

Wollongong Local Planning Panel Assessment Report | 24 September 2020

WLPP No.	Item No. 3
DA No.	DA-2019/1124
Proposal	Residential - demolition of existing dwelling house and construction of a multi dwelling housing development
Property	92-94 Princes Highway, THIRROUL NSW 2515
Applicant	Design Workshop Australia
Responsible Team	Development Assessment and Certification - City Wide Team (BM)

ASSESSMENT REPORT AND RECOMMENDATION

Executive Summary

Reason for consideration by Local Planning Panel

The proposal has been referred to the WLPP for **determination** pursuant to part 2(b) of Schedule 2 of the Local Planning Panels Direction as the application is the subject of 10 or more unique submissions by way of objection.

Proposal

The proposal is for demolition of existing structures, tree removal and construction of multi dwelling housing - 6 units with associated carparking and infrastructure.

Permissibility

The site is zoned R2 Low Density Residential pursuant to Wollongong Local Environmental Plan (WLEP) 2009. The proposal is categorised as a multi dwelling development and is permissible in the zone with development consent.

Consultation

The proposal was exhibited in accordance with Appendix 1 of the Wollongong Development Control Plan 2009 notified between the 24 October 2019 and 13 November 2019. 18 submissions were received during this period.

The submissions received are discussed at section 1.5 of the assessment report

Internal

Details of the proposal were referred to Council's Stormwater, Traffic, Landscape, Environment, Geotech and Heritage divisions for assessment. Satisfactory referral advice, comments and/or recommended conditions were provided in each instance except for Traffic.

External

Details of the proposal were referred to Transport for NSW (TfNSW) and Rural Fire Service (RFS). TfNSW provided comments subject to Council's consideration of their recommended conditions and other concerns. The NSW RFS provided conditional support.

Main Issues

The main issues are,

- Traffic and safety concerns
- Non-compliance to development controls related to side setbacks and link to traffic management matters

Recommendation

It is recommended that DA-2019/1124 be **refused**.

1 APPLICATION OVERVIEW

1.1 PLANNING CONTROLS

The following planning controls apply to the development:

State Environmental Planning Policies:

- SEPP No. 55 – Remediation of Land
- SEPP (Building Sustainability Index: BASIX) 2004
- SEPP Infrastructure -2007

Local Environmental Planning Policies:

- Wollongong Local Environmental Plan (WLEP) 2009

Development Control Plans:

- Wollongong Development Control Plan 2009

Other policies

- Wollongong City-Wide Development Contributions Plan 2019
- Biodiversity Conservation Act 2016

1.2 DETAILED DESCRIPTION OF PROPOSAL

The proposal is for the construction of a multi dwelling development comprising 6 units as detailed below:

Site preparation

- Demolition of existing dwelling and structures
- Earthworks for the preparation of the building works and tree removal.

Works / Construction / building details

- Construction of six (6) x three (3) bedroom split level double storey dwellings with rear and front upper level balconies
- On-site parking for a total of fourteen (14) cars, including twelve (12) resident parking spaces in the form of tandem garage spaces for 2 cars per dwelling, and two (2) visitor parking spaces and a holding bay;
Removal of selected trees, with the provision of associated supplementary replacement plantings/landscaping and stormwater drainage.

Landscaped areas are provided in the form of a deep soil zone at the rear southern part and perimeter landscaping.

Access for vehicles is via a driveway off the Princes Highway/Bulli Pass along the north western side boundary.

Bin Storage collection area located towards the eastern boundary of the site and adjacent to the internal driveway.

Architectural Plans are at Attachment 1.



Figure 1: Site Plan

1.3 BACKGROUND

DE-2018/214, Residential - multi dwelling housing

: PL-2019/33, Residential - multi dwelling housing

BA-1977/2044, Dwelling

: BA-1978/801, Fence

: DA-2019/1124, Residential - demolition of existing dwelling house and construction of a multi dwelling housing development

A voluntary Design Review (DE-2018/214) process and a Pre-lodgement meeting (PL-2019/33) were held. A copy is provided at Attachment 2.

Customer service actions

There are no outstanding customer service requests of relevance to the development.

1.4 SITE DESCRIPTION

The site is located at 92-94 Princes Highway, THIRROUL NSW 2515 and the title reference is Lot 2 DP 202822.

The site is an allotment with 42.48m street frontage and splayed boundaries of approximate depths 36m and 40m to the western and eastern sides. The site has moderate crossfall towards the south east corner. A number of trees located on the site are to be removed as part of this development application.

Adjoining developments are single storey dwellings. The locality is characterised by low density residential developments. A heritage listed dwelling is located opposite and across the road from the site.

Property constraints

- Council records identify the land as being Bushfire Prone, Unstable and partly Ecological Sensitive land. The NSW RFS and Council's officers have reviewed the proposal and did not raise concerns related to these constraints.

There are no restrictions on the Title.



