.1%</b>	<b>36.0</b>
<b>SITE 4</b>															
M3	Residential	3,945	0.39	4.7%	3,716						3,716	0.94	40	11.7%	101.4
	Road Reserve	686	0.07	0.9%											
<b>SITE 4 TOTAL</b>		<b>4,631</b>	<b>0.46</b>	<b>5.6%</b>	<b>3,716</b>						<b>3,716</b>	<b>0.80</b>	<b>40</b>	<b>11.7%</b>	<b>86.4</b>
<b>EXISTING ROAD RESERVE</b>		<b>13,740</b>	<b>1.37</b>	<b>16.7%</b>											
<b>TOTAL</b>		<b>82,466</b>	<b>8.25</b>	<b>100.0%</b>	<b>32,095</b>	<b>2,506</b>	<b>566</b>	<b>3,698</b>	<b>3,158</b>	<b>1,332</b>	<b>43,355</b>	<b>0.53</b>	<b>343</b>	<b>100.0%</b>	<b>41.6</b>

## INDICATIVE MASTER PLAN

# FOWLERS VILLAGE CENTRE

## VISION

The vision for Fowlers Village Centre is to create:

**“A compact and local scale retail precinct, conveniently located for daily needs and accessible via public and active transport. Characterised by a Main Street which is aligned to capture views to Mount Keira and Mount Kembla, it celebrates place and balances pedestrian and vehicle movement.”**

To enable this vision, the Village Centre is to deliver:

- **Celebrate the scenic landscape character:** Align the Main Street along view lines to Mount Keira and Mount Kembla.
- **Active and healthy lifestyles:** Provide seamless pedestrian and cyclist connections into the village centre from the adjacent roadways, open space and key destinations.
- **Day-to-day convenience:** Concentrate supermarket, retail and services along the Main Street with visibility and access from Cleveland Road.
- **Housing mix close to the centre:** Locate medium-density housing typologies close to the Village Centre providing housing choice with convenience.

The concept master plan for Fowlers Village Centre is illustrated in the opposite page.





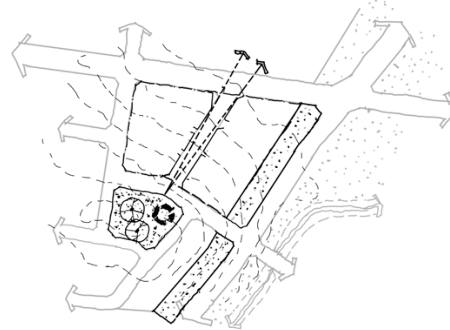
Figure 65 Fowlers Village Centre - Concept Master Plan

## FOWLERS VILLAGE CENTRE

### GUIDING PRINCIPLES

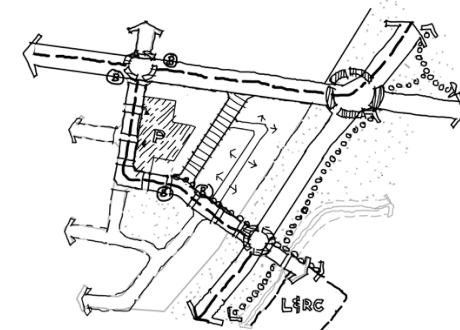
Five guiding principles were identified for the Fowlers Village Centre site which brings together the planning context, place analysis and development opportunities to underpin the master plan.

### 01 CELEBRATE COUNTRY



1. Establish a local open space on the hill to the south of the centre retaining the existing mature trees.
2. Establish a view corridor from the local open space along the Main Street, through to Mount Kembla and Mount Keira.
3. Design street layout and building configurations to respond to topography.
4. Provide linear pathways within the Eastern Gas Pipeline easement.

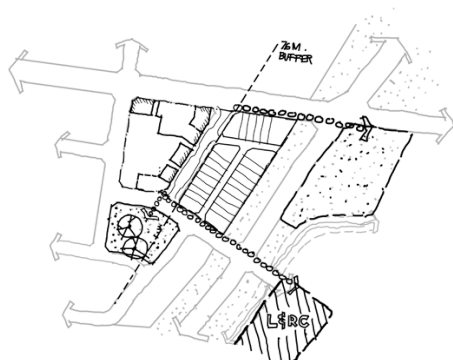
### 02 A CONNECTED AND ACCESSIBLE CENTRE



1. Design the Main Street to balance place vibrancy, pedestrian movement and vehicle access.
2. Establish a minor collector road, bypassing the centre, providing vehicular and active transport connectivity between neighbourhoods north of Cleveland Road and the future Leisure and Recreation Centre to the east of Fowlers Road (south).
3. Signalise intersections, including providing safe pedestrian and cycleway crossings, at both Cleveland Road and Fowlers Road (south) junctions with the proposed minor collector road.
4. Realign bus network through the minor collector road, locating bus stops close to the main street and future leisure centre.
5. Locate vehicle and service access off laneways and collector roads, away from the main street.
6. Consolidate at-grade parking behind retail provision.

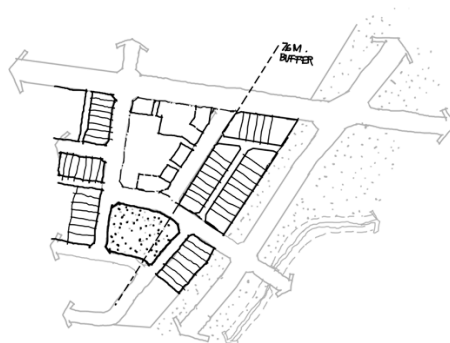


### 03 AN ACTIVATED PLACE AND VIABLE CENTRE



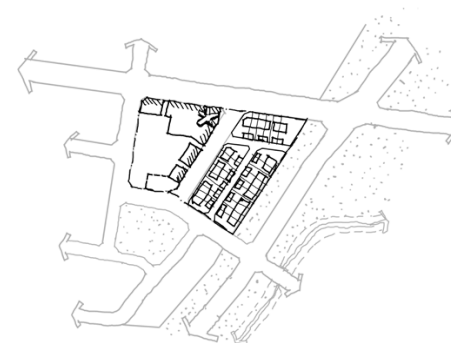
1. Create a safe and compact centre locating retail activity along the main street, beyond the 76m buffer zone to the Eastern Gas Pipeline.
2. Co-locate retail, private childcare and open space to create a compact and walkable village centre within a walkable distance to the Leisure and Recreation Centre.
3. Locate the primary retail activation along the western side of the main street and secondary activation along the southern side of Cleveland Road to maximise visibility to passing trade.
4. Promote passive surveillance and minimise vehicle and pedestrian conflicts on the Main Street by providing parking access to residential via rear laneway.

### 04 DELIVER VARIED HOUSING OFFER CLOSE TO AMENITIES



1. Locate medium density residential uses, including terraces and townhouses, within the 76m buffer zone to the Eastern Gas Pipeline.

### 05 CONTEXTUAL BUILT FORM RESPONSE

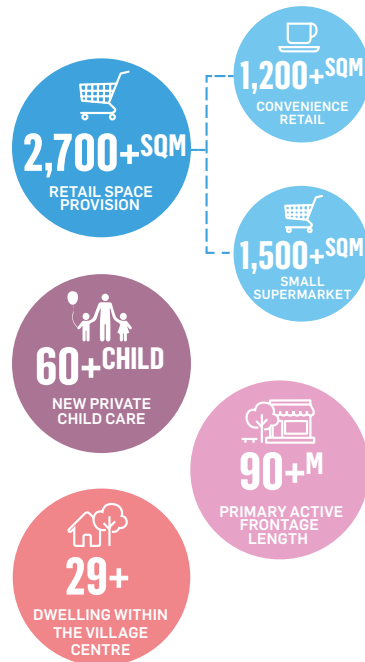


1. Locate key marker building at the north-western corner of the Main Street.
2. Designate corner lots and preferred location for 2 storey at rear and to address both streets.

## FOWLERS VILLAGE CENTRE

# STRUCTURE PLAN

The Structure Plan summarises the key elements of the Indicative Plan including the building uses, access and movement, natural assets, open space, public domain, place activation and infrastructure.



### LEGEND

Fowlers Village Centre

### BUILDING USES

Mixed Retail Supermarket and Convenience  
 Retail (Convenience)  
 Private Child Care  
 Residential (Medium Density)

### ACCESS & MOVEMENT\*

Sub-arterial Road (Type 2) - 22.9m  
 Major Collector Road (Type 3) - 21.9m  
 Minor Collector Road (Type 4) - 20.95m  
 Main Street - 20m  
 Access Road (Type 6) - 17.1m  
 Access Place (Type 7B) - 13.5m  
 Laneway (Type 8) - 8m  
 Indicative Access Point  
 Proposed Bus Stop  
 Bus Route Realignment (South-Central Loop)

At-grade Parking  
 Indicative Loading Area  
 Off-road Shared Path  
 Road-side Shared Path  
 Pedestrian Connection to the Main Street  
 Signalised Primary Intersection  
 Signalised Secondary Intersection  
 Left In Left Out  
 Roundabout

### NATURAL ASSETS

Mullet Creek Tributary  
 Core Riparian Zone  
 Proposed Retained Existing Trees  
 Recreational Open Space (Zoned RE1)  
 Visual Connections  
 2m Contour Lines

### OPEN SPACE, PUBLIC DOMAIN AND PLACE ACTIVATION

Pedestrian Link  
 Passive Open Space  
 Proposed Local Open Space  
 Private Child Care Outdoor Space  
 Ground Floor Private Open Space  
 Indicative Planting with At-grade Parking  
 Primary Active Frontage  
 Secondary Active Frontage  
 Key Marker Building

### INFRASTRUCTURE

Jemena Eastern Gas Pipeline (EGP) Easement  
 Jemena Eastern Gas Pipeline (EGP) 76m Buffer Zone

\*Note:

- Final intersection arrangement is subject to future transport analysis.
- Final layout of road network is subject to design which may differ from that show in the proposed structure plan.



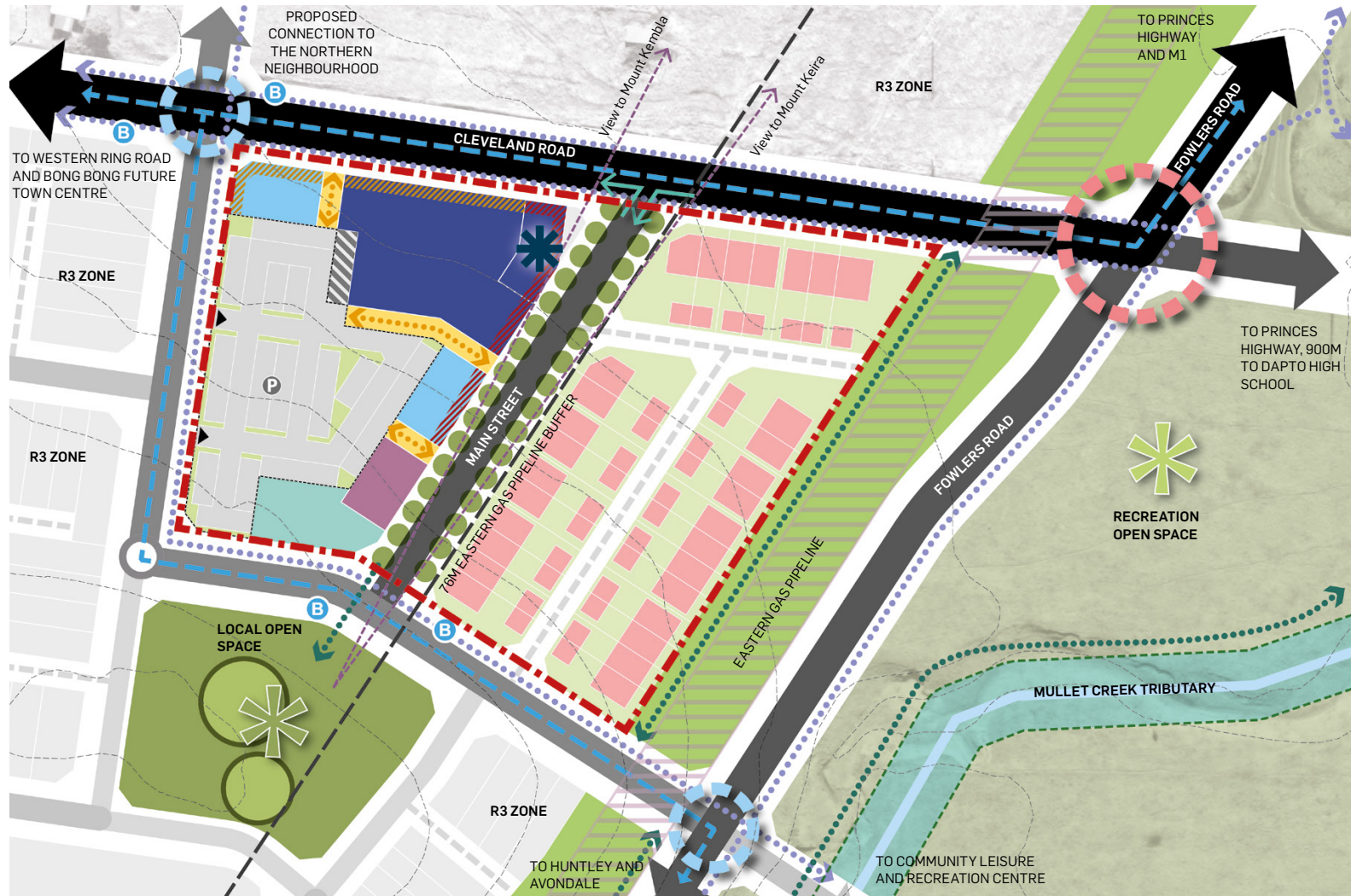
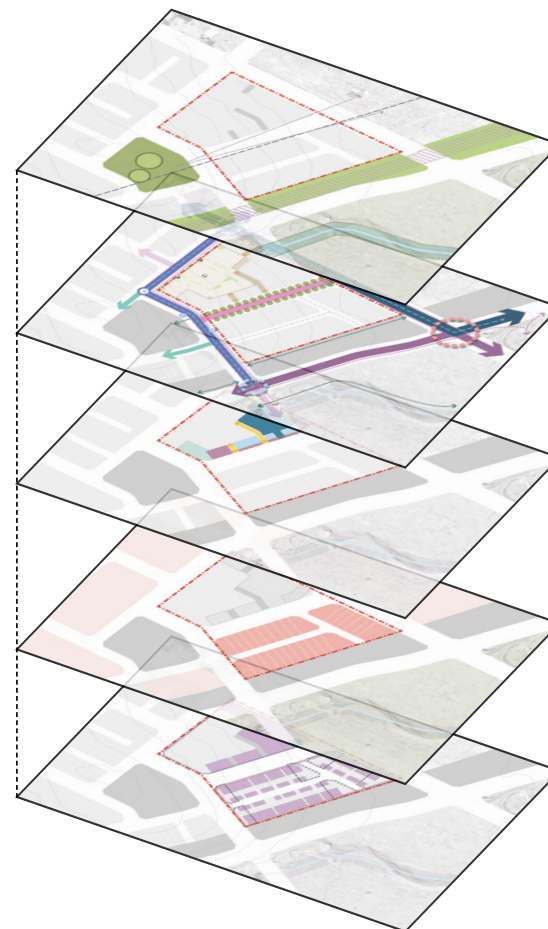


Figure 66 Fowlers Village Centre - Structure Plan

## FOWLERS VILLAGE CENTRE

### LAYERED STRATEGIES

Five layered strategies demonstrate the detailed design outcomes of a new walkable and viable Village Centre in alignment with the guiding principles.



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## FOWLERS VILLAGE CENTRE



### STRATEGY 1 NATURAL ASSETS

Celebrate the unique scenic landscape setting of the Site including protecting the existing mature trees, responding to the local topography and leveraging the infrastructure easement as landscape amenity and linear connectivity.

The key outcomes include:

	<b>Fowlers Village Centre</b>		<b>Jemena Eastern Gas Pipeline (EGP) Easement</b>
<b>NATURAL ASSETS</b>			<b>Jemena Eastern Gas Pipeline (EGP) 76m Buffer Zone</b> No Sensitive Use within 76m of the EGP.
	<b>Visual Connections</b> Established visual connections along the Main Street to Illawarra Escarpment including Mount Kembla and Mount Keira.		<b>2m Contour Lines</b>
	<b>Proposed Local Open Space</b> Proposed a 4,500 sqm new local open space to the southern end of Main Street.		<b>Mullet Creek &amp; Riparian Zone</b> Mullet Creek and its riparian zone to the southeast of Fowlers Road.
	<b>Existing Mature Trees</b> Retained existing mature trees within the proposed local open space to the south.		<b>Recreational Open Space (Zoned RE1)</b> Proposed recreational open space zone along the eastern side of Fowlers Road.
	<b>Open Space within Gas Easement</b> Provide linear pathways within the Eastern Gas Pipeline easement.		

### PRINCIPLE 1 CELEBRATE COUNTRY

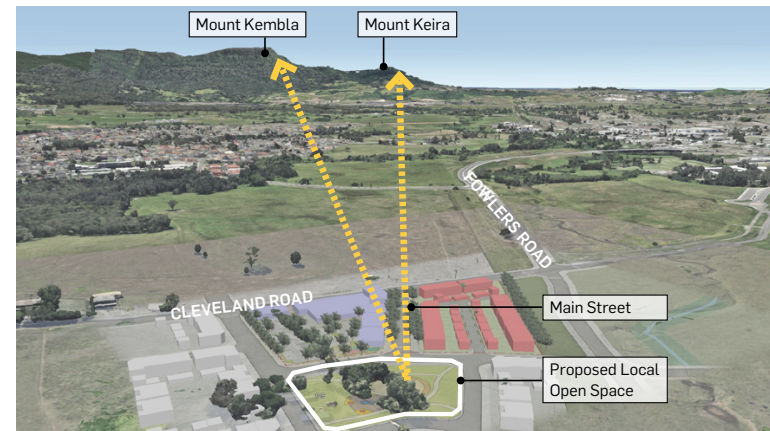


Figure 67 Visual Connections

### LOCAL OPEN SPACE AND VISUAL CONNECTIONS

The proposed local open space to the south of the Village Centre leverages the high elevation within the locality with the opportunity to create view connections to the scenic Illawarra Escarpment including Mount Kembla and Mount Keira, which inform the Main Street alignment.

The local open space is characterised by:

- Open lawn at the southern end of the Main Street for recreation;
- Viewpoint at the highest elevation of the open space; and
- Retained existing mature trees for local character.





Figure 68 Fowlers Village Centre - Natural Assets Strategy

## FOWLERS VILLAGE CENTRE



### STRATEGY 2 ACCESS AND MOVEMENT

Create convenient, safe and prioritised active and public transport connections to and from the Village Centre.

The key outcomes include:



**Fowler's Village Centre**

#### ROAD NETWORK AND INTERSECTIONS\*

Create a legible inter-connected and integrated street network with different characters and functions to balance movement and place outcomes including:



##### Sub-arterial Road (Type 2) - 22.9m

Primary west to north access route with bus service that connects to Princes Highway and M1.



##### Major Collector Road (Type 3) - 21.9m

Primary south to east access route with bus service.



##### Minor Collector Road (Type 4) - 20.95m

Proposed access route bypassing the Village Centre with bus service.



##### Main Street - 20m

Pedestrian-friendly street with wide footpath, slow traffic speed, on street parking with retail activation along the western side and residential interface along the eastern side.

-----	<b>Laneway (Type 8) - 8m</b>
	Vehicular access to the rear garage of the residential lots.
	<b>Signalised Primary Intersection</b>
	Primary intersection between Cleveland Road and Fowlers Road.
	<b>Signalised Secondary Intersection</b>
	Secondary intersection between the Minor Collector Road and Sub-arterial Road providing safe pedestrian and cycle crossing.
	<b>Left In Left Out</b>
	Access to Main Street from Cleveland Road for westbound traffic.
	<b>Roundabout</b>
	<b>Access Street (Type 6) - 17.1m</b>
	Minor local access street outside of the Village Centre with residential interface on both sides.
	<b>Access Place (Type 7B) - 14.5m</b>
	Minor local access street outside of the Village Centre with the open space on one side.

#### PUBLIC AND ACTIVE TRANSPORT



##### Proposed Bus Stop

Proposed bus stops at the north-west corner of the Site and the southern end of the Main Street providing public transport service to the Village Centre.

## PRINCIPLE 2

### A CONNECTED AND ACCESSIBLE CENTRE

	<b>Bus Route (South-Central Loop)</b>
	Realign the bus route along the Minor Collector Road.
	<b>Off-road Shared Path</b>
	Shared pedestrian / cycle paths along the Eastern Gas Pipeline easement.
	<b>Road-side Shared Path</b>
	Shared pedestrian / cycle paths within the road reserve.
	<b>Pedestrian Links to the Main Street</b>
	Create pedestrian links between Main Street and at-grade parking area to the West.

#### PARKING AND ACCESS



##### Indicative Access Point

Parking and servicing access points from Minor Collector Road.



##### Consolidated At Grade Parking

Consolidated at-grade parking behind the Village Centre buildings.



##### Indicative Loading Area

Indicative loading area for supermarket.



##### Indicative Planting within At-grade Parking

\*Note:

- Final intersection arrangement is subject to future transport analysis.
- Final layout of road network is subject to design which may differ from that show in the proposed structure plan.

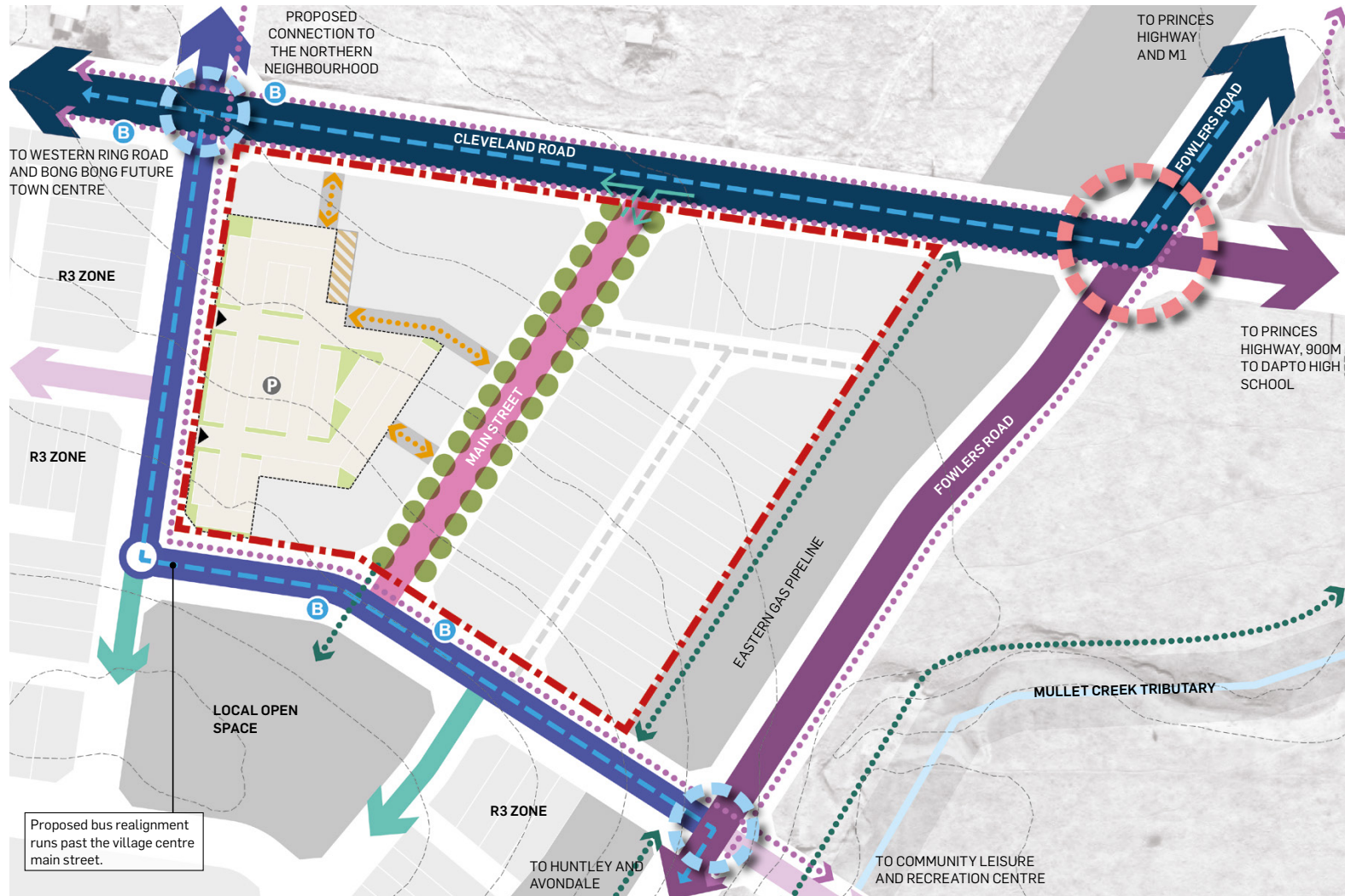


Figure 69 Fowlers Village Centre - Access and Movement Strategy

FOWLERS VILLAGE CENTRE



TYPICAL STREET SECTION

SUB-ARTERIAL ROAD - TYPE 2 / 22.9M

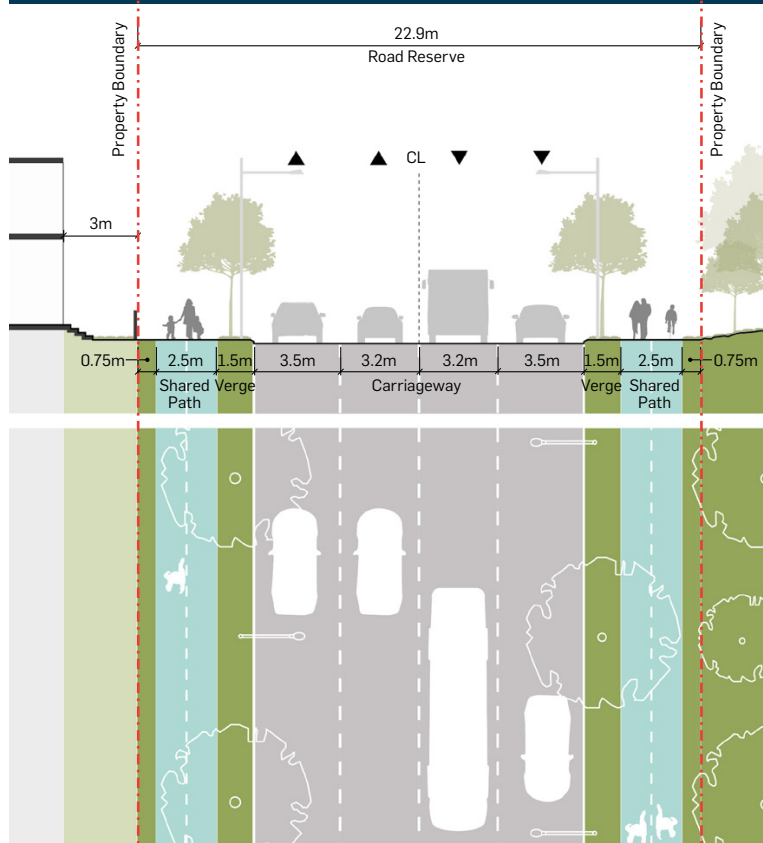


Figure 70 Indicative Street Section A-A' - Sub-Arterial Road - Type 2

MAJOR COLLECTOR ROAD - TYPE 3 / 21.9M

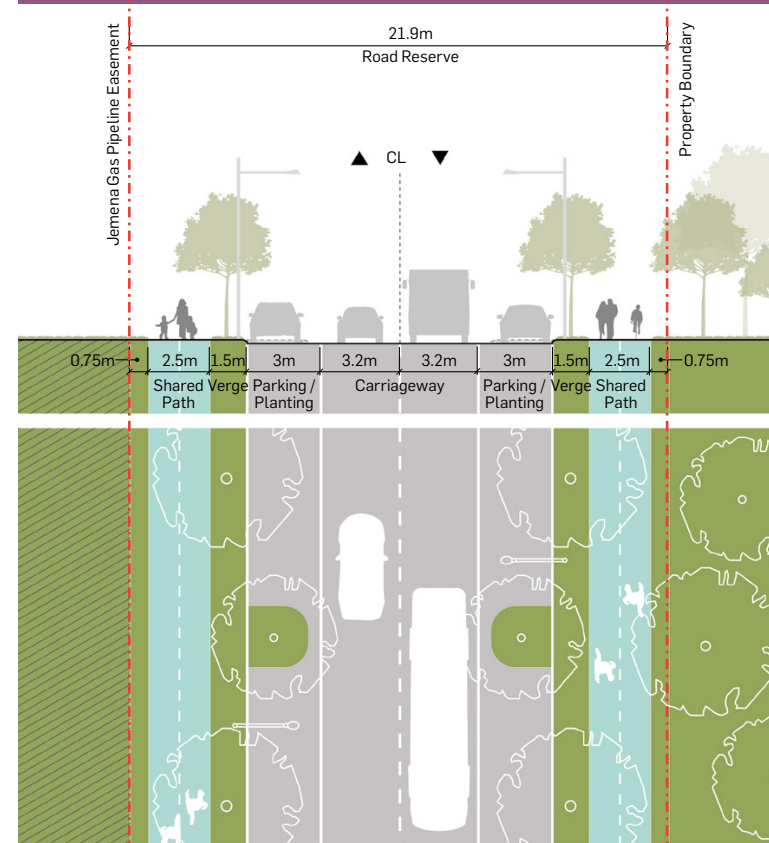


Figure 71 Indicative Street Section B-B' - Major Collector Road - Type 3



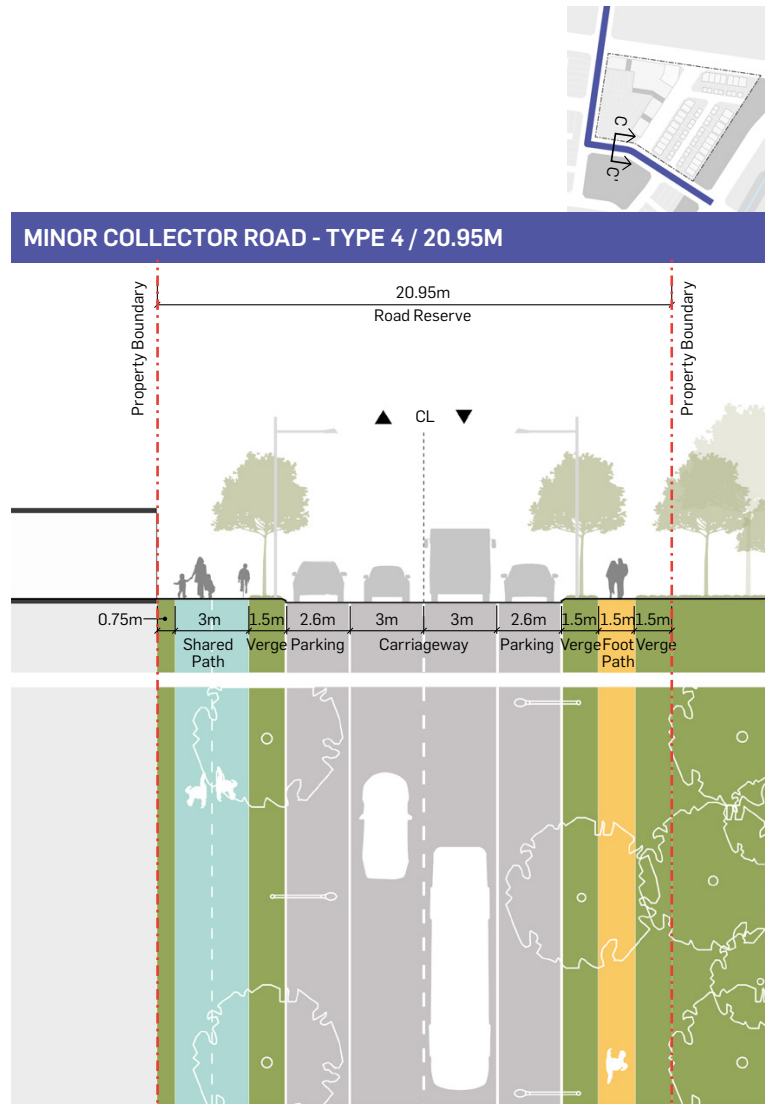


Figure 73 Indicative Street Section C-C' - Minor Collector Road - Type 4

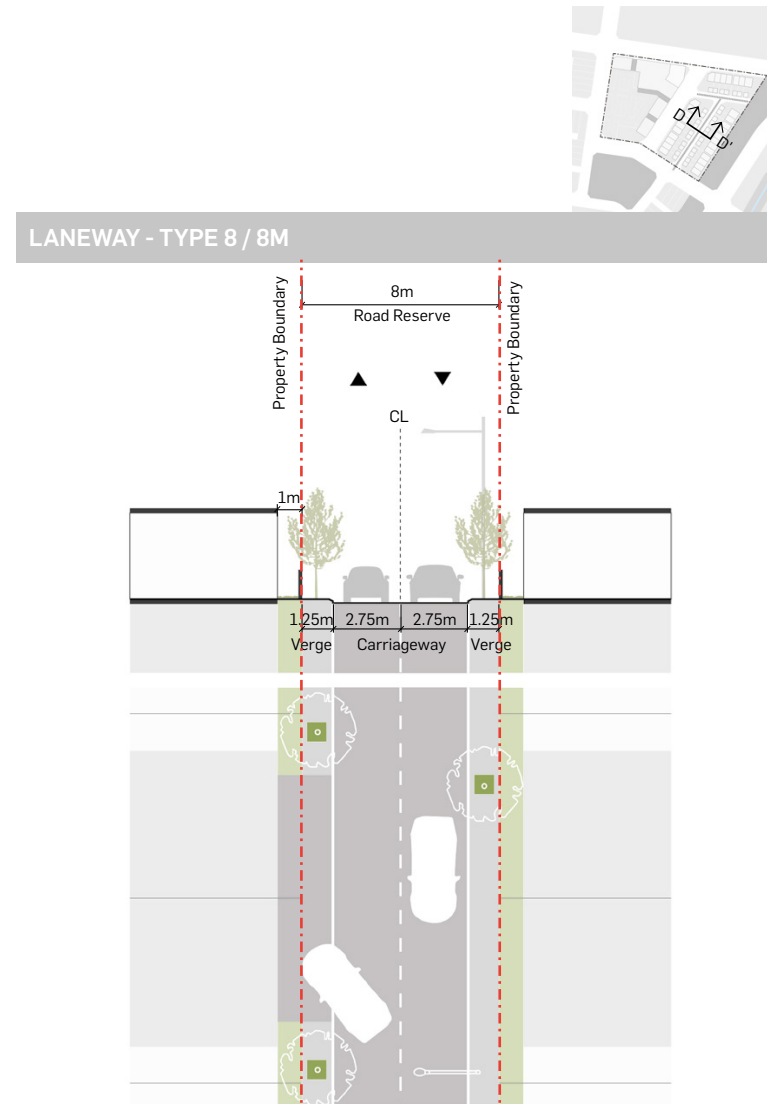


Figure 72 Indicative Street Section D-D' - Laneway - Type 8

## FOWLERS VILLAGE CENTRE



### MAIN STREET CHARACTERISTICS

The Main Street is the primary activation zone within Fowlers Village Centre characterised by the following:

- Ground floor retail activation on the western side;
- Residential frontage on the eastern side;
- Wide footpath fronting the retail to allow for outdoor dining and activation;
- Tree canopy cover;
- Slow traffic speed environment;
- On-street parking for convenience and act as buffer to the traffic movement; and
- Pedestrian priority crossing at both ends.

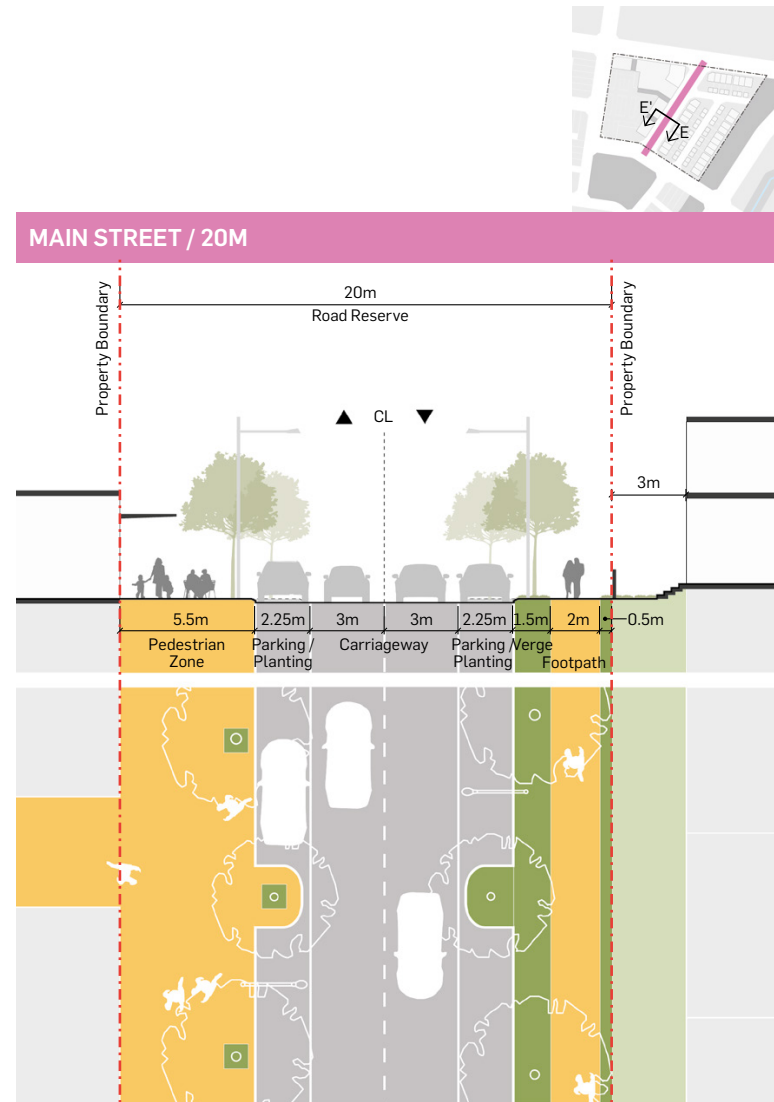


Figure 74 Indicative Street Section E-E' - Main Street



## MOVEMENT AND PLACE CLASSIFICATIONS

The Transport for NSW's Design of Roads and Streets document provide guidelines and classification of the four road and street environment identified in the Movement and Place framework. This includes:

- Main Road;
- Main Street;
- Local Street; and
- Civic Space;

The guidelines provide further sub-classifications for each environment based on its function and design parameters.

The following diagram identifies the adopted road and street environment sub-classification within Fowlers Village Centre.

### LEGEND

 Fowlers Village Centre

### MOVEMENT AND PLACE CLASSIFICATION

-  Main Street - Transit Way
-  Local Street - Connector Street
-  Local Street - Yield Street
-  Local Street - Residential Lane
-  Civic Space - Destination High Street

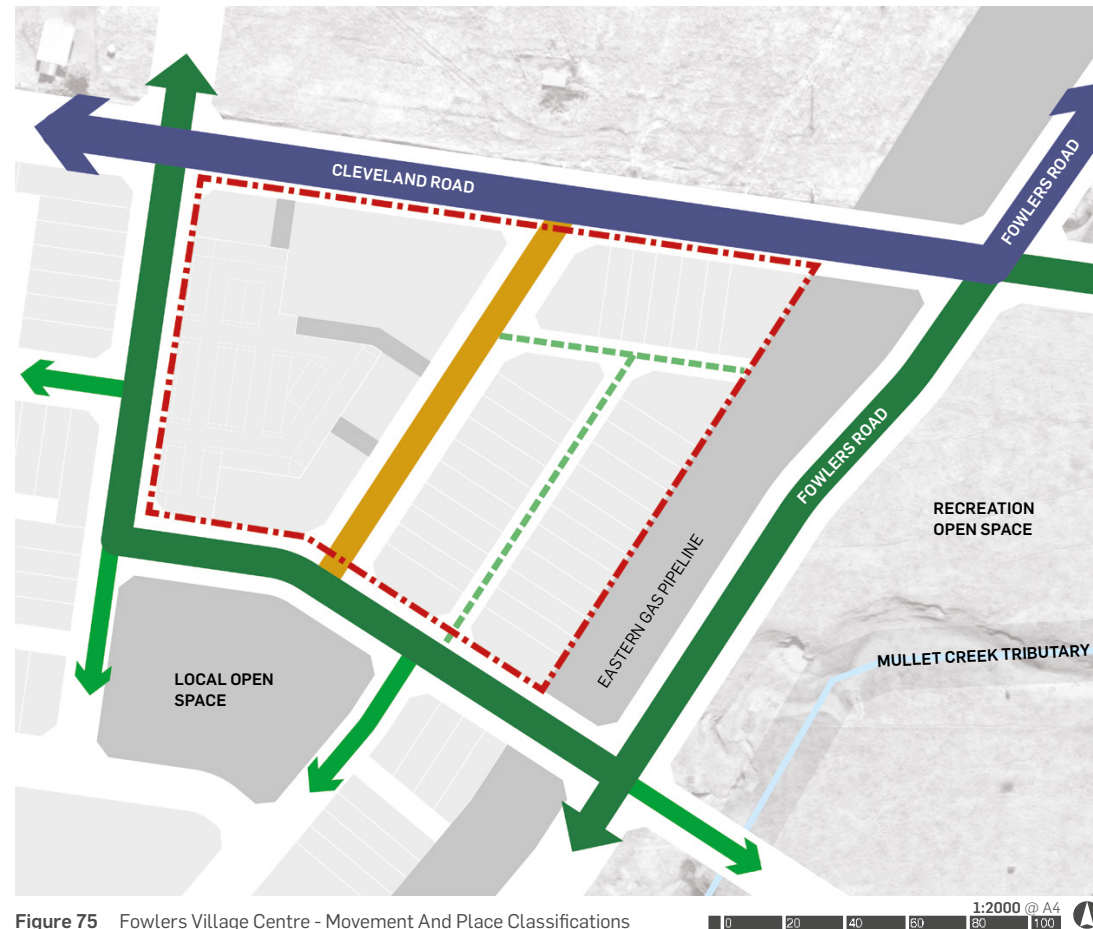


Figure 75 Fowlers Village Centre - Movement And Place Classifications

## FOWLERS VILLAGE CENTRE




### INDICATIVE PARKING STRATEGY


The parking strategy for Fowlers Village Centre proposed a consolidated at-grade parking provision behind the Village Centre buildings with additional kerbside parking along Main Street and Minor Collector Road to cater for the retail and services parking requirement.

#### LEGEND


 Fowlers Village Centre


#### PARKING AND ACCESS

 Indicative Access Points

 Indicative At-grade Parking

 Indicative Loading Area

 Indicative Kerbside Parking

 Indicative Planting for At-grade Parking

 Internal Parking Circulation

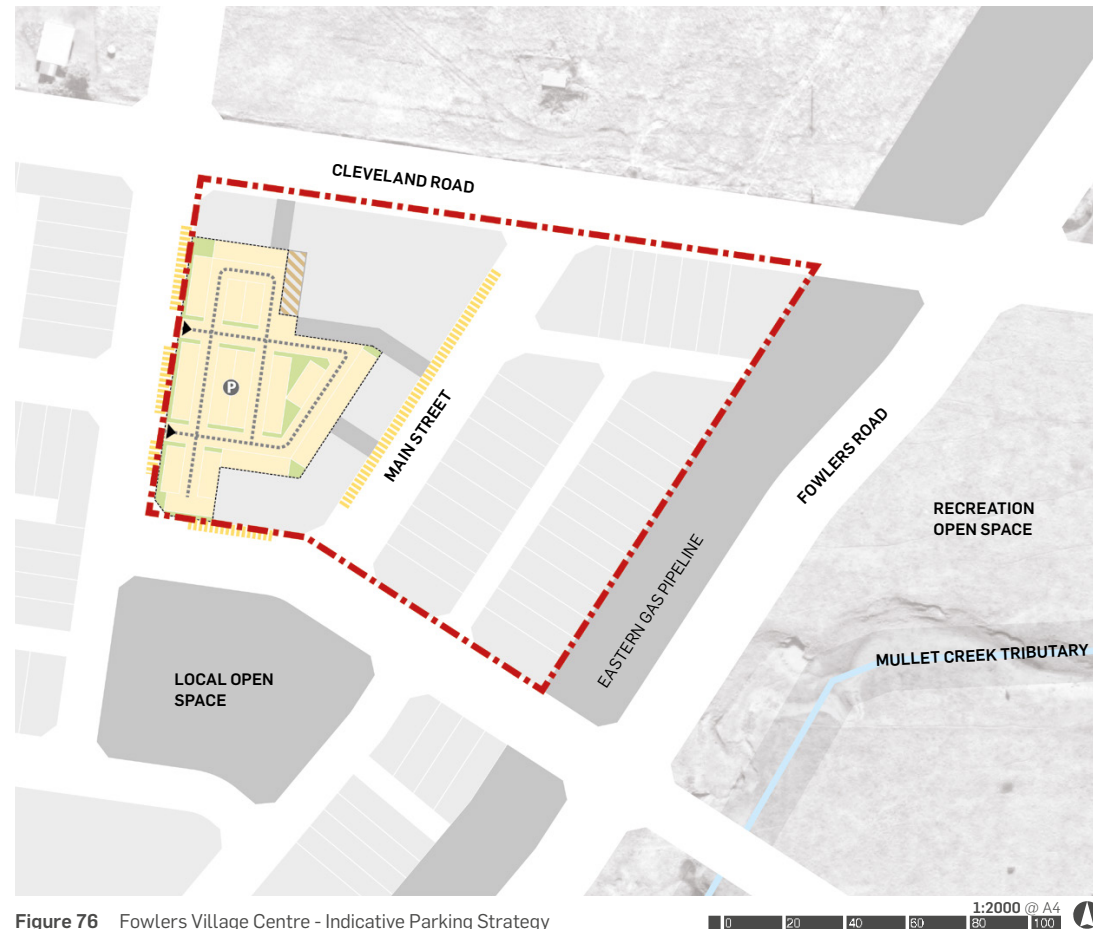


Figure 76 Fowlers Village Centre - Indicative Parking Strategy



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## FOWLERS VILLAGE CENTRE






### STRATEGY 3 CENTRE ACTIVATION

Create a walkable and compact Village Centre that is accessible to the community and benefiting from the exposure to passing trade along Cleveland Road and Fowlers Road.

The key public domain and centre activation outcomes for Fowlers Village Centre include:

	<b>Fowlers Village Centre</b>
<b>GROUND LEVEL USES</b>	
	<b>Retail (Supermarket)</b> Locate the 1,500 sqm supermarket at the northern end of Main Street providing exposure to passing trade along Cleveland Road.
	<b>Retail (Convenience)</b> Sleeve retail along the western side of the Main street, and at the corner intersection with Cleveland Road and Minor Collector Road.
	<b>Private Child Care</b> Locate the private child care at the southwest corner of Main Street.
	<b>Private Child Care Outdoor Space</b>

<b>PUBLIC DOMAIN</b>	
	<b>Pedestrian Link</b> Pedestrian link between the Main Street and the at-grade parking area.
<b>ACTIVE STREET FRONTAGE</b>	
	<b>Primary Active Frontage</b> Ground-level non-residential uses with frequent entries and predominantly glazed frontage.
	<b>Secondary Active Frontage</b> Ground-level non-residential uses with predominant glazed frontage and allow for some blank walls and less frequent entries.