Figure 2: Aerial photograph

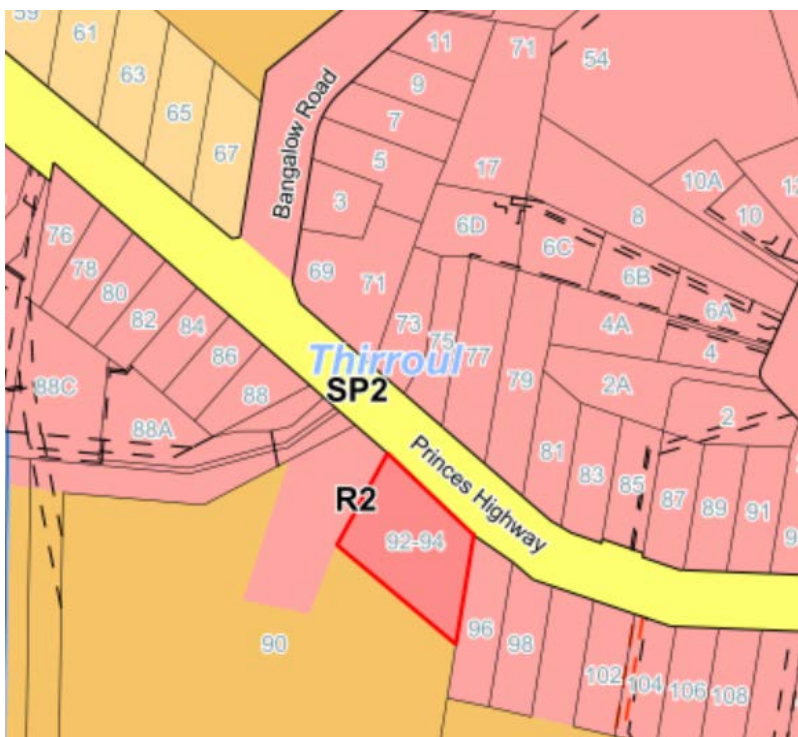


Figure 3: WLEP 2009 zoning map

1.5 SUBMISSIONS

The application was notified in accordance with WDCP 2009 Appendix 1: Public Notification and Advertising. This included a notice in The Wollongong Advertiser. Eighteen (18) submissions were received and the issues identified are discussed below.

Table 1: Submissions

Concern	Comment
1. Traffic and Safety impacts, Parking deficiency	<p>TfNSW and Council's Traffic engineer have reviewed the applicant's Traffic assessment plans and require off site road works in Bangalow Road by way of a formed cul-de-sac to ensure minimal traffic impacts.</p> <p>The proposed roundabout road works at the midpoint of Bangalow Road as presented by the applicant are not supported. Changes are warranted and would be subject to further consultations with the residents and it is considered a Planning Agreement process is the appropriate mechanism.</p> <p>Separately TfNSW have expressed concern with street waste collection at this location and cumulative impacts over time should similar development in the locality request the same consideration. Traffic queuing to enter the site is also raised as a concern.</p> <p>The development is not supported due to the unresolved traffic matters.</p>
2. Non-compliances with Setback and Number of storeys	<p>Though the development may be considered as meeting the objectives of the setback development controls the siting of the building/garages is considered to pose vehicle manoeuvring constraints at the access points that could lead to traffic queuing on Bulli Pass. The development is of 2 storeys though the side facades appear to show a portion as of 3 storeys.</p> <p>The development proposal is not supported for the traffic related matters.</p>
3. Out of character with streetscape	<p>The proposed yield involving variation to side setbacks and traffic issues are considered excessive for the site and inconsistent with the existing character for the locality - which is not multi dwelling development.</p>
4. Tree removal	<p>Council's Landscape and Environment officers have reviewed the proposal and provided satisfactory comments. Conditions are proposed for compensatory planting should consent be granted.</p>
5. Noise from balconies	<p>The balconies could be considered as being setback adequately from the site boundaries so as not cause adverse noise impacts to dwellings on neighbouring sites.</p>

Table 2: Number of concerns raised in submissions

Concern	1	2	3	4	5
Frequency	12	10	9	1	1

1.6 CONSULTATION

1.6.1 INTERNAL CONSULTATION

Traffic Engineer

Council's Traffic Engineer does not support the proposal and provided the following comments, *"From discussions with the Strategic Traffic Team, the concept of a U-turn facility adjacent to No's 3 and 5 Bangalow Road isn't supported due to the impact in lost street parking (approx. 7 spaces looking at the developer's plan 18172 T7 Rev. D). The proposed mid-block turning area doesn't provide a benefit for residents of Bangalow Road but will have additional vehicles turning across the travel lanes.*

Instead the preference is for a turning head to be constructed by the developer at the cul-de-sac end of Bangalow Road where there is not a significant loss of street parking, and the community will obtain a benefit. The road reserve is 20 metres wide and should accommodate a turning area which meets the standards. The applicant needs to provide a concept design for the cul-de-sac design for consideration by Council. Consultation would be required with affected property owners in Bangalow Road.

Based on the above, the proposal is not supported as presented and it is considered a voluntary Planning Agreement is the appropriate process in this regard.

Stormwater Engineer

Council's Stormwater Engineer has reviewed the proposal and provided a satisfactory referral subject to conditions.

Landscape Architect

Council's Landscape Officer raised initial concerns about submitted information in relation to impact to the street frontage area for landscaping and width of landscape beds on site.

Additional information submitted by the applicant has been reviewed and is satisfactory subject to conditions.

Environment Officer

Council's environment officer provided satisfactory referral comments.

Geotech

No concerns were raised subject to conditions.

Heritage Officer

The site itself does not contain heritage items but is in proximity to identified item of local heritage significance located opposite and across the road from the site.

No significant concerns were raised however commentary suggested recessive colour tones in finishes and reduced bulk/ scale would be preferable as relates to the nearby Heritage Item.

1.6.2 EXTERNAL CONSULTATION

Transport for New South Wales (TfNSW)

Princes Highway/Bulli Pass is considered a key State Road under the control of TfNSW. Initial referral advice received identified significant issues with regard to vehicular access and egress from the multi dwelling site. The applicant sought multiple opportunities to submit additional information to resolve concerns on the increased traffic congestion and safety on (Bulli Pass) fronting subject site. TfNSW on 8 September provided conditional supporting aspects of the proposal and raising further concerns for other aspects:

- *TfNSW supports the proposed U-turn facility on Bangalow Road as it will improve the safety of the existing layout for vehicles wishing to turn around to travel east on Princes Highway.*

- *TfNSW has concerns with the stacked parking arrangements, particularly within proposed Units 1 and 2 as this has the potential to create queuing at the entrance of the development. Although a holding bay is proposed, TfNSW believes this may be used as parking.*
- *TfNSW has concerns with the proposed garbage collection arrangements and proposed bin capacity provision. If the bins are not collected from the roadside collection point in a timely manner this may cause a road safety hazard. TfNSW believes occupants would be encouraged to request additional bins due to the limited capacity proposed.*

(TfNSW response letter provided at Attachment 4).

2 OTHER LEGISLATION

2.1 NSW BIODIVERSITY CONSERVATION ACT 2016

Section 1.7 of the *Environmental Planning and Assessment Act 1979* (EP&A Act) provides that Act has effect subject to the provisions of Part 7 of the *Biodiversity Conservation Act 2016* (BC Act).

Part 7 of the BC Act relates to Biodiversity assessment and approvals under the EP&A Act where it contains additional requirements with respect to assessments, consents and approvals under this Act.

The site is not identified as being of high biodiversity value on the Biodiversity Values Map.

Council's Environmental Officer has concluded that the proposed development including tree removals is not expected to impact threatened species or ecological communities, or their habitats. The development would therefore not be considered to result in adverse impacts on biodiversity and is consistent with the provisions of the *Biodiversity Conservation Act 2016*.

3 ENVIRONMENTAL PLANNING AND ASSESSMENT ACT 1979 – 4.15 EVALUATION

3.2 SECTION 4.15(1)(A)(1) ANY ENVIRONMENTAL PLANNING INSTRUMENT

3.2.1 STATE ENVIRONMENTAL PLANNING POLICY NO. 55 – REMEDIATION OF LAND

7 Contamination and remediation to be considered in determining development application

(1) A consent authority must not consent to the carrying out of any development on land unless:

(a) it has considered whether the land is contaminated, and

(b) if the land is contaminated, it is satisfied that the land is suitable in its contaminated state (or will be suitable, after remediation) for the purpose for which the development is proposed to be carried out, and

(c) if the land requires remediation to be made suitable for the purpose for which the development is proposed to be carried out, it is satisfied that the land will be remediated before the land is used for that purpose.

(2) Before determining an application for consent to carry out development that would involve a change of use on any of the land specified in subclause (4), the consent authority must consider a report specifying the findings of a preliminary investigation of the land concerned carried out in accordance with the contaminated land planning guidelines.

(3) The applicant for development consent must carry out the investigation required by subclause (2) and must provide a report on it to the consent authority. The consent authority may require the applicant to carry out, and provide a report on, a detailed investigation (as referred to in the

contaminated land planning guidelines) if it considers that the findings of the preliminary investigation warrant such an investigation.

(4) The land concerned is:

(a) land that is within an investigation area,

(b) land on which development for a purpose referred to in Table 1 to the contaminated land planning guidelines is being, or is known to have been, carried out,

(c) to the extent to which it is proposed to carry out development on it for residential, educational, recreational or child care purposes, or for the purposes of a hospital—land:

(i) in relation to which there is no knowledge (or incomplete knowledge) as to whether development for a purpose referred to in Table 1 to the contaminated land planning guidelines has been carried out, and

(ii) on which it would have been lawful to carry out such development during any period in respect of which there is no knowledge (or incomplete knowledge).

A desktop audit of previous land uses does not indicate any historic use that would contribute to the contamination of the site. There are no significant earthworks proposed and the proposal does not comprise a change of use. Council's Environment Officer has reviewed the proposal and no concerns are raised in regard to contamination as relates to the intended use of the land. The requirements of clause 7 are considered as satisfied.

3.2.2 STATE ENVIRONMENTAL PLANNING POLICY (BUILDING SUSTAINABILITY INDEX: BASIX) 2004

The proposal is BASIX affected development to which this policy applies. In accordance with Schedule 1, Part 1, 2A of the Environmental Planning and Assessment Regulation 2000, a BASIX Certificate has been submitted in support of the application demonstrating that the proposed scheme achieves the BASIX targets.

The BASIX certificate was issued no earlier than 3 months before the date on which the development application was lodged.

3.2.3 STATE ENVIRONMENTAL PLANNING POLICY (INFRASTRUCTURE) 2007

101 Development with frontage to classified road

(1) The objectives of this clause are—

(a) to ensure that new development does not compromise the effective and ongoing operation and function of classified roads, and

(b) to prevent or reduce the potential impact of traffic noise and vehicle emission on development adjacent to classified roads.

(2) The consent authority must not grant consent to development on land that has a frontage to a classified road unless it is satisfied that—

(a) where practicable and safe, vehicular access to the land is provided by a road other than the classified road, and

(b) the safety, efficiency and ongoing operation of the classified road will not be adversely affected by the development as a result of—

(i) the design of the vehicular access to the land, or

(ii) the emission of smoke or dust from the development, or

(iii) the nature, volume or frequency of vehicles using the classified road to gain access to the land, and

(c) the development is of a type that is not sensitive to traffic noise or vehicle emissions, or is appropriately located and designed, or includes measures, to ameliorate potential traffic noise or vehicle emissions within the site of the development arising from the adjacent classified road.