### PRINCIPLE 3 AN ACTIVATED PLACE AND VIALE CENTRE

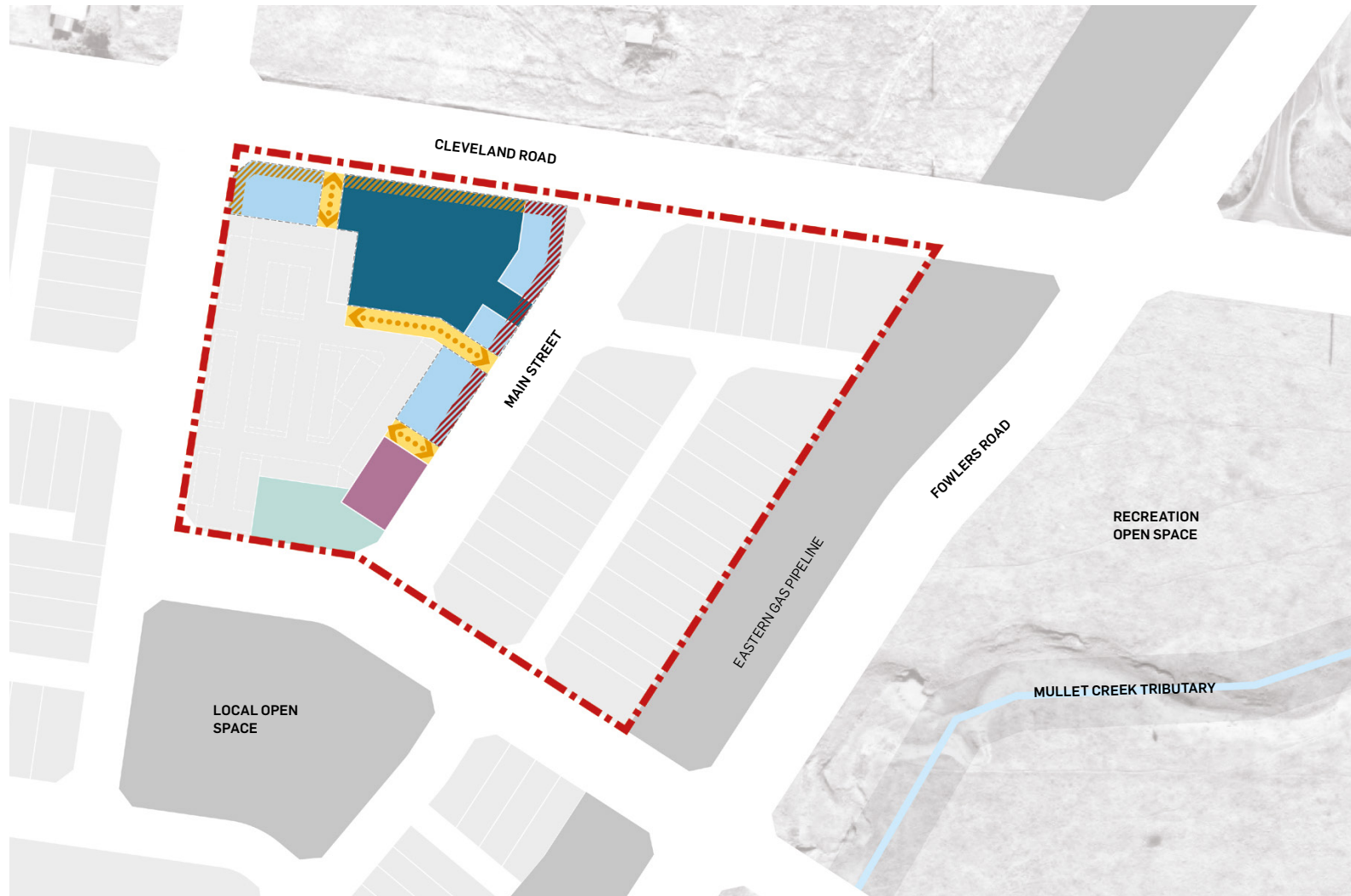


Figure 77 Fowlers Village Centre - Centre Activation Strategy

## FOWLERS VILLAGE CENTRE



### LOCAL CONTEXT AMENITIES AND RECREATIONAL OPEN SPACE

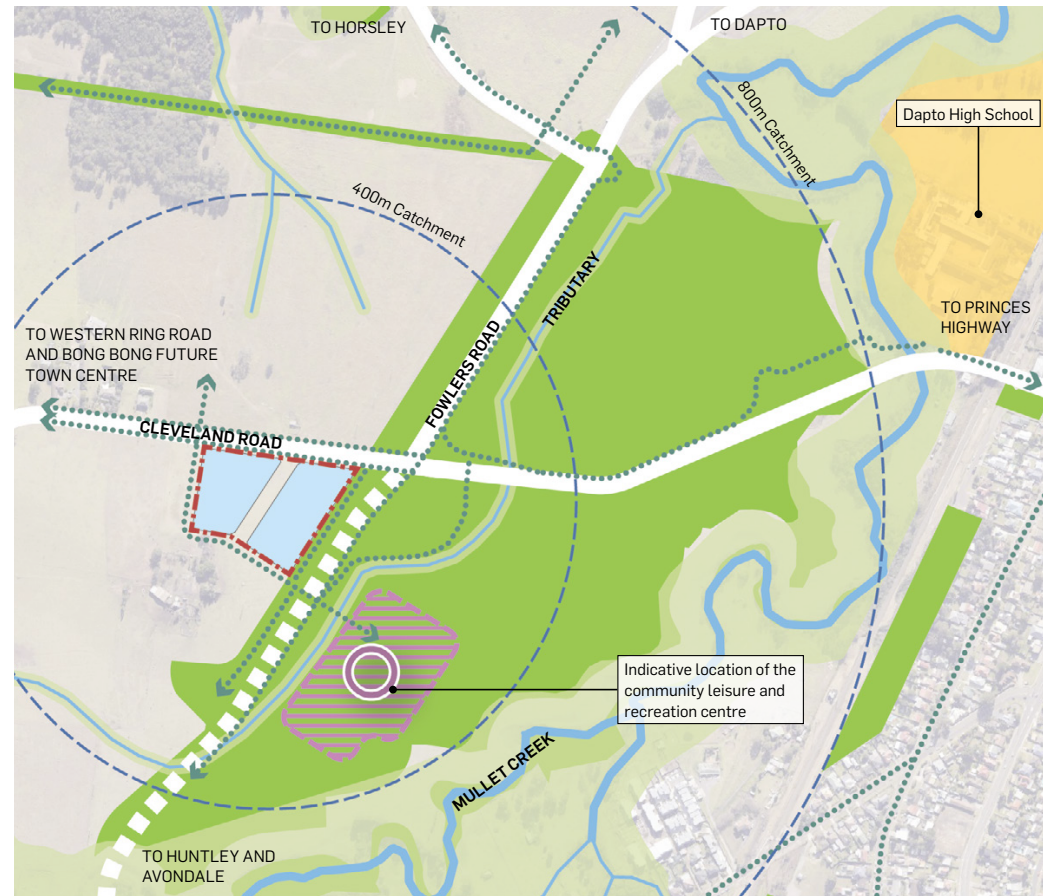
The following map illustrates Fowlers Village Centre and the surrounding education facility, amenities, and recreational open space within the local context that are connected via key roads and cycleway network.

#### LEGEND

	Fowlers Village Centre
	Existing Roads
	Proposed Roads
	Cycleway Network
	400m & 800m catchment
	Indicative Riparian Land

#### OPEN SPACE, AMENITIES AND EDUCATION FACILITY

	<b>Proposed Community Leisure and Recreation Centre</b> Indicative Location of the Community Leisure and Recreation Centre immediately to the southeast of the Centre.
	<b>Recreational Open Space (Zoned RE1)</b> Proposed Recreational Open Space zone (RE1) along the eastern side of Fowlers Road.
	<b>Education Facilities</b> Dapto High School - 800m east of the Village Centre via Cleveland Road.



**Figure 78** Fowlers Village Centre - Local Context Amenities and Recreational Open Space

1:10000 @ A4  
0 100 200 300 400 500

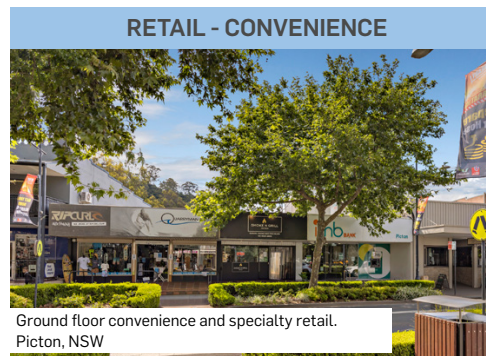




## RETAIL AND COMMUNITY SPACE PROVISION

Fowlers Village Centre provides retail and community uses comprised of small supermarket, convenience retail and potential private child care facility.

The Centre can accommodate approximately 2,700 sqm retail space, 28% more than the retail demand forecast of 2,168 sqm, catering for the retail demand beyond 2041.



## FOWLERS VILLAGE CENTRE



### STRATEGY 4 HOUSING MIX

Creating a diverse community with housing mix offer close to the Village Centre to support the Village Centre viability.

The key housing outcomes for the Village Centre include:



Fowlers Village Centre



Residential Quadrant Site

#### HOUSING TYPOLOGY (WITHIN VILLAGE CENTRE)



##### Rear Loaded Terrace Homes

Proposed rear-loaded terrace homes within the eastern quadrant of the Village Centre and within the 76m Eastern Gas Pipeline buffer zone.

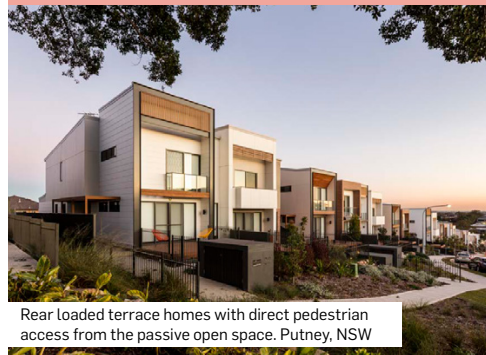
#### HOUSING TYPOLOGY (ADJACENT TO VILLAGE CENTRE)



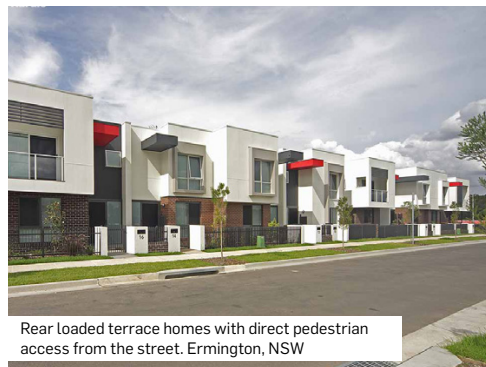
##### Medium Density Housing

Potential a mix of medium housing density housing typologies including attached homes, semi-detached and dual occupancy homes, within the surrounding R3 zone to increase the Village Centre population catchment.

#### REAR LOADED TERRACE HOMES (WITHIN VILLAGE CENTRE)



Rear loaded terrace homes with direct pedestrian access from the passive open space. Putney, NSW



Rear loaded terrace homes with direct pedestrian access from the street. Ermington, NSW

#### PRINCIPLE 4 DELIVER VARIED HOUSING OFFER CLOSE TO AMENITIES

#### MEDIUM DENSITY HOUSING (ADJACENT TO VILLAGE CENTRE)



Thornton Park, Penrith NSW



Medium density housing typologies - attached homes, semi detached, dual occupancy.

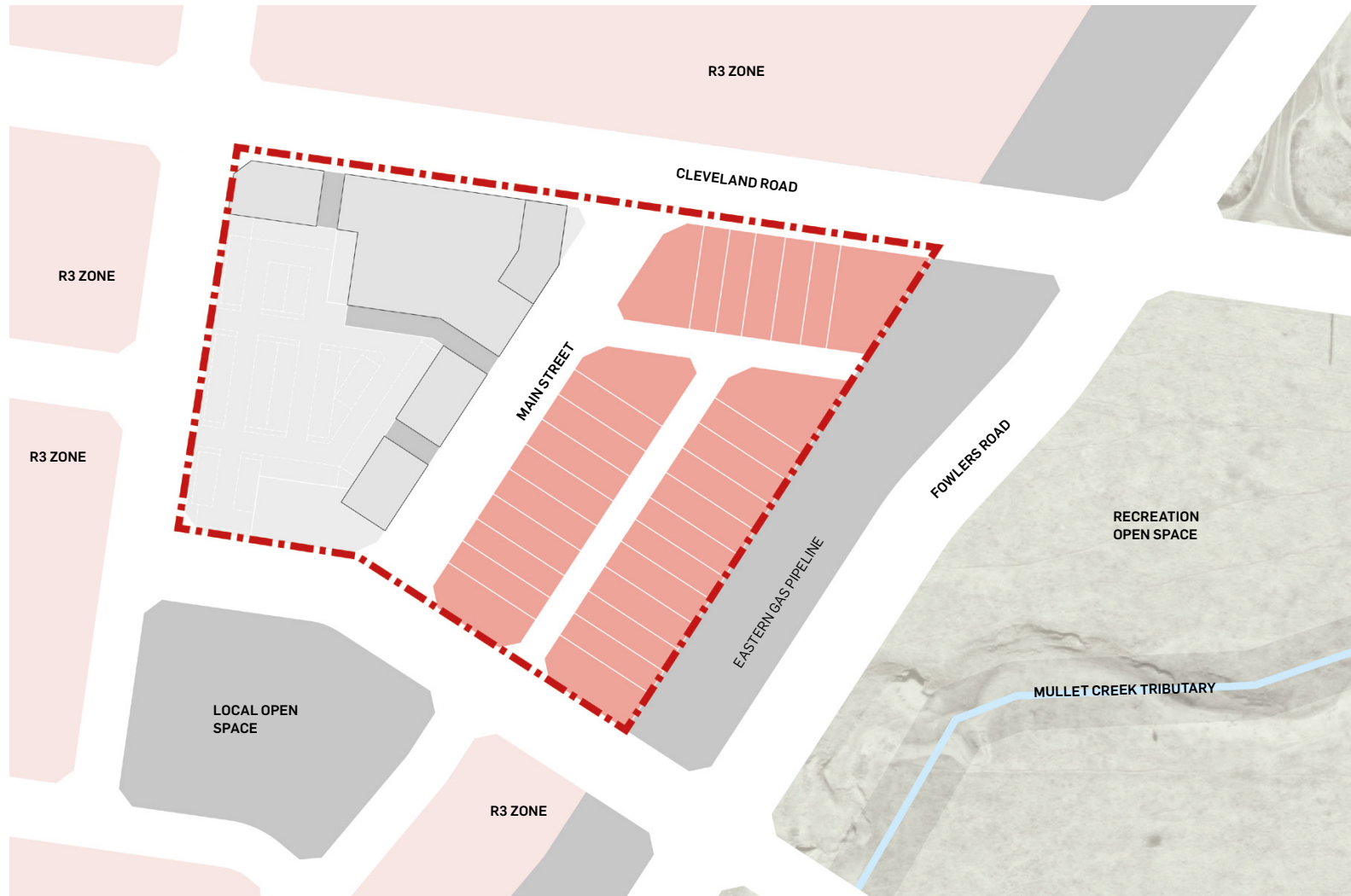


Figure 79 Fowlers Village Centre - Housing Mix Strategy

## FOWLERS VILLAGE CENTRE



### STRATEGY 5 BUILT FORM

The Village Centre built form configuration is formulated based upon the maximum permissible height, retail and housing in response to the topography, amenities and context.

The key built form outcomes for the Village Centre include:

	<b>Fowlers Village Centre</b>
<b>BUILDING HEIGHT</b>	
	<b>1-2 Storey Building</b> Proposed 1-2-storey buildings at the following location: <ul style="list-style-type: none"> <li>At the northern intersection of Main Street and Cleveland Road,</li> <li>Residential lots within the eastern quadrant; and</li> <li>At corner lots rear buildings for fonzie flat opportunities.</li> </ul>
	<b>Key Building Marker</b> Proposed key marker building at the north-west corner of Main Street with high-quality architectural design and articulation.
	<b>Indicative Building Height (Storey)</b>
	<b>Corner Residential Lots</b> Designate corner lots and preferred location for 2 storey at rear and to address both streets.

#### STREET SETBACK

	<b>0m Retail Street Setback</b> Interface with retail along west side of the Main Street and Cleveland Road.
	<b>3m Residential Primary Street Setback</b> Residential interface along east side of the Main Street, Cleveland Road and Fowlers Road.
	<b>1.5m Residential Secondary Street Setback</b> Residential interface along north side of the Minor Collector Road.

#### CORNER PLAZA

	<b>Indicative Corner Plaza</b> Improves pedestrian sight lines and opportunity for on-street dining.
--	---

## PRINCIPLE 5

### CONTEXTUAL BUILT FORM RESPONSE

#### KEY MARKER BUILDING



#### 1-2 STOREY BUILDING





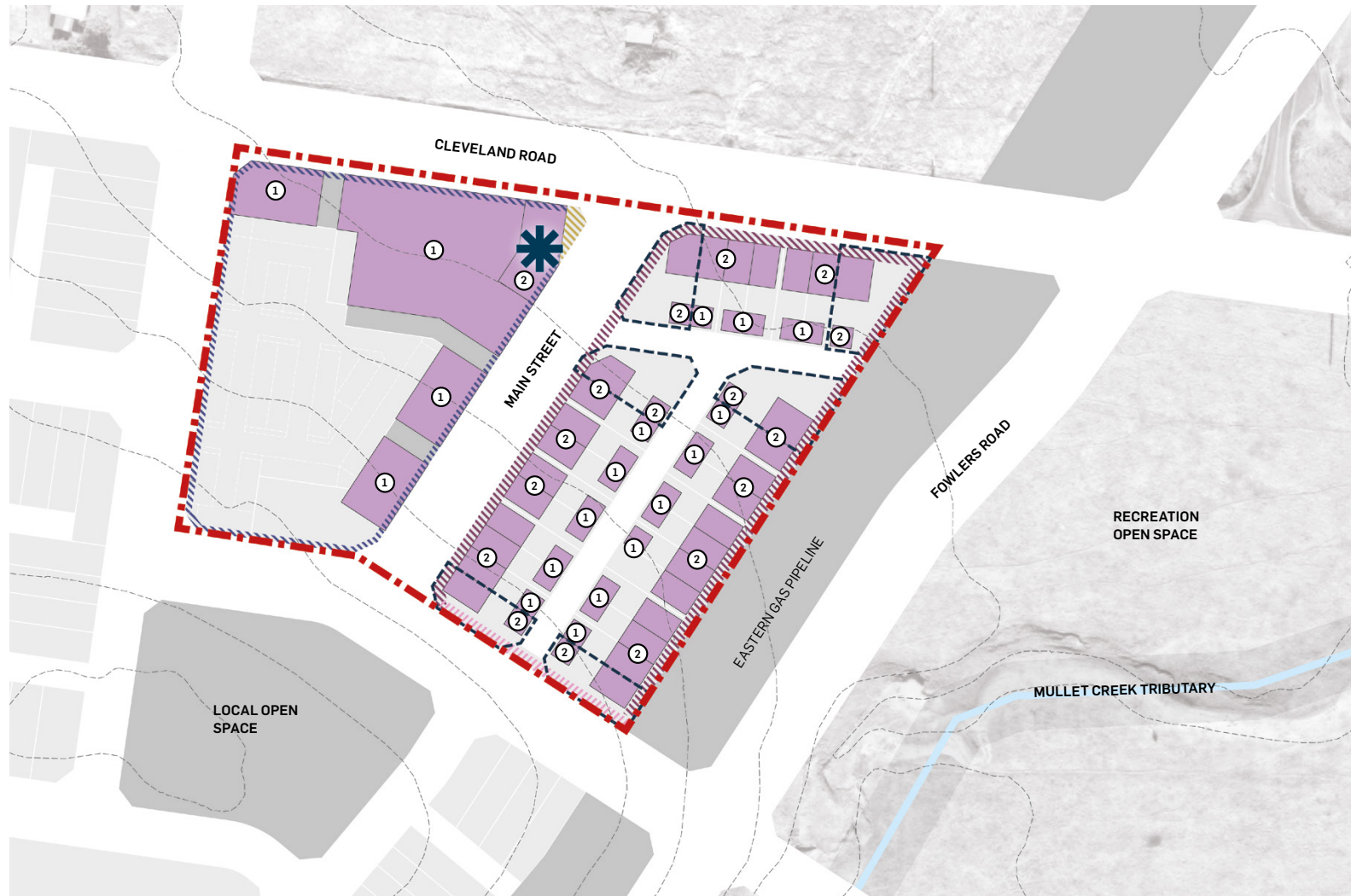


Figure 80 Fowlers Village Centre - Built Form Strategy

## FOWLERS VILLAGE CENTRE



### INDICATIVE BUILT FORM MASSING

The following illustrates the indicative built form massing of Fowlers Village Centre.



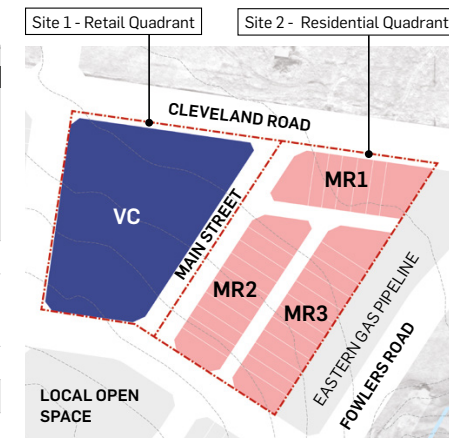
Figure 81 Fowlers Village Centre - Indicative Built Form Massing

## INDICATIVE YIELD BREAKDOWN - FOWLERS VILLAGE CENTRE

The following table and diagram provides the proposed land use and yield breakdown of Fowlers Village Centre.

**Table 9** Fowlers Village Centre - Yield Breakdown

LAND USE AND SITE AREA					GROSS FLOOR AREA							
LOT NO.	LAND USE	LAND USE AREA (SQM)	LAND USE AREA (HA)	LAND USE AREA (%)	RESI GFA (SQM)	CHILD CARE GFA (SQM)	RETAIL GFA (SQM)	RETAIL - SUPERMARKET GFA (SQM)	TOTAL GFA (SQM)	FSR (N:1)	DW. NO.	DW./HA
<b>SITE 1 - RETAIL QUADRANT</b>												
VC	Retail (Convenience & Supermarket) / Private Child Care	8,382	0.84	38.9%	271	1,258	1,527	3,056		0.36		
	Road Reserve	1,717	0.17	8.0%								
<b>SITE 1 TOTAL</b>		<b>10,099</b>	<b>1.01</b>	<b>46.9%</b>	<b>271</b>	<b>1,258</b>	<b>1,527</b>	<b>3,056</b>		<b>0.30</b>		
<b>SITE 2 - RESIDENTIAL QUADRANT</b>												
MR1	Medium Density Residential	2,231	0.22	10.3%	840			840		0.38	7	31.4
MR2	Medium Density Residential	2,791	0.28	13.0%	1,200			1,200		0.43	10	35.8
MR3	Medium Density Residential	3,387	0.34	15.7%	1,440			1,440		0.43	12	35.4
	Sub Total Developable Area	8,409	0.84	39.0%	3,480			3,480		0.41	29	34.5
	Road Reserve	3,041	0.30	14.1%								
<b>SITE 2 TOTAL</b>		<b>11,450</b>	<b>1.14</b>	<b>53.1%</b>	<b>3,480</b>			<b>3,480</b>		<b>0.30</b>		<b>25.4</b>
<b>TOTAL</b>		<b>21,549</b>	<b>2.15</b>	<b>100.0%</b>				<b>6,536</b>		<b>0.30</b>	<b>29</b>	



**Figure 82** Fowlers Village Centre - Land Use  
1:3,000 @ A4  
0 25 50 75 100 125 150

### LEGEND

  Fowlers Village Centre

### LAND USE

Village Centre - Mixed Retail / Private Child Care  
 Medium Density Residential

# APPENDIX

The following chapter summarises the Indicative Master Plan outcomes of Marshall Mount Town Centre and Fowlers Village Centre in alignment with the following:

- Indicative Yield Breakdown
- Indicative Parking Provision
- Indicative Retail and Community Space Outcome
- Indicative Dwelling Density Summary
- FSR Scenarios Investigation
- Centre Key Success Factor Principles
- Strategic Planning Directions
- Development Assumptions.



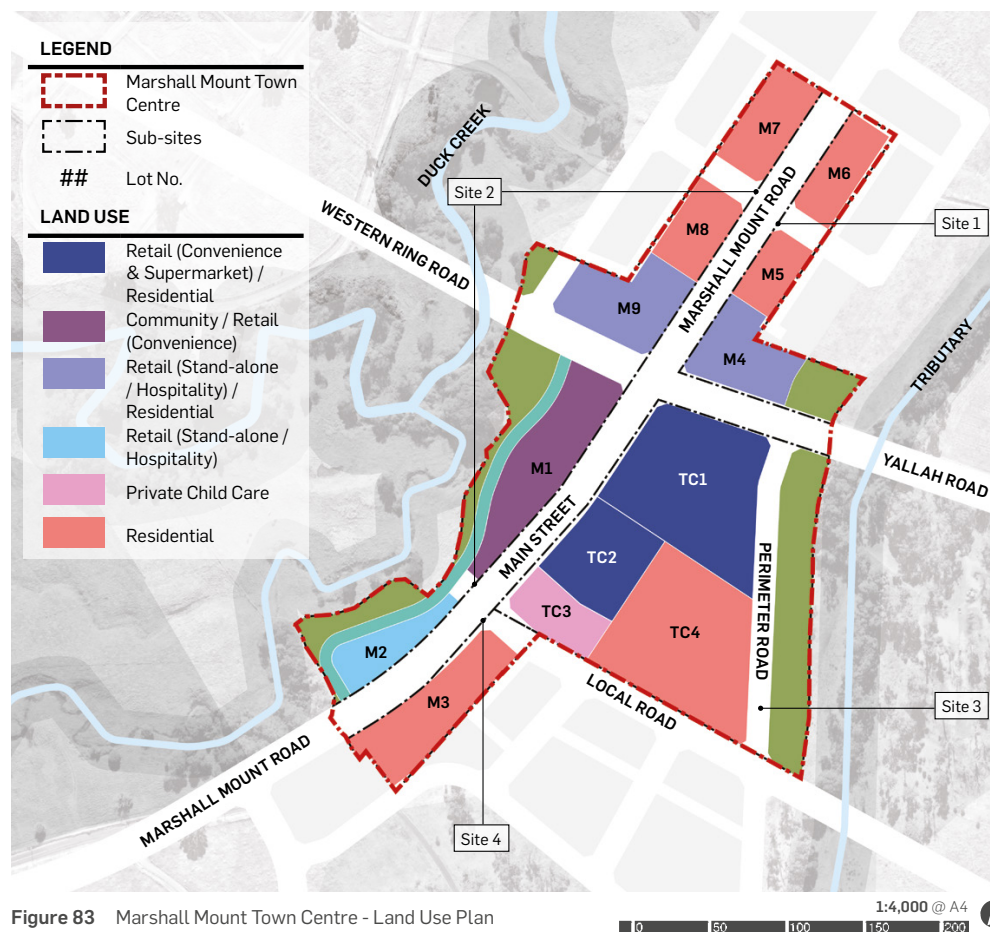


## INDICATIVE YIELD BREAKDOWN - MARSHALL MOUNT TOWN CENTRE

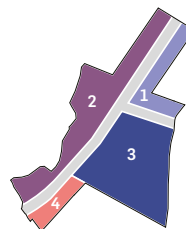
The following table and diagram provides the proposed land use and yield breakdown of Marshall Mount Town Centre.

Table 10 Marshall Mount Town Centre - Area Schedule

AREA SCHEDULE	SQM	%
<b>Total Site Area (sqm)</b>	<b>82,466</b>	<b>100.0%</b>
<b>Developable Area (sqm)</b>	<b>47,247</b>	<b>57.3%</b>
<b>Non-Developable Area (sqm)</b>	<b>35,219</b>	<b>42.7%</b>
<i>Open Space / Constrained Land (sqm)</i>	<i>10,327</i>	<i>12.5%</i>
<i>Western Promenade</i>	<i>2,366</i>	<i>2.9%</i>
<i>Road Reserve (sqm)</i>	<i>22,526</i>	<i>27.3%</i>
<b>DENSITY OUTCOME</b>	<b>N:1</b>	
<b>Gross FSR (n:1)</b>	<b>0.53</b>	
<b>Net FSR (n:1)</b>	<b>0.92</b>	
<b>GFA BREAKDOWN</b>	<b>SQM</b>	<b>%</b>
<b>TOTAL GFA (SQM)</b>	<b>43,355</b>	<b>100%</b>
<b>NON-RESI GFA (SQM)</b>	<b>11,260</b>	<b>26.0%</b>
<b>Retail GFA (sqm)</b>	<b>8,188</b>	<b>18.9%</b>
<i>Supermarket</i>	<i>3,698</i>	<i>8.5%</i>
<i>Convenience</i>	<i>3,158</i>	<i>7.3%</i>
<i>Stand-alone Hospitality</i>	<i>1,332</i>	<i>3.1%</i>
<b>Community Facility GFA (sqm)</b>	<b>2,506</b>	<b>5.8%</b>
<i>Local Multi-Purpose Community Centre and Library</i>	<i>2,506</i>	<i>5.8%</i>
<b>Private Child Care GFA (sqm)</b>	<b>566</b>	<b>1.3%</b>
<b>RESIDENTIAL GFA (SQM)</b>	<b>32,095</b>	<b>74.0%</b>
<b>Residential GFA (sqm)</b>	<b>32,095</b>	<b>74.0%</b>
<b>Total Dwelling #</b>	<b>343</b>	



## APPENDIX - MARSHALL MOUNT TOWN CENTRE



\*NOTE: It is recommended that Council consider opportunities for additional height beyond the existing LEP controls to facilitate varied building height outcomes and facilitate achievable GFA closer to Existing LEP FSR provisions. Opportunities for additional height should be considered in the context of the NSW Governments recently announced bonuses for affordable housing via State Environmental Planning Policy (Housing) 2021. The yield and parking outcomes would be expected to change if height standards are increased.

Table 11 Marshall Mount Town Centre - Yield Breakdown

LAND USE AND SITE AREA					GROSS FLOOR AREA										
LOT NO.	LAND USE	LAND USE AREA (SQM)	LAND USE AREA (HA)	LAND USE AREA (%)	RESI GFA (SQM)	COMMUNITY GFA (SQM)	CHILD CARE GFA (SQM)	RETAIL - SUPERMARKET GFA (SQM)	RETAIL - CONVENIENCE GFA (SQM)	RETAIL - F&B GFA (SQM)	TOTAL GFA (SQM)	FSR (m:1)	DW. NO.	DW. NO. (%)	DW/HA
<b>SITE 1</b>															
M4	Retail (Stand-alone / Hospitality) / Residential	2,940	0.29	3.6%	4,050					452	4,502	1.53	43	12.5%	146.2
M5	Residential	1,549	0.15	1.9%	2,075						2,075	1.34	22	6.4%	142.1
M6	Residential	2,145	0.21	2.6%	3,145						2,075	1.47	34	9.9%	158.5
OS	Open Space	1,220	0.12	1.5%											
	Open Space / Constrained Land	1,220	0.12												
	Road Reserve	895	0.09	1.1%											
<b>SITE 1 TOTAL</b>		8,749	0.87	10.6%	9,270					452	9,722	1.47	99	28.9%	149.2
<b>SITE 2</b>															
M1	Community / Retail (Convenience)	4,713	0.47	5.7%		2,506			1,016		3,522	0.75			
M2	Retail (Stand-alone / Hospitality)	1,838	0.18	2.2%						544	544	0.30			
M7	Residential	2,589	0.26	3.1%	3,144						3,144	1.21	34	9.9%	131.3
M8	Residential	2,112	0.21	2.6%	2,363						2,363	1.12	25	7.3%	118.4
M9	Retail (Stand-alone / Hospitality) / Residential	3,517	0.35	4.3%	3,281					336	3,617	1.03	35	10.2%	99.5
OS	Open Space	6,474	0.65	7.9%											
	Western Promenade	2,366	0.24												
	Open Space / Constrained Land	4,108	0.41												
	Road Reserve	3,581	0.36	4.3%											
<b>SITE 2 TOTAL</b>		24,824	2.48	30.1%	8,788	2,506			1,016	880	13,190	0.53	94	27.4%	37.9

LAND USE AND SITE AREA					GROSS FLOOR AREA										
LOT NO.	LAND USE	LAND USE AREA (SQM)	LAND USE AREA (HA)	LAND USE AREA (%)	RESI GFA (SQM)	COMMUNITY GFA (SQM)	CHILD CARE GFA (SQM)	RETAIL - SUPERMARKET GFA (SQM)	RETAIL - CONVENIENCE GFA (SQM)	RETAIL - F&B GFA (SQM)	TOTAL GFA (SQM)	FSR (N:1)	DW. NO.	DW. NO. (%)	DW./HA
<b>SITE 3</b>															
TC1	Retail (Convenience & Supermarket) / Residential	8,169	0.82	9.9%	4,641			3,698	1,257		9,596	1.17	50	14.6%	61.2
TC2	Retail (Convenience) / Supermarket / Residential	3,488	0.35	4.2%	1,407				886		2,293	0.66	15	4.4%	43.0
TC3	Private Child Care	1,954	0.19	2.4%			566				566	0.29			
TC4	Residential	8,288	0.83	10.1%	4,272						4,272	0.52	45	13.1%	54.3
OS	Open Space	4,999	0.50	6.1%											
	Open Space / Constrained Land	4,999	0.50												
	Road Reserve	3,624	0.36	4.3%											
<b>SITE 3 TOTAL</b>		<b>30,522</b>	<b>3.05</b>	<b>37.0%</b>	<b>10,321</b>		<b>566</b>	<b>3,698</b>	<b>2,142</b>		<b>16,727</b>	<b>0.55</b>	<b>110</b>	<b>32.1%</b>	<b>36.0</b>
<b>SITE 4</b>															
M3	Residential	3,945	0.39	4.7%	3,716						3,716	0.94	40	11.7%	101.4
	Road Reserve	686	0.07	0.9%											
<b>SITE 4 TOTAL</b>		<b>4,631</b>	<b>0.46</b>	<b>5.6%</b>	<b>3,716</b>						<b>3,716</b>	<b>0.80</b>	<b>40</b>	<b>11.7%</b>	<b>86.4</b>
<b>EXISTING ROAD RESERVE</b>		<b>13,740</b>	<b>1.37</b>	<b>16.7%</b>											
<b>TOTAL</b>		<b>82,466</b>	<b>8.25</b>	<b>100.0%</b>	<b>32,095</b>	<b>2,506</b>	<b>566</b>	<b>3,698</b>	<b>3,158</b>	<b>1,332</b>	<b>43,355</b>	<b>0.53</b>	<b>343</b>	<b>100.0%</b>	<b>41.6</b>

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

### INDICATIVE PARKING PROVISION - MARSHALL MOUNT TOWN CENTRE

The following table identify the indicative parking requirement / provision.

Table 12 Marshall Mount Town Centre - Indicative Parking Provision

LOT NO. USE	CAR PARKING REQUIREMENT					CAR PARKING PROVISION				BICYCLE PARKING PROVISION		MOTORCYCLE PARKING PROVISION	
	RESIDENTIAL REQUIREMENT	RETAIL REQUIREMENT	COMMUNITY REQUIREMENT	CHILD CARE REQUIREMENT	TOTAL REQUIREMENT	SURFACE/ GROUND LEVEL PODIUM	BASEMENT	INDICATIVE KERBSIDE PARKING	TOTAL	BICYCLE PARKING REQUIREMENT	BICYCLE PARKING PROVISION	MOTORCYCLE PARKING REQUIREMENT	MOTORCYCLE PARKING PROVISION
<b>TOWN CENTRE CORE</b>													
M1 Community / Retail (Convenience)		37	57		94	15	63	18	96			4	
TC1 Supermarket & Upper Level RFB	86	180			266	181	275	3	459	21	70	10	25
TC2 Shoptop Housing	26	32			58	28		5	33	7		2	
TC3 Private Child Care (120 places)				44	44			10	10			2	
TC4-1 RFB	27				27	22			22	19		2	
TC4-2 RFB	51				51	35		11	46				
SUB TOTAL - TOWN CENTRE CORE	190	249	57	44	540	281	338	47	666	47	70	20	25
<b>OTHERS</b>													
M2 Retail (Stand-alone / Hospitality)		20			20	15		11	26			1	
M3 RFB	68				68		88	13	146	17		3	
M4&5 Shoptop housing / RFB	103	17			120		118	12	195	28	50	5	12
M6 RFB	58				58		43	8	95	15		2	
M7 RFB	58				58		53	8	114	15		2	
M8&9 Shoptop housing / RFB	103	13			116		116	10	184	26	60	5	15
SUB TOTAL - OTHERS	390	50			440	15	418	62	495	101	110	18	27
<b>TOTAL</b>	<b>580</b>	<b>299</b>	<b>57</b>	<b>44</b>	<b>980</b>	<b>296</b>	<b>756</b>	<b>109</b>	<b>1,161</b>	<b>148</b>	<b>180</b>	<b>38</b>	<b>52</b>



## INDICATIVE RETAIL AND COMMUNITY SPACE OUTCOME - MARSHALL MOUNT TOWN CENTRE

The following table identify the indicative retail and community space outcome.

**Table 13** Marshall Mount Town Centre - Indicative Retail and Community Space Outcome.

<b>Marshall Mount Town Centre Retail Forecast Demand 2041</b>		<b>3,999 sqm</b>
(Wollongong Retail and Business Strategy, 2023)		
<b>RETAIL SPACE OUTCOME</b>		
<b>Master Plan Retail GFA</b>		<b>8,188 sqm</b>
	<i>Retail - Supermarket</i>	<i>3,698 sqm</i>
	<i>Retail - Convenience (including cafe, specialty shops, gym and pharmacy)</i>	<i>3,158 sqm</i>
	<i>Retail - Standalone / Hospitality</i>	<i>1,332 sqm</i>
<b>Master Plan Community Facility</b>		<b>2,506 sqm</b>
	<i>Sub-District Multi-Purpose Community Centre and Library</i>	<i>2,506 sqm</i>
<b>Private Child Care GFA</b>		<b>566 sqm</b>
<b>Private Child Care Outdoor Space</b>		<b>1,247 sqm</b>
<b>Private Child Care Capacity (# of places)</b>		<b>120</b>

## INDICATIVE DWELLING DENSITY SUMMARY - MARSHALL MOUNT TOWN CENTRE

The following table identify the indicative dwelling density summary.

**Table 14** Marshall Mount Town Centre - Indicative Dwelling Density Summary.

<b>DWELLING DENSITY SUMMARY (WITHIN TOWN CENTRE)</b>	
Total Site Area (ha)	8.25
Residential and Centre Lots Developable Area* (ha)	4.07
<b>Total Dwelling No.</b>	<b>343</b>
<b>Gross Density (dw/ha) - Total Site Area</b>	<b>41.6</b>
<b>Gross Density (dw/ha) - Residential and Town Centre Lots Developable Area*</b>	<b>84.2</b>

\*Exclude community facility and southwest retail lots

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

# FSR SCENARIOS INVESTIGATION - MARSHALL MOUNT TOWN CENTRE

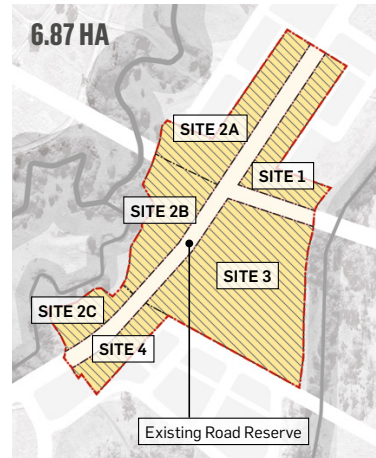
The following tables and diagrams provide further exploration of the relationship between FSR and height controls within the Marshall Mount Town Centre.

## SITE AREA DEFINITION



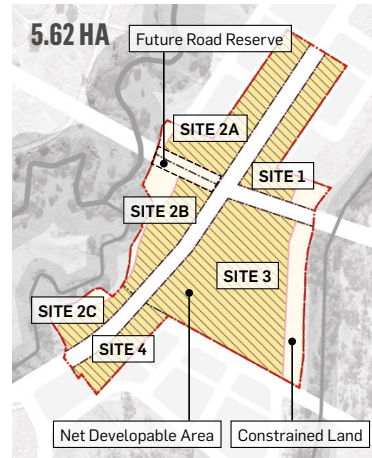
### TOTAL STUDY AREA

Total MU1 & E1 Zone Land Area



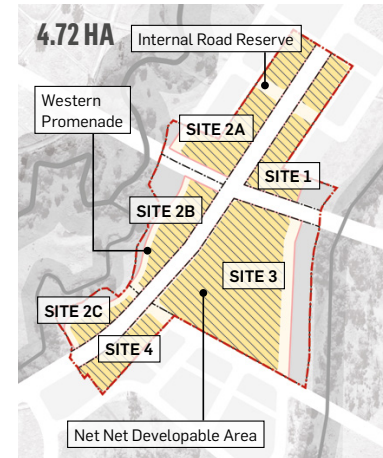
### SITE AREA

Total Study Area excludes existing road reserve (Yallah Road and Marshall Mount Road)



### NET DEVELOPABLE AREA

Total site area excludes future road reserve (Yallah Road extension) and constrained land (flooding, riparian land, biodiversity)



### NET NET DEVELOPABLE AREA


Net developable area excludes internal road and western promenade


## PLANNING CONTROLS

Table 15 Development Capacity Summary - Planning Controls


SITE	AREA (SQM)	ZONING	PERMISSIBLE FSR (N:1)	PERMISSIBLE GFA (SQM)	HOB (M / ST)
SITE 1	8,749	MU1	1.50	13,123	15m / 4 st
SITE 2A	11,039	MU1	1.50	16,559	15m / 4 st
SITE 2B	9,429	MU1	1.50	14,144	15m / 4 st
SITE 2C	4,356	MU1	1.50	6,534	15m / 4 st
SITE 3	30,522	E1	2.00	61,043	15m / 4 st
SITE 4	4,631	MU1	1.50	6,947	15m / 4 st
<b>SUB TOTAL</b>	<b>68,726</b>		<b>1.72</b>	<b>118,350</b>	
EXISTING ROAD RESERVE	13,740				
<b>TOTAL</b>	<b>82,466</b>				

### LEGEND


 Total Study Area


 Site Area

### PROPOSED LAND ZONING

 R3 - Medium Density Residential

 R2 - Low Density Residential

 RE1 - Public Recreation

 E1 - Local Centre

 MU1 - Mixed Use

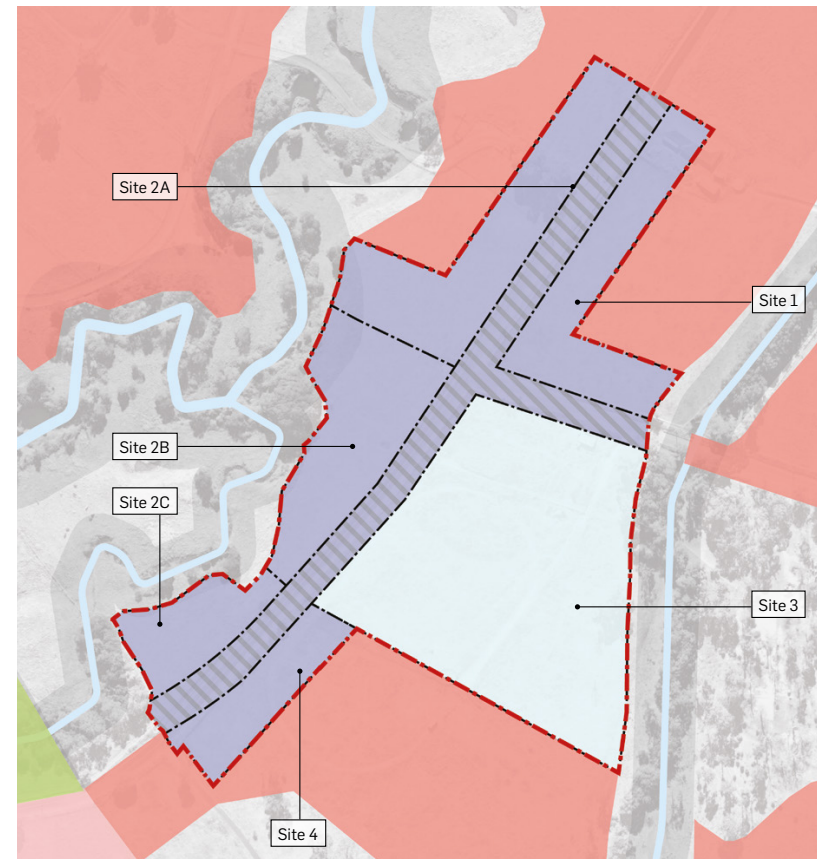
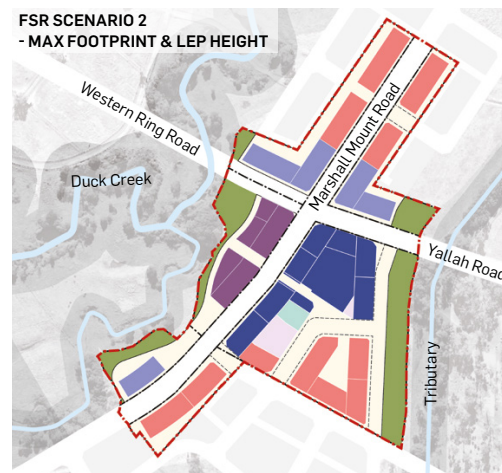
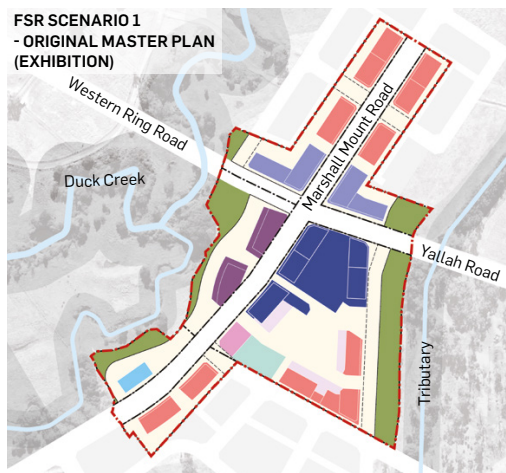


Figure 84 Marshall Mount Town Centre - Planning Controls

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## APPENDIX - MARSHALL MOUNT TOWN CENTRE

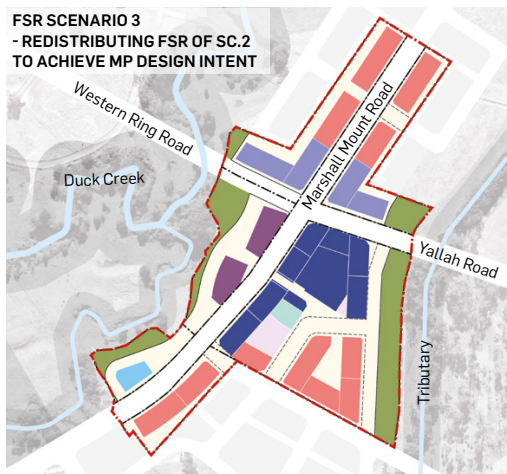
### DEVELOPMENT CAPACITY SCENARIO SUMMARY



	SQM	#DW	%		SQM	#DW	%
<b>GFA OUTCOMES</b>							
Total GFA (sqm)	43,355		36.6%		84,508		71.4%
Retail GFA	8,188				8,065		
Community & Childcare GFA	3,072				3,204		
Residential GFA	32,095	343			73,239	803	
Dwelling Density (dw/ha) -		41.6				97.4	
Indicative Population		960				2,248	
<b>FSR OUTCOMES</b>							
Achievable FSR (n:1) - GROSS	0.63		36.6%		1.23		71.4%
Achievable FSR (n:1) - NET	0.77				1.50		

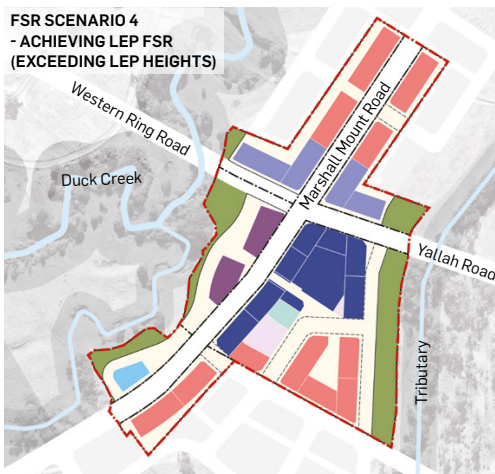


**FSR SCENARIO 3**  
- REDISTRIBUTING FSR OF SC.2  
TO ACHIEVE MP DESIGN INTENT



SQM	#DW	%
83,866		70.9%
8,965		
3,692		
71,209	777	
	94.2	
	2,176	
1.22		70.9%
1.49		

**FSR SCENARIO 4**  
- ACHIEVING LEP FSR  
(EXCEEDING LEP HEIGHTS)



SQM	#DW	%
118,737		100.3%
8,965		
3,692		
106,080	1,163	
	141.0	
	3,256	
1.73		100.3%
2.11		

## PLANNING CONTROLS

This options analysis identified that the maximum permissible GFA within the precinct would be difficult to achieve due to:







- Net developable area (i.e. removing major roads and open space land) is only 80% of the total area of land that is available within the Town Centre (excluding land already dedicated to roads).
- An additional 13% of the land will be required for future roads (approx only - based on the master plan).
- The resultant FSR for individual sites (average) throughout the town centre to realise the maximum permissible GFA would be **2.5**.

	SQM	HA	%
<b>SITE AREA</b>			
Total Site Area (sqm)	68,726	6.87	100.0%
Net Developable Area (sqm)	56,225	5.62	81.8%
Net Net Developable Area (sqm)	47,247	4.72	68.7%
<b>GFA OUTCOMES</b>			
Total Permissible GFA (sqm)	118,350		100.0%
<b>FSR OUTCOMES</b>			
Total Permissible FSR (n:1) - GROSS	1.72		100.0%
Total Permissible FSR (n:1) - NET	2.10		

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

### FSR SCENARIO 1 - ORIGINAL MASTER PLAN (EXHIBITION)

#### LEGEND

-  Total Study Area
-  Site Area
-  Net Developable Land
-  Constrained Land
-  Indicative Internal Road Reserve
-  Building Height (Number of Storey)

#### BUILDING USES

-  Mixed Retail (Convenience) / Supermarket and Residential
-  Mixed Community and Retail (Convenience)
-  Mixed Retail (Stand-alone / Hospitality) and Residential
-  Retail (Stand-alone / Hospitality)
-  Mixed Private Child Care and Residential
-  Outdoor Space for Private Child Care
-  Residential
-  Ground Level Loading / Servicing

Table 16 Development Capacity Summary - FSR Scenario 1

SITE	NET DEVELOPABLE AREA (SQM)	NET NET DEVELOPABLE AREA (SQM)	PROPOSED GFA (SQM)	GROSS FSR (N:1)	NET FSR (N:1)	HEIGHT RANGE (M/ST)
SITE 1	7,479	6,634	9,722	1.11	1.30	10.2-14m / 3-4 st
SITE 2A	9,530	8,217	9,124	0.83	0.96	10.2-14m / 3-4 st
SITE 2B	6,126	4,713	3,522	0.37	0.57	8.2m / 2 st
SITE 2C	2,982	1,838	544	0.12	0.18	4.4m / 1 st
SITE 3	25,476	21,900	16,727	0.55	0.66	4.4-14m / 1-4 st
SITE 4	4,631	3,945	3,716	0.80	0.80	10.2m / 3 st
SUB TOTAL	56,225	47,247	43,355	0.63	0.77	
CONSTRAINED LAND	10,434					
FUTURE ROAD RESERVE	2,067					
TOTAL	68,726					

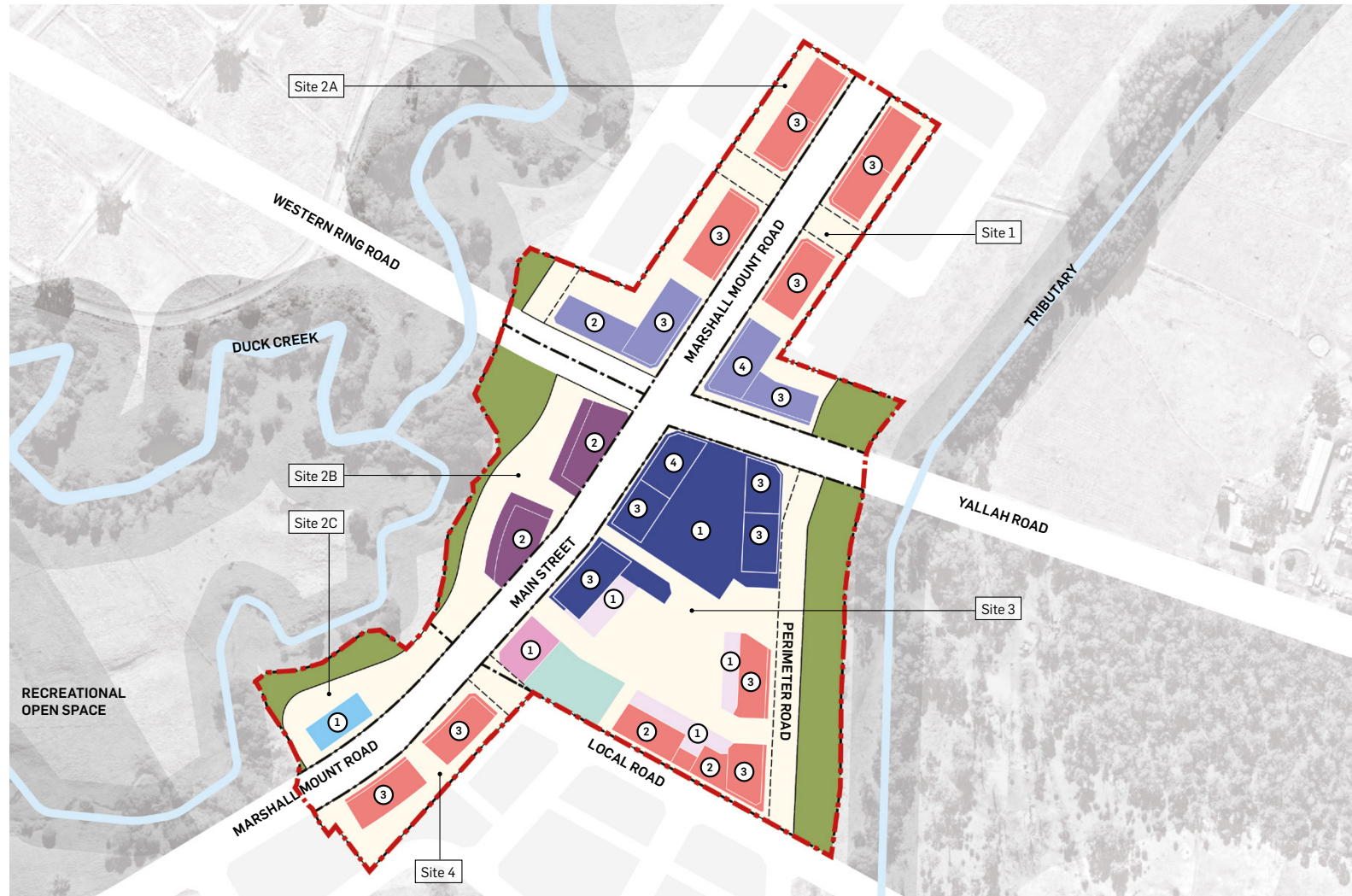








Figure 85 Marshall Mount Town Centre - FSR Scenario 1

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

### FSR SCENARIO 2 - MAX FOOTPRINT & LEP HEIGHT

#### LEGEND

-  Total Study Area
-  Site Area
-  Net Developable Land
-  Constrained Land
-  Indicative Internal Road Reserve
-  Building Height (Number of Storey)

#### BUILDING USES

-  Mixed Retail (Convenience) / Supermarket and Residential
-  Mixed Community / Retail (Convenience) and Residential
-  Mixed Retail (Stand-alone / Hospitality) and Residential
-  Outdoor Space for Private Child Care
-  Residential
-  Ground Level Loading / Servicing

Table 17 Development Capacity Summary - FSR Scenario 2

SITE	NET DEVELOPABLE AREA (SQM)	NET NET DEVELOPABLE AREA (SQM)	PROPOSED GFA (SQM)	GROSS FSR (N:1)	NET FSR (N:1)	HEIGHT RANGE (M/ST)
SITE 1	7,479	6,634	13,114	1.50	1.75	14m / 4 st
SITE 2A	9,530	8,217	16,416	1.49	1.72	14m / 4 st
SITE 2B	6,126	4,713	9,964	1.06	1.63	14.6m / 4 st
SITE 2C	2,982	1,838	3,007	0.69	1.01	14m / 4 st
SITE 3	25,476	21,900	35,039	1.15	1.38	14m / 4 st
SITE 4	4,631	3,945	6,968	1.50	1.50	13.4m / 4 st
SUB TOTAL	56,225	47,247	84,508	1.23	1.50	
CONSTRAINED LAND	10,434					
FUTURE ROAD RESERVE	2,067					
TOTAL	68,726					



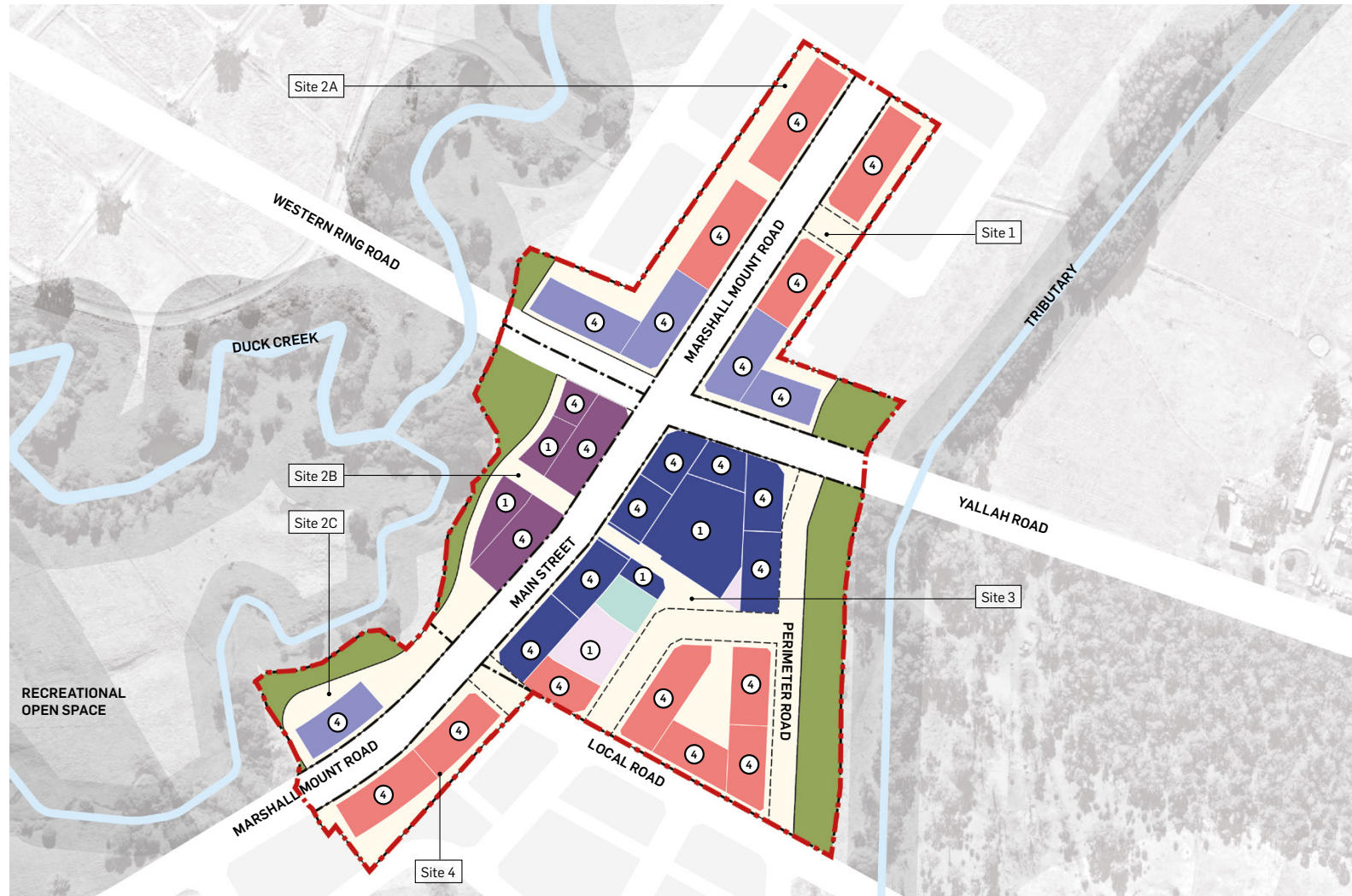








Figure 86 Marshall Mount Town Centre - FSR Scenario 2

1:3,000 @ A4  
0 25 50 75 100 125 150

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

### FSR SCENARIO 3 - REDISTRIBUTING FSR OF SC.2 TO ACHIEVE MP DESIGN INTENT

#### LEGEND

-  Total Study Area
-  Site Area
-  Net Developable Land
-  Constrained Land
-  Indicative Internal Road Reserve
-  Building Height (Number of Storey)

#### BUILDING USES

-  Mixed Retail (Convenience) / Supermarket and Residential
-  Mixed Community / Retail (Convenience) and Residential
-  Mixed Retail (Stand-alone / Hospitality) and Residential
-  Outdoor Space for Private Child Care
-  Residential
-  Ground Level Loading / Servicing

Table 18 Development Capacity Summary - FSR Scenario 3

SITE	NET DEVELOPABLE AREA (SQM)	NET NET DEVELOPABLE AREA (SQM)	PROPOSED GFA (SQM)	GROSS FSR (N:1)	NET FSR (N:1)	HEIGHT RANGE (M/ ST)
SITE 1	7,479	6,634	14,870	1.70	1.99	13.4-20.4m / 4-6 st
SITE 2A	9,530	8,217	16,378	1.48	1.72	13.4-20.4m / 4-6 st
SITE 2B	6,126	4,713	4,217	0.45	0.69	8.2m / 2 st
SITE 2C	2,982	1,838	1,458	0.33	0.49	8.2m / 2 st
SITE 3	25,476	21,900	36,490	1.20	1.43	19.8-20.4m / 6 st
SITE 4	4,631	3,945	10,453	2.26	2.26	19.8m / 6 st
<b>SUB TOTAL</b>	<b>56,225</b>	<b>47,247</b>	<b>83,866</b>	<b>1.22</b>	<b>1.49</b>	
<b>CONSTRAINED LAND</b>	<b>10,434</b>					
<b>FUTURE ROAD RESERVE</b>	<b>2,067</b>					
<b>TOTAL</b>	<b>68,726</b>					

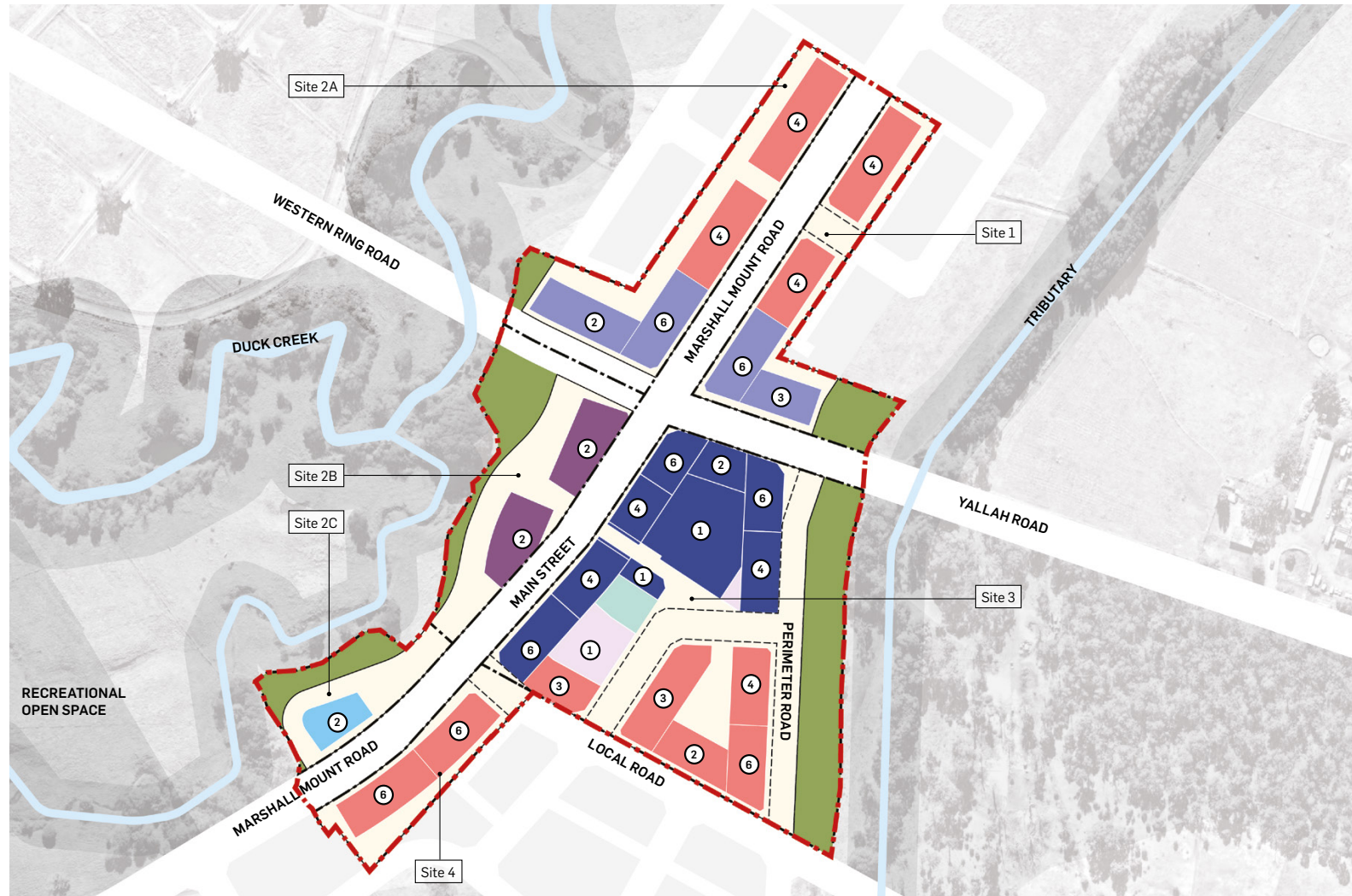








Figure 87 Marshall Mount Town Centre - FSR Scenario 3

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

### FSR SCENARIO 4 - ACHIEVING LEP FSR (EXCEEDING LEP HEIGHTS)

#### LEGEND

-  Total Study Area
-  Site Area
-  Net Developable Land
-  Constrained Land
-  Indicative Internal Road Reserve
-  Building Height (Number of Storey)

#### BUILDING USES








-  Mixed Retail (Convenience) / Supermarket and Residential
-  Mixed Community / Retail (Convenience) and Residential
-  Mixed Retail (Stand-alone / Hospitality) and Residential
-  Retail (Stand-alone / Hospitality)
-  Outdoor Space for Private Child Care
-  Residential
-  Ground Level Loading / Servicing

Table 19 Development Capacity Summary - FSR Scenario 4

SITE	NET DEVELOPABLE AREA (SQM)	NET NET DEVELOPABLE AREA (SQM)	PROPOSED GFA (SQM)	GROSS FSR (N:1)	NET FSR (N:1)	HEIGHT RANGE (M/ ST)
SITE 1	7,479	6,634	23,802	1.70	1.99	13.4-20.4m / 4-6 st
SITE 2A	9,530	8,217	27,510	1.48	1.72	13.4-20.4m / 4-6 st
SITE 2B	6,126	4,713	4,217	0.45	0.69	8.2m / 2 st
SITE 2C	2,982	1,838	1,458	0.33	0.49	8.2m / 2 st
SITE 3	25,476	21,900	51,297	1.20	1.43	19.8-20.4m / 6 st
SITE 4	4,631	3,945	10,453	2.26	2.26	19.8m / 6 st
SUB TOTAL	56,225	47,247	118,737	1.73	2.11	
CONSTRAINED LAND	10,434					
FUTURE ROAD RESERVE	2,067					
TOTAL	68,726					



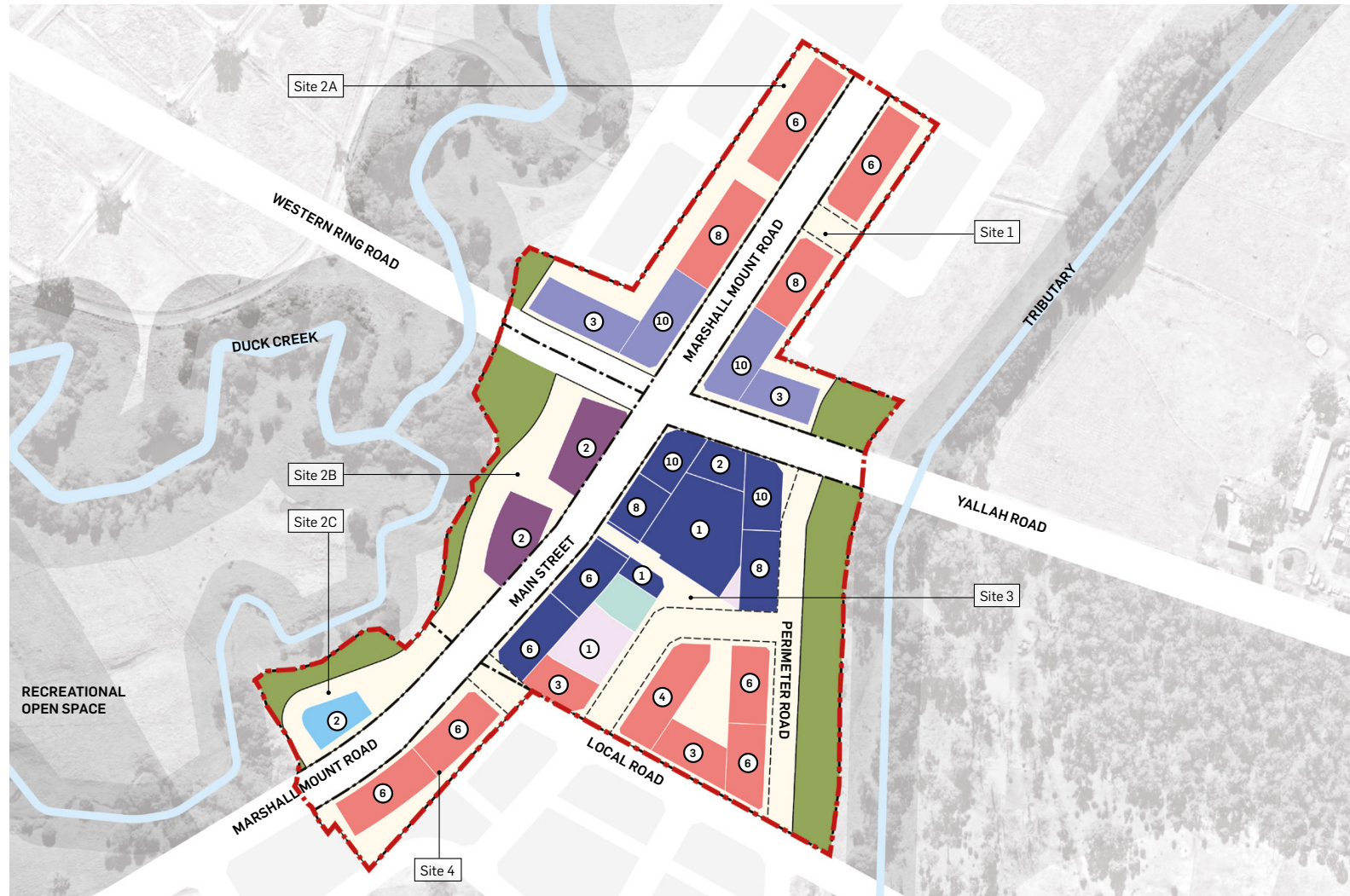









Figure 88 Marshall Mount Town Centre - FSR Scenario 4

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

### CENTRE KEY SUCCESS FACTORS ALIGNMENT - MARSHALL MOUNT TOWN CENTRE

The following table summarises the proposed Marshall Mount Town Centre Master Plan outcomes in alignment with the Centre Key Success Factors principles.

**Table 20** Centre Key Success Factors Alignment - Marshall Mount Town Centre

CENTRE KEY SUCCESS FACTORS		PROPOSED OUTCOMES – MARSHALL MOUNT TOWN CENTRE
	<b>Location</b> Locate the centre's <b>main activities / nodes</b> along the primary road connections to increase <b>visibility for passing trade</b> .	<ul style="list-style-type: none"> <li>Marshall Mount Town Centre is located at the primary intersection of Yallah Road and Marshall Mount Road.</li> <li>Yallah Road and the future western ring road is the primary passing trade.</li> </ul>
	<b>Co-location with Activity Generators</b> Co-locate the centres <b>with activity generators</b> i.e. school, child care, sporting fields, community facilities, leisure centre.	<ul style="list-style-type: none"> <li>The Town Centre is located adjacent to the proposed recreational open space and proposed private child care and community facilities within the centre.</li> </ul>
	<b>Compact and Walkable</b> Create <b>walkable and compact centre</b> that provide immediate access to the community	<ul style="list-style-type: none"> <li>Proposed primary activation is concentrated along the main street and Fig Tree Link creating a compact centre.</li> <li>The primary activation is situated in a relatively flat topography to ensure accessibility and walkability.</li> </ul>
	<b>Mix of Uses</b> <b>Promote cross-usage</b> between retail, residential, community facilities and other services to <b>increase dwell-time</b> within the centre.	<ul style="list-style-type: none"> <li>Proposed a mix of supermarket, retail, residential and community uses including multi-purpose hall and private child care within the Town Centre Core Precinct.</li> </ul>
	<b>Activation</b> Establish <b>the active retail / community uses frontage on both sides of the main street</b> with minimal movement barriers between each side.	<ul style="list-style-type: none"> <li>Proposed ground level activation on both sides of the Main Street and Fig Tree Link with minimal movement barriers.</li> </ul>
	<b>Accessibility</b> Ensure the centre is <b>accessible on various transport modes</b> including public transport, active transport (walking and cycling) and vehicle.	<ul style="list-style-type: none"> <li>The centre is surrounded by a road network that is accessible from all directions.</li> <li>Proposed bus stops located at the primary intersection adjacent to the Town Centre Core area.</li> <li>The proposed centre is connected to the wider active transport network of both on-road and off-road shared path.</li> </ul>
	<b>Population Catchment</b> Ensure the centre is <b>accessible to the largest population catchment</b> possible.	<ul style="list-style-type: none"> <li>Proposed high density residential / apartment within the centre and transitioning to the surrounding medium density housing type.</li> <li>The proposed secondary road to the west improves connectivity to the residential neighbourhood to the west.</li> </ul>

## STRATEGIC PLANNING ALIGNMENT - MARSHALL MOUNT TOWN CENTRE

The following table summarises the proposed Marshall Mount Town Centre Master Plan outcomes, in alignment with the relevant strategic planning directions.

This includes:

- West Dapto Vision 2018;
- West Dapto Social Infrastructure Needs Assessment 2023
- West Dapto Development Contribution Plan 2024; and
- Wollongong Development Control Plan 2009
- Wollongong Retail and Business Centres Strategy March 2023

Table 21 Strategic Planning Directions Alignment - Marshall Mount Town Centre

STRATEGIC PLANNING DIRECTIONS	STATEMENT / REQUIREMENT	ALIGNMENT	PROPOSED OUTCOMES
<b>West Dapto Vision 2018</b>			
	<p><i>"West Dapto will grow and develop as a series of <b>Integrated and connected communities</b>. Set against the spectacular <b>Illawarra Escarpment and a landscape of riparian valleys</b>, these communities will integrate the <b>natural and cultural heritage</b> of the area with the new urban form.</i></p> <p><i>The communities will be healthy, sustainable and resilient with active and passive open space <b>accessible by walkways, cycleways and public transport</b>. To support these new communities, <b>local centres</b> will provide shopping services, community services and jobs while employment lands will facilitate further opportunities for the region.</i></p> <p><i>West Dapto will be supported by a long-term strategy to oversee the timely <b>implementation of infrastructure</b> to deliver sustainable and high-quality suburbs with <b>diverse housing choices</b>."</i></p>	✓	<p>The vision for Marshall Mount Town Centre is to create:</p> <p>"A compact mixed retail and community precinct with housing choice, conveniently located, and accessible by active and public transport. It is characterised by a vibrant main street that balances pedestrian and vehicular movements, and local places for people that celebrate views to the scenic Illawarra Escarpment and the existing Moreton Bay Fig Tree."</p> <p>Five Guiding Principles have been identified for Marshall Mount in alignment with the West Dapto Vision, being:</p> <ol style="list-style-type: none"> <li>1. <b>Celebrate the Country.</b></li> <li>2. <b>A Connected and Accessible Centre.</b></li> <li>3. <b>Activated Place and Viable Centre.</b></li> <li>4. <b>Deliver a Housing Mix Close to Amenities.</b></li> <li>5. <b>Contextual Built Form Response.</b></li> </ol>
<b>West Dapto Social Infrastructure Needs Assessment 2023</b>			
Sub district multi-purpose community centre	Consider the delivery of library floorspace of at least 1,500 sqm in Marshall Mount TC, as part of a larger co-located facility. It is noted that this could be of a 'local scale', being between 1,500 - 2,400 sqm.	✓	Proposed approximately 2,500 sqm community space, which has enough capacity to accommodate a library.
OS23 - Yallah Recreation Area - Neighbourhood Park	Indicative Land Area - 5ha	✓	<ul style="list-style-type: none"> <li>• Proposed open space location as per zoning.</li> <li>• Provide cycleway / pedestrian connection along Duck Creek from the Town Centre to the proposed Neighbourhood Park.</li> </ul>

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

STRATEGIC PLANNING DIRECTIONS	STATEMENT / REQUIREMENT	ALIGNMENT	PROPOSED OUTCOMES
<b>West Dapto Development Contribution Plan 2024</b>			
<b>Open Space &amp; Recreation</b> OS23 - Yallah Recreation Area - Neighbourhood Park	Indicative Land Area - 5ha	✓	<ul style="list-style-type: none"> <li>Proposed open space location as per zoning.</li> <li>Provide cycleway / pedestrian connection along Duck Creek from the Town Centre to the proposed Neighbourhood Park.</li> </ul>
<b>Community Facilities</b> CF05 - Local multi-purpose community centre and library	Indicative Land Area - 1ha Indicative GFA - 2,400 sqm	✓	Proposed approximately 2,500 sqm community space along the Main Street adjacent to the main activation.
<b>Active Transport</b> Shared used pathway network and shared use pathway bridges	Support improvements to active transport (walking and cycling). Active transport, including walking and cycling, will be an important component of the future West Dapto transport system, contributing significantly to achieving the vision of a sustainable community.	✓	Connect the cycleway along Duck Creek and its tributaries to the Town Centre.
<b>Wollongong DCP Chapter B4 - Development in Centres and Peripheral Sales Precinct</b>			
<b>Marshall Mount Town Centre</b>			
Floorspace	<i>Retail floorspace - 5,000 - 20,000 sqm</i>	✓	Proposed a total retail GFA of approximately 8,100 sqm.
Population catchment	<i>10,000 - 20,000 (800m - 1km catchment)</i>	✓	(For further detail refer to the FSR scenarios investigation.) <ul style="list-style-type: none"> <li>To achieve the min. population of 10,000 people identified in the retail strategy, E1 and MU1 zoned land within the Town Centre need to deliver <b>1,312 dwelling / 3,635 population / 160 dw/ha</b>.</li> <li>Testing of scenario 4 (achieved FSR) results in <b>1,163 dw / 3,256 population</b>, slightly below the dwelling density target.</li> </ul>
Accessibility	<i>On public transport network with access to transport by train or bus. Accessible for logistics and private vehicles. Active transport connections to and within the Centre.</i>	✓	The town centre is accessible via multi modes of private vehicle, bus service and active transport (cycling and walking)
Retail Uses	<i>Generally anchored by a medium to full sized supermarket; May include a fruit and vegetable store, bakery, butcher and a range of services including a pharmacy, hairdressers, postal services, gyms etc.</i>	✓	Propose a full-size supermarket with specialty retail along the Main Street and Fig Tree Link.



STRATEGIC PLANNING DIRECTIONS	STATEMENT / REQUIREMENT	ALIGNMENT	PROPOSED OUTCOMES						
Other Uses	<i>The Town Centre provides local scale public open space. Some well located Town Centres include local scale community facilities.</i>	✓	<ul style="list-style-type: none"><li>Proposed community facilities uses (library, community centre and private child care) within the town centre core zone.</li><li>Proposed Main Street and Fig Tree Link within the primary activation area.</li><li>Proposed active transport access to the proposed recreational open space to the southwest.</li></ul>						
Wollongong DCP Chapter D16 – West Dapto Urban Release Area									
11.1 Town Centre Development Controls	Development in the West Dapto Town Centres is to comply with the following development controls								
	1. Establish a strong urban form that clearly distinguishes the centre or local node from surrounding areas.	✓	Proposed town centre sits at the intersection of the primary roads with high density mixed use residential typologies, providing a distinguish character to the surrounding context.						
	2. Taller buildings which maximise building height is encouraged in town centre core. Lower scale buildings up to 3 storeys in height should surround and support the core.	✓	Maximum height is limited to 15m in the LEP controls. Master Plan has identified the opportunity for taller, marker buildings beyond the LEP controls.						
	3. The street wall height should have a 2-3 storey building form.	✓	Proposed 2 storey street wall along Marshall Mount Road and Yallah Road / Western Ring Road.						
	4. Zero building setbacks on main streets. Other streets are generally to have a setback of between 0 – 2.5m.	✓	Proposed zero building setbacks along the Main Street.						
	5. Side and rear building setbacks are as follows:								
	<table><tr><th>Setback</th><th>Distance</th></tr><tr><td>Side</td><td>Zero</td></tr><tr><td>Rear</td><td>Zero - where lot adjoins allotment zoned E1 Local Centre; or 5-6m - where lot adjoins allotment with any residential zone</td></tr></table>	Setback	Distance	Side	Zero	Rear	Zero - where lot adjoins allotment zoned E1 Local Centre; or 5-6m - where lot adjoins allotment with any residential zone	✓	Proposed rear setback for MU-1 zone aligned with the setback control.
	Setback	Distance							
	Side	Zero							
	Rear	Zero - where lot adjoins allotment zoned E1 Local Centre; or 5-6m - where lot adjoins allotment with any residential zone							
6. Civic public spaces designed to encourage social interaction with paved areas, outdoor seating and urban green spaces are encouraged to balance the indoor building provisions.	✓	Proposed Fig Tree Link with public plaza towards the western end adjacent to the Moreton Bay Fig Tree as the key local landscape features.							
7. Shared parking facilities are encouraged, accessed from laneways of minor streets. Parking lots and parking areas should not be visible from the streets, allowing the built form to define the urban function.	✓	Proposed consolidated at-grade parking space within the Town Centre Core Precinct behind the buildings.							

## APPENDIX - MARSHALL MOUNT TOWN CENTRE

STRATEGIC PLANNING DIRECTIONS	STATEMENT / REQUIREMENT	ALIGNMENT	PROPOSED OUTCOMES
West Dapto Development Contribution Plan 2024			
<b>5.3 Our new Centres in West Dapto are well planned</b>  Recommendation 9 <i>"Communicate clear expectations for centre development outcomes"</i>	States: <i>Ensure the LEP and DCP controls for new centres in West Dapto facilitate the outcomes described for towns and villages in the updated Centres Hierarchy.</i>	✓	<ul style="list-style-type: none"> <li>The Master Plan for Marshall Mount Town Centre identified there is some misalignment between the existing LEP controls for FSR and Maximum Heights being the FSR cannot be achieved within the height controls. However, it is noted that the retail and centres hierarchy has been informed by population and dwelling outcomes that assume realisation of the FSRs.</li> </ul>

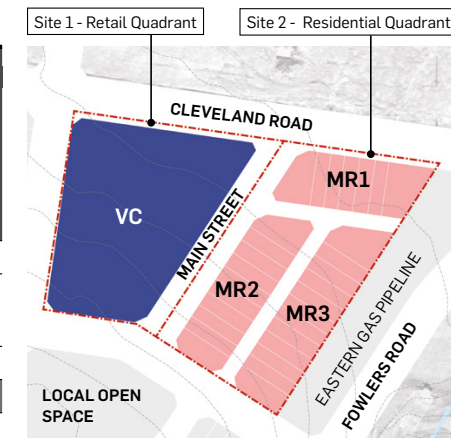
## APPENDIX - FOWLERS VILLAGE CENTRE

# INDICATIVE YIELD BREAKDOWN - FOWLERS VILLAGE CENTRE

The following table and diagram provides the proposed land use and yield breakdown of Fowlers Village Centre.

**Table 22** Fowlers Village Centre - Yield Breakdown

LAND USE AND SITE AREA					GROSS FLOOR AREA							
LOT NO.	LAND USE	LAND USE AREA (SQM)	LAND USE AREA (HA)	LAND USE AREA (%)	RESI GFA (SQM)	CHILD CARE GFA (SQM)	RETAIL GFA (SQM)	RETAIL - SUPERMARKET GFA (SQM)	TOTAL GFA (SQM)	FSR (N:1)	DW. NO.	DW./HA
<b>SITE 1 - RETAIL QUADRANT</b>												
VC	Retail (Convenience & Supermarket) / Private Child Care	8,382	0.84	38.9%	271	1,258	1,527	3,056		0.36		
	Road Reserve	1,717	0.17	8.0%								
<b>SITE 1 TOTAL</b>		<b>10,099</b>	<b>1.01</b>	<b>46.9%</b>	<b>271</b>	<b>1,258</b>	<b>1,527</b>	<b>3,056</b>		<b>0.30</b>		
<b>SITE 2 - RESIDENTIAL QUADRANT</b>												
MR1	Medium Density Residential	2,231	0.22	10.3%	840			840		0.38	7	31.4
MR2	Medium Density Residential	2,791	0.28	13.0%	1,200			1,200		0.43	10	35.8
MR3	Medium Density Residential	3,387	0.34	15.7%	1,440			1,440		0.43	12	35.4
	Sub Total Developable Area	8,409	0.84	39.0%	3,480			3,480		0.41	29	34.5
	Road Reserve	3,041	0.30	14.1%								
<b>SITE 2 TOTAL</b>		<b>11,450</b>	<b>1.14</b>	<b>53.1%</b>	<b>3,480</b>			<b>3,480</b>		<b>0.30</b>		<b>25.4</b>
<b>TOTAL</b>		<b>21,549</b>	<b>2.15</b>	<b>100.0%</b>				<b>6,536</b>		<b>0.30</b>	<b>29</b>	



**Figure 89** Fowlers Village Centre - Land Use  
1:3,000 @ A4  
0 25 50 75 100 125 150

### LEGEND

  Fowlers Village Centre

### LAND USE

Village Centre - Mixed Retail / Private Child Care  
 Medium Density Residential

## APPENDIX - FOWLERS VILLAGE CENTRE

### INDICATIVE PARKING PROVISION - FOWLERS VILLAGE CENTRE

The following table and diagram identify the indicative parking provision breakdown.

**Table 24**    Fowlers Village Centre - Indicative Parking Provision

PARKING PROVISION		%
Parking Requirement (space)	125	
Parking Provision (space)	160	100%
At-grade Parking	128	80%
Kerbside Parking	32	20%

### INDICATIVE RETAIL AND COMMUNITY SPACE OUTCOME - FOWLERS VILLAGE CENTRE

The following table identify the indicative retail and community space outcome.

**Table 23**    Fowlers Village Centre - Indicative Retail and Community Space Outcome.

<b>Fowlers Village Centre Retail Forecast Demand 2041 (sqm)</b>		<b>2,168</b>
(Wollongong Retail and Business Strategy, 2023)		
<b>RETAIL AND COMMUNITY SPACE OUTCOME</b>		
Retail GFA		2,785 sqm
Retail - Supermarket		1,527 sqm
Retail - Convenience (including cafe, specialty shops, gym and pharmacy)		1,258 sqm
Private Child Care GFA		271 sqm
Private Child Care Outdoor Space		563 sqm
Private Child Care Capacity (# of places)		60



## APPENDIX - FOWLERS VILLAGE CENTRE

**INDICATIVE DWELLING DENSITY SUMMARY  
- FOWLERS VILLAGE CENTRE**

The following table identify the indicative dwelling density summary.

**Table 25**    Fowlers Village Centre - Indicative Dwelling  
Density Summary








DWELLING DENSITY SUMMARY (WITHIN VILLAGE CENTRE)	
Residential Quadrant Site Area (ha)	1.14
Developable Residential Lot Area (ha)	0.84
<b>Total Dwelling No.</b>	<b>29</b>
<b>Gross Density (dw/ha) - Residential Quadrant Site Area</b>	<b>25.4</b>
<b>Net Density (dw/ha) - Residential Lot Area</b>	<b>34.5</b>

## APPENDIX - FOWLERS VILLAGE CENTRE

### CENTRE KEY SUCCESS FACTORS ALIGNMENT - FOWLERS VILLAGE CENTRE

The following table summarises the proposed Fowlers Master Plan outcomes alignment with the Centre Key Success Factors principles.

**Table 26** Fowlers Village Centre - Centre Key Success Factors Alignment

CENTRE KEY SUCCESS FACTORS		PROPOSED OUTCOMES – FOWLERS VILLAGE CENTRE
	<b>Location</b> Locate the centre's <b>main activities / nodes</b> along the primary road connections to increase <b>visibility for passing trade</b> .	<ul style="list-style-type: none"> <li>Fowlers Village Centre is located at the primary intersection of Cleveland Road and Fowlers Road.</li> <li>The Centre is highly visible from Fowlers Road and Cleveland Road being the primary movement corridor / passing trade.</li> </ul>
	<b>Co-location with Activity Generators</b> Co-locate the centres <b>with activity generators</b> i.e. school, child care, sporting fields, community facilities, leisure centre.	<ul style="list-style-type: none"> <li>The centre is located within 100m / &lt;5 mins walking to the proposed leisure and recreational centre to the southwest.</li> </ul>
	<b>Compact and Walkable</b> Create <b>walkable and compact centre</b> that provide immediate access to the community	<ul style="list-style-type: none"> <li>Proposed primary activation is concentrated along the main street creating a compact centre.</li> <li>The primary activation is situated in a gentle slope topography to ensure accessibility and walkability.</li> </ul>
	<b>Mix of Uses</b> <b>Promote cross-usage</b> between retail, residential, community facilities and other services to <b>increase dwell-time</b> within the centre.	<ul style="list-style-type: none"> <li>Proposed a mix of mid size supermarket, retail, residential, and private child care within the primary activation zone.</li> </ul>
	<b>Activation</b> Establish <b>the active retail / community uses frontage on both sides of the main street</b> with minimal movement barriers between each side.	<ul style="list-style-type: none"> <li>Proposed ground level activation along the main street with minimal movement barriers.</li> </ul>
	<b>Accessibility</b> Ensure the centre is <b>accessible on various transport modes</b> including public transport, active transport (walking and cycling) and vehicle.	<ul style="list-style-type: none"> <li>The centre is surrounded by a road network that is accessible from all directions.</li> <li>Proposed bus stops located to the south end of the main street and adjacent to the local park.</li> <li>The proposed centre is connected to the wider active transport network of both on-road and off-road cycleway.</li> </ul>
	<b>Population Catchment</b> Ensure the centre is <b>accessible to the largest population catchment</b> possible.	<ul style="list-style-type: none"> <li>Proposed a mix of 2 storeys residential apartment and terrace homes within the village centre core zone transitioning to medium density housing type of the surrounding neighbourhood.</li> </ul>

## APPENDIX - FOWLERS VILLAGE CENTRE

# STRATEGIC PLANNING ALIGNMENT - FOWLERS VILLAGE CENTRE

The following table summarises the proposed Fowlers Master Plan outcomes alignment with the relevant strategic planning directions.

This includes:

- West Dapto Vision 2018;
- West Dapto Development Contribution Plan 2020; and
- Wollongong Document Control Plan 2009

Table 27 Fowlers Village Centre - Strategic Planning Alignment

STRATEGIC PLANNING DIRECTIONS	STATEMENT / REQUIREMENT	ALIGNMENT	PROPOSED OUTCOMES
<b>West Dapto Vision 2018</b>			
	<p><i>"West Dapto will grow and develop as a series of <b>Integrated and connected communities</b>. Set against the spectacular <b>Illawarra Escarpment and a landscape of riparian valleys</b>, these communities will integrate the <b>natural and cultural heritage</b> of the area with the new urban form.</i></p> <p><i>The communities will be healthy, sustainable and</i></p> <p><i>Resilient with active and passive open space <b>accessible by walkways, cycleways and public transport</b>. To support these new communities, <b>local centres</b> will provide shopping services, community services and jobs while employment lands will facilitate further opportunities for the region.</i></p> <p><i>West Dapto will be supported by a long-term strategy to oversee the timely <b>implementation of infrastructure</b> to deliver sustainable and high-quality suburbs with <b>diverse housing choices</b>."</i></p>	✓	<p>The vision for Fowlers Village Centre is to create:</p> <p><i>"A compact local retail precinct within connected and diverse communities with public and active transport connections to key recreational open spaces and amenities along Fowlers Road and a visual connection to Mount Keira and Mount Kembla."</i></p> <p>Five Site Key Directions have been identified for Fowlers Village Centre in alignment with the West Dapto Vision, being:</p> <ol style="list-style-type: none"> <li>1. <b>Celebrate the Country.</b></li> <li>2. <b>A Connected and Accessible Centre.</b></li> <li>3. <b>Activated Place and Viable Centre.</b></li> <li>4. <b>Deliver a Housing Mix Close to Amenities.</b></li> <li>5. <b>Contextual Built Form Response.</b></li> </ol>
<b>West Dapto Development Contribution Plan 2020</b>			
<b>Open Space &amp; Recreation</b>			
OS13 – Community Leisure and Recreation Centre		✓	<ul style="list-style-type: none"> <li>• Proposed leisure recreation centre location to the southeast of the Village Centre (outside the centre boundary).</li> <li>• Provide cycleway / pedestrian connection between the Village Centre and the Community Leisure and Recreation Centre.</li> </ul>

## APPENDIX - FOWLERS VILLAGE CENTRE

STRATEGIC PLANNING DIRECTIONS	STATEMENT / REQUIREMENT	ALIGNMENT	PROPOSED OUTCOMES
<b>Community Facilities</b>	Indicative Land Area - 1,500 sqm		
CF04 - Neighbourhood multi-purpose community centre	Indicative GFA - 350 sqm	✓	<ul style="list-style-type: none"> <li>Proposed location within the Leisure and Recreation Centre.</li> <li>Provide cycleway / pedestrian connection between the Village Centre and the Community Leisure and Recreation Centre.</li> </ul>
<b>Wollongong DCP Chapter B4 - Development in Centres and Peripheral Sales Precinct</b>			
<b>Ch 4 Centres Hierarchy</b>			
<b>4.2 Development Controls Village Centre</b>			
Floorspace	2,000-5,000 sqm	✓	Proposed a total retail GFA of approximately 2,700 sqm.
Population catchment	up to 10,000 (~ catchment radius of 600 – 800m)	✓	Refer to catchment analysis for Marshall Mount on challenges achieving these populations within the 600-800m catchment based on existing LEP controls).
Accessibility	On public transport network with access to transport by train or bus. Accessible for logistics and private vehicles. Active transport connections to and within the Centre. Slow through traffic and walkable catchment (5-10 minutes). Pedestrians are prioritised.	✓	The village centre is accessible via multi modes of private vehicle, bus service and active transport (cycling and walking).
Retail Uses	<p>Village Centres are served by a small to medium sized neighbourhood supermarket and or mixed business type shop, fruit and vegetable retailers, newsagents, restaurant, bakery, butcher and some limited services for example hairdresser.</p> <p>Individual tenancies are small scale with a smaller floor area than their Town Centre counterparts.</p> <p>Food and Drink Premises with drive through facilities are not permitted in Village Centres (Wollongong LEP).</p>	✓	Propose a mid-size supermarket with specialty retail along the main street.
Residential Uses	Village Centres have lower height limits, restricting upper story residential development to traditional shop-top housing, with a small number of apartments above a retail or business tenancy. These apartments provide passive surveillance within the Centre, and offer housing diversity and relative affordability in the context of the surrounding neighbourhood.	✓	Proposed up to two storeys shop top housing, townhouse and terrace homes within the village centre core zone.



STRATEGIC PLANNING DIRECTIONS	STATEMENT / REQUIREMENT	ALIGNMENT	PROPOSED OUTCOMES
Other Uses	<i>The Village Centre provides local scale public open space in the form of small landscaped plazas or similar.</i>	✓	Proposed local open space to the local hill to the south with visual connection to Illawarra Escarpment, Mount Kembla and Mount Keira..
<b>Wollongong DCP Chapter D16 – West Dapto Urban Release Area</b>			
<b>11.2 Village Centre Development Controls</b>	Development in the West Dapto Village Centres is to comply with the following development controls:		
	1. Building setbacks can be either street aligned (zero) or setback up to 5m to create commercial forecourts or residential courts to the street.	✓	Proposed zero building setbacks along the Main Street with ground level retail and 3m setback for ground level residential.
	2. Varied building setbacks are encouraged to create an informal organic character.	✓	Proposed zero building setbacks for ground level retail / community uses and 3m setback for ground level residential provide a varied setback.
	3. Building heights of up to 2 storeys are encouraged to create an urban village character with upper floor uses including small scale commercial and residential developments.	✓	Proposed 1-2 storeys streetwall along the Main Street.
	4. Parking to be provided at the rear of buildings in the form of rear laneways and parking areas accessed from the rear laneways / car courts.	✓	Proposed consolidated at-grade parking space within the Village Centre core zone behind the buildings.
	5. All shops should address and be entered from the major street where possible, or the secondary street.	✓	Retail frontage along the Main Street providing street address and activation.
	6. Community congregation areas to be north facing and take advantage of escarpment views where possible.	✓	North-South Main Street alignment and proposed local open space to the local hill to the south provide public space with visual connection to the Mount Kembla and Mount Keira to the north and Illawarra Escarpment to the west.
	7. Street parking to be maximised around villages.	✓	Proposed on-street parking along the Main Street and Minor Collector Road.
	8. Parking lots and parking areas are generally not to be visible from the main collector roads, allowing the built form to define the urban function.	✓	Proposed consolidated at-grade parking space behind the Main Street to ensure that it is not visible from the primary roads.

## APPENDIX

# DEVELOPMENT ASSUMPTIONS

Development assumptions contained within following table underpin the master plans for both Marshall Mount Town Centre and Fowlers Village Centre.