The proposal was referred to TfNSW under Cl.101 as the development fronts Princes Highway/Bulli Pass which is a classified road.

TfNSW did not support the proposal twice initially as the development is considered to generate additional traffic and the impact of this traffic needed to be considered and adequately mitigated.

The final submission to TfNSW by the applicant on 26 August 2020 was supported on conditions and Council's further consideration to matters related to traffic mitigation works were requested in their response dated 8 September 2020. See also section 1.6

3.2.4 WOLLONGONG LOCAL ENVIRONMENTAL PLAN 2009

Clause 1.4 Definitions

Multi dwelling housing means 3 or more dwellings (whether attached or detached) on one lot of land, each with access at ground level, but does not include a residential flat building.

Part 2 Permitted or prohibited development

Clause 2.2 – zoning of land to which Plan applies

The zoning map identifies the land as being zoned R2 Low Density Residential

Clause 2.3 – Zone objectives and land use table

The objectives of the zone are as follows:

- *To provide for the housing needs of the community within a low density residential environment.*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents.*

The proposal is considered to provide housing needs within the zone.

The land use table permits the following uses in the zone.

*Attached dwellings; Bed and breakfast accommodation; Boarding houses; Boat launching ramps; Child care centres; Community facilities; Dual occupancies; Dwelling houses; Environmental facilities; Exhibition homes; Exhibition villages; Group homes; Health consulting rooms; Home-based child care; Hospitals; Hostels; Information and education facilities; Jetties; **Multi dwelling housing**; Neighbourhood shops; Places of public worship; Recreation areas; Recreation facilities (indoor); Recreation facilities (outdoor); Residential flat buildings; Respite day care centres; Roads; Semi-detached dwellings; Seniors housing; Shop top housing; Signage; Veterinary hospitals*

The proposal is categorised as a multi dwelling housing as described above and is permissible in the zone with development consent.

Clause 2.7 Demolition requires development consent

The application proposes demolition of existing building.

Part 4 Principal development standards

Clause 4.3 Height of buildings

The proposed building height of maximum 8.8m does not exceed the maximum of 9 permitted for the site.

Clause 4.4 Floor space ratio

Maximum FSR permitted for the zone: 0.5:1

Site area: 1709.1 m²

GFA of Units:

Each Unit 138 m²

Total GFA 855.1 m²

FSR: $855.1 \text{ m}^2 / 1709.1 \text{ m}^2 = 0.5:1$

**areas confirmed via software Trapeze and 216sqm of GFA excluded for car parking purposes.

The proposal complies with the development standard.

Clause 5.10 Heritage Conservation

The site itself does not contain heritage items but is in proximity to identified item of local heritage significance located opposite and across the road from the site.

No significant concerns were raised however commentary suggested recessive colour tones in finishes and reduced bulk/ scale would be preferable as relates to the nearby Heritage Item.

Part 7 Local provisions – general

Clause 7.1 Public utility infrastructure

The development site is already serviced by electricity, water and sewage services which can be augmented to service the development.

Clause 7.2 Natural Resource Sensitive Lands - Biodiversity

The site is identified partially Natural Resource Sensitive Land along the south-western boundary. Council's Environment officer has provided comments that as the small number of trees in this area are part of the managed landscape and mostly exotic or landscape trees this is not considered to be a limiting constraint due to the nature of the vegetation on site.

Clause 7.6 Earthworks

The proposal comprises earthworks related to the construction of the buildings and related infrastructure and landscaping.

3.3 SECTION 4.15(1)(A)(II) ANY PROPOSED INSTRUMENT

N/A

3.4 SECTION 4.15(1)(A)(III) ANY DEVELOPMENT CONTROL PLAN

3.4.1 WOLLONGONG DEVELOPMENT CONTROL PLAN 2009

CHAPTER A1 – INTRODUCTION

8 Variations to development controls in the DCP

Clause 5.4 Side Setbacks:

The control being varied is *R2 Low Density Residential Zone 0.8 x ceiling height*

(variation statement submitted by applicant at Attachment 3)

Encroachment- Ground levels of Unit 1 & Unit 6 and Level 1 of Unit 1

<i>Boundary</i>	<i>Calculated setback</i>	<i>Proposed setback</i>
<i>Western side Unit 1</i>	<i>Varies from: Ground level: 2.64m to 4.8m Level 1: 4.8m to 6.4m</i>	<i>Ground level: Consistent 3.1m Level 1: Consistent 3.1</i>
<i>Eastern side Unit 6</i>	<i>Variable from: Ground level: 3.2m to 4m Level 1: 5.52m to 6.16m</i>	<i>Ground level: 2.65m to 9m Level 1: 2.65m to 6.9m</i>

Though the reduced setbacks (which has resulted from desired yield) may be considered to have minimal adverse impacts on directly adjoining neighbours in terms of overlooking or overshadowing, the siting of development components, particularly Unit 1 and Unit 2 and vehicle movement generated from the development are cumulative reasons for additional traffic impacts which is the underlying unresolved assessment issue of this proposal.

The requested variation is not supported for the following reasons:

- TfNSW has raised concerns on manoeuvring of cars in and out of the tandem garage arrangements particularly from Unit 1 and Unit 2 that could cause potential queuing at the entry point and create traffic impacts on Princes Highway/Bulli Pass.
- A compliant setback can be achieved with a reduced yield and/ or lesser smaller dwelling units on site, thereby reducing consequent traffic issues.

CHAPTER A2 – ECOLOGICALLY SUSTAINABLE DEVELOPMENT

Development controls to improve the sustainability of development throughout Wollongong are integrated into the relevant chapters of this DCP.

Generally speaking, the proposal is considered to be consistent with the principles of Ecologically Sustainable Development.

CHAPTER B1 – RESIDENTIAL DEVELOPMENT

4.0 General Residential controls

<i>Controls/objectives</i>	<i>Comment</i>	<i>Compliance</i>
<u>4.1 Maximum Number of Storeys</u>		
<ul style="list-style-type: none">• R2 max height of 9m or two storey• R3 max height of 13m or 3 storeys	R2 zone – Units are of 2 storeys at any point on ground.	Yes

<ul style="list-style-type: none"> • Battle axe allotments - 1 storey • Ancillary structures – 1 storey • Built form that has a positive impact on the visual amenity of the area and addresses site constraints and overlooking of neighbouring properties • In R2 Low Density Residential zones, where development occurs within the 8m rear setback the development is limited to single storey 	<p>The development appears to have 3 storeys on side elevations where the stairwell is located for the side units.</p>	
<p><u>4.2 Front Setbacks</u></p> <ul style="list-style-type: none"> • 1. Infill 6m min but less dependent on street character • Garages and carports 5.5m min • Greenfield sites 4m min • 2. Corner allotments • Infill setback 6m min • Secondary buildings setback 3m min except garages 5.5m min 	<p>Front building line of more than 6m is maintained</p>	<p>Yes</p>
<p><u>4.9 Fences</u></p> <ul style="list-style-type: none"> • Fences must be constructed to allow natural flow of stormwater or runoff. • Fences within front and secondary building lines should be mainly constructed of transparent fence materials. • Any fence or related retaining wall within the front setback from the primary road frontage must be a max 1.2m in height 	<p>Complies</p>	<p>Yes</p>
<p><u>4.11 Storage Facilities</u></p> <ul style="list-style-type: none"> • Studio/1 bedroom- 6m³ storage volume to 3m² storage area • 2 bedroom- 8m³ storage volume to 4m² storage area • 3 bedroom- 10m³ storage volume to 5m² storage area 	<p>Storage considered to be adequate.</p>	
<p><u>4.15 Development near the coastline</u></p>	<p>NA</p>	

<p><u>4.16 View sharing</u></p> <ul style="list-style-type: none"> To protect and enhance view sharing, significant view corridors A range of view sharing measures to be considered for building design 	<p>No adverse view impacts anticipated from the proposed development.</p>	
<p><u>4.17. Retaining walls</u></p> <ul style="list-style-type: none"> To ensure well designed retaining walls that are structurally sound 	<p>retaining walls within the site proposed</p>	<p>Yes</p>

5.0 Attached dwellings and multi -dwelling housing

<i>Controls/objectives</i>	<i>Comment</i>	<i>Compliance</i>
<p><u>5.1 Minimum Site Width Requirement</u></p> <p>Minimum 18m</p>	<p>A minimum site width of 42.48m at the street front.</p>	<p>Yes</p>
<p><u>5.2 Number of Storeys</u></p> <p>R2 zone – 2 storeys</p>	<p>The application proposes 2 storeys for the development and height below 9m.</p> <p>Units are of 2 storeys at any point on ground.</p> <p>The development displays 3 storey appearance on side elevations where the stairwell is located for the side units.</p>	<p>Yes</p>
<p><u>5.3 Front Setbacks</u></p> <p>6m min required to facade</p>	<p>>6m from the street frontage</p>	<p>Yes</p>
<p><u>5.4 Side and Rear Setbacks</u></p> <p>R2 low density residential zone requires a minimum side/rear setback of 0.8 x ceiling height</p> <p>Where balconies or windows of living areas face the rear boundary at first floor level or above, a minimum 1.0m x ceiling height is required</p>	<p>Variations proposed are not capable of support (see section A1 under 3.4.1 of this report)</p>	<p>No</p>
<p><u>5.5 Building Character and Form</u></p>	<p>Proposal is not considered to fit sympathetically with the existing street character. The scale and form of the development exhibits visual dominance of garages on to the street and non-compliant setbacks to the side boundaries.</p>	<p>No</p>

<p><u>5.6 Access / Driveway Requirements</u></p>	<p>Building (garages for units) located close to the access point from the street is considered to create traffic impacts on the road and within the site in terms of manoeuvring of vehicles in and out of the site.</p> <p>The proposal is considered to impact on traffic flow on Princes Highway (See Chapter E3 below).</p>	No
<p><u>5.7 Car Parking Requirements</u></p>	<p>Refer chapter E3</p>	
<p>Car parking to be located behind front setback</p>	<p>Car parking for Unit 1 and Unit 2 located close to the entry and exit point to the development is considered to create traffic impacts on the road and within the site in terms of manoeuvring of vehicles in and out of the site.</p>	No
<p><u>5.8 Landscaping Requirements</u></p>	<p>Appears to achieve compliance</p> <p>Council's Landscape Architect has provided a satisfactory referral.</p>	Yes
<p>Min. 30% of site area must be provided as landscaped area</p> <p>Min. 1.5m wide landscaping beds alongside & rear boundaries</p>	<p>Sufficient DSZ is provided.</p>	Yes
<p><u>5.9 Deep Soil Planting</u></p>	<p>POS is provided on upper level directly off living areas.</p> <p>Shadow diagrams have been lodged indicating that the POS of at least 70% of the dwellings can receive a minimum of three hours of sunlight on June 21 to 50% of the POS.</p>	Yes
<p><u>5.11 Private Open Space</u></p>	<p>Ground level POS with 4m x 5m minimum dimensions</p> <p>70% of dwellings must receive minimum 3 hours direct sunlight to POS between 9am-3pm on June 21</p> <p>Design private open spaces so that they act as direct extensions of the living areas of the dwellings they serve.</p>	Yes

Clearly define private open space through use of planting, fencing or landscaping features.