Table 28 Development Assumptions

DEVELOPMENT ASSUMPTIONS		SOURCE / REMARKS
<b>BUILDING HEIGHT (M)</b>		
4.40 m	Ground level commercial floor to floor height	Apartment Design Guide
3.70 m	Upper level commercial floor to floor height	Apartment Design Guide
3.70 m	Ground level residential level floor to floor height	Apartment Design Guide
3.10 m	Upper level residential level floor to floor height	Apartment Design Guide
<b>BUILDING EFFICIENCY (%)</b>		
85%	Commercial / Retail GBA to GFA	Industry practice
85%	Community GBA to GFA	Industry practice
75%	Residential Apartment GBA to GFA	Industry practice
<b>RESIDENTIAL UNIT (SQM)</b>		
90 sqm	Average GFA/ Apartment unit - Built to Sell	Higher average apartment unit size outside of metropolitan area
120 sqm	Average GFA/ rear loaded terrace homes	
<b>LOW-MEDIUM DENSITY RESIDENTIAL LOT SIZE (SQM)</b>		
200 sqm	Average terrace homes	

DEVELOPMENT ASSUMPTIONS		SOURCE / REMARKS
<b>CHILD CARE</b>		
13 sqm	GFA (indoor and outdoor) child care / child	3.25 sqm indoor space + 7 sqm outdoor space (unencumbered) per child (Wollongong DCP 2009 Ch C5) / 80% efficiency
5	Children / employee (average age 2-3)	NSW Government Required Staff Ratios - Education and Care Services National Regulations
<b>PARKING REQUIREMENTS (#SPACE)</b>		
1.5	Average Car Parking Space/ Dwelling	Wollongong DCP 2009 Ch E3
0.2	Car Visitor Parking Space/ Dwelling	Wollongong DCP 2009 Ch E3
27.78 sqm	GFA retail/ Car Parking space	10% reduction within 400m bus stop (25sqm / parking space, Retail premises - Wollongong DCP 2009 Ch E3)
44.44 sqm	GFA community centre/ Car Parking space	10% reduction within 400m bus stop (Assumed business premises parking req. 40 sqm / parking space - Wollongong DCP 2009 Ch E3)
6	Children/ child care visitor parking space	Wollongong DCP 2009 Ch E3
1	Child care employee / parking space	Wollongong DCP 2009 Ch E3
0.33	Residents bicycle park/ dwelling	1 bicycle parking / 3 dwellings - Wollongong DCP 2009 Ch E3

DEVELOPMENT ASSUMPTIONS		SOURCE / REMARKS
0.0833	Visitor bicycle parking/ dwelling	1 bicycle parking / 12 dwellings - Wollongong DCP 2009 Ch E3
0.040	Motorcycle Parking / Carpark	Wollongong DCP 2009 Ch E3, Business & retail premises, child care
0.067	Motorcycle Parking / dwelling	Wollongong DCP 2009 Ch E3, residential
<b>PARKING SPACE PROVISION (SQM)</b>		
35 sqm	Basement Carpark Gross Area per space -Residential	Parking space, circulation and servicing
30 sqm	At-grade Carpark Gross Area per space	Parking space, circulation and planting
1 sqm	Per bike parking space	Assume vertical bicycle stand
2 sqm	Per motorcycle parking space	
<b>APARTMENT DESIGN GUIDE (ADG) REQUIREMENTS (%)</b>		
25%	Minimum Communal Open Space Provision	
7%	Minimum Deep Soil Zone Provision	
50%	Min. COS receiving at least 2hr sunlight in mid winter	

LIST OF ABBREVIATION	
ADG	Apartment Design Guide
AEP	Annual Exceedance Probability
APZ	Asset Protection Zone
DCP	Development Control Plan
DW.	Dwelling
EGP	Eastern Gas Pipeline
FSR	Floor Space Ratio
GBA	Gross Building Area
GFA	Gross Floor Area
HA	Hectare
LEP	Local Environmental Plan
REQ.	Requirement
RESI	Residential
RFB	Residential Flat Building
SQM	Square Meter
TC	Town Centre
VC	Village Centre







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2	27/7/2011	3/8/2011	Incorporate Shone Ave Neighbourhood Plan
3	26/11/2012	8/12/2012	Update Wongawilli North Neighbourhood Plan
4	27/5/2013	1/6/2013	Incorporate Sheaffes Rd Neighbourhood Plan
5	9/12/2013	14/12/2013	Incorporate Reddalls Rd Industrial Neighbourhood Plan
6	24/3/2014	2/4/2014	Incorporate Darkes Rd South West Neighbourhood Plan and updated road network diagrams
7	3/8/2015	12/8/2015	Incorporate Avondale Road North, Huntley Neighbourhood Plan
8	24/8/2015	9/9/2015	Incorporate Shone Avenue / West Dapto Road Neighbourhood Plan
9	19/10/2015	24/10/2015	Incorporate West Dapto Rd / Sheaffes Rd (south) Neighbourhood Plan
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15	2/8/2021	30/8/2021	Incorporate additional area into Hayes Land and Iredell Road Neighbourhood Plan.
16	18/10/2021	12/11/2021	Neighbourhood Planning Review
17	27/05/2024	03/06/2024	Elm Park Neighbourhood Plan
18	24/06/2024	-	West Dapto Centres Master Plans



## 1. INTRODUCTION

This chapter of the Wollongong Development Control Plan 2009 (DCP) is intended to provide structure and guidance for the future development of the West Dapto Urban Release Area. It is aimed at achieving the vision for West Dapto which is:

*West Dapto will grow and develop as a series of integrated and connected communities. Set against the spectacular Illawarra Escarpment and a landscape of riparian valleys, these communities will integrate the natural and cultural heritage of the area with the new urban form.*

*The communities will be healthy, sustainable and resilient with active and passive open space accessible by walkways, cycleways and public transport. To support these new communities, local centres will provide shopping services, community services and jobs while employment lands will facilitate further opportunities for the region.*

*West Dapto will be supported by a long-term strategy to oversee the timely implementation of infrastructure to deliver sustainable and high-quality suburbs with diverse housing choices.*

Other parts of this DCP continue to apply to the West Dapto Release Area in conjunction with this chapter. Part A of the DCP contains the Introduction and Part B provides Land Use Based Planning Controls. Part C provides Specific Land Use Controls and Part E provides the General (City Wide) Controls.

This document is set out around groups of planning principles. The principles are designed to outline expectations around elements for consideration while planning for development of the West Dapto Urban Release Area. The principles, objectives and applicable controls will guide the growth of new suburbs and neighbourhoods, protect the environment and integrate with existing communities.

## 2. LAND TO WHICH CHAPTER APPLIES

This chapter applies to all land within the West Dapto Release Area (**Figure 1**).

Part D – Locality Based DCPs / Precinct Plans

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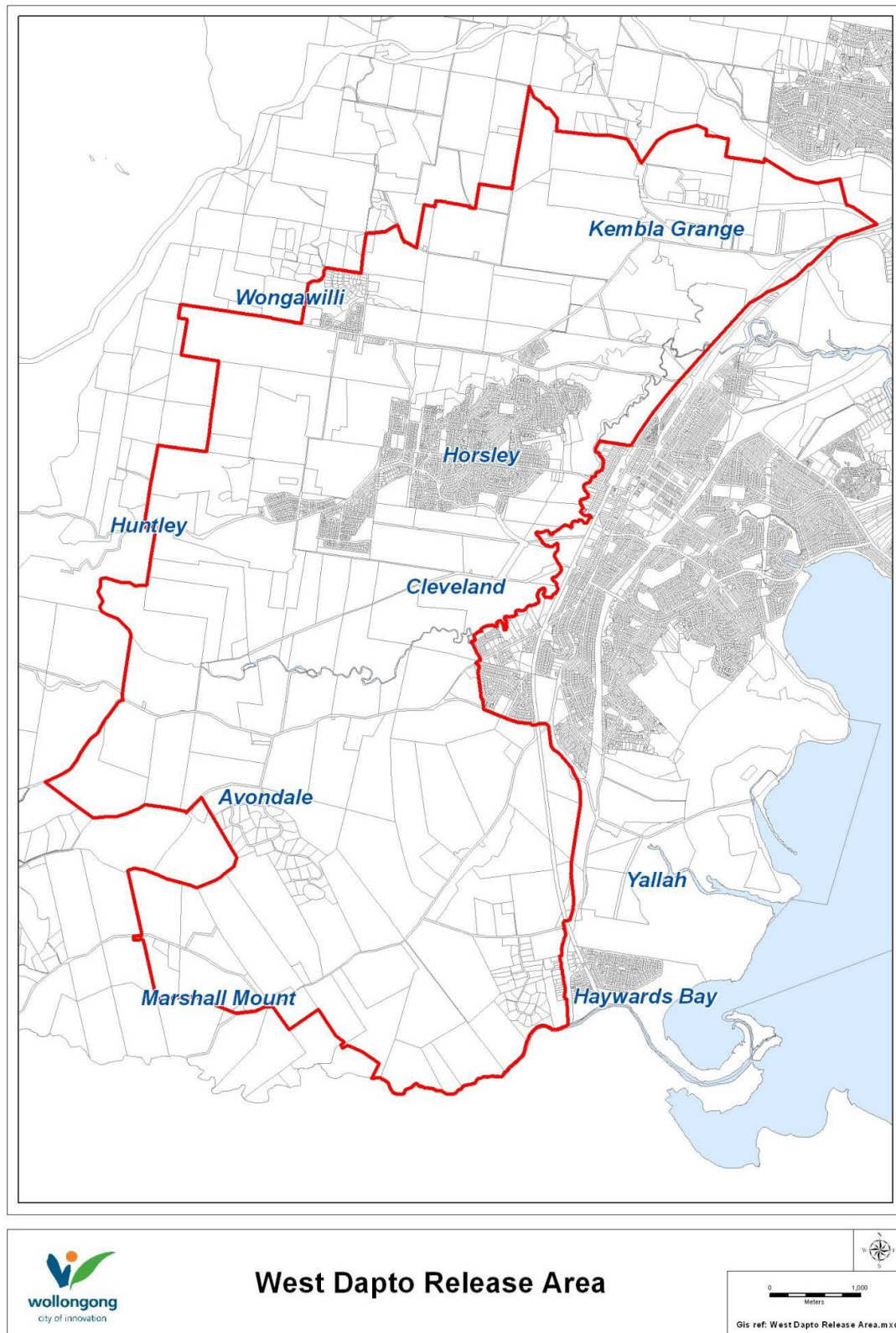


Figure 1. West Dapto Release Area

### 3. OBJECTIVES

The objectives of this chapter are to:

- (a) enable the development of the West Dapto Urban Release Area for residential, employment, industrial and environmental conservation areas in a manner consistent with the Wollongong LEP 2009 the West Dapto Vision 2018 and the West Dapto Structure Plan (**Figure 2**).
- (b) ensure development incorporates the principles of Ecologically Sustainable Development, promoting the retention and enhancement of the area's unique environmental features, to shape the desired future urban setting.
- (c) support the provision of safe and efficient road networks that promotes long term sustainability and active transport, with public transport services linking surrounding areas.
- (d) implement Water Sensitive Urban Design (WSUD) for effective water management and protect development in the area from flooding.
- (e) recognise the existing environmental and landscape qualities of the release area and establish future urban characteristics to shape ongoing development.
- (f) protect, conserve and enhance riparian and environmentally sensitive areas and only allow for development compatible with the conservation values of these areas.
- (g) protect areas of high scenic value, notably the Illawarra Escarpment and Lake Illawarra with developments that contribute and promote the areas visual and aesthetic values.
- (h) conserve and enhance the environmental, cultural and built heritage of West Dapto.
- (i) guide the development of open space to meet future community needs and facilitate a network of open space connected by off road cycleways and shared paths throughout the release area.
- (j) ensure that development in the Darkes Road, Bong Bong and Marshall Mount town centres contributes to the creation of retail, business, commercial and community hubs and provides significant local employment and community service opportunities.
- (k) provide village centres with localised businesses and higher density residential opportunities at key places or intersections where bus stops, community facilities and open space come together as local urban focal points.
- (l) ensure the community's social and cultural needs are met through the provision of a range of community facilities across the release area (co-located with other facilities in 'hubs', creating urban focal points).
- (m) guide planning and development of well-located schools, childcare centres, and adult education facilities to support the community's educational needs.
- (n) stimulate diversity in development types and styles to provide a range of different dwellings to increase housing choice and design quality in the Illawarra.
- (o) ensure safe, secure, liveable, and resilient urban environments are established considering future climate and other potential environmental vulnerabilities.
- (p) improve employment opportunities and economic growth in the Illawarra region whilst ensuring that commercial and industrial development is ecologically sustainable, energy efficient and of a high design standard.

## 4. STRUCTURE PLAN

The West Dapto Structure Plan (**Figure 2**) shows the land use setting which will house the future urban structure and guide the development of the release area.

It is characterised by a series of residential precincts estimated to generate around 19,500 dwellings. The precincts come together to form five distinct stages, separated by riparian corridors connecting through the release area from the Illawarra Escarpment framing the western extent to Lake Illawarra in the east. The release area will also integrate and protect heritage landscapes and items into the urban structure.

The West Dapto Structure Plan identifies:

- Town and village centres
- Conservation land
- Heritage items and potential curtilages
- Transition land (environmental constraints exist but may be appropriate for sympathetic developments)
- Development land
- Employment land
- Large open space facilities (neighbourhood parks 2-5 ha, and district parks 5-8 ha)
- Structural road network
- Creek lines and flood extents (1% AEP)

In the Wollongong LEP 2009, Stage 1–2, some of Stages 3 and 4, and all of Stage 5 of the release area are zoned for residential development (**Figure 3**). There is also 175 hectares of employment land zoned at Kembla Grange (**Figure 3**).

### Stages 1 and 2

Stages 1 and 2 are located in the northern extent of the release area (**Figure 3**) and have a number of unique features, including:

- Potential development of around 6,700 dwellings.
- Employment land (industrial zones) located within close proximity to Unanderra light industrial area and well connected to Dapto Regional Centre along the Princes Highway, the Port of Port Kembla and the M1 Motorway.
- South of the employment lands will be home to Darkes Town Centre with approximately 7,500m<sup>2</sup> of commercial/retail floor space providing for a range of shops and services as well as community facilities and active open space for community recreation.
- Protection and rehabilitation of riparian corridors and conservation areas (vegetation and heritage conservation depending on site features and opportunities) to improve water quality, recreational opportunities and connectivity of remnant vegetation along these structural spurs through the release area.
- Structural road network that will connect from outside the release area, through Stages 1 and 2 and into the southern reaches of the release area.
- Active transport facilities (cycleways and shared paths) connecting residential areas with open space provisions via riparian corridors and along the structural road network.
- Two village centres (Wongawilli and Jersey Farm) providing local convenience shops and urban focal points within the residential areas of Stage 2.
- Two primary schools, one located close to the Darkes Town Centre and one located in the Wongawilli and Jersey Farm Roads area to service the future residential families.
- Bong Bong Town Centre, at the southern extent of Stage 2 on the south side of Bong Bong Road will provide retail needs, local services and community facilities with employment opportunities in the local context. It will be the urban focal point supporting opportunity for denser housing products located convenient to public and active transport links.



### Stage 5 or Yallah-Marshall Mount Precinct

The vision for the Yallah-Marshall Mount Precinct is to create a vibrant, compact, environmentally sustainable atmosphere. The precinct will be concentrated around a compact, walkable town village centre reflecting low carbon footprint principles. The Yallah-Marshall Mount precinct will utilise traditional urban design principles, with relatively high densities around the town centre and concentrated along the main access roads. The precinct will have a diverse range of housing types and densities.

The bulk of higher density development will be focussed around the proposed town village centre, with opportunities for small lot housing along the main transport links through the precinct. The fringe areas will contain rural and rural-residential development. The aim is to have the new community focussed on transport links, rather than a 'blanket' of suburbia. The desire is to have a variety of housing types and styles to provide for a wide diversity in population, allow for increased "ageing in place" opportunities and make an interesting urban environment. The biodiversity corridors and Duck Creek will be significant attributes of the new community, with the escarpment as a visual backdrop.

Yallah-Marshall Mount precinct is characterised by:

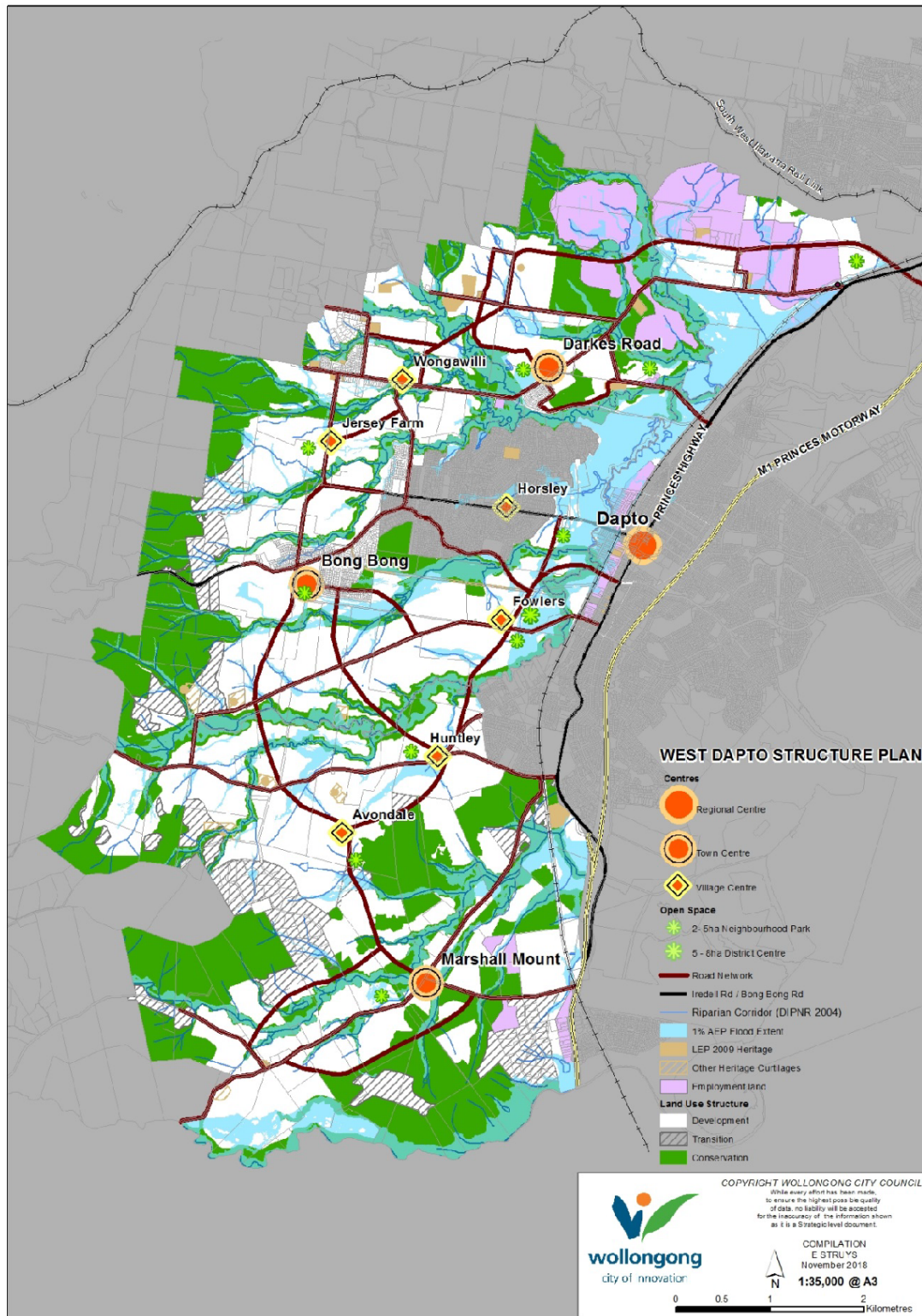
- Potential development of around 4,000 new dwellings.
- Marshall Mount Town Centre comprising approximately 8,000+m2 3,500m2 retail floor space. Refer to Section 14.6.8 for more Town Centre characteristic details. In a traditional main street format providing retail shops, local convenience needs, local services, community facilities and the like.
- Connection of the precinct into Avondale and Cleveland, with access via an extension of Yallah Road (Western Ring Road, formerly referred to as Road No. 8) as part of the overall West Dapto road network.
- Protection of significant vegetation and unique landscape features of the area.
- Utilisation of Duck Creek as a focal feature of the community.
- A primary school located near Marshall Mount Town Centre to meet the educational needs of the future residential families.

### Stages 3 and 4

Stages 3 and 4 are located in the existing rural suburbs of Cleveland and Avondale, in the middle of the release area south of Horsley. They are well connected to the Dapto Regional Centre to the east via Fowlers Road into Cleveland Road. Part of Stage 3, and a small part of Stage 4 have been rezoned. Stages 3 and 4 will ultimately include:

- Potential development of approximately 8,800 new dwellings.
- Community facilities including a district level recreational centre and youth services facility.
- Two primary schools to meet the educational needs of future residential families.
- Three well connected village centres (Fowlers, Huntley and Avondale) that will provide local convenience shops and urban focal points within the residential areas.
- Unique Mullet Creek Catchment environmental features providing the riparian corridor spur supporting and defining the surrounding urban form.
- Structural road network that will connect from the southern extent of Stage 2 at Bong Bong Town Centre down to the southern extent of Stage 4 and into Stage 5 of the release area. There will also be road connections spanning from Dapto Regional Centre into the release area along the east, branching into the village centres supporting surrounding residential development.
- Active transport facilities (cycleways and shared paths) connecting residential areas with open space provisions via riparian corridors and along the structural road network.
- High school and primary School facilities for the future population of the stages. Ideally the school will be located near the Bong Bong Town Centre (in Stage 2 or 3) to create a relationship with the town centre and provide education services for the future children and youth population residing between Stages 1, 2 and 3 of the release area.

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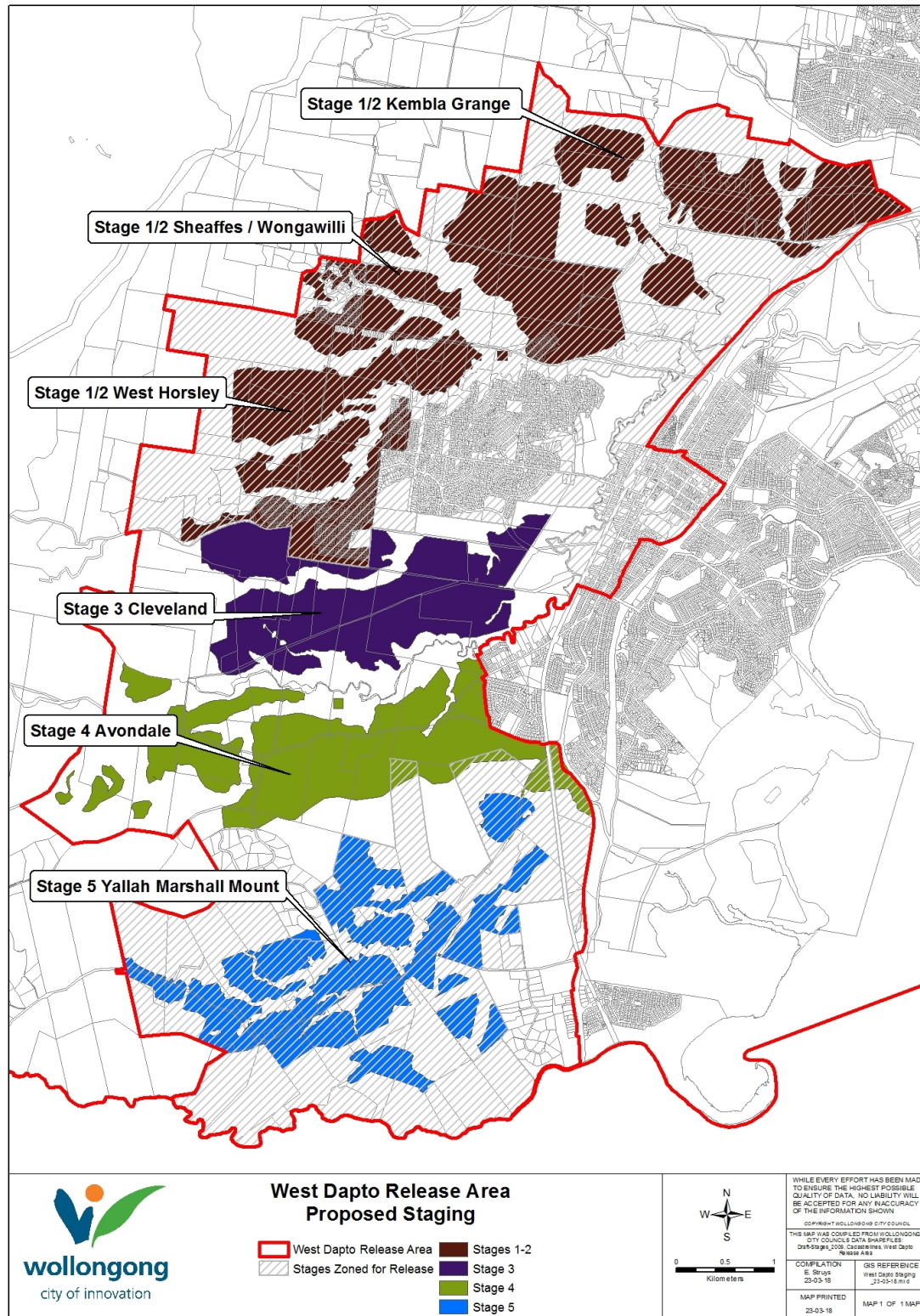


Figure 3. Stages of West Dapto Release Area

## 5. PLANNING PRINCIPLES

The West Dapto Planning Principles are intended to guide land use planning decision associated with the release area. They provide a statement of a desirable outcome for the development of the release areas and provide a basis of reasoning to support making planning decisions. Principles are important considerations when there may be more than one interpretation or contradictions between any qualitative requirements or development controls defined in other chapters of the DCP.

There are eight groups of principles originally outlined in the West Dapto Vision 2018. This chapter is structured in a similar way, building on principles with additional details. **Figure 4** outlines the key components and how they relate to Council's planning policies.

The groups of principles include:

- Transport
- Water management
- Conservation
- Open space
- Community facilities (and Education)
- Town centres
- Employment
- Housing



Figure 4. Principles and planning tools relationship



## 6. TRANSPORT

### 6.1 The Road Network

The future road network for West Dapto will be the backbone of the community, providing for all types of access and movement through the release area. The road network form and provision contribute significantly to achieving the vision of long-term sustainability.

An integrated transport system is proposed that caters for private cars, freight, public transport, pedestrians and cyclists. Road types have been developed based on a functional hierarchy, where the road designs support the transport modes in various ways. The road network has been developed to cater for future urban land uses and to deliver a safe, connected and legible transport framework that complements the natural environment and facilitates sustainable transport outcomes.

The Structure Plan (**Figure 2**) outlines the structural road network through the release area. The road network structure was modelled in TRACKS to understand the demand and supply requirements to service the release area. The modelling informed road typology requirements for the structural road network as shown in **Figure 5** and **Figure 6**. The road typology for the release area is informed by road hierarchy and cross sections which detail how the roads are configured for designs. Road hierarchy and cross sections are covered in DCP Chapter B2 Residential Subdivision.

In accordance with the following road network principles (specifically 2 and 3), the release area needs to be accessible in emergency situations. Flooding events present a specific challenge to urban development in a flood plain area and specific design response is needed provide safe and connected residential areas. **Figure 7** illustrates which structural roads or sections of road will be designed and constructed to provide 1% Annual Exceedance Probability (1% AEP) flood immunity.

#### Principle 1 - Supportive land use patterns

- 1.1 Plan higher residential densities and mixed land use in and adjacent to town and village centres and major public transport nodes, to reduce reliance on the private car and reduce overall road network requirements and costs.
- 1.2 Plan the co-location of compatible land uses to reduce private car reliance and reduce overall road network requirements and costs.

#### Principle 2 - A safe, connected and legible road network for all users

- 2.1 Provide a road network based on the modified grid layout to maximise accessibility and efficiency.
- 2.2 Implement a clear hierarchy of road types (see DCP Chapter B2 Residential Subdivision for road type details) that responds to relevant transport requirements and road function, creating a highly legible road network for all users (**Figure 5** Road typology and **Figure 7** Flood reliable roads).
- 2.3 Implement intersection designs appropriate to the road types **Figure 5**, surrounding land uses and environments.
- 2.4 Ensure the West Dapto structural road network supports the town and village centres hierarchy.
- 2.5 Ensure the integrated road system, caters for all road users including private cars, freight, public transport (buses), pedestrians and cyclists.
- 2.6 Implement driveway access restrictions and manage on-road parking on the higher-order roads (access-denied roads) to improve traffic efficiency and pedestrian/cyclist safety and amenity.
- 2.7 Ensure built form controls on adjacent properties to roads deliver active frontages to maximise passive surveillance and personal safety in the road environment. For example, road layouts that include lanes, service roads and so on to ensure houses front the primary road.
- 2.8 Ensure roads and intersections are designed to meet requirements of the DCP Chapter B2: Residential Subdivision, AustRoads and Australian Standards.

#### Principle 3 - Design roads to complement the environment

- 3.1 Ensure roads fit with the landform (topography), complement local character and land use, and minimise visual, ecological and noise impacts.
- 3.2 Ensure road alignments take advantage of views and visual stimuli for the motorist to enhance

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legibility, sense of place and create a positive experience in movement.

- 3.3 Consider the role of road networks in structuring precincts, including both transport and community needs to maximise liveability and quality urban outcomes.
- 3.4 Incorporate Water Sensitive Urban Design (WSUD) into transport infrastructure design and consider options to increase permeability of hard surfaces.

**Principle 4 - Quality infrastructure**

- 4.1 Use robust and durable materials, quality finishes and ancillary infrastructure, with neat, uncomplicated designs that minimise maintenance requirements and discourage vandalism.
- 4.2 Consider innovative technologies in road and transport infrastructure design, construction, and operation.

**Principle 5 - Road network to support sustainable transport outcomes**

- 5.1 Staging of additional car based infrastructure to encourage public and active transport and maximise the use of existing infrastructure.
- 5.2 Use an established 15% transport mode shift target when planning for road network requirements within West Dapto, to encourage a shift towards reduced car dependence.
- 5.3 Ensure that roads are designed to provide a high level of safety, access and amenity for pedestrians, cyclists and public transport (bus services).

## 6.2 Bridge and culvert design

Bridges and culverts form important structural components supporting the road network as it traverses the flood plain landscape. While there are design limits and prefabrication conditions the infrastructure are built to, it is also important that design decisions on materials, placements, modification to standards and any other specifics take into account desired outcomes for the areas they are in and who they will cater to.

**Principle 1 – Good design is context sensitive design**

Design that is sensitive to context is valued by communities. Bridges and culverts that are functional and fit the landscape are good for community pride and local identity.

- 1.1 Consider the influence of locational context and functional requirements in the design process. For example, if the crossing is traversing land that is zoned C2 or C3 and there are known ecological communities or fauna groups recorded there, fauna crossings should be a design component and construction must be sensitive to these outcomes.
- 1.2 Bridge and culvert alignment should integrate with environmental features.
- 1.3 Construction over or within waterways should have regard to the Fish Passage Guidelines developed by NSW Fisheries.
- 1.4 Ensure storm immunity standards are met, and design and construction provides longevity and minimises maintenance requirements.
- 1.5 Design finishes and overall appearance should respond to and incorporate character of the area.

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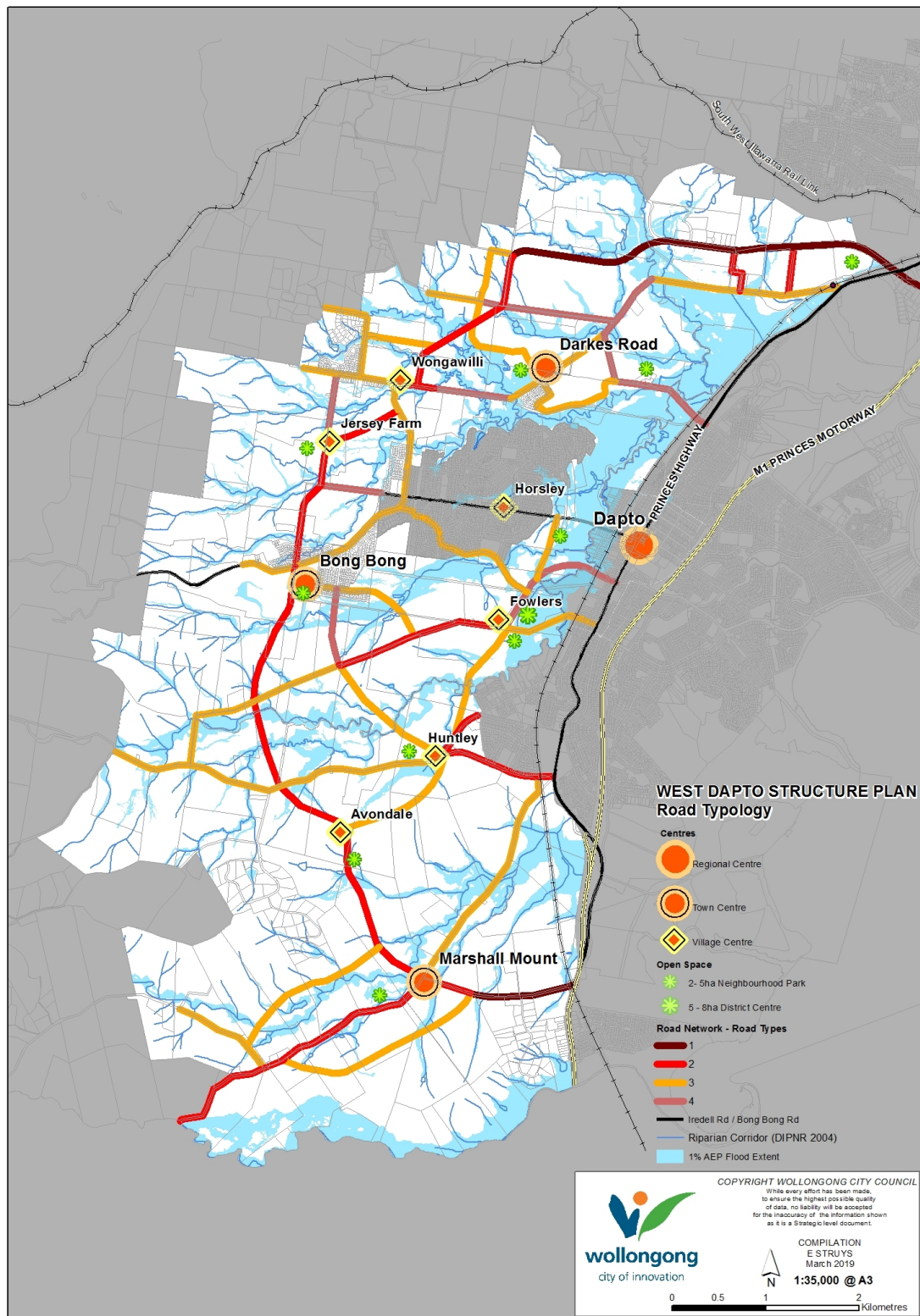


Figure 5. Road typology



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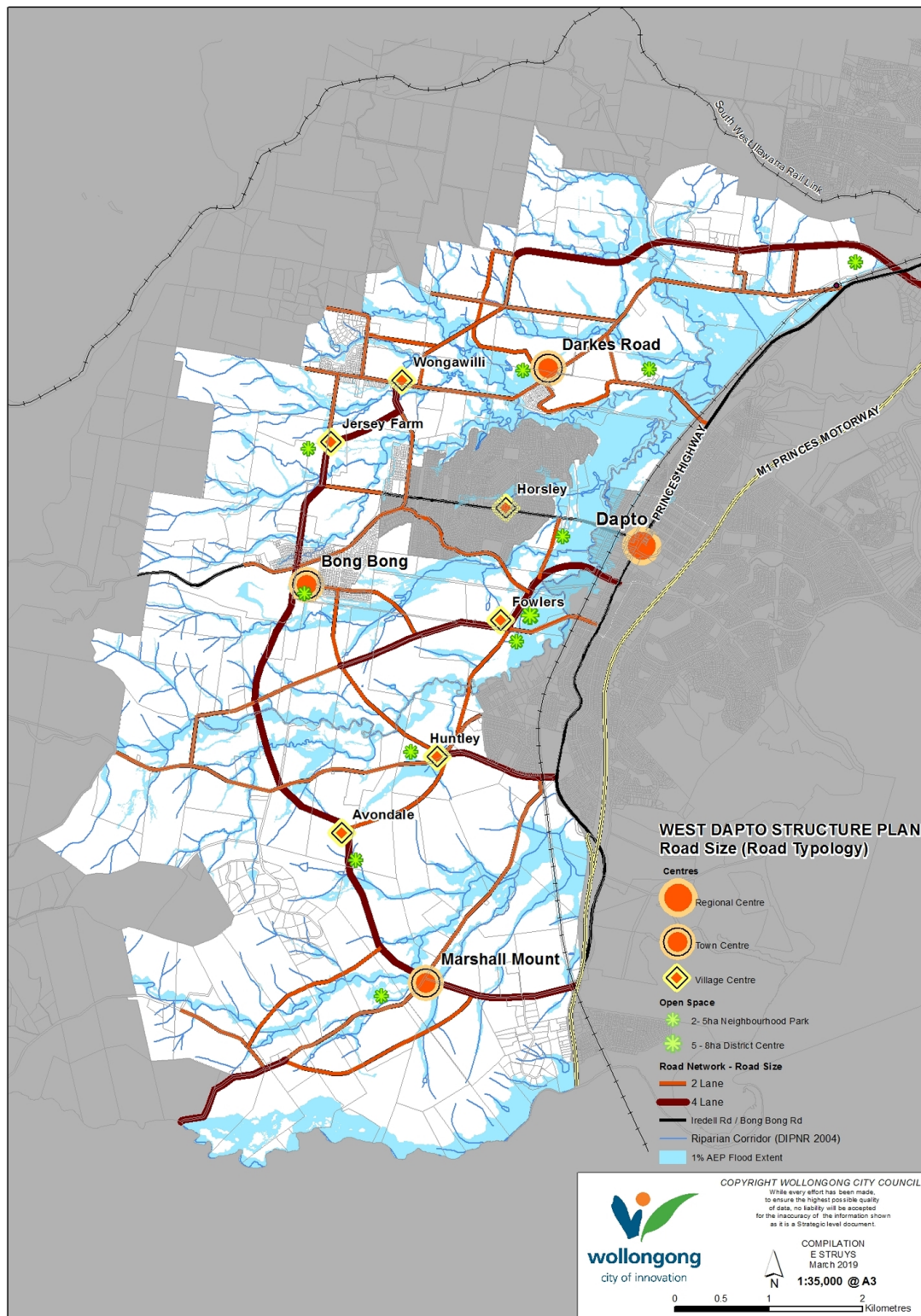


Figure 6. Road sizes



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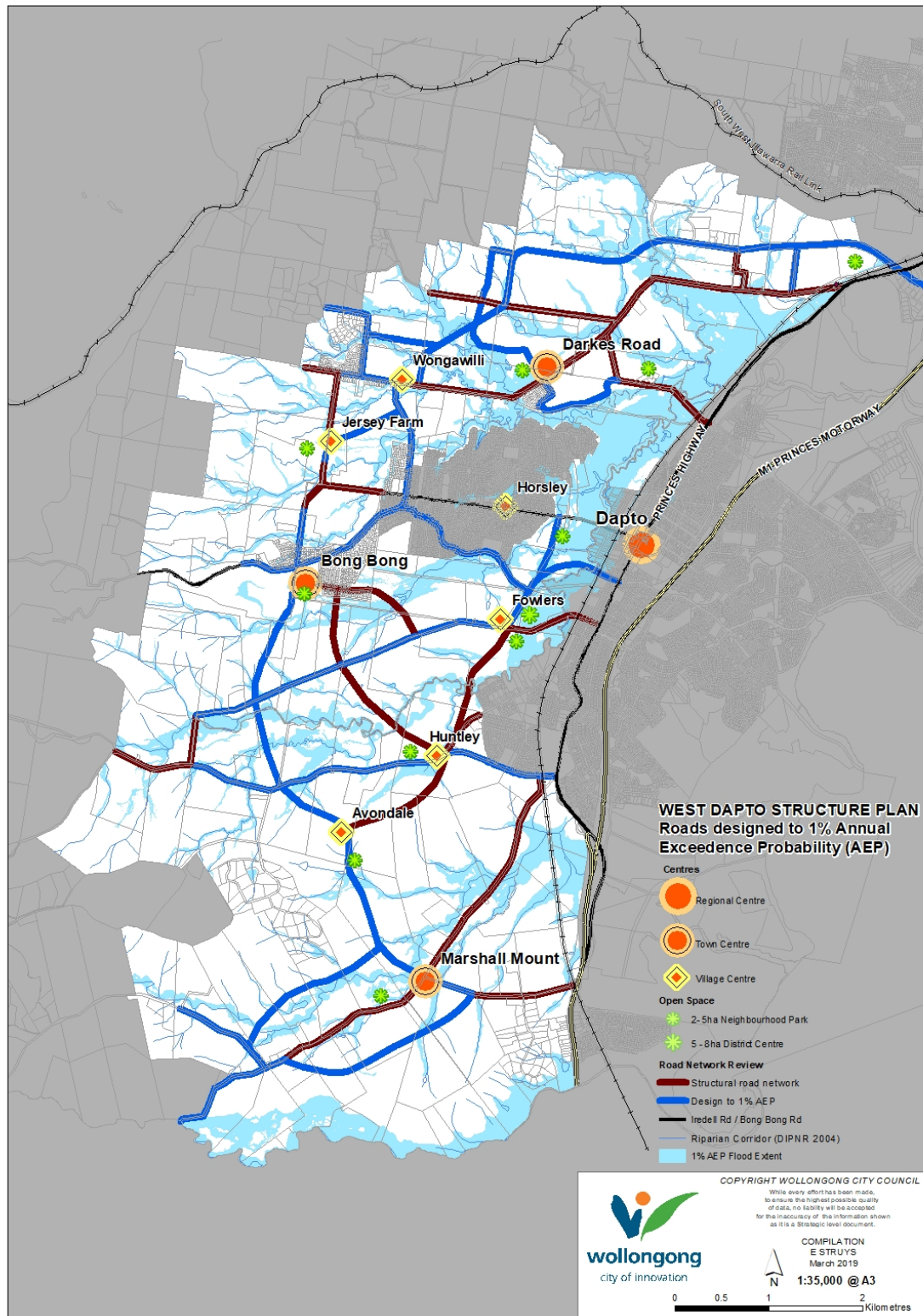


Figure 7. Flood reliable roads

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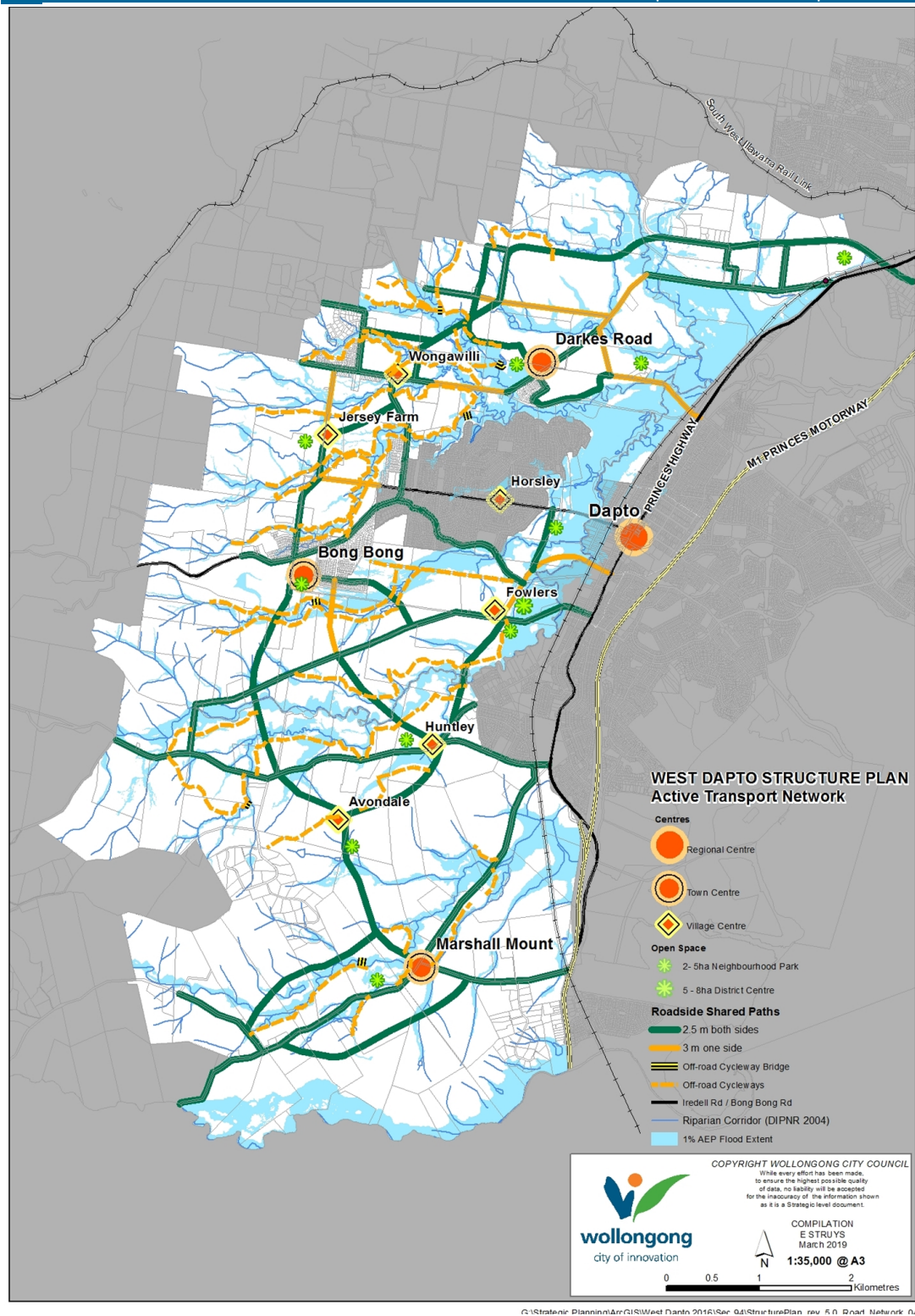


Figure 8. Active transport network

### 6.3 Active transport

Walking and cycling (referred to as Active Transport) will be an important component of the future West Dapto transport system, contributing significantly to achieving the vision of a sustainable community. The planning and design for West Dapto emphasises walkable communities which enables sustainable living. Walking is also an important factor in the success of public transport.

Active transport at the local level will deliver convenient and attractive travel options especially for short trips, which will not only assist in reducing the reliance on and impacts of private car use but will contribute to the health and resilience of the community.

The riparian corridors will be structural open space areas, to convey water and connect ecology, to promote walking and cycling with a series of pathway systems clearly linking key destinations such as schools from residential areas to promote walkability.

The Active Transport Network (**Figure 8**) identifies off-road cycleways, and active travel links on shared paths as part of the road network, connecting neighbourhoods and residential areas to parks and town centres (road cross sections include roads with shared paths as outlined in DCP Chapter B2 Residential Subdivision). Shared paths and cycleways should be located outside of core riparian areas, with selected cycleway bridges spanning riparian core land connecting key destinations through an open space network.

#### Principle 1 – Supportive land use patterns

- 1.1 Plan residential land uses close to town and village centres and major public transport nodes with higher residential densities adjacent to these locations to maximise walking and cycling catchments
- 1.2 Promote shared parking across uses in town and village centres to encourage walking when undertaking multiple activities in these centres. Avoiding fragmented parking will also improve utilisation of spaces and improve walkability through more compact town centre layouts and fewer driveway crossings.

#### Principle 2 – Connected, functional pedestrian and cycle network

- 2.1 Provide a convenient and legible movement network for pedestrians (including people living with disabilities) and cyclists, ensuring excellent connectivity and directness between residences and attractors such as schools, shops, public transport nodes, sports ovals, and employment centres.
- 2.2 Include footpaths or shared paths on all roads in the road types hierarchy except laneways and minor access streets (refer to Road Network Principles and DCP Chapter B2: Residential Subdivision).
- 2.3 Take advantage of easements, riparian areas and open space areas to create convenient pedestrian and cycle links (or “short-cuts”) that maximise accessibility between different precincts and land uses.
- 2.4 Implement a wayfinding strategy to provide clear and coordinated information for access to facilities and services within the West Dapto Release Area and surrounding areas.
- 2.5 Provide safe and secure bicycle parking or storage facilities at key destinations in town and village centres, sports ovals, community facilities, transport interchanges and key open space areas.
- 2.6 Include bicycle parking and end-of-trip facilities as part of the development of employment, business and commercial sites, particularly in town and village centres.
- 2.7 Ensure that the West Dapto cycleway network integrates with the wider surrounding regional cycle routes.

#### Principle 3 – Attractive and safe environment

- 3.1 Design streets to provide a high level of pedestrian and cyclist amenity and safety, creating public space where people want to be.
- 3.2 Provide convenient and safe road crossing points, traffic calming (where appropriate) and tree planting to enhance the pedestrian and cycle environment.
- 3.3 In high pedestrian demand areas such as town and village centres, further increase pedestrian amenity and safety through path widening, driveway access controls and other site-specific actions to improve pedestrian priority.
- 3.4 Incorporate Crime Prevention Through Environmental Design (CPTED) principles in the planning of walking and cycling facilities.



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- 3.5 Consider innovative technologies for lighting key off-road paths, including solar lighting and luminescent pathway materials, etc.
- 3.6 Construct pedestrian and cycle infrastructure according to AustRoads and Australian Standards, with attractive and durable materials and well-designed landscaping treatments.
- 3.7 Incorporate supporting infrastructure such as seats, bike rails, shade structures, bubblers and viewing or rest areas into the active transport network where appropriate.

As part of Council's commitment to the transport principles and active transport outcomes, additional initiatives will be explored that will help promote and encourage the take up of active transport in our community.

## 6.4 Public transport

The establishment of efficient and attractive public transport options for West Dapto is imperative to achieve sustainable growth outcomes. West Dapto Urban Release Area presents an opportunity to promote 'best practice' in public transport and non-motorised modes, reducing reliance on the private car, contributing to a mode shift target and creating a more resilient, interesting and liveable community.

These high level principles inform and guide public transport planning for the new growth area, to ultimately ensure that the vision for sustainable transport in West Dapto is achieved. This will also require partnerships beyond council, with public transport providers and Transport for NSW.