Screen private open space where appropriate to ensure privacy.

5.12 Solar Access Requirements

Windows to living rooms of adjoining dwellings must receive 3 hours of sunlight between 9.00am and 3.00pm on 21 June.

At least 50% of the private open areas of adjoining residential properties must receive at least 3 hours of sunlight between 9.00am and 3.00pm on June 21.

The primary balcony of at least 70% of the dwellings within a multi dwelling housing development shall receive a minimum of three hours of direct sunlight between 9.00am and 3.00pm on June 21.

Windows to north facing living rooms for each of the subject dwellings in the development must receive at least 3 hours of sunlight between 9.00am and 3.00pm on 21 June.

At least 50% of the private open space area for each of the subject dwellings in the development must receive at least 3 hours of sunlight between 9.00am and 3.00pm on 21 June.

Submitted shadow diagrams indicate that the proposed units are situated at adequate distances from neighbouring dwellings so as not to cause shadow impact on to the living rooms and POS.

Yes

CHAPTER D1 – CHARACTER STATEMENTS

Thirroul

Future desired character for Thirroul is mainly for residential development will remain primarily low density in nature. Some restricted medium density within close vicinity to the Thirroul village centre and the railway station is envisaged. Any new housing must be sympathetic with the existing Thirroul village built form and streetscape, particularly in older areas with weatherboard houses, pitched roofs and wide eaves.

Any dwellings should be designed to minimise the scale and bulk of the development through well articulated building forms.

The proposal is considered to have impacts on the existing and desired future character for the locality. The bulk and scale of the development with portions of 3 storey appearance and the units sited in a single row is considered to have uncharacteristic impacts to the streetscape.

CHAPTER E2: CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN

The design and layout of the development is expected to be consistent with this Chapter providing casual surveillance opportunities to the street and throughout the site.

CHAPTER E3: CAR PARKING, ACCESS, SERVICING/LOADING FACILITIES AND TRAFFIC MANAGEMENT

The car parking rates under Chapter E3 for multi dwelling housing are as follows:

1 car parking space per dwelling (<70m²) or 1.5 car parking spaces per dwelling (70-110m²) or 2 car parking spaces per dwelling (>110m²), plus 0.2 car parking spaces per dwelling for visitors, 1 bicycle space per 3 dwellings (residents) and 1 bicycle space per 12 dwellings (visitors), 1 motorcycle space per 15 dwellings, Large Rigid Vehicle (Waste Contractor), >10 dwellings – side loading waste collection vehicle

Adequate parking is provided on site for the development as per the above requirement.

However the assessment made by TfNSW and Council's Traffic engineer have raised significant concerns on the traffic impacts generated with the development as detailed within this report.

Further off-site signage and traffic related works on Bangalow Road to form a cul-de-sac are required to be undertaken to facilitate this type of development in this location. Council's Traffic Committee and Strategic Traffic Team are required to be involved in the process including consultations with nearby residents. It is considered a Planning Agreement process is the appropriate mechanism.

The proposed development cannot be supported for these reasons.

CHAPTER E7: WASTE MANAGEMENT

Site Waste Minimisation and Management Plan provided.

CHAPTER E11: HERITAGE CONSERVATION

The site itself does not contain heritage items but is in proximity to identified item of local heritage significance located opposite and across the road from the site.

No significant concerns were raised however commentary suggested recessive colour tones in finishes and reduced bulk/ scale would be preferable as relates to the nearby Heritage Item.

CHAPTER E12: GEOTECHNICAL ASSESSMENT OF SLOPE INSTABILITY

Council's Geotechnical Officer has reviewed the proposal. No concerns were raised.

CHAPTER E16 BUSHFIRE MANAGEMENT

The proposal is considered to be consistent with this Chapter. The NSW RFS have reviewed the proposal and provided satisfactory advice with conditions.

CHAPTER E17 PRESERVATION AND MANAGEMENT OF TREES AND VEGETATION

Proposal involves removal of 13 trees located within the site. Council's Landscape and Environmental officer have provided conditionally satisfactory comments and conditions including compensatory plantings.

Future work proposed on Bangalow Road to form a cul-de-sac as part of the required traffic mitigation is identified to have possible impacts on vegetation that require further assessment consideration.

CHAPTER E19 EARTHWORKS

Proposal involves excavations related to constructions, infrastructure and other landscaping works. Council's Geotechnical Officer has reviewed the proposal. No concerns were raised.

CHAPTER E21 DEMOLITION AND ASBESTOS MANAGEMENT

Demolition of existing dwelling house located on the property forms part of this application and a demolition plan has been prepared.

Council's Environmental officer has provided conditionally satisfactory comments and conditions.

3.4.2 WOLLONGONG CITY- WIDE DEVELOPMENT CONTRIBUTIONS PLAN - 2019

Development contributions are applicable, however the DA is recommended for refusal.