Neighbourhood Plans and development applications must demonstrate they have planned to facilitate public transport by responding to these principles at each level of development planning.

### Principle 1 – Accessible public transport

- 1.1 Major public transport nodes located in town and village centres where the greater residential densities and employment opportunities are centred.
- 1.2 Ensure that major generators of travel are well serviced by public transport.
- 1.3 Promote co-location of different destination assets around public transport nodes and in centres, to enable multiple trip purposes.

### Principle 2 – Effective bus network, service provision and integration

- 2.1 Provide coordinated, frequent and reliable bus services to destinations within and surrounding West Dapto.
- 2.2 Create an efficient, seamless travel experience through integrated ticketing, minimising transfer times, and intuitive and easily accessible service information.
- 2.3 Ensure street networks are interconnected and allow permeability for buses.
- 2.4 Ensure the bus network is highly accessible and services the majority of residences (with bus stops every 400m, see DCP B2 Residential Subdivision), town and village centres, employment areas, sporting facilities and Dapto Station.
- 2.5 Incorporate bus priority measures as necessary to ensure highly efficient, prioritised bus transport.

### Principle 3 – Quality infrastructure

- 3.1 Provide comfortable, attractive, safe, and secure buses and bus related infrastructure with clear timetable and service information which cater for all users including people living with a disability and the elderly.
- 3.2 Ensure pedestrian and cycle links to bus stops are of a high standard (refer also to Active Transport Principles).
- 3.3 Encourage the use of innovative and efficient public transport technology.



## 7. WATER MANAGEMENT

This section outlines the guiding principles, objectives, outcomes and development controls relating to Water Management across the West Dapto Release Area.

The approach behind 'water management' is to consider both floodplain and stormwater principles in an integrated way to achieve a better overall 'water management' strategy for the West Dapto Urban Release Area.

West Dapto is bisected by a series of watercourses that form part of the Mullet Creek and Duck Creek catchments. During heavy rain they can experience intense floods of short duration (rapid rise and fall of the creek levels). The residential areas of West Dapto will be designed to be above the 1% Annual Exceedance Probability (1% AEP) flood level and include provisions to protect future residents against flood risk.

### **Principle 1 - Integrate floodplain and stormwater management into the urban development process.**

#### Objectives

- Adopt a water management approach by integrating floodplain and stormwater management, which meets the needs for hydraulic capacity, managing floods and maintaining water quality.
- Develop an overall 'water management' strategy for the urban release area by integrating both stormwater and floodplain management strategies, to guide progressive development within West Dapto without causing adverse impacts to downstream areas by way of flooding or reduction in water quality.
- Manage stormwater runoff such that flood damage and adverse effects on both development and the natural environment is minimised.

#### Outcomes

- The creation of a water management strategy for West Dapto with consideration of but not limited to existing and new urban development, flooding, stormwater runoff, minimising impact of flooding and stormwater, water sensitive urban design, the environment, and water quality in receiving waters including Lake Illawarra.
- The successful implementation of a water management strategy for West Dapto.

### **Principle 2 - Improve the management of water quantity relating to urban development inclusive of stormwater, wastewater, water supply and recycled water.**

#### Objectives

- Maintain or minimise changes to natural hydrology of catchments which drain to waterways or neighbouring catchments.
- Manage stormwater runoff using a combination of at-source and regional systems rather than a single scale system where possible.
- Minimise stormwater runoff volumes.
- Incorporate Water Sensitive Urban Design principles in managing stormwater quantity.
- Mitigate potential stormwater impacts from future urban development.
- Reduce the probability and impact of downstream flooding to a level acceptable to the community.
- Manage stormwater discharge in a manner that minimises impacts on downstream receiving waters.
- Ensure that stormwater runoff is treated as a valuable resource and that its use for non-potable purposes is encouraged.
- Encourage stormwater reuse and harvesting.
- Reduce potable water consumption.

#### Outcomes

- Any retention/detention basins, if required, are strategically located within the neighbourhood, precinct and/or regional scale to attenuate flows to pre-development conditions for events between the 1 year and 100-year storm events.

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- Developments which use re-use water, infiltration, retention and/or detention strategies to limit the increase in runoff volume.
- Limiting the increase in stormwater runoff volume from urban development through the use of water sensitive urban design measures.
- Minimised impervious areas to 60% on individual lots to promote infiltration and reduce peak flows downstream.
- Grassed swales incorporated at the subdivision scale to promote infiltration and reduce peak flows downstream.
- Rainwater tanks utilised on a large scale on individual lots for house and garden reuse to reduce runoff volume and reliance on potable water supplies.
- The use of buffers such as landscaping, detention and retention structures between impervious surfaces and receiving waters.
- The use of landscaped features to direct runoff from impervious areas into vegetated areas.

**Principle 3 – Develop the floodplain and surrounding areas in a sustainable way.**

Objectives

- Develop strategies that will cater for progressive urban development within West Dapto without causing adverse impacts to downstream areas by way of flooding, increase in flow rates or reduction in water quality.
- Identify the extent of the floodplain based on post development flooding scenarios to enable key planning for sustainable urban development.
- Prevent the intensification of the use of floodways, watercourses and overland flow paths for residential/commercial/industrial development use.
- Design development layouts with consideration to the existing floodplain and natural landform.
- Promote multifunctional and appropriate land use of the floodplain.
- Address the potential impacts of climate change.
- Increase the public awareness of flooding within the West Dapto Urban Release Area and the existing urban catchment of Dapto.
- Ensure that flood fringe areas are sustainably managed.

Outcomes

- Urban developments in the release area are resilient to flooding in both the short and long term.
- Residential developable areas that are located outside of the 1% AEP flood extents and elevated using a freeboard of 500mm plus a predetermined climate change factor, based on a detailed catchment wide flood investigation for the post development (ultimate) flooding scenario.
- Urban developments are designed with minimal disturbance to the natural land form.
- Recreational open space areas which are located adjacent to riparian areas and/or within the natural floodplain storage areas.
- Development which has been controlled by specific guidelines to ensure sustainable development in the floodplain.
- Increased public awareness of the hazard and extent of land affected by all potential floods, including floods greater than the 1% AEP event and to ensure essential services and land uses are planned appropriately in recognition of all potential floods.
- No adverse impacts to downstream areas from either flooding or reduction in water quality.

**Principle 4 - Preserve the natural function of the floodplain, natural waterways and riparian corridors.**

Objectives

- Ensure that the natural function of the floodplain to convey and store floodwaters during flood events is preserved and enhanced where possible along with any associated flood dependant ecosystem.
- Prevent any filling and/or development within high flood risk areas.

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- Ensure no net increase in fill within the floodplain.
- Protect key creeks and riparian corridors from degradation and improve their environmental function where possible.
- Ensure that rehabilitation of key riparian corridors is consistent with the adopted 'water management' strategy for West Dapto and the DCP Chapter E23 Riparian Land Management.

Outcomes

- All residential, commercial and industrial development is located outside of the identified flood conveyance and flood storage areas.
- The revegetation of riparian corridors does not increase flood risk to the existing surrounding urban areas.
- Natural drainage paths and infiltration basins utilised as much as possible.
- Revegetation of key riparian areas is undertaken in accordance with the Riparian land management chapter in the Wollongong DCP.
- Waterways are protected by providing a vegetation buffer to urban development.
- Potential increase in developable land within the shallow floodplain (< 0.5m depth in a 1% AEP event and of low hydraulic hazard) by way of implementing a local cut/fill strategy only where compliance with all relevant floodplain management controls can be demonstrated.
- The natural functions of flood dependant ecosystems are preserved where possible.

**Principle 5 - Protect people and property from flooding in a strategic way.**

Objectives

- Minimise the risk to human life and property damage caused by flooding through appropriately locating urban development.
- Ensure flood risk and flood impacts to both existing and future development within West Dapto and surrounding catchment areas are minimised.
- Minimise the risk to human life by ensuring the provision of safe vehicular access and egress for residents and emergency services in times of flood.
- Develop practical floodplain and stormwater management solutions for future urban development and associated infrastructure within West Dapto.
- Locate residential urban development areas outside of the 1% AEP flood extents and elevate using a freeboard of 500mm plus a predetermined climate change factor, based on the post development (ultimate) flooding scenario.
- Design specific roads within the urban release area to achieve a 1% AEP flood event immunity including a pre-determined climate change factor or greater flood event.
- Ensure new development does not increase the flood risk to existing development areas.

Outcomes

- Specific guidelines which have been created to locate development within West Dapto without putting people and property at flood risk.
- Residential developable areas that are located outside of the 1% AEP flood extents and elevated using a freeboard of 500mm plus a predetermined climate change factor, based on a detailed catchment wide flood investigation for the post development (ultimate) flooding scenario.
- Floor levels for all residential habitable buildings are set at or above the post development flood planning level corresponding to the 1% AEP flood level plus 0.5m freeboard plus a pre-determined climate change factor.
- Specific roads (shown in **Figure 7**) are designed to connect urban development and provide safe vehicular flood access to higher ground in times of flood up to and including the 1% AEP event plus a pre-determined climate change factor, or where feasible the Probable Maximum Flood (PMF) event.
- Identification of potential flood risks to people and property in West Dapto through the undertaking of a detailed Floodplain Risk Management Study.

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- Sheltered refuge areas are incorporated into building designs, with the floor level of the refuge set above the PMF where applicable to protect occupants from extreme floods.

**Principle 6 - Protect water quality of surface and groundwater from urban development and avoid any adverse effects on water quality to downstream watercourses and Lake Illawarra.**

Objectives

- Enhance the long-term environmental protection of the receiving waters and Lake Illawarra.
- Manage stormwater quality using a combination of at-source and regional systems, rather than single scale systems where possible.
- Incorporate best practice Water Sensitive Urban Design (WSUD) and proven innovative solutions to ensure there is no adverse impact on water quality discharging from the site or to natural streams.
- Utilise higher stormwater quality targets through best practice stormwater treatment systems, as proposed by the stormwater risk management framework being developed for the Lake Illawarra catchment.
- Prioritise stormwater quality management strategies to meet load reduction targets for nitrogen, the limiting nutrient for water quality issues in the receiving waters.
- Manage stormwater in accordance with the Lake Illawarra Coastal Management Program.

Outcomes

- The use of appropriate WSUD measures both at the source of subdivision runoff and at a regional scale to minimise the water quality impacts downstream.
- The use of a treatment train approach including systems such as bio-retention, swales, wetlands and raingardens which exceed current stormwater quality targets.
- No reduction in water quality in Lake Illawarra related to stormwater arriving from the release area.
- A water quality monitoring system that monitors the effectiveness of stormwater treatment systems within the urban release area, the quality of water entering receiving waters and agreed systems and processes for addressing any inadequate water quality issues.
- Stormwater quality reduction targets are verified through focussed monitoring, evaluation, and reporting activities.
- The flood risk to existing development is not increased.

**Principle 7 - Integrate stormwater management into the natural and urban land form in an unobtrusive way.**

Objectives

- Manage the flow of stormwater from the urban release area using both natural and artificial drainage networks to a formal point of discharge.
- Integrate Water Sensitive Urban Design (WSUD) into roads, landscape, and open space only where practical to collect and treat runoff prior to discharge into receiving waters and Lake Illawarra.
- Identify, manage and enhance the function of existing watercourses and natural trunk drainage paths where possible.
- Make provision in the Neighbourhood Planning phase of the urban release area for adequate proportion of land that serves stormwater management functions.
- Minimise the use of 'hard engineered' stormwater infrastructure. If this type of infrastructure is necessary, avoid locating 'hard engineering' stormwater infrastructure within existing vegetation or riparian corridors where possible.
- Ensure stormwater systems are safely integrated with parks, conservation areas and riparian buffers in a visually appealing way to achieve quality environmental and social outcomes.
- Promote the community acceptance of places which integrate stormwater systems with the environment.

Outcomes

- A network of interconnected multi-functional drainage corridors within West Dapto which act as watercourses, floodways, flora and fauna habitat and water quality treatment areas.



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- Stormwater treatment systems which are integrated within public open spaces and streetscapes to enhance visual and public amenity.
- Online stormwater basins only where environmental impacts are minimised, and development benefits maximised.
- Artificial drainage infrastructure which has been designed and landscaped to mimic natural ponds and waterways, and also provides public amenity.
- Places which are safe, visually appealing and encourage active and passive uses by the community.
- Places that provide access to and awareness of the total stormwater system for the community.
- Native vegetation used within stormwater management infrastructure.
- Road corridors located above the 1% Annual Exceedance Probability (AEP) which have incorporated WSUD measures.

**Principle 8 - Provide efficient and sustainable stormwater infrastructure for the urban release area.**

Objectives

- Develop regional stormwater management solutions in combination with at-source based systems where possible.
- Ensure that the stormwater infrastructure is practical, cost effective and maintainable, with preference given to options achieving the maximum benefit-cost ratio over their lifecycle.
- Ensure stormwater infrastructure is designed to remain viable for the long term and under the widest range of probable climate futures.
- Ensure that lifetime maintenance costs are factored into decision making processes and strategies are in place to ensure adequate maintenance over the life of the system.
- Incorporate best practice stormwater management principles and strategies in developments, including monitoring regimes that can demonstrate the effectiveness of the system.
- Discourage interim stormwater management solutions unless it can be replaced with an ultimate strategic solution.
- Ensure that stormwater management systems applied within West Dapto achieve aesthetic, recreational, environmental and economic benefits and avoid introducing social risks;
- Achieve a uniform standard of stormwater drainage design for all urban developments.
- Increase public convenience and public safety as well as protection of property.
- Ensure stormwater infrastructure is designed with consideration to blockage and maintenance access.

Outcomes

- The overall number of stormwater systems is tailored for the neighbourhood and regional zones to detain or retain as much of the catchment runoff as possible.
- Infrastructure such as swales, basins, wetlands and gross pollutant traps which have been designed with consideration to maximum functionality and longevity, minimal construction and ongoing maintenance costs, infrequent maintenance periods and low potential for attracting mosquitos and algal blooms.
- Stormwater infrastructure such as trunk drainage and basins that are designed to fit within the existing topography, with minimal impact upon the environment.
- Reduced capital costs due to implementation of soft engineering treatments.
- Installation of stormwater infrastructure which has been designed with consideration to climate change in a practical, sustainable and cost-effective manner.
- Stormwater infrastructure designed and constructed with consideration to the ultimate strategic stormwater plan for West Dapto.

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**Principle 9 - Preserve the natural environment and enhance where possible in keeping with stormwater quantity and quality management objectives and targets.**

Objectives

- Protect and enhance the habitat value of the surrounding environment and downstream waterways, by controlling water quality and water quantity.
- Improve key riparian corridors and ensure the ecological values of the creek systems are enhanced without flooding impact on existing development.
- Protect and enhance where possible natural watercourses, riparian corridors and wetlands.
- Minimise the disturbance to the natural landform and existing vegetation.
- Reduce the impacts typically associated with urbanisation on receiving waterways and wetlands, including a reduction in streamflow erosion potential.
- Adopt the treatment of all watercourse corridors including widths according to DCP Chapter E23: Riparian Land Management.
- Maintain riparian connectivity of key category 1 watercourses by using piered deck structures where road crossings are proposed.
- Minimise the number of road crossings across category 2 watercourses to preserve riparian connectivity.
- Minimise the edge effects at the riparian corridor/urban interface by providing a suitable riparian corridor width and integrated transition at the urban and riparian interface (for example, perimeter roads with houses fronting, gentle batters if needed, otherwise avoid batters and retaining walls).
- Protect and rehabilitate existing waterways into 'living' waterways.
- Enhance urban areas by applying Council's 'Urban Greening Strategy'.
- Enhance the appeal of the natural environment to the community by introducing adjacent open spaces.

Outcomes

- Key watercourses within development neighbourhoods which have been enhanced with natural bed stability and sympathetic re-vegetation to minimise erosion and promote habitat without causing adverse impacts to surrounding urban development in times of flood.
- Watercourses protected by providing a buffer of natural vegetation to urban development.
- Urban development which has minimal disturbance to soils and vegetation by maintaining the natural landform.
- Waterways that are rehabilitated and provide fish habitat, pools and riffles and adequate riparian buffers in line with best practice.
- Appropriate monitoring systems in place to demonstrate the habitat value of downstream waterways is being protected including agreed systems and processes to manage stormwater quality and/or quantity if the habitat values are shown not to be protected.
- Community open space areas located adjacent to riparian buffers that provide a natural visual backdrop.
- Clear connectivity between riparian corridors, residential areas and roads by avoiding steep batters and retaining walls or opaque fences.
- Community access to selected waterways.

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**Principle 10 - Promote liveability and amenity for the community by using water in all environments.**

Objectives

- Promote the community acceptance of places which integrate stormwater systems with the environment.
- Protect and rehabilitate existing waterways into 'living' waterways.
- Locate communal open space adjacent to natural and artificial waterways.

Outcomes

- Places which are safe, visually appealing and encourage active passive use by the community.
- Places that provide access to and awareness of the total stormwater system for the community.
- Waterways that are rehabilitated and provide fish habitat, pools and riffles and adequate riparian buffers. in line with best practice.
- Safe community access to selected waterways.

**Other General requirements**

1. A water cycle management report is to be submitted with development applications for subdivision in accordance with the currently adopted Water Cycle Management Study and Floodplain Risk Management Study and Plan for the urban release area. The report must address water cycle management, water quality management, watercourse and corridor management, conservation and rehabilitation of aquatic habitat, and floodplain management.
2. Land that remains below the 1% AEP flood level for the post development flooding scenario as approved by the consent authority is not suitable for residential development. The post development flooding scenario refers to the ultimate development scenario inclusive of a fully developed catchment across the urban release area, riparian corridor enhancement and floodplain management works (e.g. basins).
3. Subdivision of land is not to create any additional flood affected residential allotments. A flood affected allotment is defined as being wholly or partly below the Flood Planning Level (FPL) (i.e. the 1% AEP flood level plus a freeboard of 500mm) or the Probable Maximum Flood (PMF) – whichever is the greater.
4. There is to be no net increase in fill within the floodplain.
5. There is to be no filling or development located within the high flood risk areas.
6. Compensatory excavation may be used to offset fill; however, the compensatory excavation must be taken from an adjacent area of similar flood function that is lower in the floodplain (i.e. at a lower AEP inundation extent) than the proposed fill areas. Cut and fill drawings and volume calculations must be supplied to Council.
7. Filling of individual sites within the floodplain in isolation without consideration of the cumulative effects is not permitted unless the floodplain risk management plan (FRMP) for the catchment has been adopted which allows filling to occur. Where no FRMP is applicable, any proposal to fill a site must be accompanied by an analysis of the effect on flood levels of similar filling of developable sites in the area.
8. Any development and/or filling proposed within the floodplain shall be commensurate with the land use, flood risk, flood hazard and hydraulic category.
9. Enhanced riparian corridors cannot be used to offset any floodplain storage in the flood modelling.
10. The minimum habitable floor level of buildings to be set at the post development flood planning level (FPL) - (i.e. the 1% AEP flood level based on the post development flooding scenario plus a freeboard of 500mm plus a predetermined climate change factor). *Note: the allowance for climate change is determined from the current and relevant Flood Risk Management Study and Plan.*
11. Subdivisions are to be designed according to Water Sensitive Urban Design principles. Development applications are to include a detailed statement indicating how the proposed design complies with these principles. Refer to Chapter E15: Water Sensitive Urban Design.
12. Detention basins created offline to watercourses are required as necessary where peak flows are predicted to increase. Consideration will be given to proposals for larger basins that serve multiple precincts and sub-catchments or other innovative design. The location of basins needs to be agreed to by adjoining land owners as part the Neighbourhood Plan. Where a basin is on an adjoining

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property, owner's consent and the creation of an easement is required.

13. Developments shall be demonstrated to have reliable access in a 1% AEP event to Council's designated flood reliable roads within the West Dapto Release Area.
14. Development Proposals shall consider flood events larger than the 1% AEP event.
15. The Lake Illawarra Risk Based Framework water quality targets shall be used as a minimum for all water quality modelling. Note: This general requirement is subject to the outcome of the Office of Environment and Heritage (now Department of Planning, Industry and Environment) Project: *Applying the OEH / EPA Risk Based Framework in the Lake Illawarra Catchment*.
16. Refer to Chapter E13: Floodplain Management and Chapter E14: Stormwater Management and Chapter E15: Water Sensitive Urban Design for additional controls relating to floodplain and stormwater management.



## 8. CONSERVATION PRINCIPLES

### 8.1 Environment conservation

In adopting the concept of ecologically sustainable development (ESD, see DCP Chapter A2), regionally significant releases, such as the West Dapto Urban Release Area, present opportunities to preserve remanent vegetation and enhance its ecological connectivity (structural and functional). This section identifies the strategic environmental priorities to guide planning and development of the West Dapto Release Area integrating conservation priorities with opportunity for a future West Dapto Biodiversity Conservation Strategy (BCS) and Biodiversity Conservation Strategy Structure Plan (BCSSP).

A BCS provides opportunity for Council to achieve biodiversity certification (bio certification) in a coordinated approach for the whole release area, improving the overall conservation outcomes beyond what would be achievable developing site by site. Council will continue to work closely with the NSW Department of Planning, Industry and Environment and the Environment, Energy and Science Group to achieve this strategic outcome. The principles should also be used to guide site by site considerations.

#### **Principle 1: Prioritise areas that offer high environmental value for conservation**

Consider information that identifies areas of threatened ecological communities or stands of habitat greater than 4ha (considered to present high environmental value in terms of habitat size and area) and avoid impacts as a result of land use changes to these areas.

#### **Principle 2: Connectivity of habitat areas**

Connecting patches of habitat that have high biodiversity value will provide opportunity for ecological migration over time as well as opportunity for improvement to habitat quality and values. These are more commonly known as biodiversity corridors providing strategic connection of larger and better condition patches of vegetation either by re-establishing continuous native vegetation cover in one or more strata over an alignment or designing stepping stones of habitat that traverse local corridors recognised in planning instruments and studies.

#### **Principle 3: Protect Environmental Values**

Community values of environmental function in a landscape are aided by planning and providing complementary land uses alongside conservation sites to assist in improving and protecting the ecological function of the site and enhancing its resilience.

Secure areas that present high environmental value as areas for conservation and long term management (ideally through a bio certification process).

Main development interfaces with the escarpment on the western edge of the release area are considered environmentally sensitive and zones reflect C2 Environmental Conservation. Environmental Conservation land will form a transitional development edge with lower densities of development adjacent to these areas. Increased opportunity for planting will be accommodated to complement the wooded slopes and riparian corridors.

Development interfaces with the predominantly west-east running riparian corridors which are where revegetation and ongoing management is required, or will be, zoned C3 Environmental Management.

Environmentally sensitive design and siting will be required for development in the C4 Environmental Living zone.

Refer to Chapters E17: Preservation and Management of Trees and Vegetation, E18: Native Biodiversity Impact Assessment and E23: Riparian Land Management.

### 8.2 Heritage conservation

Understanding and conserving the heritage values of the West Dapto presents an opportunity to enrich the social values of the release area and to promote cultural understanding of our shared heritage.

The Australian Heritage Commission (2000) states the aim of both natural and cultural heritage conservation is to retain the significance of place and in the case of West Dapto the natural and cultural heritage values are deeply entwined and cannot be separated. (Australian Government, Department of Environment and Energy, 2017). Impacts to heritage significance are a key consideration for development of the release area at each planning stage. Land use changes should retain, integrate and enhance heritage values. The principles for West Dapto aim to promote heritage conservation and meaningful consideration of the significance of place to ensure future development enhances the heritage values of West Dapto.

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**Principle 1: Prioritise the Conservation of Heritage Items and sites of Aboriginal Heritage Significance**

Local Heritage items listed in Schedule 5 of the Wollongong LEP 2009, sites of Aboriginal Cultural Heritage significance as well as areas of potential archaeological significance should be retained and conserved within new development areas and appropriate curtilages and visual settings established. Development planning should account for the significance of sites and places and their visual relationships to each other or key landforms and key sites that contribute to the historic setting or cultural significance of newly developing neighbourhoods should be retained.

**Principle 2: Respect the Cultural Landscape**

The West Dapto Urban Release Area has a rich and diverse history of Aboriginal and non-Aboriginal occupation. The area retains a range of key landscape elements, landforms, natural features such as creeks and ridgelines, important views and visual connections, and historic road and transport corridors that are important and unique aspects of the local area. The elements contribute to the character and significance of West Dapto through connection to Dreaming stories and by telling the stories of the area. Proposed development should be guided by an understanding of, and respect for significant features of the natural landform and historic setting. This will assist new communities in understanding and appreciating the unique visual and physical connections between places and features, within and outside of their development areas.

Views and Vistas

Generally, land in the release area around and above the 50-60m contours is considered of High Scenic Quality. Development within these areas must be sympathetic to that visual quality as the ability of the area to absorb change is low.

**Principle 3: Embed Local History and Character in New Communities**

Developments should strive to feature historic sites and places of significance within development areas to provide a unique sense of identity and character for developing neighbourhoods. The adaptation and re-use of historic buildings in an appropriate manner, conserving and integrating them into new developments, is encouraged. The retention and integration of significant Aboriginal sites as well as significant trees and landforms into natural area reserves, parks and as conservation areas is also encouraged. The use of historically relevant street names, integration of interpretation and the celebration of aspects of a site's Indigenous and post settlement history, are encouraged to ensure that the rich history of the area is celebrated and recognisable in the identity of developing communities.

**Other Requirements**

Developers must complete various **heritage studies** to understand the significance of Indigenous and European heritage sites and the potential impacts of the proposed development. These studies determine further conservation management requirements and approval needs.

Neighbourhoods will include visual character and cultural landscapes and ensure:

1. Design of subdivision patterns and road layouts are to have regard to the retention of view corridors and vistas through, and to, areas of high scenic quality.
2. Primary street planting is to be undertaken and established prior to the commencement of individual lot development or housing construction to minimise the visual impacts of proposed development.
3. A **visual impact assessment** is to be prepared by the applicant and submitted with any development application in areas of high scenic quality (at or above 50-60m contours). The visual impact assessment is to assess any potential impact to the visual quality and how the design will respond to this. The assessment will include recommendations for the development design. The development application will demonstrate how the visual quality of the visual catchment will be protected and incorporated through design responses.
4. An **Aboriginal Cultural Heritage Assessment Report (ACHAR)** is to be prepared for any proposed development where, or if:
  - the site has been identified having moderate to high archaeological potential or cultural significance.
  - an Aboriginal site or object has been recorded in the vicinity.
  - an area of potential archaeological deposit (PAD) has been identified through a Due Diligence Assessment or other study undertaken on the site.

The recommendations of the ACHAR should inform the development outcomes.

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Wollongong LEP 2009 identifies a number of heritage items within the West Dapto Release Area. In addition to the statutory controls contained under the LEP the Wollongong DCP 2009 contains requirements in relation to these items.

1. Refer to Chapter E10: Aboriginal Heritage and Clause 5.10 of the Wollongong LEP 2009 for specific controls relating to Aboriginal Heritage.
2. Refer to Chapter E11: Heritage Conservation, Clause 5.10 of the Wollongong LEP 2009, The NSW Heritage Act 1977 and The Burra Charter.

### 8.3 Riparian Corridors

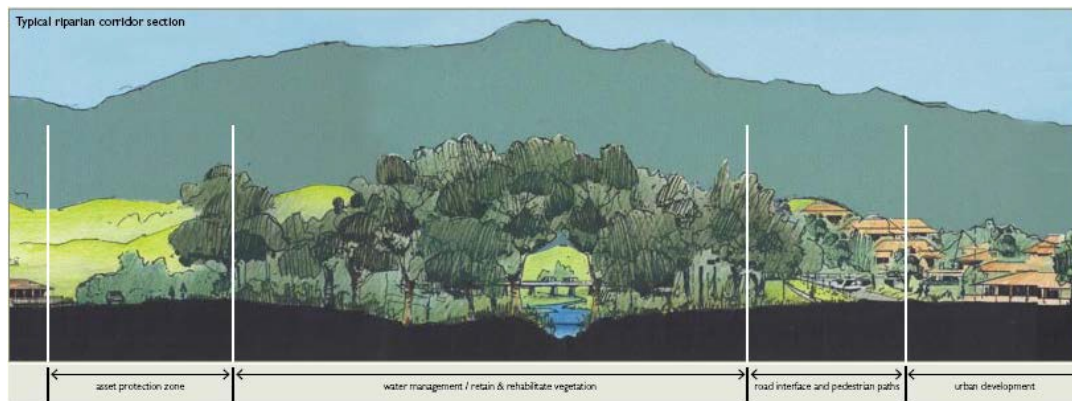


Figure 9. Typical riparian corridor cross section

West Dapto is dissected by fast flowing creeks and extensive areas of flood prone land, and riparian corridors. These areas offer an opportunity for recreation, visual separation and conservation. The corridors will result in significant amounts of open space creating wider landscapes within easy reach of all parts of the new development areas, meaning walking, cycling, recreation and nature will form a part of daily life. These riparian corridors have been, or will be, zoned for environmental protection. Limited development is allowed in these areas.

The riparian corridors will link the escarpment to Lake Illawarra through the release area. They will be vegetated with avenues of intensive planting and water management running through the urban street pattern to create a connected web of open space. This will encourage walking and create a sense of nature interacting with urbanity (see cross section in **Figure 9**).

Land between the watercourse and the 1% Annual Exceedance Probability flood extent can either be:

1. retained in private ownership and used for grazing, recreational activities or other permissible uses, or
2. dedicated at no cost to Council, for use as bushland, agricultural or recreational purposes. There are no development contributions off-set for the dedication or transfer of this land.

**Controls:**

1. Neighbourhood Plans will identify proposed land uses and ownership of the riparian land.
2. Riparian corridors are to be revegetated to instate a healthy ecological structure and function and to enhance resilience to flooding and improving water quality.
3. Refer to Chapter E23: Riparian Land Management for controls relating to riparian lands.

## 9. OPEN SPACE PRINCIPLES

This section establishes the open space principles for the West Dapto Urban Release Area. This section should also be read in conjunction with Community and Education Facilities principles and at a landscape structure level, will contribute to delivering against Council's Urban Greening Strategy. An overarching framework with four inter-related principles is designed to achieve the open space objectives for the West Dapto Urban Release Area. Open spaces need to be considered places that are designed responding to principles to ensure they provide for a resilient community.

There are more details regarding Council's specific requirements for each open space facility and subdivision requirements provided in the **West Dapto Open Space Design Manual and the West Dapto Open Space Technical Manual**.

### Principle 1: Functionality

- Open space needs to be of an appropriate size and flexible footprint for multiple functions and uses (Hierarchy of facilities).
- Open space and recreation outcomes are not compromised by other competing functional elements. For example, flooding and water management, traffic and road infrastructure, cultural heritage and biodiversity.

### Principle 2: Accessibility

- Walkable distances from residential areas, universal design principles used for facilities with a focus on 'play' and diverse experience (resident catchments).
- There is a well-distributed network of accessible (in both location and design), attractive and useable public open spaces and natural areas within the existing and future neighbourhoods of West Dapto.

### Principle 3: Connectivity, movement and flow

- Open space must be connected spaces with shared paths and trails linking to other facilities or places of interest, including centres, heritage sites (if not sensitive), riparian areas, natural areas, employment centres, transport nodes, community facilities and the like.
- The open space areas are highly connected to create a network of open spaces with a range of functions to complement the existing landscape features and provide opportunities for ecological connectivity.

### Principle 4: Value and amenity

- Future uses complement and add to existing values – for example, open space may present opportunities to preserve remanent vegetation or support the regrowth of bushland vegetation. Land uses must not have conflicting outcomes – for example, an active play facility may jeopardise a threatened ecological community, water management may restrict active use, etc.).
- That public open space and natural areas will provide opportunity for interaction filling a variety of recreational, sporting, play, and meeting the physical and social needs of the community.

### 9.1 Hierarchy and catchments

Based on the principles of functionality, accessibility, connectivity and values, there are some guides to the level of open space based on size and characteristics of projected population and its recreational needs categorises relevant population catchment distances for each level of open space provision (hierarchy) and how it generally relates to size requirements in the future urban and residential areas based on NSW Recreation and Open Space Planning Guidelines for Local Government (2010).

It is important to emphasise that any benchmark standards cannot be used as a 'one size fits all' assessment tool. Through analysis of local context and community needs, these standards can and should be varied if based on sound evidence.



**Table 1. Open space provision standards (based on NSW Recreation and Open Space Planning Guidelines for Local Government (2010) and the Elton Report (2007) recommendations).**

Function and service	Size	Catchment radius (distance)
Local Passive	0.5-2 ha	400-600m
Local Active	1-2 ha	400-600m
Neighbourhood Passive	2-4 ha	2km
Neighbourhood Active	3-5 ha	2km
District Active	5-8 ha	Southern ward of LGA
City wide Active	8 + ha	Facility to serve the whole LGA

Note: If stormwater infrastructure is proposed to be co-located with open space the general size requirements in **Table 1** should be considered with reference to Council's **West Dapto Open Space Design Manual**.

The relationship can also be understood in catchments for community populations. **Figure 10** shows proposed open space catchments of West Dapto (based on methods established in NSW Recreation and Open Space Planning Guidelines for Local Government, 2010). These catchments are indicative and are shown for illustration purposes only to guide how location of facilities will be planned to be located within a walkable catchment. In the release areas open space network, open space will need to be provided at all hierarchy and catchment levels.

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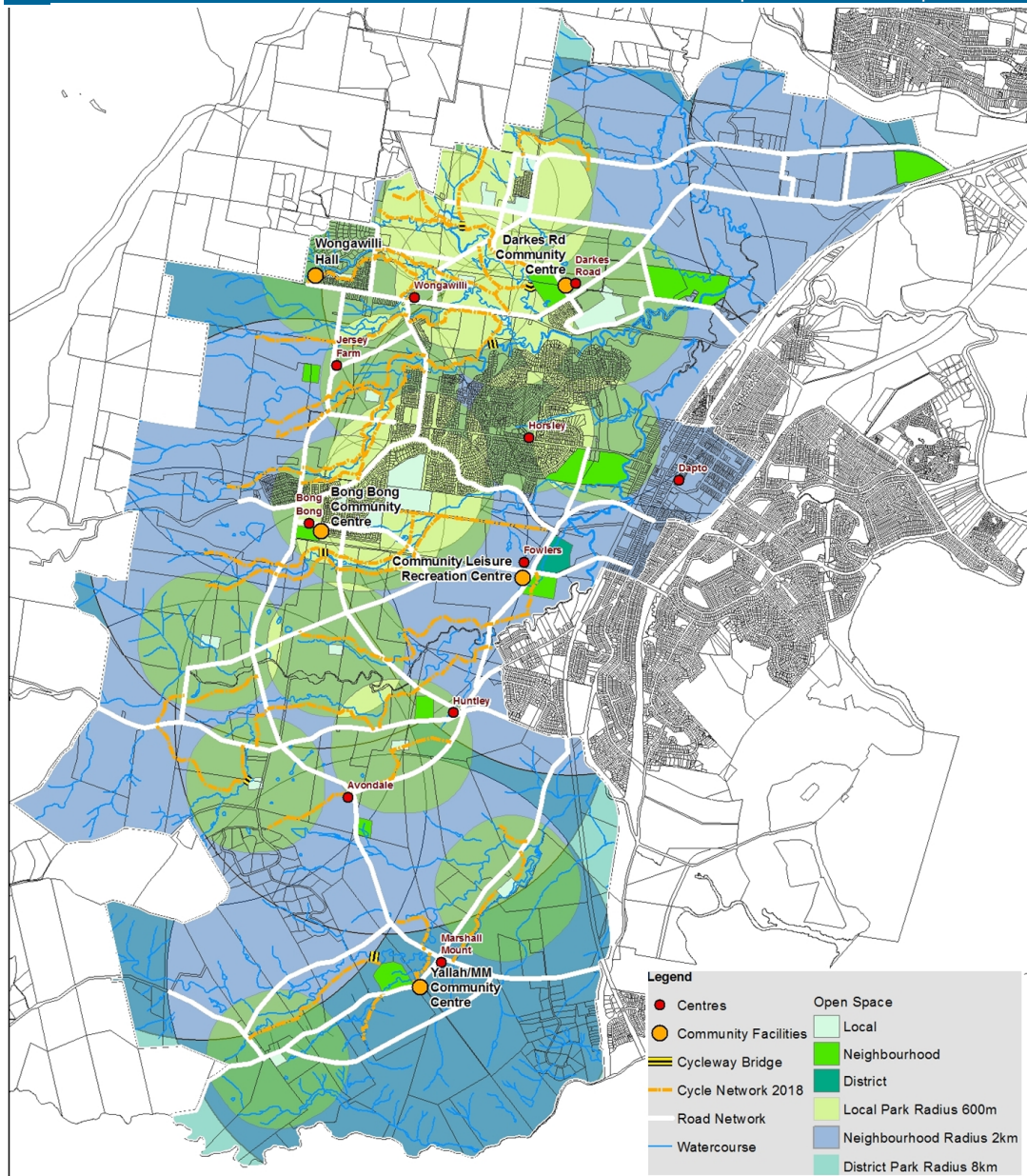


Figure 10. Planning for open space in West Dapto Urban Release Area

## 10. COMMUNITY AND EDUCATION PRINCIPLES

Community facilities are an increasingly important component of local service provision across a range of areas in the public and private sector. For example, there is a clear trend in public health and alternative education to use local community facilities for regular and specialist community services rather than develop individual facilities. Council understands this increases the importance of flexible design, location and efficiencies to be achieved by these facilities for them to make the best contribution to community outcomes.



**Figure 11. Community hub concept – co-location, joint use and multi-purpose centres**

### **Principle 1: Healthy, diverse and resilient**

Community facilities contribute to quality of life to support healthy, diverse and resilient community.

### **Principle 2: Efficient**

Making efficient use of resources through shared or co-located facilities and multiple use agreements (multi-purpose community hubs) with flexible design that can respond, expand and adapt as needs change.

### **Principle 3: Safety, security and adding to civic identity and sense of place**

Promote safety, security and provide focal points adding to civic identity and sense of place through clustering community facilities.

### **Principle 4: Self-sufficient and resilient community**

Community facilities provide opportunity for self-sufficiency to build capacity and social capital and to actively contribute to community resilience.

### **Principle 5: Vibrant and accessible**

Placing facilities in convenient, central locations, adjacent to open space promotes access and contributes to the vibrancy of the development and allows for overflow activities such as children's play.

### **Principle 6: Equitable**

Provide equitable access for all sections of the population, through the distribution, design and policies of facilities.

### **Principle 7: Diversity**

Community facilities promote diversity and encourage people from culturally and linguistically diverse backgrounds to participate in the social and economic life of the community.

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**Principle 8: Viable and sustainable**

Developing sustainable funding, ownership, governance, management and maintenance arrangements, including private partnership arrangements where community benefit is achieved.

**Principle 9: Coordination**

Council will work with the State Government and non-government schools sector to promote best-practice education outcomes for the community of West Dapto. This will include sharing data and integrating asset solutions, such as opportunity for shared and joint-use facilities.

Planning for the provision of education is important for West Dapto's growing community. In NSW, the Department of Education provides funds and regulates education services for NSW students from early childhood to secondary school. The Department of Education provided previous support for the six primary schools and two high schools based on the projected housing provision and related future population estimates.

**Figure 12** shows some indicative school locations within the release area.

The distribution of schools in the release area ideally would be spread across the residential areas of the five stages. There would be two new primary schools in the vicinities of Darkes Road/West Dapto Road and Wongawilli Village.

In Stage 3 of the release area, a new secondary school close to Bong Bong Town Centre will be supported by three primary schools; potentially Jersey Farm - Robins Creek (Bong Bong Primary School on **Figure 12**, Bong Bong/Cleveland (Stage 3 Primary School on **Figure 12**) and Avondale/Mooreland (Stage 4 Primary School in **Figure 12**).

A third secondary school potentially located in Calderwood Urban Release Area has been approved and will service a primary school in Marshall Mount area (shown in **Figure 12** as Yallah Primary School) as well as the required primary schools in Calderwood.

A special needs school should be either co-located with or separate to a mainstream school in the release area.



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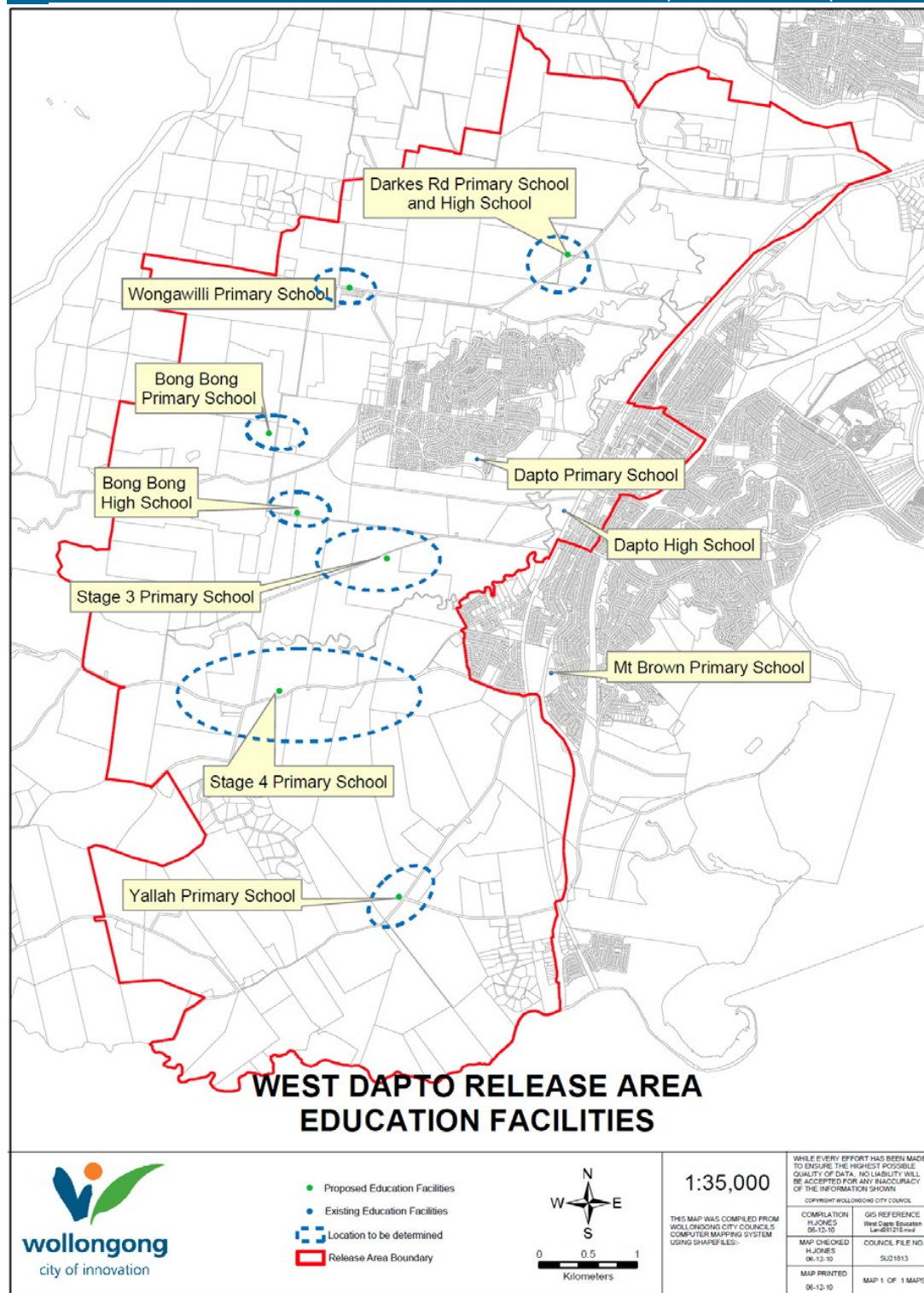


Figure 12. Potential school locations (Council to work with NSW Department of Education)

## 11. TOWN CENTRE PRINCIPLES

As a new release area, there is an opportunity to ensure that the ideal treatments are made to establish sustainable, appealing and functional residential living as well as commercial and light industrial areas providing employment, social and cultural opportunities with sufficient flexibility to cater for the future populations needs. As with other previous principles the town centre principles should not be considered in isolation.

The town centres of West Dapto will fill diverse roles, functions and mixed uses. The key objective of town centre principles is to help identify centre locations, function and existence. Configurations will reflect the town centre hierarchy with a focus on pedestrian priority. Supported with a decision process (zoning, neighbourhood planning, etc.), appropriate locations will promote the social and economic functions and outcomes sympathetic to character and 'place'.

There are three principles, Hierarchy (**Figure 13**), Movement sensitive and Identity and diversity, each to be considered in planning of town centres to meet the objectives for West Dapto Release Area.

Council also expects the town and village centres of West Dapto to be:

1. Master planned with the plan responding to the release areas planning principles.
2. Subdivision design modelled for walkability with plans that demonstrate public and employment base have easy access to active and public transport.

### Principle 1: Hierarchy

Hierarchy provides a basis for which to establish functions, order, and visions as well as allowing the protection of these. Hierarchy is not the only way to understand or to set direction in planning for centres, we understand that the centres are also a connected network, which can support each other in an interlocking way.

Each level of the hierarchy represents the size and general characteristics of the centres commercial, retail and business roles (see **Figure 13**). The Hierarchy reinforces role and function, supports the Wollongong City Centre and higher order centres and provides certainty for investment decisions. Hierarchy reinforces character and identity as well as provides direction around appropriate residential density sympathetic to community facilities and service locations.

Requirements for development in Business zones must comply with contents of the DCP Chapter B4: Development in Centres and Peripheral Sales Precincts. Chapter B4 outlines the Hierarchy for the LGA as well as other studies or assessments needed to support development applications.

#### Regional Centres

Wollongong Local Government Area has two major regional centres Warrawong and Dapto. It forms and important commercial and business centre role in Wollongong LGA.

A series of new centres will be located close to Dapto's existing urban landscape. Supporting Dapto's development as a regional centre will be an important consideration in the planning of new town and village centres. These lower order centres must be sensitive to the levels of hierarchy to maintain existing functions and minimising any negative impact on the hierarchy.

#### Town Centres

There are three town centres planned for the urban release area. The objective of the town centres is to ensure that development in the Darkes Road (1), Bong Bong (2) and Marshall Mount (3) town centres contribute to the creation of retail, business, commercial, and community hubs that act as public transport nodes and provide significant local employment opportunities.

Major town centres (~15,000m<sup>2</sup>) are planned within the central western (Bong Bong Road) and the southern (Marshall Mount) parts of the release area. A local town centre is planned in the northern (Darkes Road) area. These are intended to create local retail, business, commercial and community hubs providing significant local employment opportunities. They will need to complement rather than compete with the higher order major regional centre of Dapto.

Council expects the town centres of West Dapto to be master planned demonstrating how the plan responds to these planning principles. Section 14.6 outlines the West Dapto Centres Master Plans that have been adopted into the DCP for the purposes of this Part. Full West Dapto Centres Master Plan packages are available on Council's website (West Dapto Project Page)

#### Village Centres

The West Dapto further review of release area centres and controls (Urbacity, 2014) noted the role of  
[Draft Wollongong Development Control Plan 2009 – June 2024](#)

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Villages, as a lower order centre, is to “provide convenient alternative to the supermarket based town centres for daily goods and services with a focus on amenity for housing density and improved public transport use”.

Village and local centres will develop localised business opportunities at key places and intersections where bus stops, community facilities and local open space come together to create an urban focal point for the local community. Small villages are proposed ~2,500 m2 of floor space and accommodate a 1,000-1,500m2 supermarket and variety shops.

Council expects the village centres of West Dapto to be master planned demonstrating how the plan responds to these planning principles. Section 14.6 outlines the West Dapto Centres Master Plans that have been adopted into the DCP for the purposes of this Part. Full West Dapto Centres Master Plan packages are available on Council's website (West Dapto Project Page).