3.5 SECTION 4.15(1)(A)(IIIA) ANY PLANNING AGREEMENT THAT HAS BEEN ENTERED INTO UNDER SECTION 7.4, OR ANY DRAFT PLANNING AGREEMENT THAT A DEVELOPER HAS OFFERED TO ENTER INTO UNDER SECTION 7.4

A Planning Agreement is deemed necessary to facilitate the proposed development (see Chapter E3 above). There are no planning agreements entered into or any draft agreement offered to entered into under S7.4 as part of the proposal.

3.6 SECTION 4.15(1)(B) THE LIKELY IMPACTS OF DEVELOPMENT

Context and Setting:

The proposed development is not considered to conform to the context or setting for the existing residential precinct as relates to bulk / scale and is likely to introduce traffic management impacts that are unresolved.

Access, Transport and Traffic:

The proposal has not satisfactorily addressed safe vehicular access and egress with impacts on the Princes Highway/Bulli Pass. It is considered a Planning Agreement process is the appropriate mechanism for mitigation works proposed in Bangalow Road.

Public Domain:

The proposal will likely impact the public domain in that the development and the associated works could have adverse traffic impacts on the neighbouring properties and streets.

Utilities:

The proposal is not expected to have negative impact on the utilities.

Heritage:

The site itself does not contain heritage items but is in proximity to identified item of local heritage significance located opposite and across the road from the site.

No significant concerns were raised however commentary suggested recessive colour tones in finishes and reduced bulk/ scale would be preferable as relates to the nearby Heritage Item

Other land resources:

The proposal is not considered to contribute to the orderly development of the site and is therefore envisaged to impact upon adjoining land resources.

Water:

The site is presently serviced by Sydney Water. The proposal is not envisaged to have unreasonable water consumption.

Soils:

The proposal is not expected to have some negative impact on soils.

Air and Microclimate:

The proposal is not expected to have negative impact on air or microclimate in principle.

Flora and Fauna:

Whilst there may be possible impacts on fauna via tree removals, conditions of consent could be provided for fauna management during clearing works by a qualified ecologist.

Waste:

TfNSW has concerns with the proposed garbage collection arrangements and bin capacity provision causing possible road safety hazard.

Energy:

The proposal is not envisaged to have unreasonable energy consumption.

Natural hazards:

There are no natural hazards affecting the site that would prevent the proposed development.

Technological hazards:

There are no technological hazards affecting the site that would prevent the proposal.

Safety, Security and Crime Prevention:

The application is not expected to result in greater opportunities for criminal or antisocial behaviour. Natural surveillance has been provided for in the design as well as minimal areas for entrapment and concealment.

Social Impact:

The proposal may have social impact in principle due to additional traffic generated.

Site Design and Internal Design:

The application requests a variation to Council's development control plan for reduced setbacks as detailed in Section 3.4.1 which are not supported as it is considered there are links to unresolved traffic management matters.

Cumulative Impacts:

The proposal may be expected to have negative cumulative impacts by way of unresolved traffic management matters, bulk and scale in the locality.

3.7 SECTION 4.15(1)(C) THE SUITABILITY OF THE SITE FOR DEVELOPMENT

Does the proposal fit in the locality?

The proposal is not considered appropriate with regard to the site and is expected to have negative impacts on the amenity of the locality or adjoining developments.

Are the site attributes conducive to development?

There are no site constraints that would prevent the proposal except for the frontage to a key state road and unresolved traffic management matters.

3.8 SECTION 4.15(1)(D) ANY SUBMISSIONS MADE IN ACCORDANCE WITH THIS ACT OR THE REGULATIONS

Details of the proposal were publicly exhibited in accordance with Appendix 1 of the Wollongong Development Control Plan (WDCP) 2009. Eighteen (18) submissions were received during the notification period. A detail discussion on submissions received is provided at section 1.5.

3.9 SECTION 4.15(1)(E) THE PUBLIC INTEREST

The application is expected to have unreasonable impacts on the environment and the amenity of the locality. It is not considered appropriate with consideration to the character of the area and is therefore considered to be not in the public interest.

4 CONCLUSION

This application has been assessed having regard to the Heads of Consideration under Section S4.15(1) of the Environmental Planning and Assessment Act 1979, the provisions of Wollongong Local Environmental Plan 2009 and all relevant Council DCPs, Codes and Policies.

Pursuant to clause 2.3 of WLEP 2009, Multi dwellings are permissible in the R2 Low Density Residential zone with development consent. The proposal is not considered to satisfy Council's development control requirements and traffic impacts are not resolved. The development is also regarded an overdevelopment on site in terms of number of units resulting in a variation request to the side setback development controls which is considered as causing cumulative impacts on parking and traffic management which are unresolved.

The most recent assessment made by TfNSW and Council's Traffic engineer of additional information submitted by the proponent have identified that further off site signage and traffic related works on Bangalow Road to form a cul-de-sac are required to be undertaken to facilitate this type of development in this location.

Council's Traffic Committee and Strategic Traffic Team are required to be involved in the process including consultations with nearby residents. It is considered a Planning Agreement process is the appropriate mechanism.

Since neither a Planning Agreement or draft agreement are in place a Deferred Commencement consent is also not considered suitable in this instance.