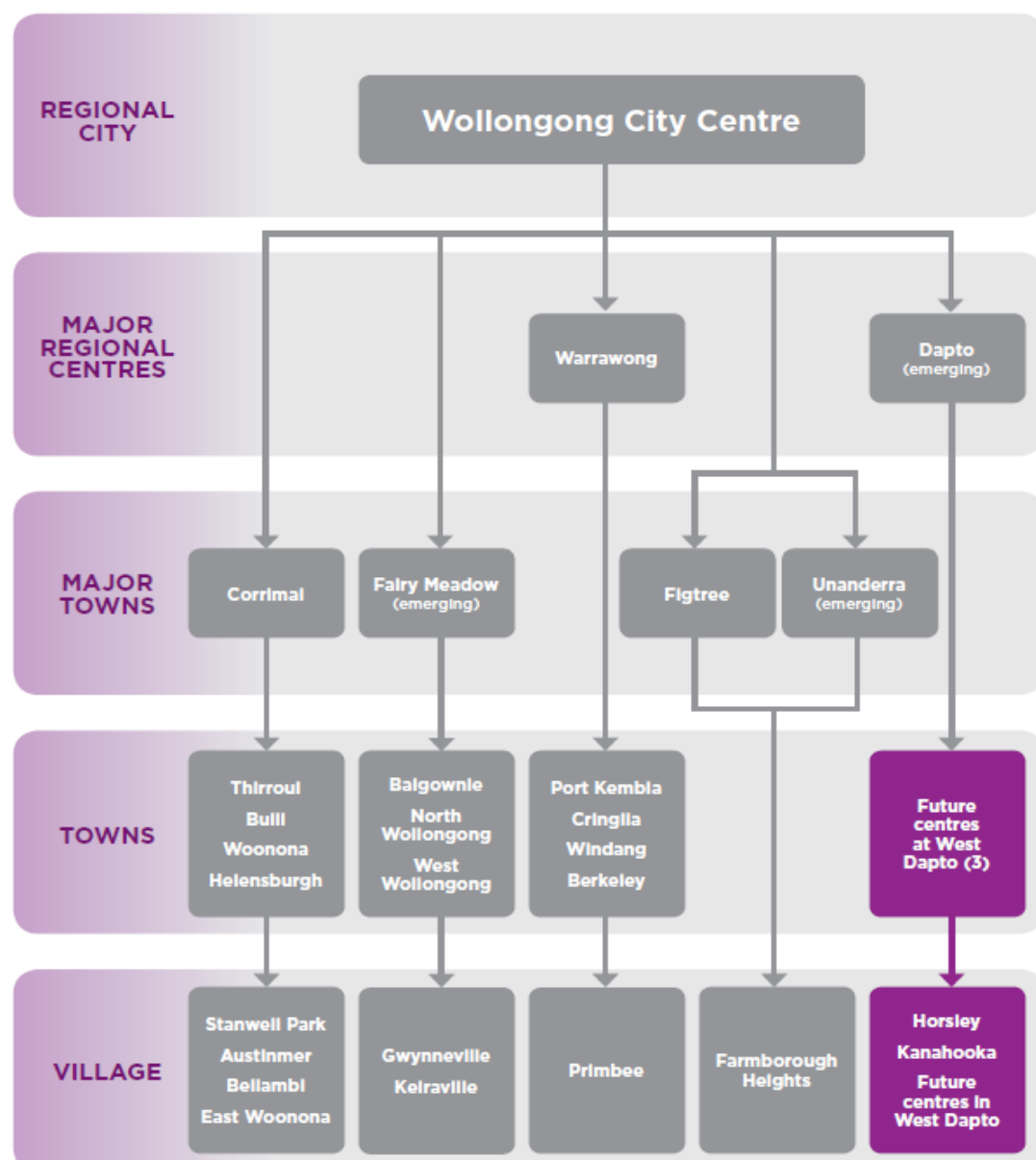


Figure 13. Wollongong Town Centres Hierarchy

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**Principle 2: Movement sensitive**

The town centres of West Dapto are expected to facilitate social contact, employment, and living needs in a sustainable manner. That is, the town centres will be located to promote active transport, public transport and healthy lifestyles. Living within 400-800m of a mix of destinations is consistently associated with higher levels of active transport in adults and older adults (Heart Foundation, 2017).

Movement sensitive means movement (accessibility, location, etc.) will be a key consideration for co-location of a mix of destinations (or land uses) within a centre. Centres will provide a location for activity, attraction, service for people to walk or cycle to. A focal point and community hub and transport node within the neighbourhood that allows for multiple activities to be undertaken and different daily needs (i.e. live, work, play) to be met in the one location.

Centres must also be supported and surrounded by a network of connected streets, paths and cycle ways, providing and promoting opportunities for active transport, and convenient access to public transport rather than private vehicles. The network will link open space works with Open Space and Recreation principles.

Neighbourhood Plans must consider their interface with adjoining areas and their ability to develop. The Plans must consider how different land use parcels such as centres are linked by the road network and pedestrian / cycle paths within and between different residential neighbourhoods.

**Principle 3: Diversity and identity**

Centres are vital to the social fabric of a neighbourhood and will facilitate a diverse range of activities by prioritising places and spaces where people of all ages can gather, meet friends and family and engage in social activities.

A vision that encourages diversity and shapes and reflects a centre's character is especially important for new centres. Centres will be diverse from each other (through hierarchy, features and visions). The vision can be understood as capitalising on existing features of heritage, environment (vegetation, topography, etc.) contributing to a new theme expressing the centre's role in the new urban residential landscape. In other words, a vision and purpose for people to create from, understanding that activity, physical setting and meaning come together to create a 'sense of place' framed by the built forms that provide a variety of building types.

The town centres will have a variety of building typologies with urban characteristics such as increased height, minimal or zero street setbacks and street level awnings and verandahs. The public domain is intended to reflect an urban character, with high quality hard and soft landscaping and paved footpaths with advanced planting of shade trees. Parking will be at the rear of blocks and underground as well as good on street provision of kerbside parking – building setbacks to accommodate front parking lots will not be permissible, as these detract from the street qualities sought in these centres.

**Refer to Section 14.6 West Dapto Centres Master Plans for specific centre intended characteristics.**

Other Chapters of Wollongong DCP 2009 containing development controls relating to the developments within the town and village centres include:

1. Chapter B3: Mixed Use Development for specific controls relating mixed use developments.
2. Chapter B4: Development in Centres and Peripheral Sales Precincts for specific controls relating to business and town centre developments.

**11.1 Town centre development controls**

Development in the West Dapto Town Centres is to comply with the following development controls, **unless otherwise specified in Section 14.6 West Dapto Centres Master Plans:**

1. Establish a strong urban form that clearly distinguishes the centre or local node from surrounding areas.
2. Taller buildings of 4-6 storeys are encouraged in the town centre core. Lower scale buildings up to 3 storeys in height should surround and support the core.
3. The street wall height should have a 2-3 storey building form.
4. Zero building setbacks on main streets. Other streets are generally to have a setback of between 0 – 2.5m.



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5. Side and rear building setbacks are as follows:

Setback	Distance
Side	Zero
Rear	Zero where lot adjoins allotment zoned E1 Local Centre or 5-6m where lot adjoins allotment with any residential zone.

6. Civic public spaces designed to encourage social interaction with paved areas, outdoor seating and urban green spaces are encouraged to balance the indoor building provisions.
7. Shared parking facilities are encouraged, accessed from laneways of minor streets. Parking lots and parking areas should not be visible from the streets, allowing the built form to define the urban function.

## 11.2 Village centre development controls

Development in the West Dapto Village Centres is to comply with the following development controls, unless otherwise specified in Section 14.6 West Dapto Centres Master Plans:

- Building setbacks can be either street aligned (zero) or setback up to 5m to create commercial forecourts or residential courts to the street.
- Varied building setbacks are encouraged to create an informal organic character.
- Building heights of up to 2 storeys are encouraged to create an urban village character with upper floor uses including small scale commercial and residential developments.
- Parking to be provided at the rear of buildings in the form of rear laneways and parking areas accessed from the rear laneways / car courts.
- All shops should address and be entered from the major street where possible, or the secondary street.
- Community congregation areas to be north facing and take advantage of escarpment views where possible.
- Street parking to be maximised around villages.
- Parking lots and parking areas are generally not to be visible from the main collector roads, allowing the built form to define the urban function.

## 12. EMPLOYMENT PRINCIPLES

Five of the Seven Employment Principles in the West Dapto Vision 2018 intend to guide the development and creation of sustainable employment outcomes for the West Dapto Urban Release Area.

The creation of employment opportunities within and near West Dapto allows people to work close to where they live and can reduce the overall traffic generated by the development.

The Structure Plan (**Figure 2**) shows the main employment area, in the northern end of the release area at Kembla Grange, in purple. The land is mostly zoned for light and heavy industrial land uses. There are some limited areas of light industrial land arranged in an enterprise corridor north and south of the Dapto Regional Centre, and some west of the M1 and along Yallah Road, Yallah to provide additional local employment opportunities.

### Other Considerations

1. Wollongong Economic Development Strategy and Implementation Plans, and Advantage Wollongong, Invest Wollongong.
2. Chapter B5: Industrial Development for controls relating to development on industrial lands.

### Principle 1: Support local sustainable and accessible employment

- 1.1 Support a variety of employment opportunities accessible to the whole community.
- 1.2 Employment containment to reduce commuting out of the release area and region.
- 1.3 Local access to higher order (career generating) employment opportunities.
- 1.4 Encourage high density employment opportunities within walking distance of existing or proposed public transport services.
- 1.5 Encourage employment area developments adjoining the structural road network to take advantage of accessibility and exposure.

**Principle 2: Attract, facilitate and support industries, enterprises and business to locate in West Dapto** (this principle is supported by Council, Business Chambers and other organisations as required).

### Principle 3: Ensure Town and Village centre employment outcomes are prioritised

- 3.1 Town and village centres are to ensure planning decisions (such as master plans and spatial arrangements) support and prioritise employment outcomes.
- 3.2 Encourage professional service type jobs and roles that are beyond the normal retail type jobs normally expected in new urban release areas.

### Principle 4: Protect existing employment land

- 4.1 Maintain existing zoned employment land within the release area to ensure a supply is maintained over time and is available for employment generating opportunities.
- 4.2 Create a strategy to enable appropriate interim uses of employment areas that also allows for gradual intensification over time.
- 4.3 Support the preservation of large lots and clusters of light and heavy industrial land and ensure business parks are not located within light industrial zones.

### Principle 5: Take advantage of and encourage employment innovations

- 5.1 Planning decisions to anticipate, be responsive to and cater for innovative employment solutions.

### Principle 6: Improve employment opportunities and ensure a high standard

- 6.1 Ensure developments are considerate of their context and are compatible with residential and sensitive land uses as well as conservation outcomes of the urban release area.
- 6.2 Apply a merit-based approach when assessing employment generating activities.
- 6.3 Encourage development for employment which provides a range of goods and services without adversely affecting the amenity, health or safety of any adjoining area.

## 13. HOUSING PRINCIPLES

Any specific controls for Neighbourhood Plans must consider the ability to develop adjoining areas including linkages to those areas.

The Housing Principles should be read in conjunction with Council's LGA-wide Housing Policy setting, which is updated from time to time to ensure we are addressing the challenges of a changing housing environment.

These principles should be considered in conjunction with all other planning principles as they all contribute to achieving the vision for the West Dapto Release Area and ultimately sustainable housing outcomes.

*"The communities will be healthy, sustainable and resilient and will have access to diverse housing choice and active or passive open space accessible by walkways, cycle ways and public transport."*

### **Urban Residential Density Distribution**

The intention for West Dapto's urban structure is to provide varying housing densities with increased densities located around town and village centres, and community hubs. Targeting the delivery of medium residential densities in the release area encourages population diversity, makes public transport more viable and supports the sustainability of the town and village centres. A range of housing types are to be provided to ensure that the housing needs of all household types are met. A diverse demographic profile will help ensure a sustainable and vibrant community in the long term.

The areas of lower residential density (R2 Low Density Residential zone) should provide an average of 13 dwellings per hectare and then in later stages 15 dwellings per hectare. In the more sensitive areas, such as the "transition" areas shown on the structure plan (**Figure 2**). Council proposes densities around 5 to 10 dwellings per hectare to enable the protection of environmental values and minimise visual impacts. The areas of medium residential density (R3 Medium Density Residential zone) should provide an average of 20 to 25 dwellings per hectare. Density measures such as Gross Density help inform and set targets at a precinct level (based on Landcom, 2011, Residential Density guide and supporting charts, (**Figure 15**). Net density (**Figure 14**) will show if the desired mix and ultimate (finished development) housing is being achieved. These are not site by site or zone controls as the aim is for diversity. They help to inform infrastructure planning, and to understand the intensity of built forms and population.

### **Principle 1: Encourage housing diversity**

Diversity can be delivered through different products at different stages of planning by promoting and providing a range of density and lot size and shapes to offer a range of choice to better meet changing community needs.

Mixture of density low to high, single dwellings, dual occupancy, town houses and apartments in appropriate locations should all be considered in neighbourhood planning and subdivision design stages.

Promote increased densities and innovative design types close to town and village centres and transport infrastructure where possible.

A variety of lot sizes and dimensions must be provided to achieve diversity in products to suit a range of household structures and to meet the density targets relating to the residential zones (Refer to **Figure 14**).

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**Reference chart | Residential density and planning controls**

TYPICAL FSR & LOT AREAS FOR HOUSE TYPES



**Figure 14. Net Residential Density Chart (Landcom, 2011).**



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Reference chart | Gross residential density

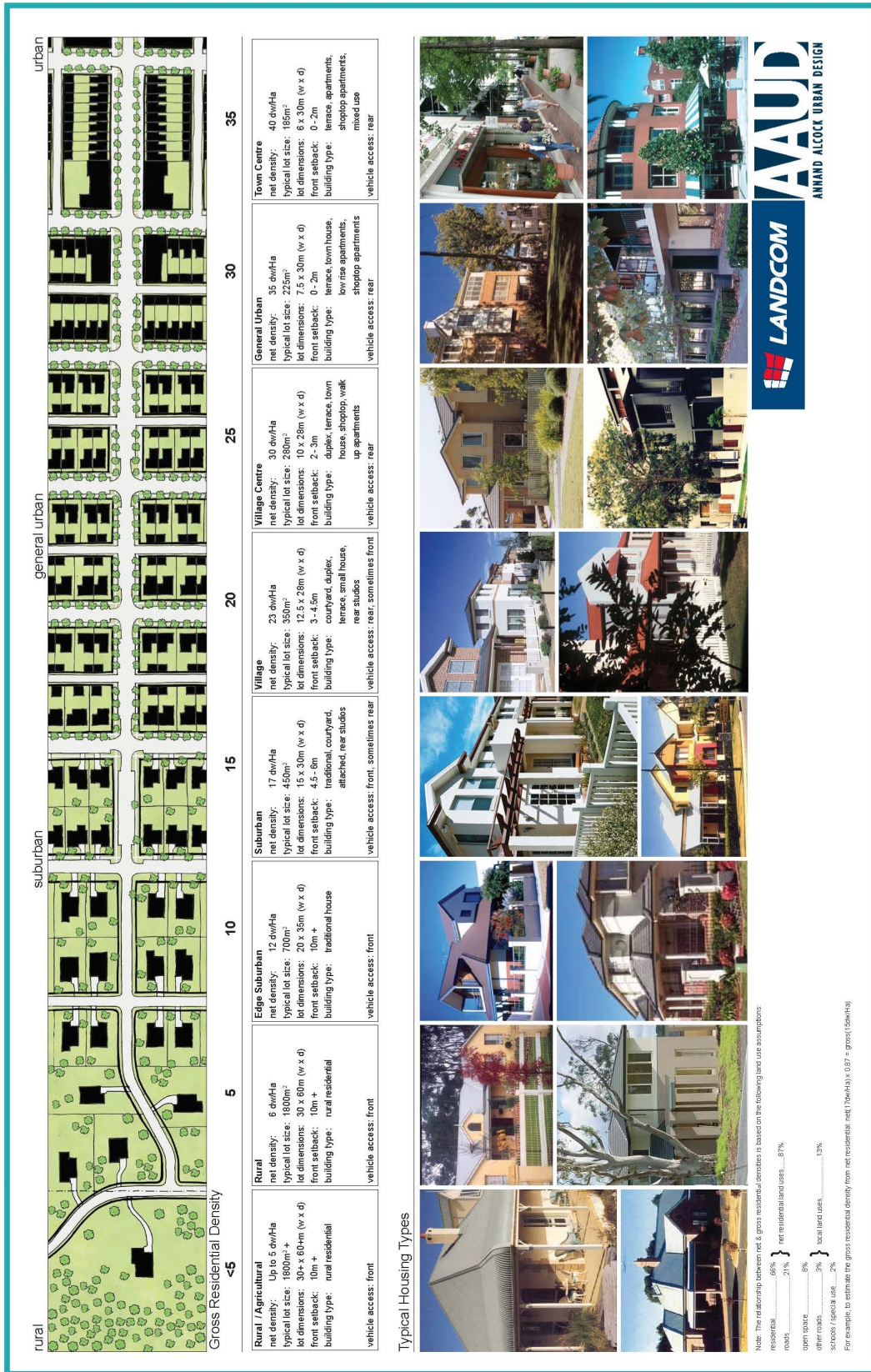


Figure 15. Gross Residential Density (Landcom, 2011)

Draft Wollongong Development Control Plan 2009 – June 2024

### **Principle 2: Promote housing affordability**

Residential neighbourhoods cater for a variety of demographic and socio-economic characteristics. Promoting housing mixture is one tool that provides opportunity for more affordable housing options and reduces housing stress.

### **Principle 3: Establish sustainable, energy efficient, appealing and functional residential living**

- 1 Seek to promote design excellence in housing provision and neighbourhood planning.
- 2 Target an increased use and uptake of renewable energy through housing and neighbourhood design.
- 3 Target smart design solutions for housing with passive heating/cooling (housing placement in lots, responsive floor plans), light coloured roofing and light paving or ground covering materials (both reduces ambient air temperatures in the neighbourhoods and roof cavity temperatures).
- 4 Lots must have the appropriate area and dimensions for the siting of dwellings, canopy trees and other vegetation, private outdoor open space, rainwater tank, and vehicular access and on-site parking.

### **Principle 4: Creating local amenity and a sense of place**

Design safe, healthy and active neighbourhoods with interactive interfaces between residences, the streets and surrounds. It is about ensuring there is visual connection between housing and the streets, parks and activity areas they are adjoining or interfacing with.

Encouraging and supporting housing design that responds to place. Creating site responsive built form and lot layouts that consider existing features and landscape context, natural land form and surrounding land uses.

Manage housing growth to protect and promote the conservation values that contribute to concepts of 'place' in West Dapto.

1. Lot size and layout must respond to the physical characteristics of the land, such as slope and existing significant vegetation, and site constraints including bush fire risk.
2. Lot design is to facilitate housing fronting onto creek line corridors and other areas of public open space, to incorporate these spaces into the living environment, facilitate surveillance, and prevent isolation and degradation of these spaces.

### **Principle 5: Housing transition to the Illawarra Escarpment**

Reduce housing density on the fringe of the urban release area to provide delineation to the housed urban areas and a buffer to the escarpment and important environmental features.

## 14. NEIGHBOURHOOD PLANS

Neighbourhood Planning is a progressive step between the West Dapto Structure Plan, the urban zoning process, and a development application for subdivision. It allows issues to be considered on a neighbourhood or catchment scale. A Neighbourhood Plan must be adopted into this Development Control Plan before Council determines a development application for land within the West Dapto Urban Release Area (WDURA). Neighbourhood Planning is the preferred approach of Council to meet the requirements of Clauses 6.1(2) and 6.2(2) of the WLEP 2009. Council acknowledges that a concept development application can also satisfy this obligation when proposed at a Neighbourhood scale (**Figure 17**).

Neighbourhood Planning is intended to:

- ensure adjoining land owners jointly (or on behalf of another) consider common constraints and design issues.
- provide a means to work through issues such as transport access and the staging of development.
- allow Council and other agencies to better align infrastructure planning and delivery to where development is occurring or is ready to occur.
- achieve efficiencies of shared infrastructure requirements delivered at a Neighbourhood scale to allow for shared servicing, shared cost/contribution and efficient ongoing maintenance (for example water management).
- set urban density targets at an appropriate scale that supports town and village centre development.
- align neighbourhood boundaries with ownership/cadastre and water sub-catchments reflecting the existing environmental setting.
- identify and protect environmental and heritage conservation areas.
- manage the interface between urban development and adjoining land uses.
- provide guidance for the preparation of a development application for land contained within the Neighbourhood Plan.

Draft Neighbourhood Plans will be exhibited as proposed amendments to this chapter. After exhibition of a draft and adoption of a final Neighbourhood Plan into this chapter, development applications for lots within the defined area of the Neighbourhood Plan can be lodged. A development application within a defined Neighbourhood (**Figure 17**) will not be accepted unless there is an adopted Neighbourhood Plan.

Development applications must align with any relevant Neighbourhood Plan, identified Sequencing or Staging Plans. A development application can be lodged on behalf of many land owners, if the owner's consent is provided for each affected lot. Any proposed variation to an agreed Neighbourhood Plan will require justification and will be considered on merit. Council may require the agreement of adjoining owners for variations to an adopted Neighbourhood Plan on or near property boundaries particularly where variations to an adopted Neighbourhood Plan arise as a result of a development application. The variation needs to be adopted before the development application is determined, or at the next opportunity.

### 14.1 Neighbourhood Plan Requirements

Neighbourhood Plans are required to:

- support and reflect the West Dapto Vision 2018 Planning Principles and Structure Plan.
- confirm the developable areas within the defined Neighbourhoods outlined in **Figure 17**. Council will also consider proposals to consolidate neighbourhoods.
- supplement the previous information prepared to support rezoning of West Dapto. For example, information prepared by Council in 2007 did not have sufficient resources or site access to allow detailed consideration of every property in the WDURA.  
You can request copies of the studies from Council's Urban Release or Customer Service teams (The West Dapto Aboriginal Heritage Study is not a public document).
- consider all potential constraints holistically, mitigate impacts, or propose solutions to managing constraints on a neighbourhood or catchment scale, rather than property by property.
- define the desired future character for the neighbourhood.
- plan the development sequence for all lots within a neighbourhood to ensure adjoining land owners consider each other's proposals, concepts and development timeframes (planning through any access issues, etc).

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- encourage the integration of development sites, development sequencing and economies of scale (eg opportunities for efficiencies through shared infrastructure, integrated outcomes with well-considered interfaces between land uses). Avoid exclusion of adjoining lots that may result in development isolation or disjointed development outcomes.
- provide more detailed neighbourhood specific information such as future residential density, proposed landform, open space functions, conservation areas, water management structures and neighbourhood transport network. The specific information shall be guided by the West Dapto Structure Plan, the West Dapto Development Contributions Plan and other relevant sections of this DCP chapter (for example 6.1 The Road Network).
- ensure that the impact of earthworks on the natural topography, landform and vegetation is minimised.
- ensure sufficient space is provided in a neighbourhood plan for water management, open space and any other land uses, or infrastructure required (considering the Principles in the West Dapto Vision 2018) to support safe and sustainable communities.
- ensure interfaces between land uses and delivery of large infrastructure is well coordinated within and with adjacent neighbourhoods.
- identify additional specific information that is required to be addressed at a future development application stage.

Where a Centre Master Plan has been adopted into this Chapter, that Centre Master Plan must be incorporated into the relevant defined neighbourhood or concept development application.

## 14.2 Pathways to Neighbourhood Planning

There are two options for neighbourhood planning in the West Dapto Urban Release Area:

- Preparing a Neighbourhood Plan (lodged with or without a Planning Proposal), or
- Lodging a Concept Development Application (CDA).

In West Dapto, the preparation of a Neighbourhood Plan has traditionally been pursued and is Council's preferred option, however the *Environmental Planning and Assessment Act, 1979* also identifies CDAs as an option.

### 14.2.1 Pre-lodgement and Lodgement Pathway

The following steps apply to all draft Neighbourhood Plans and Concept Development Applications:

- Optional Pre-meeting** Discuss the site with Council staff to gauge the general acceptability of the overall proposal. Council will consider the current Sequencing Plan, Staging Plan and planned infrastructure delivery. This discussion is offered to support a holistic approach to planning and to streamline the process.
- Mandatory Pre-lodgement meeting** held with the applicant and representatives from relevant Council Divisions to discuss the preliminary neighbourhood plan proposal. Detailed notes from the meeting will generally be given to the applicant within 10 business days. All pre-lodgement advice will include as a minimum reference to the matters to be addressed in a Neighbourhood Plan or CDA (Section 14.2.3 below). A fee may be payable to Council subject to Council's Fees and Charges.
- Applicant refines their proposal, guided by the pre-lodgement notes.
- Lodgement:** The draft neighbourhood plan or concept development proposal is lodged with Council, accompanied by the applicable assessment fee. The application must also include the completed lodgement checklist, confirming that the necessary information accompanies the application.
- Council will process the application within 14 days. If an application is incomplete or inadequate it will be rejected. If this occurs, Council will return the rejected application to the applicant, with a letter outlining the reasons for rejection.



### 14.2.2 Neighbourhood Planning Assessment Pathway

The following steps apply only to Neighbourhood Plans:

- a. **Assessment period:** If an application is accepted, the draft neighbourhood plan's assessment period will begin. A Council officer will seek expert advice from various internal staff. Together, they will review the proposal to decide if it is supportable or if more information is required. Council staff are generally given 21 days to provide referral comments.

If more information is needed, a request for information (RFI) will be sent to the applicant. The applicant's response must be made, in full to Council, within 21 days. When the new information is received, it will be reviewed to determine its completeness. The new information will be re-referred to Council staff for review and comments. The assessment period should include only one RFI.

A report to Council will be prepared, at the end of the assessment period, recommending that the proposal be:

- I. **Exhibited.** This will occur if the draft plan is considered generally supportable to progress. Any outstanding RFI issues that can be resolved post exhibition would also be detailed in the Council report.
- II. **Rejected.** If significant outstanding issues remain unresolved after the RFI process, a recommendation to reject the draft plan will be made. If it is rejected, it cannot progress further, and the applicant will be notified.

An alternate resolution may also be made by the Council.

*Where a Neighbourhood Plan is lodged with a concurrent Planning Proposal the Planning Proposal will be subject to Local Planning Panel review before the report to Council for exhibition is prepared.*

- b. **Exhibition:** If the proposal is supported by Council for exhibition, the draft Neighbourhood Plan will be exhibited for a minimum 28 days. External referral agencies will be directly notified of the proposal and given copies of the draft plan with the relevant supporting documents.
- c. **Post-exhibition period:** Council will review all submissions received at the end of the exhibition period. An RFI will be forwarded to the applicant, if required, to allow the resolution of any issues raised.

A report to Council will be prepared at the end of the post exhibition period, outlining any issues raised in submissions. The report will recommend adoption of the proposal, or rejection. An alternate resolution may also be recommended by the Council. If the proposal is rejected, it will not progress any further, and the applicant will be notified. Council staff will be available to discuss the reasons for an application's rejection, where necessary.

### 14.2.3 Concept Development Applications Assessment (CDA) Pathway

Division 4.4 of the *Environmental Planning and Assessment Act (EP&A Act) 1979* sets out the statutory requirements for making concept development applications. Section 4.23 of the EP&A Act 1979 provides that a concept development application can satisfy the requirement that a DCP must be prepared before land can be developed, as required by Part 6 of WLEP 2009. Therefore, Council can consider a CDA as an alternative to a Neighbourhood Plan.

A CDA sets out concept proposals for the development of a site, similar to a Neighbourhood Plan. Detailed proposals for the site or for separate parts of the site are to be the subject of a subsequent development application, or applications. These subsequent applications must be consistent with the original consent.

A concept development application must also:

- demonstrate consistency with the West Dapto Vision 2018 and all relevant chapters of Wollongong DCP 2009.
- be proposed at a Neighbourhood scale consistent with the defined Neighbourhood applying to the land, or larger, as shown in **Figure 17** Defined Neighbourhoods.
- be compliant with the relevant planning controls, or where there are non-compliances these are either minor or satisfactorily justified.

The following steps apply only to CDAs:

- a. **Exhibition:** If the proposal is supported for exhibition, the CDA will be exhibited for a minimum 28 days in accordance with Council's Community Participation Plan.

External referral agencies will be directly notified of the proposal.

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- b. **Assessment period:** Council will collaborate with internal specialist staff to review the proposal to decide if the proposal is supportable, or if more information is required.

If more information is needed, a request for additional information will be sent to the applicant. An applicant's response must be made, in full to Council, within 21 days. When the information is received, it will be reviewed to determine its completeness.

- c. **Post-exhibition period:** Council will review all submissions received, including agency submissions, at the end of the exhibition period. An RFI will be forwarded to the applicant, if required, to allow the resolution of any issues raised.

A report to Council will be prepared, at the end of the post-exhibition period, recommending adoption of the proposal into this chapter of the DCP, or rejection. An alternate resolution may also be recommended by the Council. If the proposal is rejected, it will not progress any further, and the applicant will be notified.

- d. **Determination:** Following the end of the assessment period, Council staff, where possible under delegations, will determine the CDA.

- e. **Post determination:** In the event that a CDA has been approved in accordance with Section 4.23 of the EP&A Act 1979 and is deemed to have satisfied the requirements of Part 6 of WLEP 2009, Council can consider that approval as an alternative to a Neighbourhood Plan. The Concept Approval shall be adopted into this chapter of the DCP as soon as practical.

See Figure 16 below:

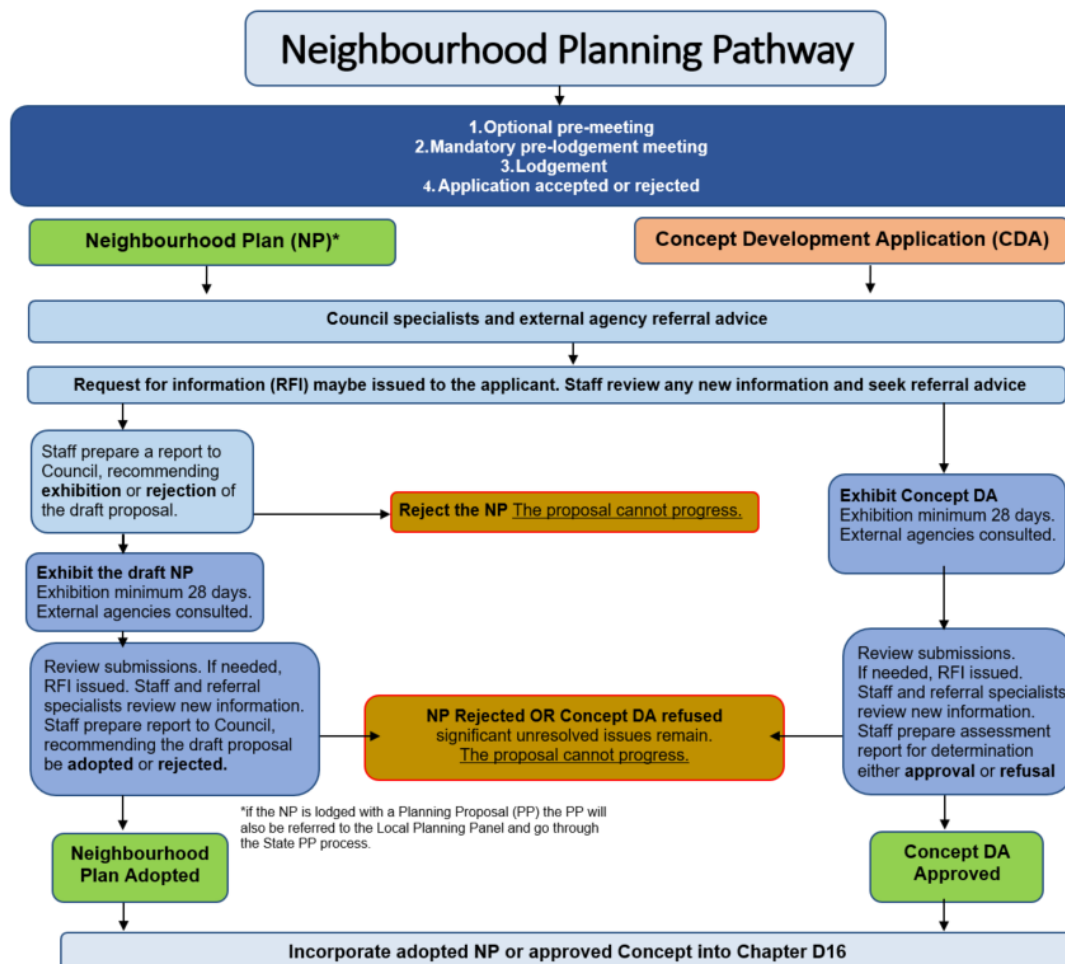


Figure 16. Neighbourhood Planning Pathways flowchart

### 14.3 Matters to be Addressed in Neighbourhood Planning Applications

An application for a Neighbourhood Plan must include:

1. Site location and description, and general land capability assessment, addressing existing issues such as:
  - Wollongong LEP 2009 provisions (including Zoning, Minimum Lot Size, FSR, Building Height, Flooding, Heritage, Acid Sulfate Soils, riparian corridors, etc)
  - other relevant legislation
  - the setting within West Dapto, for example the proximity to commercial centres, main roads, community services
  - flooding and bush fire constraints
  - topography, known geotechnical constraints, known contamination constraints
  - biodiversity (EECs, bushland, significant trees, habitat)
  - heritage - historical land use, heritage sites, including Indigenous Heritage cultural issues and visual character
  - existing road network
  - available utilities, services and existing easements
  - need for community and recreation facilities
  - noise impacts (e.g. from main roads, industrial areas, or public and private railways).
2. A concept Neighbourhood Plan and supporting documentation, showing the proposed:
  - land use areas including, but not limited to, residential, retail, employment, recreation and conservation areas
  - road layout and hierarchy
  - indicative dwelling density (**Figure 15**) and yield
  - public transport, bicycle and pedestrian routes demonstrating walkability
  - drainage management concept plan based on modelling (water quantity, quality, and flood behaviour) inclusive of indicative locations and sizing of infrastructure

Note – where a drainage/water quality solution is developed at a catchment or neighbourhood level, Council will consider acquisition where the agreed detention basin site is consistent with the West Dapto Development Contributions Plan

  - buffers to heritage items or other proposed heritage conservation management measures
  - riparian corridors, buffers and proposed future uses
  - location of schools, community facilities, recreation facilities and parks, including any proposed public land
  - conceptual Bulk Earthworks Plan is required. The Bulk Earthworks Plan shall also demonstrate the feasibility of the drainage (stormwater) infrastructure and road layout plans. Justification will be required where there are changes to the existing landform.
  - the outcomes identified in a Centre Master Plan where a Centre Master Plan has been adopted into Section 14.6 of this Chapter.
3. In collaboration with Council advice, a staging or sequencing plan supporting the concept Neighbourhood Plan showing:
  - all existing site boundaries within the neighbourhood, and
  - proposed development staging within the planned area, taking into consideration delivery of essential infrastructure, access and logical progression as a development front.
4. Submission in electronic and PDF form.
  - All the above data layers are required to be presented in electronic form. The electronic Neighbourhood Plan package will include either a set of Shapefiles, a Geodatabase or set of

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CAD files or be provided in another form as required by Council. The applicant shall also seek advice regarding Council's specific naming conventions, coordinate system and metadata requirements prior to lodgement of the Neighbourhood Plan.

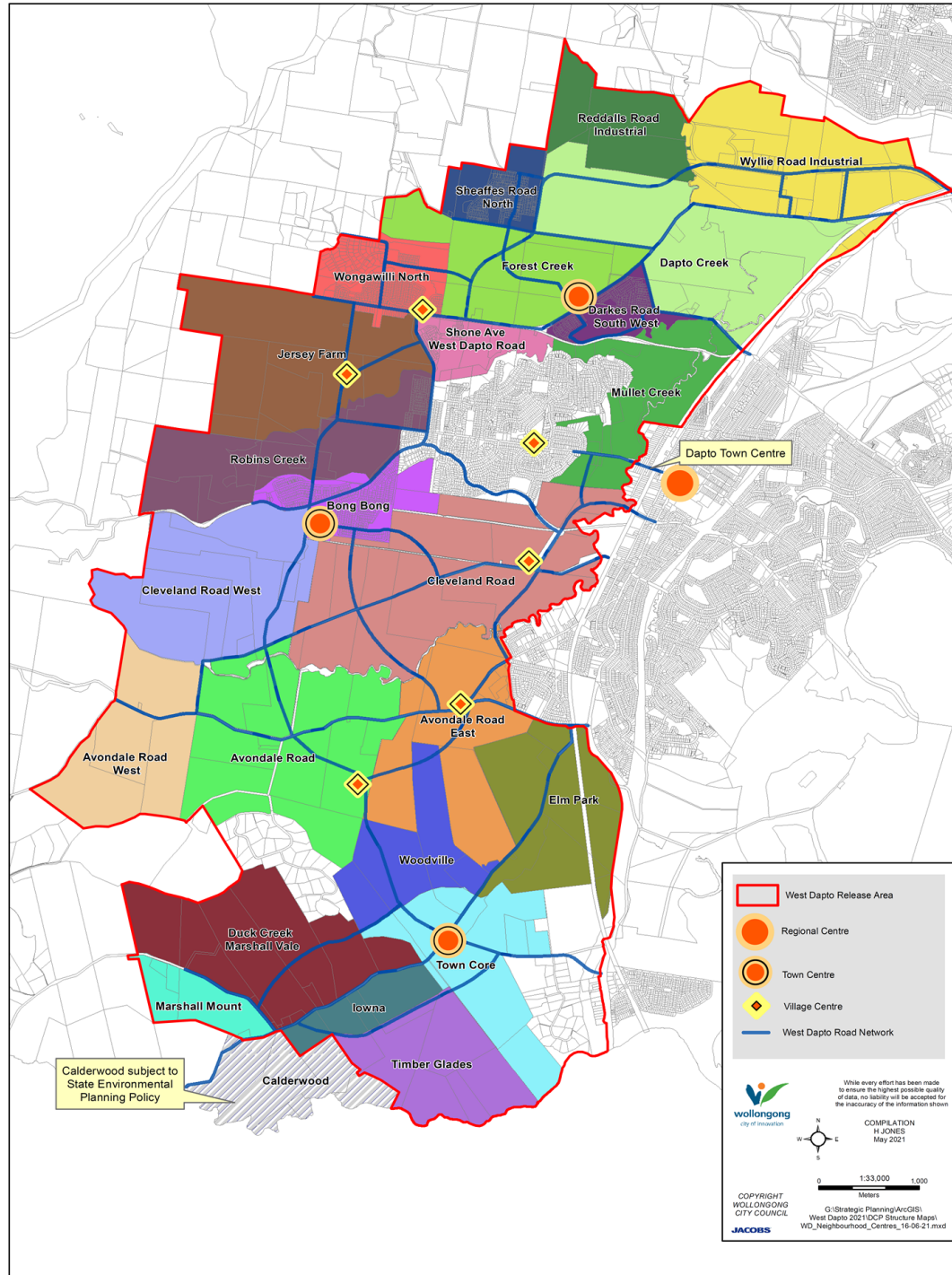
- Council also requires the Neighbourhood Plan and supporting plans (staging, infrastructure plan, etc) to be provided in PDF form.



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Defined Neighbourhoods in West Dapto Urban Release Area



**Figure 17. Defined Neighbourhoods in West Dapto Urban Release Area.**

*The transport network displayed in Figure 17 is correct as at the time of adoption and represents the overall transport structure depicted through the West Dapto Development Contributions Plan. The West Dapto Development Contributions Plan provides additional information relating to infrastructure requirements and should be read in conjunction with the Wollongong DCP 2009 Chapter - for example, apportionment, infrastructure costed in the plan, and infrastructure not included in the plan. The West Dapto Development Contributions Plan is available on Council's website.*

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## 14.4 Transitional Arrangements

Council recognises that the WDURA is an active release area where much development has already occurred, and new neighbourhoods are forming. Council also recognises that new Neighbourhoods, as shown in **Figure 17** Defined Neighbourhoods in West Dapto Urban Release Area, will be developed over time.

All existing adopted Neighbourhood Plans in Section 14 remain the guiding step between the West Dapto Structure Plan and Development Applications. A Neighbourhood Plan is repealed by an amendment to this chapter (Chapter D16: West Dapto Urban Release Area) where the guiding function of that Neighbourhood Plan is replaced by a subsequent adopted Neighbourhood Plan. Where a Neighbourhood Plan is under assessment or pre-lodgement meetings have been held, those applications can continue (these are listed within the Council Business Paper of 18 October 2021).

In the event that a Concept Development Application has been approved in accordance with Section 4.23 of the EP&A Act and is deemed to have satisfied the requirements of Part 6 of WLEP 2009, Council can consider that approval as an alternative to a Neighbourhood Plan. The Concept Approval shall be adopted into this chapter of the DCP as soon as practical.

All Neighbourhood Planning applications lodged after 31 December 2021 must reflect **Figure 17** Defined Neighbourhoods unless they are subject to the transitional arrangements outlined in this Section.

## 14.5 Adopted Neighbourhood Plans

Section 14.5 outlines all neighbourhood plans that have been adopted into the DCP for the purposes of this Part.

All adopted Neighbourhood Plans remain in force until such time as they are superseded by a subsequent Neighbourhood Plan. In some circumstances, adopted Neighbourhood Plans represent part of the larger defined neighbourhood shown in **Figure 17**.

The table below lists adopted Neighbourhood Plans and identifies how these relate to Defined Neighbourhoods depicted in **Figure 17**.

Defined Neighbourhood (Figure 17)	Neighbourhood Plans adopted before 1 October 2021 These form part of the Defined Neighbourhood area.	Submitted by	Adoption Date
Bong Bong	14.3.1. Bong Bong East and North	Stockland	14 Dec 2010
Bong Bong	14.3.2. Bong Bong Town Centre	Vinta Group / Bong Bong Town Centre	14 Dec 2010
Wongawilli North	14.3.3. Wongawilli north	Cardno Forbes Rigby and Jones Flint and Pike.	26 Nov 2012
Jersey Farm	14.3.5 Shone Avenue south	KF Williams	26 Jul 2011
Reddalls Road Industrial and Solar Radio Station	14.3.6 Reddalls Road Industrial	Beadnell	9 Dec 2013
Sheaffes Road North	14.3.7 Sheaffes Road North	SMEC Urban	8 Apr 2013
Darkes Road South West	14.3.8 Darkes Road South West	Don Fox Planning	24 Mar 2014
Avondale Road West	14.3.9 Avondale Road North, Huntley	Urbis	3 Aug 2015
Shone Avenue West Dapto Road	14.3.10 Shone Avenue / West Dapto Road	KF Williams	24 Aug 2015
Forest Creek	14.3.11 West Dapto Road / Sheaffes Road (south)	Watts Consulting for Wollongong City Council	19 Oct 2015
Cleveland Road West	14.3.12 Bong Bong South	Stockland	19 Nov 2018
Robins Creek	14.3.13 Hayes Lane and Iredell Road	Cardno	6 Apr 2020 & 2 Aug 2021
Woodville, Elm Park, Duck Creek Marshall Vale, Town Core, Iowna, Timber Glades	14.3.14 Stage 5 Yallah, Marshall Mount future neighbourhood precincts	Neighbourhoods yet to be planned.	9 Dec 2019

### 14.5.1 Bong Bong East and North



Figure 18. Neighbourhood Plan 1 - Bong Bong East and North

The following variations to development controls have been accepted:

**Chapter B1 Residential Development - Section 4.2 Front setbacks** – controls 1 and 2 are replaced with:

1. The following setback requirements apply from the primary street frontage to the front façade of the building:
  - (a) Front building line: 4.5m minimum setback, except for garages which must be setback at least 5.5m from the property boundary on the primary road.
  - (b) Articulation zone: An articulation zone up to a maximum of 1.5m measured from the foremost edge of the building line may be incorporated within the front setback zone. The following building elements are permitted in the articulation zone:
    - i) an entry feature or portico
    - ii) a balcony, deck, patio, pergola, terrace or verandah
    - iii) a window box treatment
    - iv) a bay window or similar feature
    - v) an awning or other feature over a window
    - vi) a sun shading feature.
  - (c) A building element must not extend above the eave gutter line, other than a pitched roof to an entry feature or portico that has the same pitch as the roof on the dwelling house.
  - (d) The maximum area of all building elements within the articulation zone, other than a building element listed in (v) or (vi) above, must not be more than twenty five percent of the area of the articulation zone, measured through the horizontal plane of the elements.
2. For corner allotments a 2m minimum setback requirement applies from the secondary street frontage to the façade of the building.

**Chapter B1 Residential Development - Section 4.3 Side and rear setbacks** – controls 1 to 3 are replaced with:

1. A dwelling house and any carport, garage, balcony, deck, patio, pergola, terrace or verandah that is attached to the dwelling house with a building height at any point up to 3.8m on an allotment with an area greater than or equal to 450m<sup>2</sup> must have a setback from a side boundary of at least 900mm. This control does not apply to a secondary street frontage.
2. Any part of a dwelling house that has a building height in excess of 3.8m and any carport, garage, balcony, deck, patio, pergola, terrace or verandah that is attached to a dwelling house on an allotment with an area greater than or equal to 450m<sup>2</sup> must have a setback from a side boundary of at least the sum of 900mm and an amount that is equal to one quarter of the additional building height above 3.8m. This control does not apply to a secondary street frontage.

Note - A two storey dwelling house may have its ground floor component (up to 3.8m in height) setback 900mm from the side boundary with the second storey setback further as required by the formula in (2).

A dwelling house that is part two storey and part single storey may have the single storey portion of the dwelling house (up to 3.8m) setback 900mm from the side boundary and the two storey portion of the dwelling house setback further as required by the formula in (2).

3. On an allotment with an area less than 450m<sup>2</sup> and a lot width 10m or less, where an easement for access and maintenance as well as driveway crossing locations (which are located so as not to adversely impact on-street parking capacity) are provided on title, a zero side setback may be applied to one side for the single storey component of the dwelling. The two storey component of the dwelling is to be setback further as required by the formula in (2). This control does not apply to a secondary street frontage.

The following additional controls to apply:

1. A dwelling house and any carport, garage, balcony, deck, patio, pergola, terrace or verandah that is attached to the dwelling house with a building height at any point up to 3.8m must have a setback from the rear boundary of at least 3m.
2. A dwelling house with a building height of more than 3.8m and any carport, garage, balcony, deck, patio, pergola, terrace or verandah that is attached to the dwelling house must have a setback from the rear boundary of at least 3m, plus an amount that is equal to three times the additional building height above 3.8m up to a maximum setback of 8m.
3. Despite (6) and (7), an allotment that has a rear boundary with a laneway may have a building line that abuts that boundary for up to 50 per cent of the length of that boundary.

**Chapter B2 Residential Subdivision – Section 5 Topography, Landform Conservation, Cut and Fill**  
– does not apply to master planning of land and precinct subdivision applications.

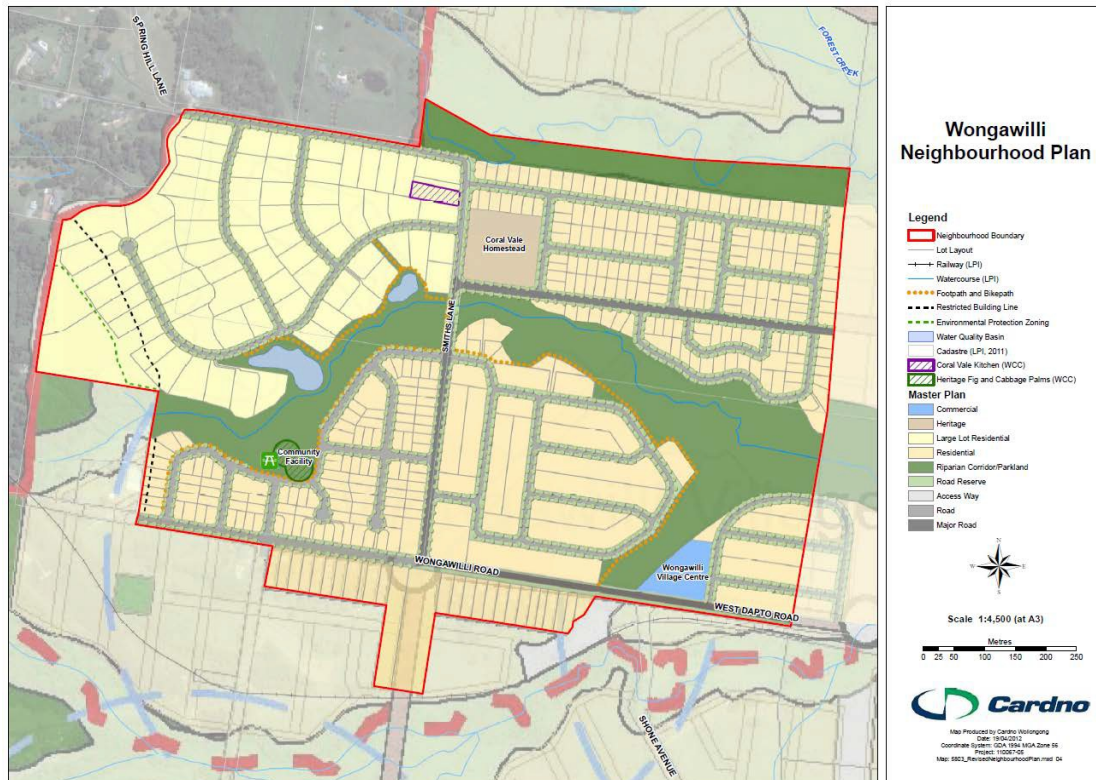
### 14.5.2 Bong Bong Town Centre

In the area where Bong Bong Road adjoins the north-south arterial route a new district town centre is to be established, based on a north-south orientated main street (see **Figure 18** Bong Bong East and North Neighbourhood Plan). The Bong Bong Town Centre is to be the primary town centre in the release area. The Bong Bong Town Centre is to be a supermarket based centre with a range of shops and would accommodate around 15,000m<sup>2</sup> of retail floor space.



### 14.5.3 Wongawilli – North

Wongawilli North will provide a mix of housing densities from large lot housing towards the escarpment and becoming denser towards the east and surrounding the village centre. The riparian corridor will create some structural form for passive recreation and active transport links along shared paths between the residential pockets.



**Figure 19. Wongawilli North Neighbourhood Plan**

The following modified and additional controls to apply:

1. Minimum lot width fronting Wongawilli Road and Smiths Lane of 15m;
2. Minimum front building line setback of 4.5m for all lots fronting Wongawilli Road and Smiths Lane;
3. The maximum length of cul-de-sacs that provide access to lots fronting Wongawilli Road Should not exceed 130m;
4. For lots with a dual road frontage:
  - (a) Wongawilli Road and Smiths Lane is considered to be the primary road frontage and the internal unnamed road is considered to be the secondary road frontage and the rear of the lots;
  - (b) All dwellings must face, address and activate the primary road frontage of Wongawilli Road and Smiths Lane;
  - (c) Carports or garages must be located and accessed from the secondary road frontage rear of the lots;
  - (d) Minimum rear setbacks are to remain in accordance with Chapter B1, garages and carports are to have a minimum rear setback of 5.5m in accordance with the principles shown in **Figure 20**;
  - (e) Fencing and landscaping treatment of the secondary road frontage is in accordance with the principles shown in **Figure 20**. Examples of Articulated fencing include, but are not limited to:
    - i) Masonry to 1.2m high with open type lattice or slats above with masonry elements no wider than 150mm;
    - ii) Timber Lap and Cap;
    - iii) Colorbond solid to 1.2m with Colorbond lattice style top sections.

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5. For lots backing onto or adjoining the Rural Fire Service (RFS) property:
- Dwelling house, secondary dwelling and any habitable areas must be setback at least 10m from the rear or common property boundary that adjoins the RFS property.
  - Outbuildings and garages must be setback at least 5m from the rear of common boundary that adjoins the RFS property.

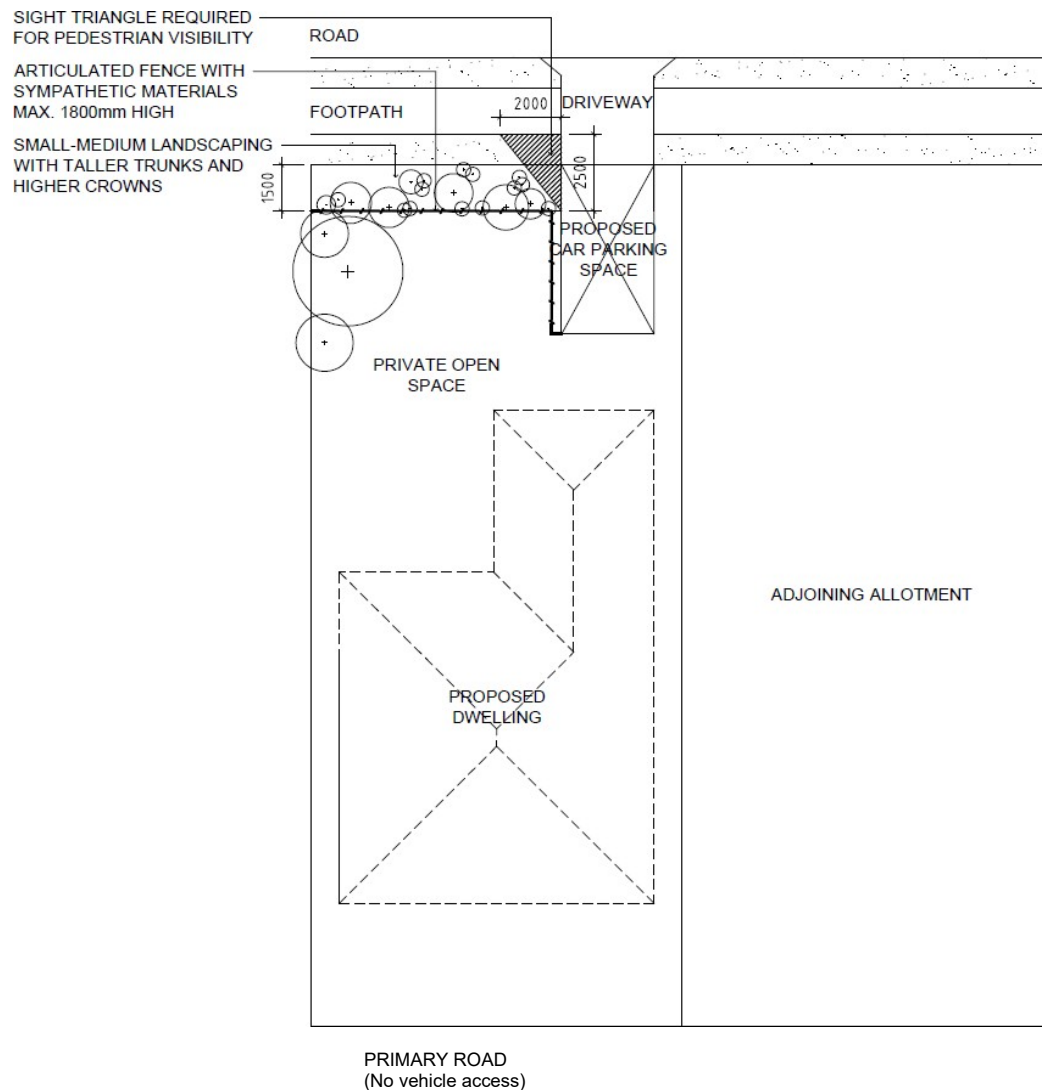


Figure 20. Dual frontage property secondary frontage treatment

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**Wongawilli Village centre** will provide a small convenience centre with a small grocer and some mixed retail shops. The centre will be designed focusing on activating the interface with the riparian boundary and its West Dapto Road frontage. Parking will be included in the village design with street parking along the secondary street and a parking lot area provided along the north, generally not visible from West Dapto Road, allowing built form to perform a clear street defining urban function. The village form will be guided by the concept design presented in **Figure 21**.



**Figure 21. Wongawilli Village Centre – Conceptual design**

#### 14.5.4 Wongawilli Mine Spur Rail line

It is anticipated that the Wongawilli Mine will continue to operate for the next 30 years, or longer. Coal is transported from the mine to Port Kembla via the rail network. Future urban development should be designed to recognise the continued use of the rail spur line and include measure to mitigate noise and other potential impacts. Division 15 of SEPP Infrastructure 2007 applies to development near the spur line.

##### Objectives:

- (a) To facilitate the transport of coal from Wongawilli Mine to Port Kembla by rail transport.
- (b) To minimise rail noise, vibration and other impacts on dwellings near the rail spur line.

##### Controls:

1. Development Applications for subdivision and dwelling houses within the rail buffer area (Refer to **Figure 22**), must satisfy the requirements of SEPP Infrastructure Division 15, and are to include:
  - a. sound attenuation measures that achieve a maximum of 35dBA within the dwelling.
  - b. consideration of vibration impacts and include mitigation measures.

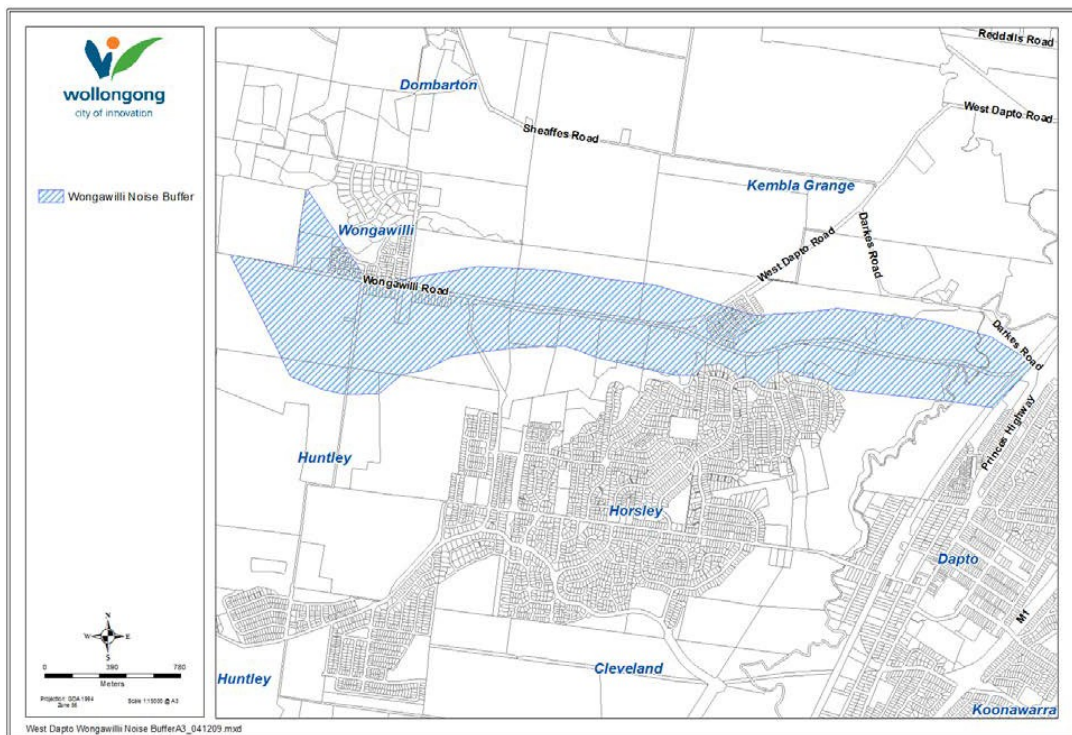


Figure 22. Wongawilli rail noise area



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14.5.5 Shone Avenue - South

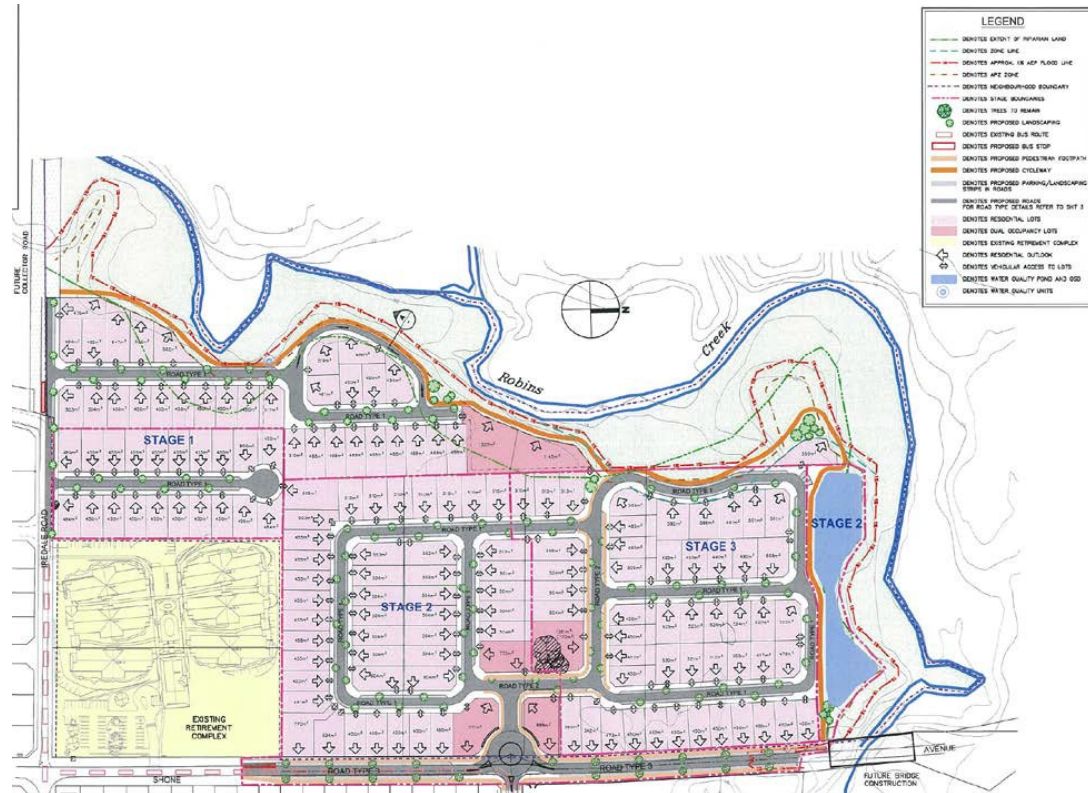


Figure 23. Shone Avenue South Neighbourhood Plan

The following modified and additional controls to apply:

1. For lots with a dual road frontage:
  - (a) Shone Avenue and Iredell Road are considered to be the primary road frontage and the internal unnamed road is considered to be the secondary road frontage and the rear of the lots;
  - (b) all dwellings must face, address and activate the primary road frontage of Shone Avenue and Iredell Road;
  - (c) carports or garages must be located and accessed from the secondary road frontage rear of the lots;
  - (d) minimum rear setbacks are to remain in accordance with Chapter B1, garages and carports are to have a minimum rear setback of 5.5m in accordance with the principles shown in **Figure 24**;
  - (e) fencing and landscaping treatment of the secondary road frontage is in accordance with the principles shown in **Figure 24**. Examples of Articulated fencing include, but are not limited to:
    - i) Masonry to 1.2m high with open type lattice or slats above with masonry elements no wider than 150mm;
    - ii) Timber Lap and Cap;
    - iii) Colorbond solid to 1.2m with Colorbond lattice style top sections.

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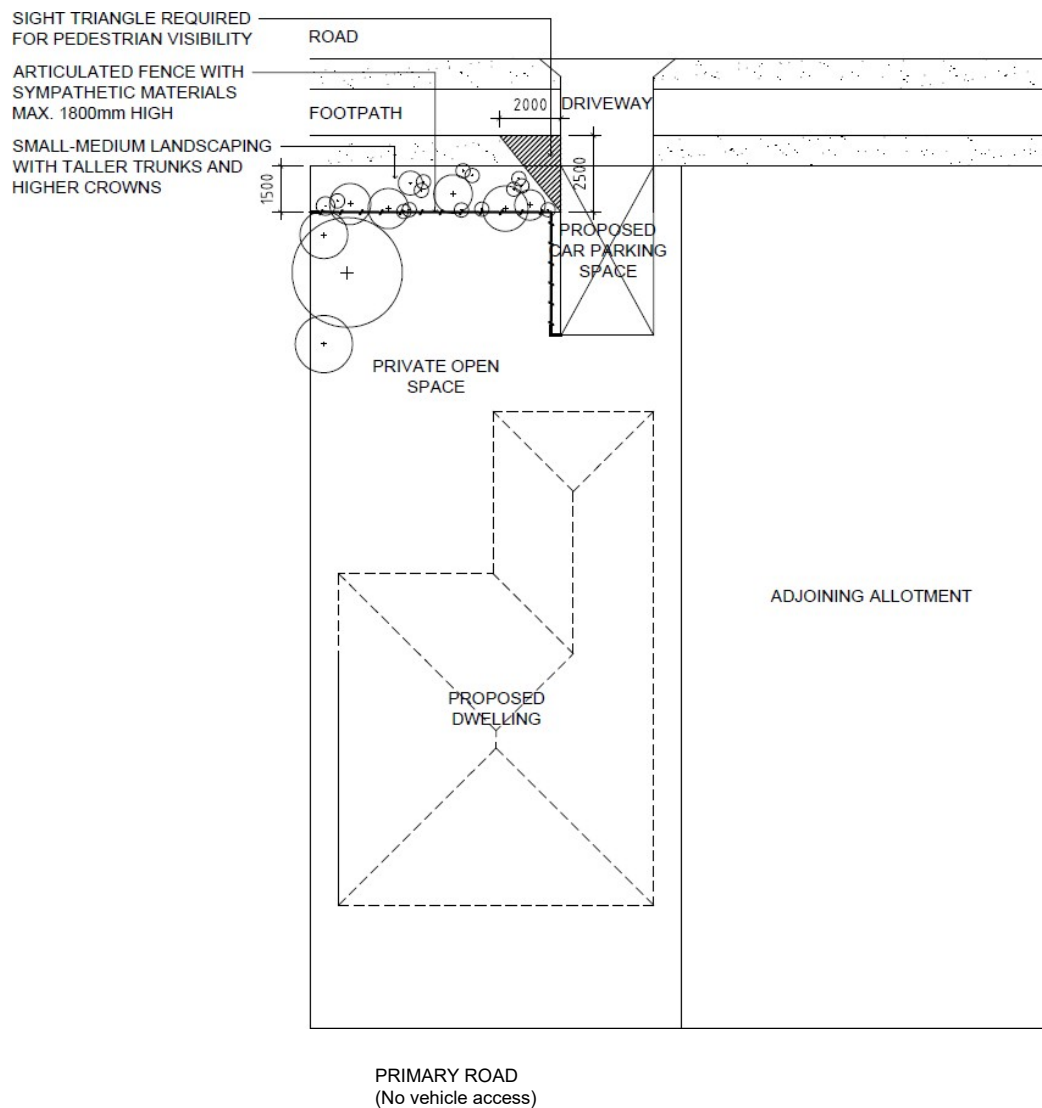


Figure 24. Dual frontage property secondary frontage treatment

### 14.5.6 Reddalls Road Industrial



Figure 25. Reddalls Road Industrial Neighbourhood Plan

The following additional controls to apply:

1. The proposed cycleway must have adequate passive surveillance to ensure safety by design.
2. Indicative future bus stop locations should be identified and shown on road types capable of handling bus routes. A minimum number of stops should be located in a manner to ensure that the majority of lots are within 400m of a bus stop.
3. Any proposed development of the neighbourhood will require the applicant upgrading the relevant section of Reddalls Road to a standard that is suitable for the normal range of Heavy vehicles at no cost to Council. These upgrade works would also include any required intersection treatment to Reddalls Road and the new proposed Access Road as well as any necessary road safety works.

### 14.5.7 Sheaffes Road North



**Figure 26. Sheaffes Road North Neighbourhood Plan**

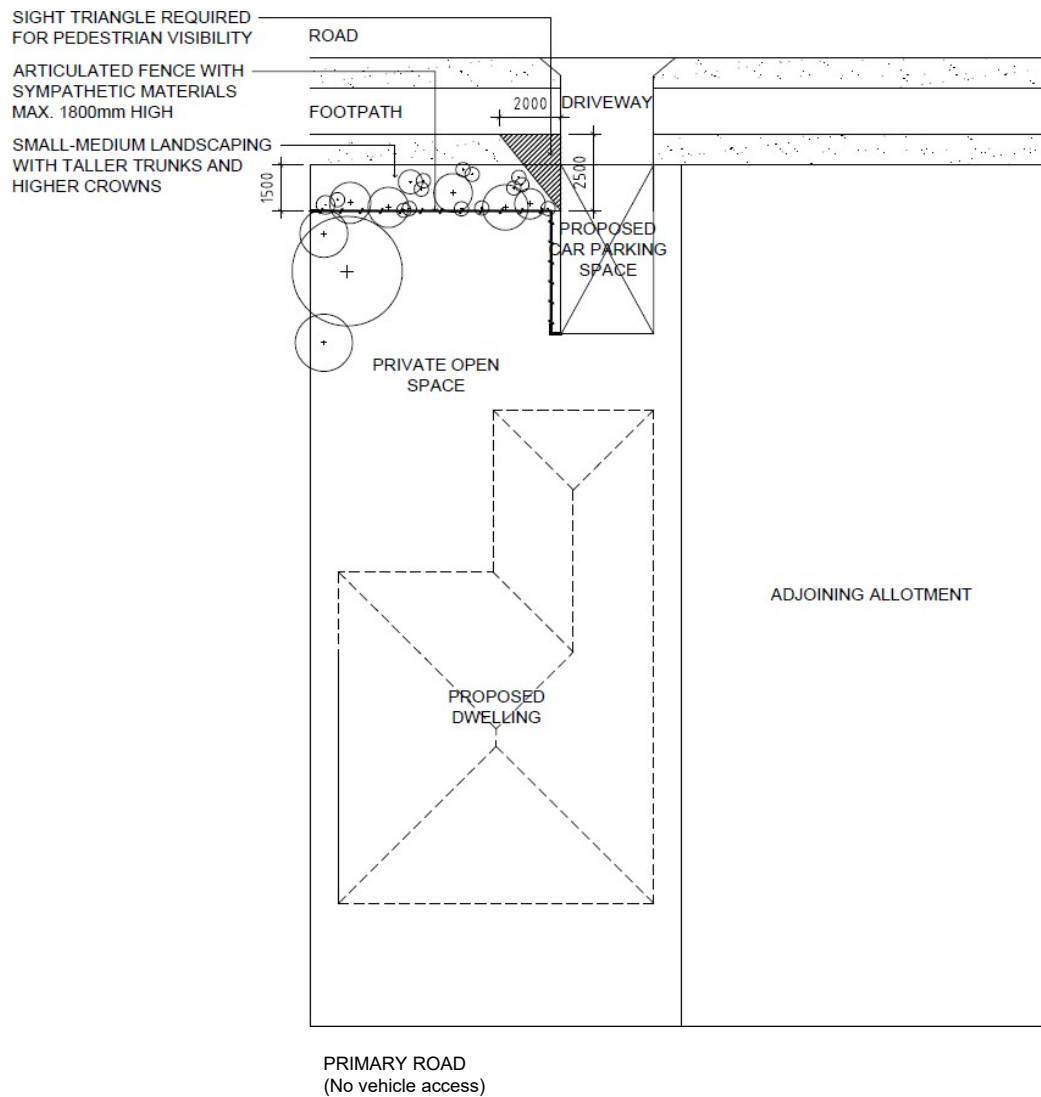
The following modified and additional controls to apply:

1. For lots with a dual road frontage:
  - (a) Sheaffes Road and Paynes Road are considered to be the primary road frontage and the internal unnamed road is considered to be the secondary road frontage and the rear of the lots;
  - (b) All dwellings must face, address and activate the primary road frontage of Sheaffes Road and Paynes Road;
  - (c) Carports or garages must be located and accessed from the secondary road frontage rear of the lots;
  - (d) Minimum rear setbacks are to remain in accordance with Chapter B1, garages and carports are to have a minimum rear setback of 5.5m in accordance with the principles shown in **Figure 27**;
  - (e) Fencing and landscaping treatment of the secondary road frontage is in accordance with the principles shown in **Figure 27**. Examples of articulated fencing include, but are not limited to:
    - i) Masonry to 1.2m high with open type lattice or slats above with masonry elements no wider than 150mm.
    - ii) Timber Lap and Cap;
    - iii) Colorbond solid to 1.2m with Colorbond lattice style top sections.



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**Figure 27. Dual frontage property secondary frontage treatment**

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### 14.5.8 Darkes Road South West

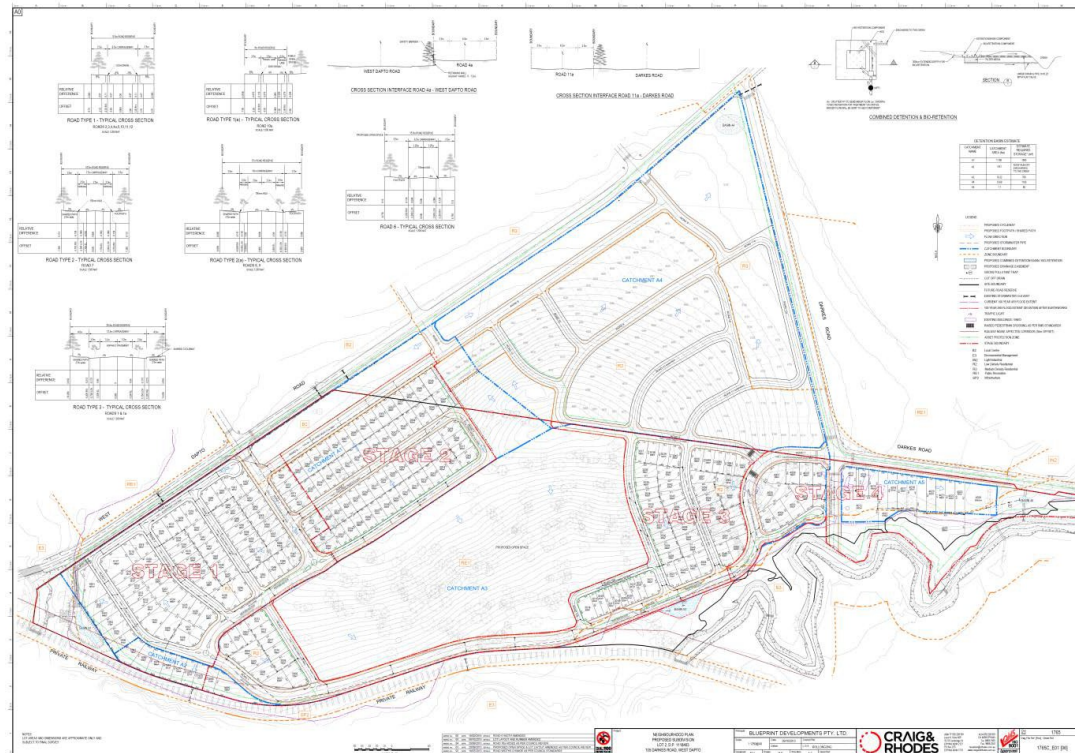


Figure 28. Darkes Road South West Neighbourhood Plan

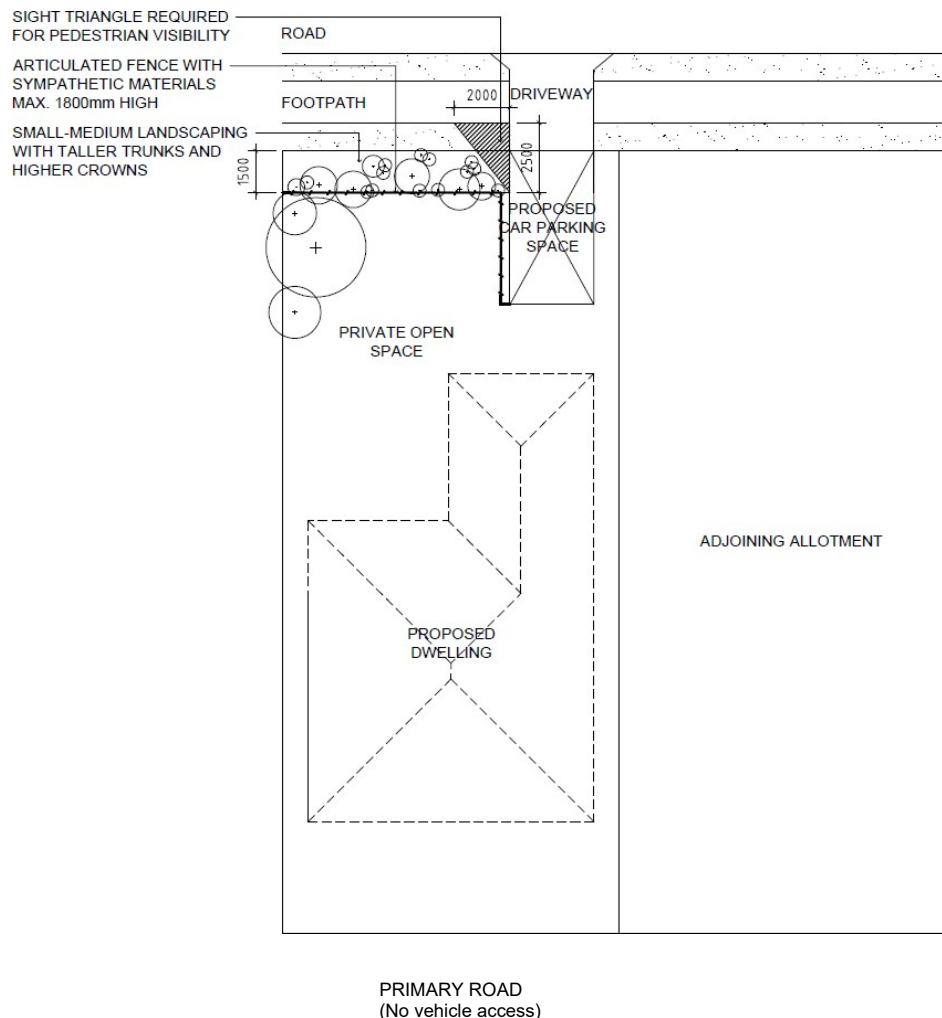
The following modified and additional controls to apply:

1. Lot width, depth and aspect are to be in accordance with **Figure 28** above and are not required to comply with Chapter B2 Section 6 Subdivision Lot Layout – Aspect & Solar Access Orientation as well as Section 8 Lot Width & Depth Requirements. The relevant issues have been considered and the lot layout and details shown are considered acceptable. Should the lot layout depart substantially from that shown then compliance with Chapter B2 is required unless variation is sought in accordance with Chapter A1.
2. On an allotment with an area less than 450m<sup>2</sup> and a lot width 10m or less, where an easement for access and maintenance as well as driveway crossing locations (which are located so as not to adversely impact on-street parking capacity) are provided on title, a zero side setback may be applied to one side for the single storey component of the dwelling. The two storey component of the dwelling is to be setback further as required by the formula in (2). This control does not apply to a secondary street frontage.
3. For lots with a dual road frontage:
  - (a) West Dapto Road and Darkes Road is considered to be the primary road frontage and the internal unnamed road is considered to be the secondary road frontage and the rear of the lots;
  - (b) All dwellings must face, address and activate the primary road frontage of West Dapto Road and Darkes Road;
  - (c) Carports or garages must be located and accessed from the secondary road frontage rear of the lots;
  - (d) Minimum rear setbacks are to remain in accordance with Chapter B1, garages and carports are to have a minimum rear setback of 5.5m in accordance with the principles shown in **Figure** below;

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- (e) Fencing and landscaping treatment of the secondary road frontage is in accordance with the principles shown in **Figure** . Examples of articulated fencing include, but are not limited to:
- Masonry to 1.2m high with open type lattice or slats above with masonry elements no wider than 150mm;
  - Timber Lap and Cap;
  - Colorbond solid to 1.2m with Colorbond lattice style top sections.



**Figure 29. Dual frontage property secondary frontage treatment**

- For all development applications outside of the area denoted as Stage 1:
  - An Aboriginal Heritage Assessment is to be undertaken in accordance with the Wollongong Development Control Plan 2009 Chapter E10.
  - Additional archaeological investigations are required to be undertaken to the previously recorded archaeological sites and three (3) potential archaeological deposits (PADs) identified. This work is required in order to better determine the significance and extents of these areas.
  - In-principle support for the intended mitigation or Aboriginal Heritage Impact Permit (AHIP) proposals is to be gained from the NSW Office of Environment and Heritage (OEH) prior to the determination of the associated development application.
  - Further consultation with Local Aboriginal Groups is to be undertaken within the assessment of any future Development Applications.

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- (e) Consideration of the impacts of the proposal on identified Non-Indigenous Archaeological Deposits located on the site during the preparation of the Heritage reports and which are subject to Section 140 of the NSW Heritage Act 1977.
  - (f) Conservation planning related to any retained structures or features on the site (e.g. The Silo and gardens).
  - (g) Interpretation planning relating to the history and heritage significance of the development area.
2. Bush Fire Matters
- (a) Certain construction standards apply for development on Bush Fire Prone Land. The applicable Construction Standards for proposed development are to reflect the Bushfire Attack Level (BAL) as identified at **Figure 30** below.
  - (b) Given that the site is identified as Bush Fire Prone Land, when a development application for subdivision is made, the development will require a Bush Fire Safety Authority to be issued by the NSW RFS under Section 100B of the Rural Fires Act 1997. The RFS has indicated that it is likely that by condition of the Bush Fire Safety Authority, restriction on the titles of the lots requiring the provision and maintenance of the necessary APZ's will be required.

APPENDIX 1 (Map 2):

AS 3959 – 2009 (Table 2.4.2) BAL Construction Standards relevant to the Neighbourhood Plan

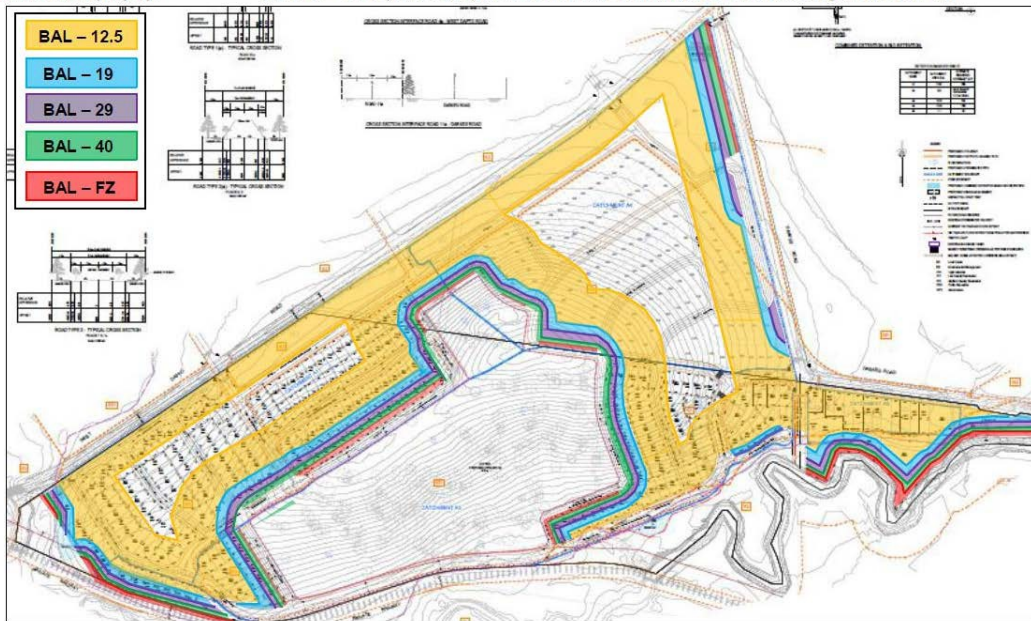


Figure 30. BAL Construction Standards relevant to the Neighbourhood Plan

1. Access
  - (a) An appropriate access track is to be provided to the Detention Basin A1 to facilitate sufficient maintenance access for Council.
  - (b) Appropriate access is also to be provided to the Wongawilli Rail Spur Line from the Detention Basin A1 and from Road 01.
  - (c) The final form of the access track is to be determined in conjunction with Council Engineering Officers within the assessment of future Development Applications. Hardstand access will be required.
2. There may be scope to amend the current Council Drainage Acquisition Maps to reflect more up to date flood mapping of the area. This is to be further investigated within future Voluntary Planning Agreements (VPAs) and assessment of development applications.



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### 14.5.9 Avondale Road North, Huntley

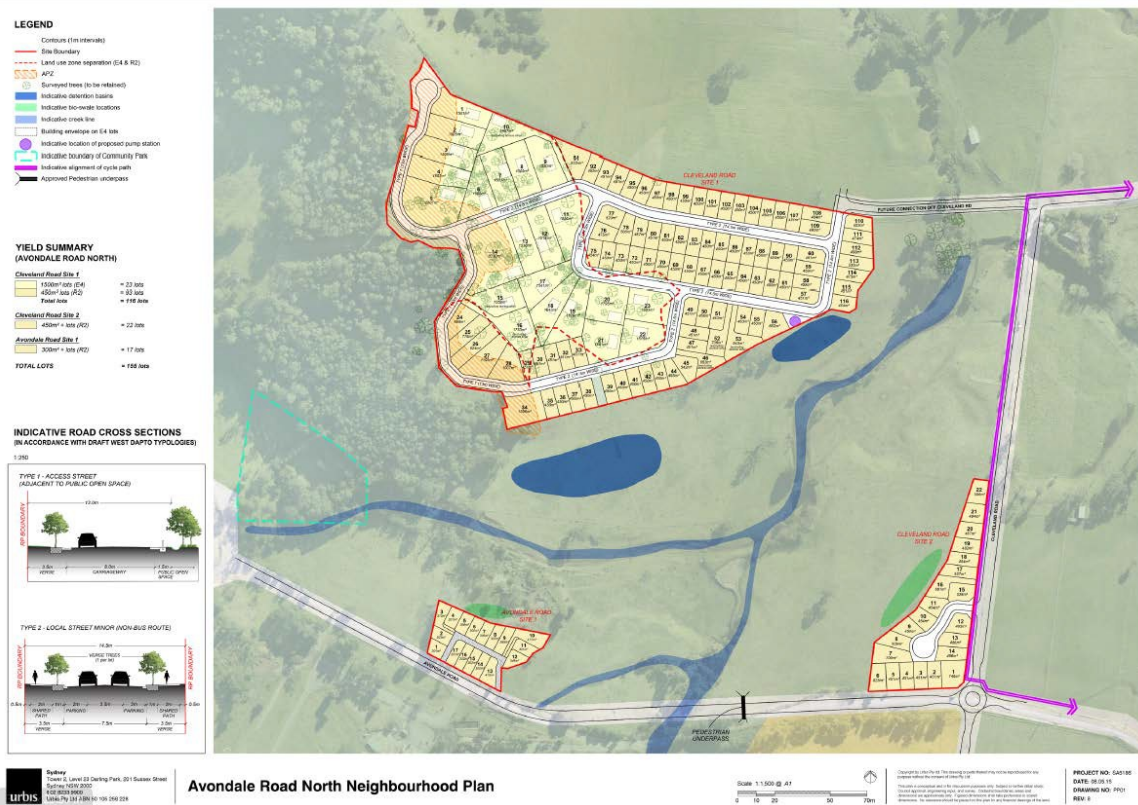
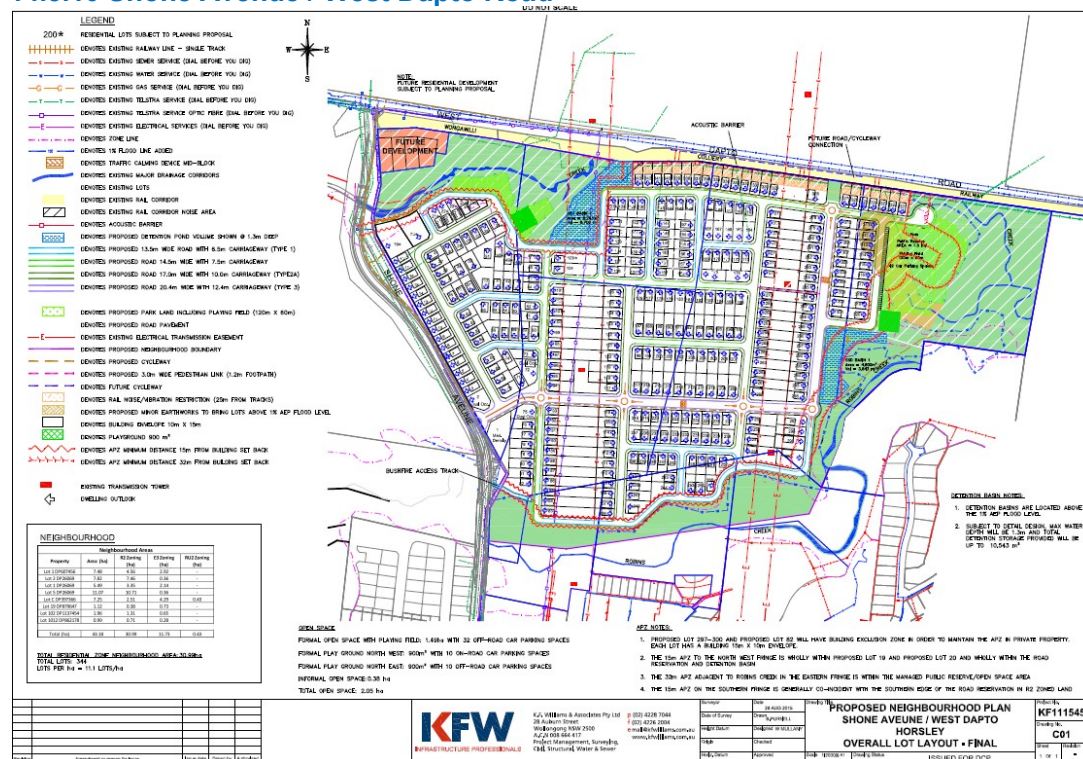


Figure 31. Avondale Road North, Huntley Neighbourhood Plan

#### 14.5.10 Shone Avenue / West Dapto Road



**Figure 32. Shone Avenue and West Dapto Road Neighbourhood Plan**

The following modified and additional controls apply:

1. For lots with a dual road frontage:
  - (a) Shone Avenue is considered to be the primary road frontage and the internal unnamed road is considered to be the secondary road frontage and the rear of the lots.
  - (b) All dwellings must face, address and activate the primary street frontage of Shone Avenue – this is the main address of the dwelling.
  - (c) The minimum front setback on Shone Avenue is 4m (being a greenfield site) and the minimum setback from the secondary road is 4m.
  - (d) No car access to residential lots is permitted from Shone Avenue (i.e. lots are access denied). Carports or garages must be located and accessed from the secondary road frontage rear of the lots.
  - (e) Garages and carports must be setback a minimum of 5.5m from the property boundary on the secondary road to enable a vehicle to park or stand in front of the garage or carport (i.e. allow off street parking that does not impede the footpath) and in order to be a non-dominant component of the streetscape.
  - (f) Fencing controls for the primary street frontage of Shone Avenue are outlined in Chapter B1: Residential Development and are designed to complement the objectives of passive surveillance.
  - (g) Fencing and landscaping treatment of the secondary road frontage must ensure that clear lines of sight are maintained for motorists and pedestrians to and from the lot, and ensure the design complements the objectives of passive surveillance. To help soften the visual impact and improve the streetscape appearance of the fence and allow visual connection between the dwelling and the street, any fence will be required to be well articulated and landscaped with appropriate planting. Articulated fencing should be provided to a maximum height of 1.8m.

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Examples of articulated fencing include, but are not limited to:

- i) Masonry to 1.2m high with open type lattice or slats above with masonry elements no wider than 150mm;
- ii) Timber Lap and Cap
- iii) Colorbond solid to 1.2m with Colorbond lattice style top sections.

**NB.** Fences in bush fire prone areas shall be of a metal or masonry construction only.

- (h) Any gates associated with the secondary street fence should open inwards and not obstruct the road reserve.
  - (i) Where garage door openings face the secondary road, they shall be a maximum of 50% of the width of the dwelling. Refer to Chapter B1: Residential Development for other car parking and access controls.
2. For lots backing onto West Dapto Road:
- (a) An acoustic building exclusion zone of 25m applies along the length of the rail corridor to reflect Noise Report recommendations.
  - (b) A sound wall is to be erected by the developer along the length of the rail corridor, as indicated in the Neighbourhood Plan.

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### 14.5.11 West Dapto Road / Sheaffes Road (south)

Along West Dapto Road a town centre (large local town centre) is to be established to the west. The town centre will interface with large neighbourhood open space provisions that will cater for active organised sporting needs. The town centre will perform an important role in the provision of public transport, as a node with active transport facilities will meet with the public transport network. The core part will contain the primary retail and commercial functions and be surrounded by some business and medium density housing. It is envisaged that this centre would accommodate around 7,500m<sup>2</sup> of retail floor space to support the employment land.

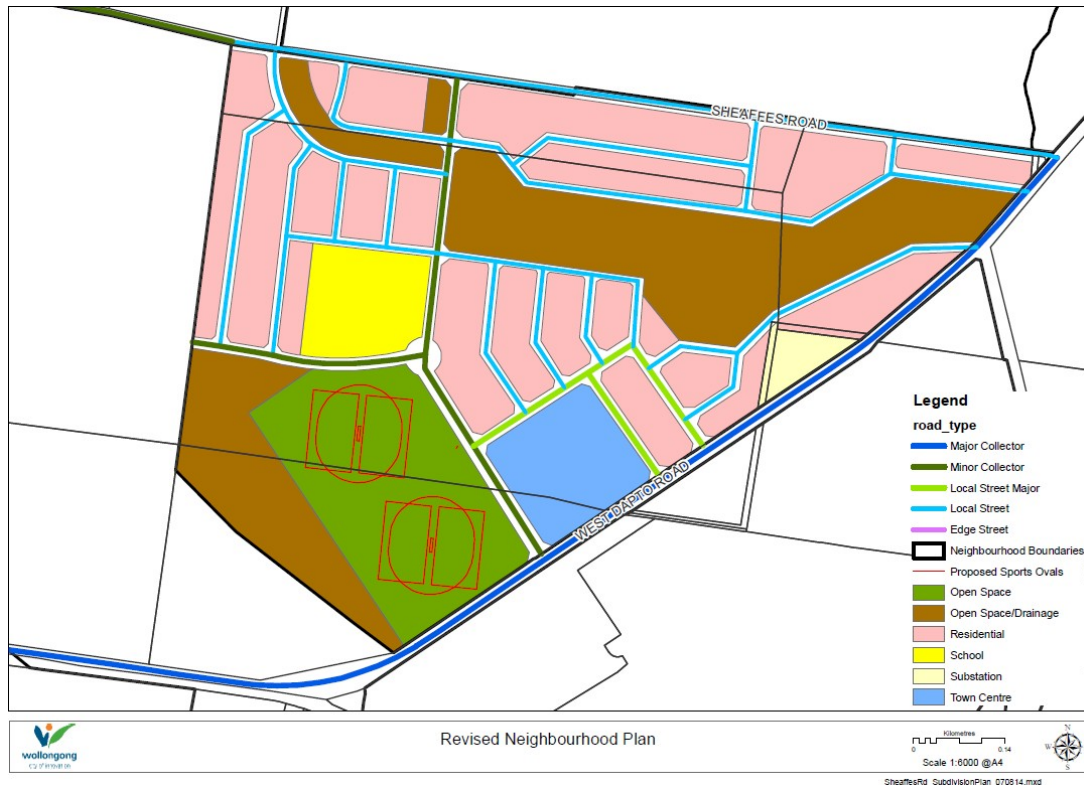


Figure 33. West Dapto Road / Sheaffes Road (south) Neighbourhood Plan



### 14.5.12 Bong Bong South

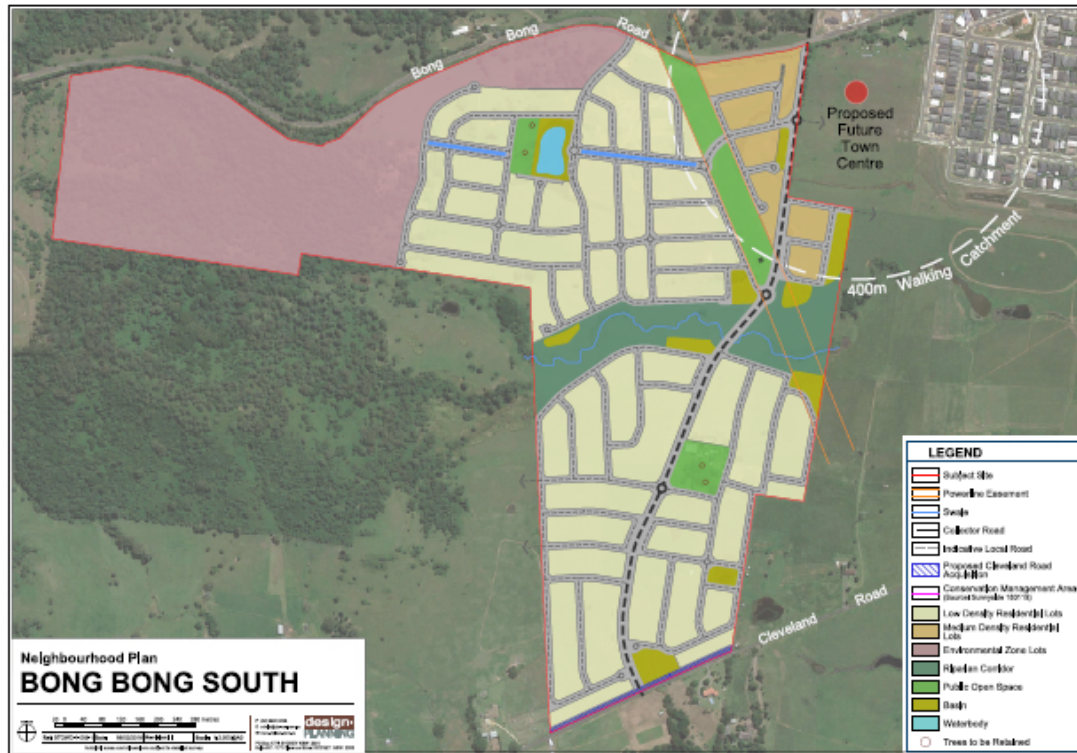


Figure 34. Bong Bong South Neighbourhood Plan

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14.5.13 Hayes Lane and Iredell Road



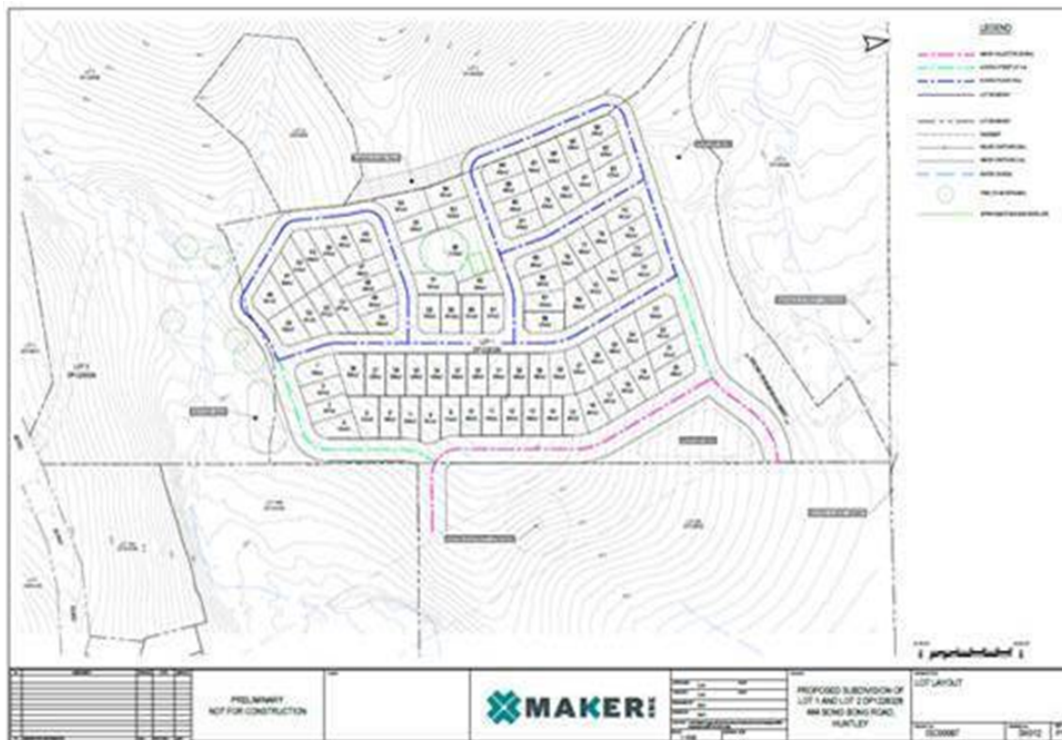
Figure 35a. Hayes Lane and Iredell Road Neighbourhood Plan

1. The future subdivision of the land should be generally in accordance with Figures 35a and 35b.
2. Future residential development on the land shall be in accordance with Chapter B1, except where variations or additional controls are identified below.
3. A development application for subdivision shall demonstrate the manner in which it is compatible with the current and likely future orderly and economic development of adjoining lands including:
  - Connectivity to pedestrian, cycling and public road networks
  - Earthworks, retaining walls and future cut/fill requirements
  - Stormwater management including any necessary works on adjoining land
  - A construction management plan maintaining connections to all essential services and public road network during and post-construction
  - Management of Asset Protection Zones and connectivity to public roads for bushfire emergency and evacuation
4. Minimum setback of four metres from primary road frontage, except for garages which must be setback at least five metres from the property boundary on the primary road.
5. Minimum setback of two metres from a secondary road on a corner lot.
6. For lots with more than one road frontage, the primary frontage is that which is adjacent to the road with the widest reserve width [internal roads/laneway are considered to be the secondary road frontage and the rear of the lots].
  - All dwellings must face, address and activate the primary road frontage
  - Car ports, garages and on-site parking must not be located within the setback to the primary frontage and not be accessed from the primary frontage.

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- Fencing and retaining walls fronting controlled access roads are to present a consistent streetscape and should be constructed prior to the issue of a Subdivision Certificate to ensure consistency of materials, construction and delivery.
7. Residential development on lots less than 300sqm must be developed as Integrated Housing.
  8. If developed in an integrated manner, a zero side setback will be considered for attached dwellings.
  9. Fencing and landscaping treatment of a road frontage that is not the primary road frontage must ensure that clear lines of sight are maintained for motorists and pedestrians and ensure the design achieves passive surveillance. Any fence will be required to be well designed and landscaped. The maximum fence height is 1.8 metres of which a maximum 1.5m from the ground is solid form and elements above 1.5m are open form. Any gates should open so as not to obstruct the road reserve.  
These fencing requirements are to be included as a Restriction-as-to-user on the title of affected lots.
  10. Fencing to 'Access Denied' primary frontages (as shown in the Neighbourhood Plan) shall be constructed prior to the issue of a Subdivision Certificate. Where retaining walls are required they are to be constructed in stepped design of masonry blocks or Council-approved equivalent and fencing as indicated in the Neighbourhood Plan and DCP controls.  
These fencing requirements are to be included as a Restriction-as-to-user on the title of affected lots.
  11. Where rear or side boundary fences adjoin land to be dedicated as open space, fences are to be of a design and materials which allow for passive surveillance between the private lot and the open space.  
These fencing requirements are to be included as a Restriction-as-to-user on the title of affected lots.
  12. Where garage door openings face the secondary road they shall be a maximum of 50% of the width of the dwelling façade.
  13. Western lots inclusive of transmission easement should have rural, timber style fences fronting the new road.
  14. Parts of the site are identified as bushfire prone land. Certain construction standards apply for development on Bushfire Prone Land. The applicable Construction Standards for proposed development are to reflect the applicable Bushfire Attack Level (BAL). The BAL will be finalised at DA stage.
  15. The north and south edges of the Neighbourhood Plan interface with existing vegetation. This requires the provision of an Asset Protection Zone (APZ) in accordance with Rural Fire Service (RFS) requirements. There is sufficient space within each development site fronting the riparian and woodland areas to ensure the provision of an APZ. The final location of any required APZ will be identified at the DA design stage.
  16. Design of local parks must integrate with the proposed OSD. The design and relationship will be finalised as a part of the subdivision DA.



**Figure 35b. Hayes Lane and Iredell Road Neighbourhood Plan – western area**

- 17.1. The future subdivision of the land should be generally in accordance with Figures 35a and 35b.
- 17.2. Future residential development on the land shall be in accordance with Chapter B1.



#### 14.5.14 Stage 5 – Yallah / Marshall Mount

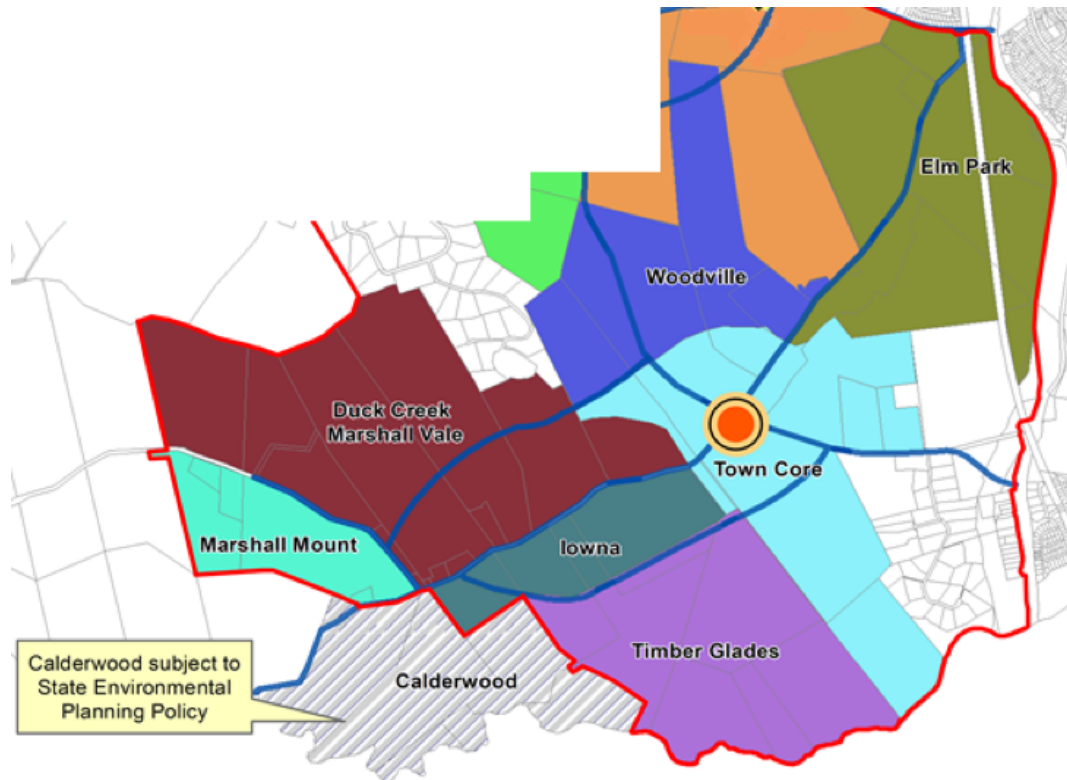


Figure 36. Boundaries of future Neighbourhoods in Stage 5 to be planned.

There are a number of defined neighbourhoods that make up Stage 5 Yallah/Marshall Mount. During the rezoning process, visions and strategic decisions were made to provide strategic level structure to future development in the area (see Page 8 summary of structure). The Neighbourhood plans will all be required to deliver against the strategic intents established during these processes. Applicants planning in Stage 5 can obtain a copy of the [West Dapto Centres Master Plan under the West Dapto Page on Council's website Yallah-Marshall Mount Vision Statement \(Council reference: Z14/418278\)](#).

##### Marshall Mount Town Centre

The new [village](#) centre will be focussed around the intersection of Yallah Road and Marshall Mount Road on lower lying land adjacent to Duck Creek. The Stage 5 Yallah/Marshall Mount visions proposes that the focal point of the new community will be in this neighbourhood. Other developable land near this central intersection will be available for development for a mixture of housing types, with densities ranging from 50-75 dwellings per hectare near the [town village](#) centre, with 20-30 dwellings per hectare further away. The vision needs to achieve these higher densities to create a critical mass of population within a walkable catchment of the proposed [town village](#) centre to assist in economic viability of the centre and reduced car dependence.

There are opportunities for smaller lot housing and terraces to take advantage of future public transport routes along Marshall Mount Road. Duck Creek provides opportunities for passive open space and walking and cycling tracks, but also includes land with significant flood hazard and is not suitable for development. The corner of Marshall Mount Road and North Marshall Mount Road contains heritage items, including a community hall, which provides opportunities for a community focus around this point. Flat land adjacent to the town centre may have potential for a school and playing fields. Land further from the main public transport routes will be for low density residential and rural-residential development.

The steeper slopes and more timbered areas provide a scenic green backdrop to the Duck Creek valley and provide a bushland link along the ridgelines from the escarpment to Lake Illawarra.

[Refer to Section 14.6.8 for specific Marshall Mount Town Centre characteristic detail.](#)

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14.5.15 Elm Park

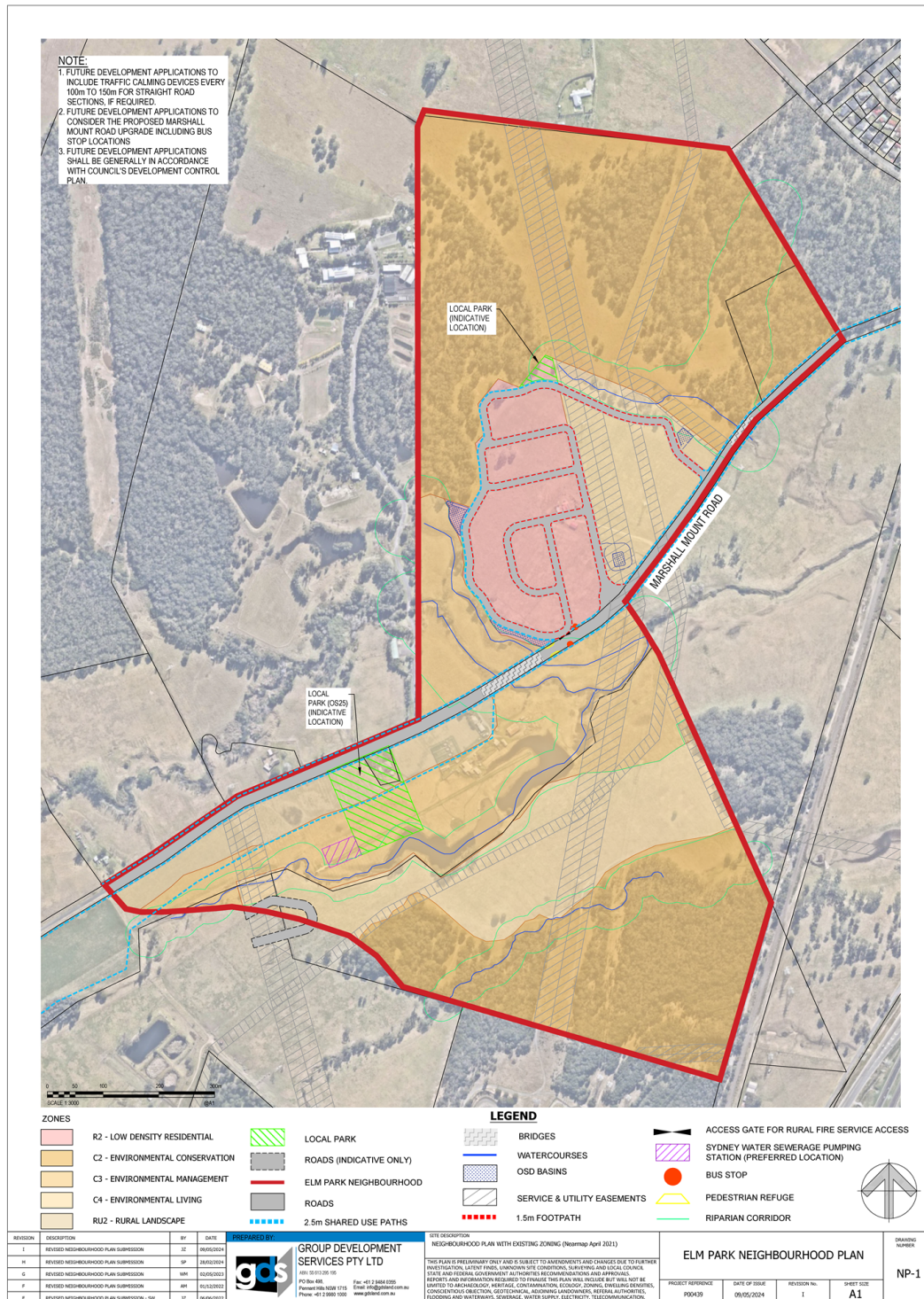


Figure 37. Elm Park Neighbourhood Plan.



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1. The proposed footprint of the Neighbourhood Plan will result in impacts on known Aboriginal Cultural Heritage Sites protected under the NSW National Parks and Wildlife Act 1974 as well as areas identified as having Moderate to High Aboriginal Archaeological Potential. Future development applications which have the potential to impact directly or indirectly on these areas identified within the Ecological Aboriginal Cultural Heritage Assessment Report dated 20 March 2024 will require an Aboriginal Cultural Heritage Impact Permit under Section 90 of the NSW National Parks and Wildlife Act 1974 and should be lodged as Integrated Development. These applications must be supported by updated Aboriginal Cultural Heritage Assessment Documentation that addresses the recommendations of the Ecological Report, and responds to the advice of Heritage NSW dated 3 August 2023.
2. An Aboriginal Cultural Heritage Assessment Report (ACHAR) will be required for the land southeast of Marshall Mount Road zoned C4 Environmental Living, prior to any development application.

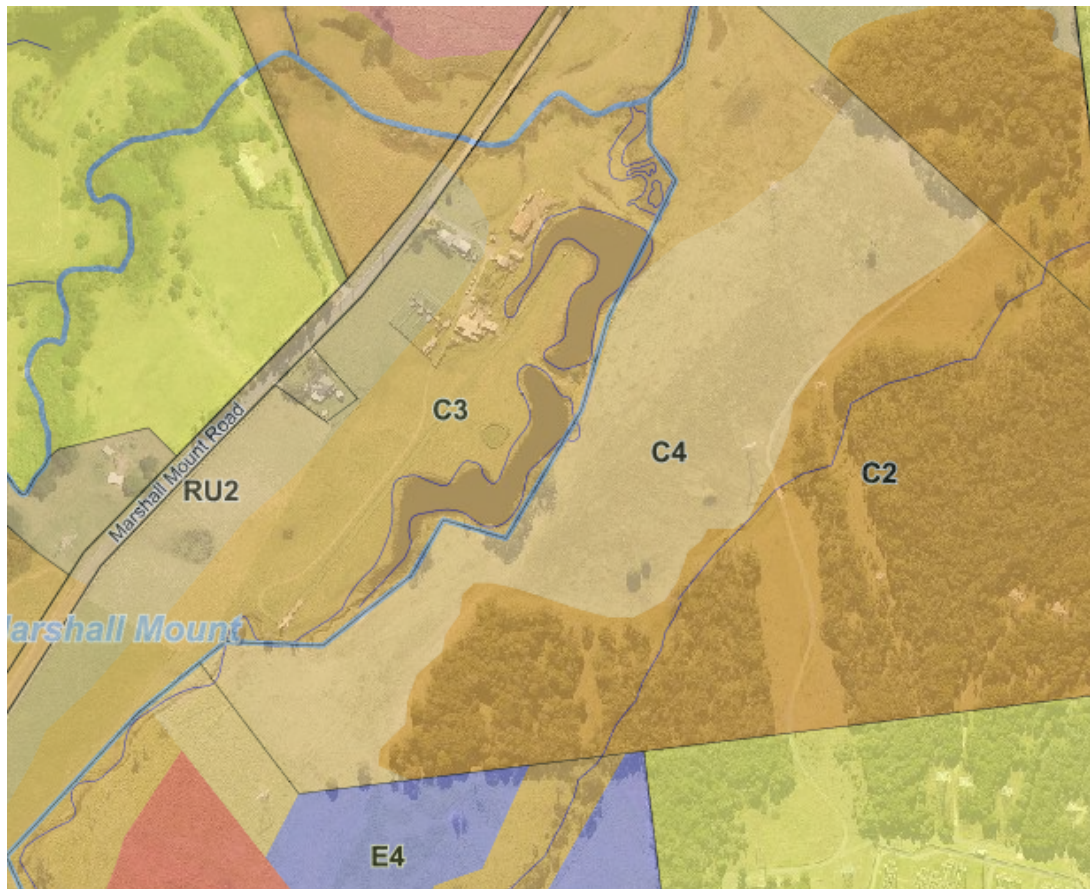
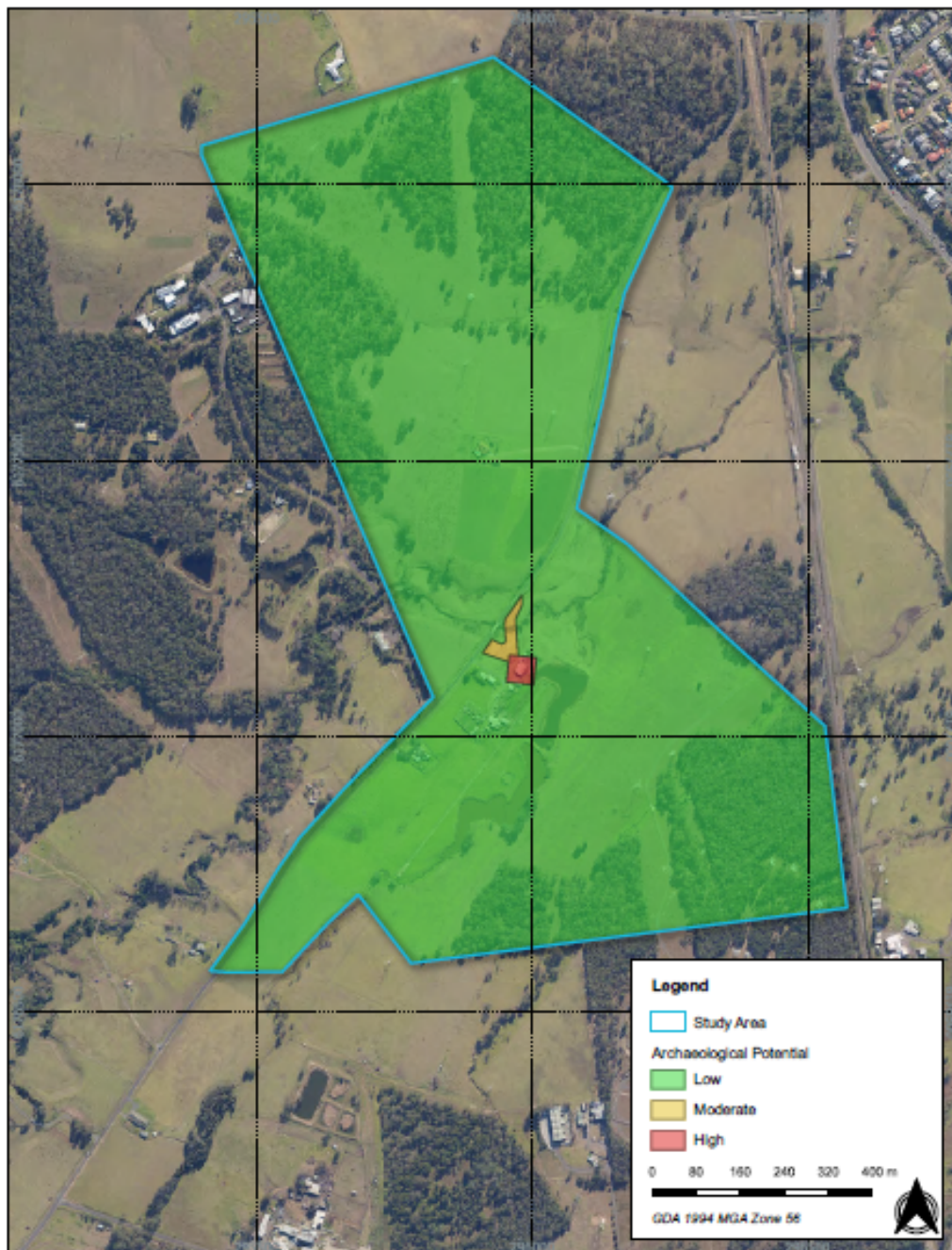


Figure 38. C4 Environmental Living area, requiring Aboriginal Cultural Heritage Assessment Report.

3. Any future proposed development that will require impacts or works within the areas identified as moderate or high within **Figure 10** of the Austral Archaeology Preliminary Historical Heritage Assessment dated 28 April 2021 will require additional archaeological investigations and considerations in line with the recommendations of that report (see **Figure 39** below).



**Figure 10 Archaeological sensitivity**  
21005 Elm Park Neighbourhood Plan, Avondale PHAA

Source: NSW LPI Aerial

Drawn by: WA Date: 2021-04-09



**AUSTRAL**  
ARCHAEOLOGY

Figure 39. Austral Archaeological Sensitivity Map.



## 14.6 West Dapto Centres Master Plans

Section 14.6 outlines the West Dapto Centres Master Plans that have been adopted into the DCP for the purposes of this Part. Adopted West Dapto Centres Master Plan packages are available on Council's website under the West Dapto Project Page.

The West Dapto Centres Master Plans intend to deliver the overarching Chapter D16 objective:

- (a) enabling the development of the West Dapto Urban Release Area for residential, employment, industrial and environmental conservation areas in a manner consistent with the Wollongong LEP 2009, the West Dapto Vision 2018 and the West Dapto Structure Plan (**Figure 2**).

This Section provides structure and guidance for the future development of the West Dapto Town and Village Centres. It is aimed at achieving the vision for West Dapto:

*West Dapto will grow and develop as a series of integrated and connected communities. Set against the spectacular Illawarra Escarpment and a landscape of riparian valleys, these communities will integrate the natural and cultural heritage of the area with the new urban form.*

*The communities will be healthy, sustainable, and resilient with active and passive open space accessible by walkways, cycleways, and public transport. To support these new communities, local centres will provide shopping services, community services and jobs while employment lands will facilitate further opportunities for the region.*

*West Dapto will be supported by a long-term strategy to oversee the timely implementation of infrastructure to deliver sustainable and high-quality suburbs with diverse housing choices.*

Where a Centre Master Plan has been adopted into this Chapter, that Centre Master Plan must be incorporated into the relevant defined neighbourhood or concept development application.

### 14.6.1 Darkes Town Centre

Subject to future planning.

Located within the Forest Creek Defined Neighbourhood.

### 14.6.2 Wongawilli Village Centre

Located within the Wongawilli Defined Neighbourhood.

Wongawilli Village centre will provide a small convenience centre with a small grocer and some mixed retail shops. The centre will be designed focusing on activating the interface with the riparian boundary and its West Dapto Road frontage. Parking will be included in the village design with street parking along the secondary street and a parking lot area provided along the north, generally not visible from West Dapto Road, allowing built form to perform a clear street defining urban function. The village form will be guided by the concept design presented in **Figure 21**.

### 14.6.3 Jersey Farm Village Centre

Subject to future planning.

Located within the Jersey Farm Defined Neighbourhood.

### 14.6.4 Bong Bong Town Centre

Located within the Bong Bong Defined Neighbourhood.

In the area where Bong Bong Road adjoins the north-south arterial route a new district town centre is to be established, based on a north-south orientated main street (see **Figure 18** Bong Bong East and North Neighbourhood Plan). The Bong Bong Town Centre is to be the primary town centre in the release area. The Bong Bong Town Centre is to be a supermarket-based centre with a range of shops and would accommodate around 15,000m<sup>2</sup> of retail floor space.

### 14.6.5 Fowlers Village Centre

Within the Cleveland Road East Defined Neighbourhood the Fowlers Village Centre is a 1.9 ha site

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characterised by a local foothill and low-lying land. It is located on the southern side of Cleveland Road, approximately adjoining the intersection of Fowlers and Cleveland Road and is well connected to the Dapto Regional Centre to the east via Fowlers Road into Cleveland Road.

It is located within Stage 3 of West Dapto and intends to deliver:

- A well-connected village centre providing approximately 2,700sqm of local convenience shops and urban focal points within the residential areas.
- 29 Dwellings within the Village Centre (subject to supporting Local Environmental Plan planning controls).



LEGEND

 Fowlers Village Centre

Figure 40. Fowlers Village Centre - boundary.

## 1. Objectives

The key objectives of the Fowlers Village Centre Master Plan are:

1. To align streets along views to the Illawarra Escarpment including Mount Keira and Mount Kembla.
2. To create a walkable and compact centre that is accessible to the community via various transport modes.
3. To concentrate retail activities and support retail frontage along the Main Street.
4. To create a village character with a safe pedestrian environment.
5. To facilitate connectivity to the future Leisure and Recreational Centre and recreational open spaces along Fowlers Road.
6. To provide increased housing density and mix close to the Village Centre to support the Centre's viability.
7. To comply with the requirement of development siting adjacent to the Eastern Gas Pipeline.
8. To maintain efficient traffic movement whilst enhancing pedestrian amenity along Main Street.
9. To provide a consolidated parking strategy for retail uses.
10. To acknowledge and celebrate Aboriginal and Torres Strait Islander people's contribution and connection to country.

## 2. Relationships to other parts of the DCP

- Part B applies to development in centres, mixed, and residential uses within the Site. Chapter B4 Development in Centres and Peripheral Sales Precincts identifies Fowlers as a village centre and outlines the desired characteristics for these centres.
- Parts A and E of the DCP generally apply to all lands contained within the Fowlers Village Centre.
- In the event that the provisions contained in the Fowlers Village Centre Master Plan are inconsistent with the provisions of any other part of the DCP, the provisions of the Fowlers Village Centre Master Plan shall prevail to the extent of the inconsistency.

## 3. Future Desired Character of Fowlers Village Centre

**The vision for Fowlers Village Centre is to create:**

"A compact and local scale retail precinct, conveniently located for daily needs and accessible via public and active transport. Characterised by a main street which is aligned to capture views to Mount Keira and Mount Kembla, it celebrates place and balances pedestrian and vehicle movement."

**To enable this vision, the Village Centre is to deliver:**

- (a) **Celebrate the scenic landscape character:** Align the Main Street along view lines to Mount Keira and Mount Kembla.
- (b) **Active and healthy lifestyles:** Provide seamless pedestrian and cyclist connections into the village centre from the adjacent roadways, open space, and key destinations.
- (c) **Day-to-day convenience:** Concentrate supermarket, retail, and services along the Main Street with visibility and access from Cleveland Road.
- (d) **Housing mixes close to the centre:** Locate medium-density housing typologies close to the Village Centre providing housing choice with convenience.
- (e) **A high-quality main street:** that is vibrant and active by providing wide and comfortable areas for walking and staying and welcoming building frontages that allow visual and physical access to the goods and activities within.
  - public domain areas with visual links to the escarpment, Mount Keira and Mount Kembla through orientation of streets, breaks in buildings and distribution of building height.

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- development of an appropriate scale for a Village Centre, noting its place in the Centres Hierarchy and taking into account the impact of heights, setbacks and building separation on the surrounding public and private domain.
- a clear and efficient movement network for people, public transport, freight movements and private cars through clear roles and functions for streets.
- Physical and visual connections within the broader precinct between the Village Centre and the future West Dapto Leisure and Recreation Centre, Sports fields, and local park.
- A main street named to acknowledge and celebrate Aboriginal and Torres Strait Islander contribution and connection to country.

The indicative master plan for Fowlers Village Centre is illustrated in **Figure 41**.



**Figure 41. Fowlers Village Centre– Illustrative Master Plan.**

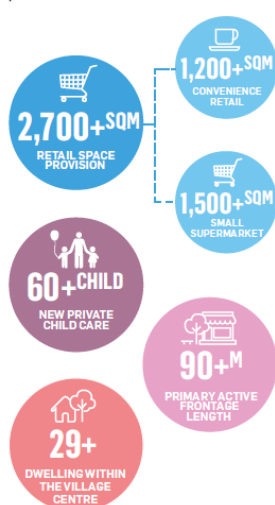
The Structure Plan for Fowlers Village Centre is shown at **Figure 42**.




## FOWLERS VILLAGE CENTRE

## STRUCTURE PLAN

The Structure Plan summarises the key elements of the Indicative Plan including the building uses, access and movement, natural assets, open space, public domain, place activation and infrastructure.













### LEGEND



 Fowlers Village Centre

## BUILDING USES







- Mixed Retail Supermarket and Convenience
- Retail (Convenience)
- Private Child Care
- Residential (Medium Density)

## ACCESS & MOVEMENT\*

-  Sub-arterial Road (Type 2) - 22.9m
-  Major Collector Road (Type 3) - 21.9m
-  Minor Collector Road (Type 4) - 20.95m
-  Main Street - 20m
-  Access Road (Type 6) - 17.1m
-  Access Place (Type 7B) - 13.5m
-  Laneway (Type 8) - 8m
-  Indicative Access Point
-  Proposed Bus Stop
-  Bus Route Realignment (South-Central Loop)

	At-grade Parking
	Indicative Loading Area
	Off-road Shared Path
	Road-side Shared Path
	Pedestrian Connection to the Main Street
	Signalised Primary Intersection
	Signalised Secondary Intersection
	Left In Left Out
	Roundabout



## NATURAL ASSETS

-  Mullet Creek Tributary
-  Core Riparian Zone
-  Proposed Retained Existing Trees
-  Recreational Open Space (Zoned RE1)
-  Visual Connections
-  2m Contour Lines

## OPEN SPACE, PUBLIC DOMAIN AND PLACE ACTIVATION

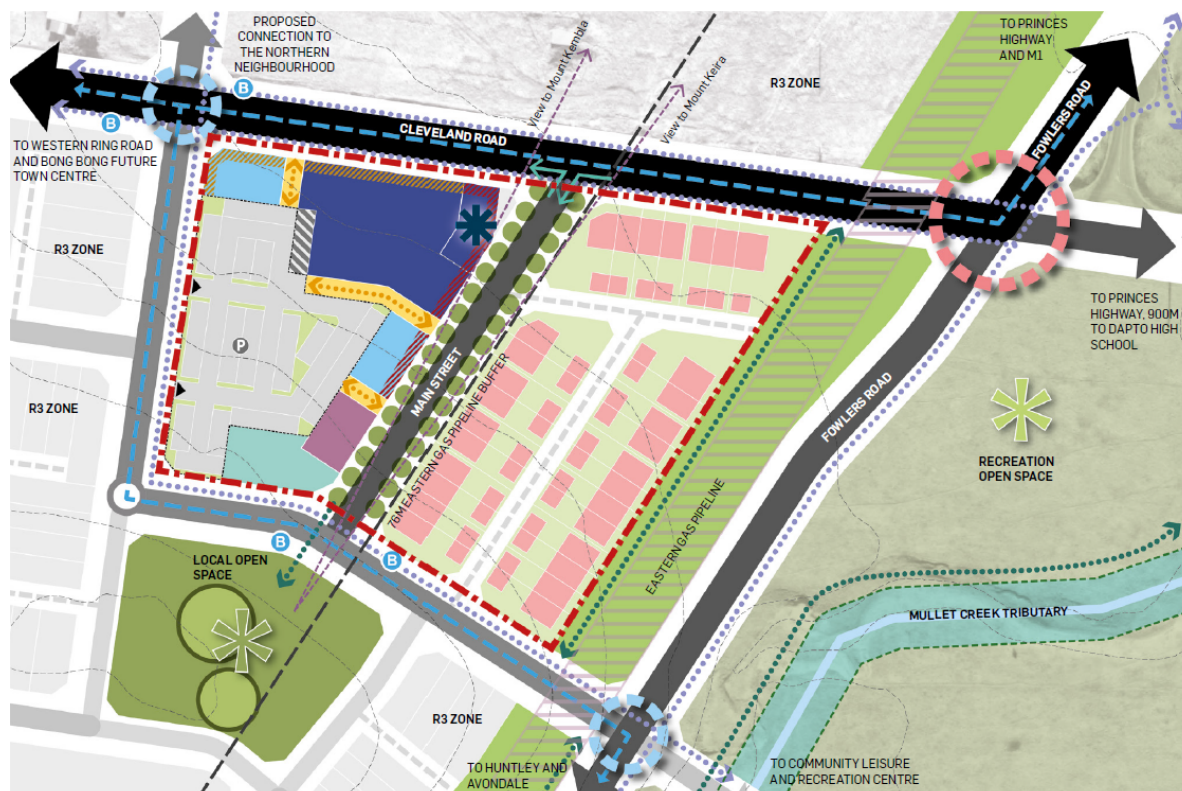
-  Pedestrian Link
-  Passive Open Space
-  Proposed Local Open Space
-  Private Child Care Outdoor Space
-  Ground Floor Private Open Space
-  Indicative Planting with At-grade Parking
-  Primary Active Frontage
-  Secondary Active Frontage
-  Key Marker Building

## INFRASTRUCTURE

 Jemena Eastern Gas Pipeline (EGP) Easement  
 Jemena Eastern Gas Pipeline (EGP) 76m Buffer Zone

**\*Note:**

- Final intersection arrangement is subject to future transport analysis.
- Final layout of road network is subject to design which may differ from that shown in the proposed structure plan.



**Figure 42. Structure Plan Fowlers Village Centre**

#### 14.6.6 Huntley Village Centre

Subject to future planning.

Located within the Avondale Road East Defined Neighbourhood (see **Figure 17**).

#### 14.6.7 Avondale Village centre

Subject to future planning.

Located within the Avondale Road Defined Neighbourhood (see **Figure 17**).

#### 14.6.8 Marshall Mount Town Centre

Located within the Town Core Defined Neighbourhood (see **Figure 17**).

Marshall Mount Town Centre is an 8.2 ha site in between Duck Creek and its tributary, nestled amongst local undulating hills and located at the intersection of Yallah Road and Marshall Mount Road.

It is located within Stage 5 of West Dapto and intends to deliver:

- 343 Dwellings within the Town Centre (some dwelling outcomes would be subject to supporting Local Environmental Plan planning controls).
- Marshall Mount Town Centre comprising approximately 8,100sqm of retail floor space in a traditional main street format providing retail shops, local convenience needs, local services, community facilities and the like.
- Protection of significant vegetation and unique landscape features of the area.
- Utilisation of Duck Creek as a focal feature of the community.

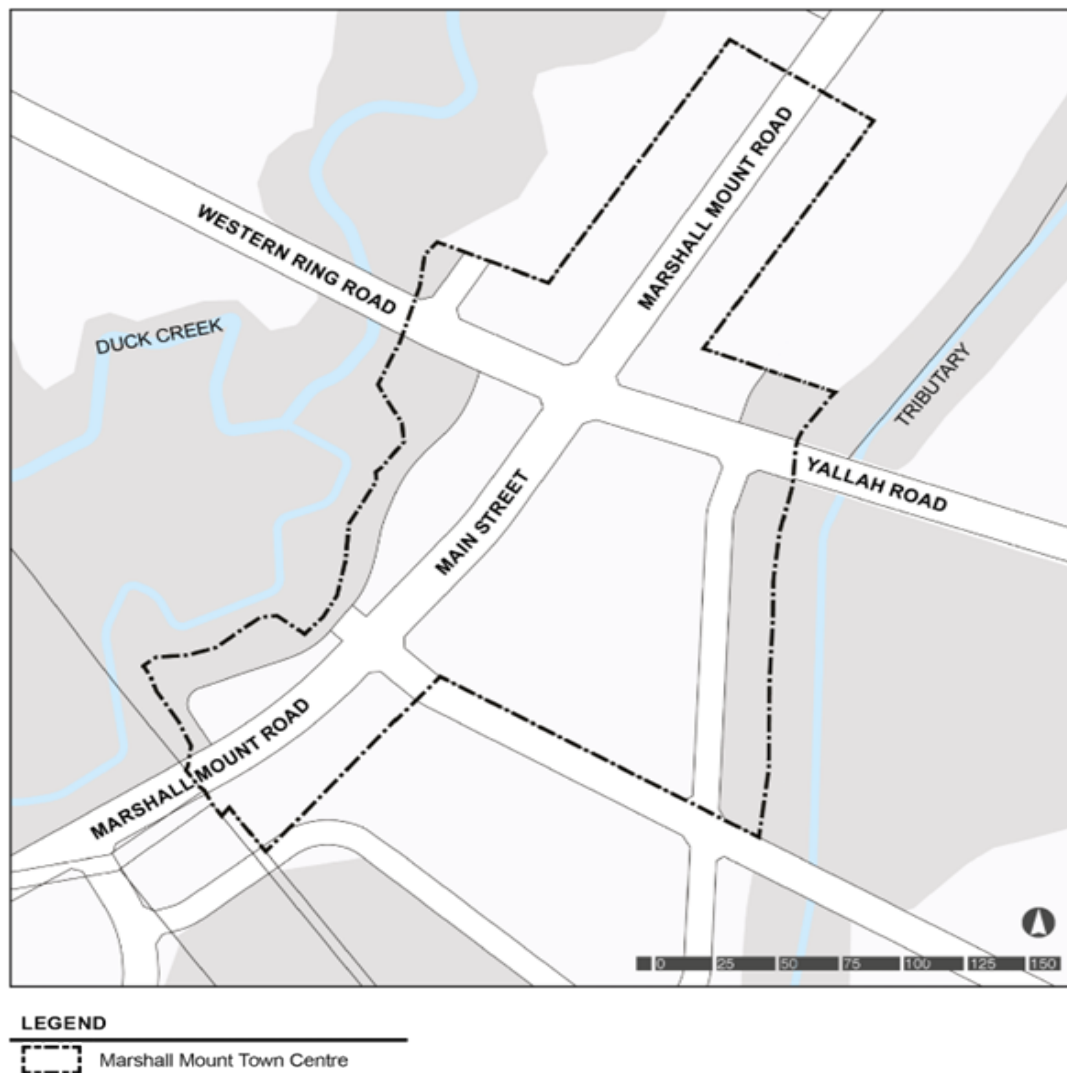


Figure 43. Marshall Mount Town Centre - boundary.

## 1. Objectives

The key objectives of the Marshall Mount Town Centre Precinct Plan are:

- To retain key views and orientate development to capture key views to the Illawarra Escarpment and existing Moreton Bay Fig Tree.
- To protect the biodiversity value along Duck Creek, its tributaries and riparian corridor.
- To minimise flood risk to the centre.
- To create a walkable and compact town centre that is accessible to the community via various transport modes.
- To encourage a viable Town Centre through a variety of uses and activities.
- To concentrate retail and community activities to support active frontages on both sides of the Main Street.
- To facilitate visual and physical connectivity to the adjacent recreational open space.
- To provide increased housing density and choice within and adjacent to the Town Centre to support the Centre's viability.

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- (i) To maintain efficient traffic flows whilst enhancing pedestrian amenity along the Main Street.
- (j) To provide a consolidated parking strategy within the Town Centre Core Precinct.
- (k) To acknowledge and celebrate Aboriginal and Torres Strait Islander contribution and connection to country.

## 2. Relationship to other parts of the DCP

- Part B applies to development in centres, mixed, and residential uses within the Site. Chapter B4 Development in Centres and Peripheral Sales Precincts identifies Marshall Mount as a town centre and outlines the desired characteristics for these centres.
- Parts A and E of the DCP generally apply to all lands contained within the Marshall Mount Town Centre.
- Part C of the DCP relates to Development within the E1– Local Centre and hence, applies to the lands contained within the Marshall Mount Town Centre.
- In the event that the provisions contained in the Marshall Mount Town Centre Master Plan are inconsistent with the provisions of any other part of the DCP, the provisions of the Marshall Mount Town Centre Master Plan shall prevail to the extent of the inconsistency.

## 3. Future Desired Character of Marshall Mount Town Centre

### The vision for Marshall Mount Town Centre is to create:

“A compact, mixed retail and community precinct with housing choice, conveniently located and accessible by active and public transport. It is characterised by a vibrant main street that balances pedestrian and vehicular movements, and local places for people that celebrate views to the scenic Illawarra Escarpment and the existing Moreton Bay Fig Tree.”

### To enable this vision, the Town Centre:

- (a) **Celebrates the local and scenic landscape character:** Establish visual connections to the existing Moreton Bay Fig Tree along the Fig Tree Link and a distant view to the Illawarra Escarpment including Mount Kembla from the Western Promenade.  
  
Aboriginal Cultural Heritage Assessment Report would be required and should inform final project outcomes for any project in the vicinity of the Moreton Bay Fig Tree.  
  
The Fig Tree Link Public Plaza will be named to acknowledge and celebrate Aboriginal and Torres Strait Islander people contribution and connection to country.
- (b) **Encourages active and healthy lifestyles:** Provide seamless pedestrian and cyclist connections into the Town Centre from the adjacent roadways, open space and key destinations.
- (c) **Provide day-to-day convenience:** Concentrate the supermarket and convenience retail along the Main Street and Fig Tree Link with visibility and access from Yallah Road.
- (d) **Become the community heart:** Deliver community facilities including a multi-purpose hall and a potential library within the Town Centre Core precinct co-located with the outdoor public plaza fronting the Moreton Bay Fig Tree.
- (e) **Provides a mix of housing choice close to the centre:** Medium to high-density housing typologies of shop top housing and residential flat building (apartments) providing housing choice with convenience.

The indicative master plan for Marshall Mount Town Centre is illustrated in **Figure 44**.



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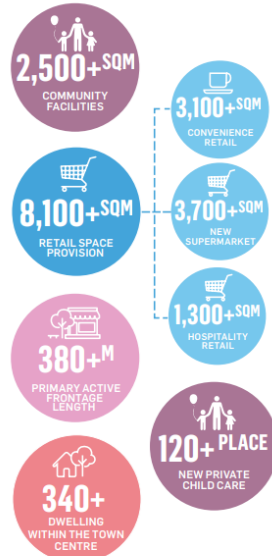
Figure 44. Marshall Mount Town Centre – Illustrative Master Plan.

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MARSHALL MOUNT TOWN CENTRE

STRUCTURE PLAN

The Structure Plan summarises the key elements of the Indicative Master Plan including natural assets and open space, access and movement, public domain and place activation and building uses.



LEGEND

Marshall Mount Town Centre

NATURAL ASSETS & OPEN SPACE

- Duck Creek and Tributary
- Core Riparian Zone
- 2m Contour Lines
- Existing Moreton Bay Fig Tree\*
- High Biodiversity Value Area
- Potential Indicative Stormwater Basin
- Western Promenade
- Passive Open Space
- Recreational Open Space (Zoned RE1)
- C2/C3 Zone Boundary
- Visual Connections

ACCESS & MOVEMENT\*\*

- Sub-arterial Road (Type 2) - 22.9m
- Sub-arterial Road (Type 2A) - 22.9m
- Major Collector Road (Type 3) - 21.9m
- Main Street - 22.9m
- Local Road (Type 5) - 18.8m

- Access Road (Type 6) - 17.1m
- Perimeter Road - 14.5m
- Laneway (Type 8) - 8m
- Indicative Access Point
- Indicative Access to Basement Parking
- Proposed Bus Stop
- Bus Route (Southern Loop)
- At-Grade Parking
- Ground Level Podium Parking
- Indicative Loading Area
- Off-road Shared Path
- Road-side Shared Path
- Pedestrian Link
- Signalised Primary Intersection
- Signalised Secondary Intersection
- Left in Left Out with Right Turn
- Left in Left Out
- Roundabout
- Indicative Signalised Pedestrian Crossing Location

OPEN SPACE, PUBLIC DOMAIN AND PLACE ACTIVATION

- Fig Tree Link
- Hardscape Open Space
- Ground Level Communal Open Space
- Podium Rooftop Communal Open Space
- Outdoor Space for Private Child Care
- Primary Retail Active Frontage
- Secondary Retail Active Frontage
- Primary Community Active Frontage
- Secondary Community Active Frontage
- Key Building Markers

BUILDING USES

- Mixed Retail (Convenience) / Supermarket and Residential
- Mixed Community and Retail (Convenience)
- Mixed Retail (Stand-alone / Hospitality) and Residential
- Retail (Stand-alone / Hospitality)
- Private Child Care
- Residential Flat Building

\*Note:

- Aboriginal Cultural Heritage Assessment Report would be required and should inform final project outcomes for any project in the vicinity of the Moreton Bay Fig Tree, including but not limited to, future community facilities, stormwater basin, shared paths and road.

\*\*Note:

- Final intersection arrangement is subject to future transport analysis.
- Final layout of road network is subject to design which may differ from that shown in the proposed structure plan.

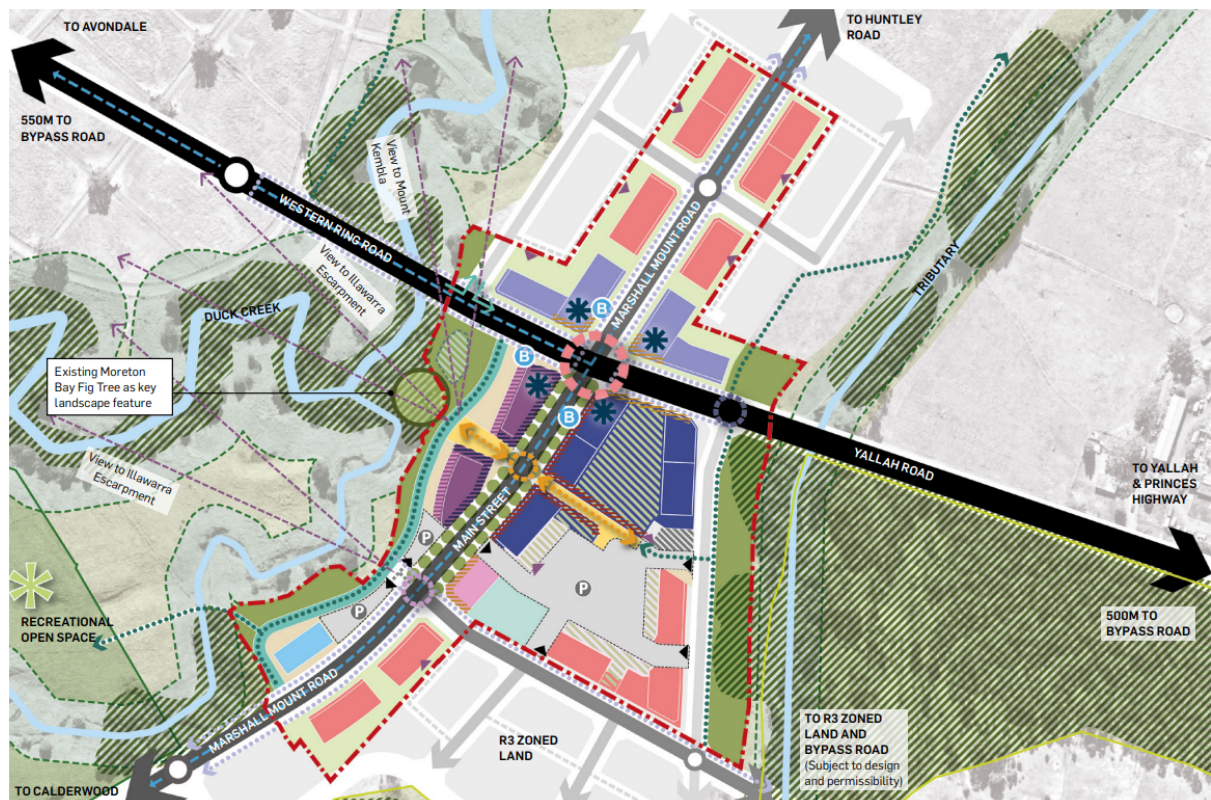


Figure 45. Structure Plan Marshall Mount Town Centre



## 15. Matters to be addressed in Development Applications

This chapter applies to development applications in the West Dapto Urban Release Area.

Documentation accompanying a development application for subdivision will also have to provide more detailed site-specific information and specialist reports, addressing issues and **all relevant matters required by this chapter and include detailed information** such as:

- Detailed site survey prepared by a registered surveyor.
- Development plans – lot layout, earthworks, detailed road designs, landscape plans, subdivision stages (if any) (Chapters B1, B2, B3, B4, D16 and E19).
- Flora and fauna assessment and future management (Chapter E18).
- Riparian land management (Chapter E23).
- Drainage/flooding/water quality modelling, WSUD (Chapters E13, E14, and E15).
- Soil erosion and sediment control (Chapter E22).
- Land contamination assessment (Chapter E20).
- Bushfire management (Chapter E16).
- Traffic assessment (Chapter E3).
- Aboriginal Heritage assessment (Chapters E10 and E11).
- Noise assessment (where relevant) (Infrastructure SEPP).
- Pedestrian and bicycle routes, including accessibility for persons living with a disability (Chapter E1).
- Crime Prevention through Environmental Design (Chapter E2), etc.

The documentation accompanying a development application for a dwelling house on a newly subdivided lot should address Parts A and B1 (dwelling houses) of this DCP and any variations to the generic controls under this chapter (e.g. the standard setbacks in individual neighbourhoods may have been varied).

An application for a dwelling house can also be undertaken in accordance with the requirements of SEPP Exempt and Complying Development, which can be assessed by Council or a Private Certifier.

**Documentation accompanying a Development Application for development proposed within a centre identified in Section 14.6 of this Chapter is to demonstrate how the proposed development will contribute to the objectives and desired future character of the relevant centre and where required meet any specific centre development controls.**