5 RECOMMENDATION

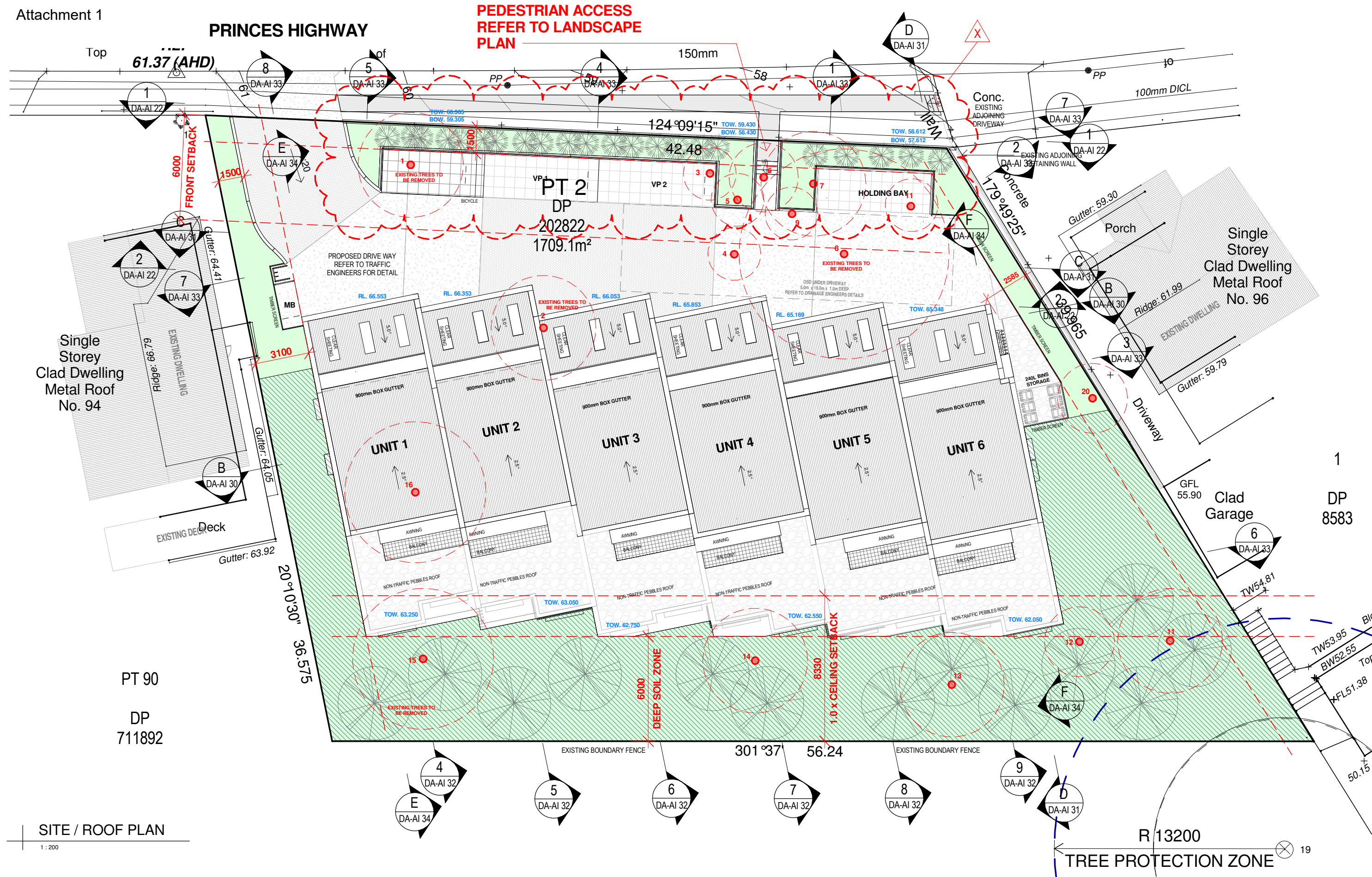
Having regard to the above information, the application is considered to be unsatisfactory and is recommended for **refusal** for the following reasons:

- a Pursuant to the provisions of Section 4.15 (1)(a)(i) of the Environmental Planning and Assessment Act, 1979, it is considered that the proposal fails to demonstrate consistency with Wollongong Local Environmental Plan 2009:
 - Clause 1.2 Aims of the Plan – (2)(c) and (2)(g)
- b Pursuant to the provisions of Section 4.15 (1)(a)(i) of the Environmental Planning and Assessment Act, 1979, it is considered that the proposal fails to demonstrate consistency with State Environmental Planning Policy – Infrastructure:
 - Clause 101 Development with frontage to classified road as relates to the uncertain outcome of required cul-de-sac works in Bangalow Road to resolve traffic impacts on Bulli Pass
- c Pursuant to the provisions of Section 4.15 (1)(a)(iii) of the Environmental Planning and Assessment Act 1979, it is considered that the proposal fails to demonstrate consistency with the provisions of the Wollongong Development Control Plan 2009:
 - Chapter B1: Residential Development
 - Chapter E3: Car Parking, Access, Servicing/ Loading Facilities and Traffic Management;
 - Chapter E17: Preservation and Management of Trees and Vegetation; (required works in (Bangalow Road)

- d Pursuant to the provisions of Section 4.15 (1)(a)(iia) of the Environmental Planning and Assessment Act 1979, it is considered that a Section 7.4 Planning Agreement is required to satisfy TfNSW and Council's traffic management concerns via works in Bangalow Road and no Agreement or draft Agreement is in place.
- e Pursuant to the provisions of Section 4.15 (1)(b) of the Environmental Planning and Assessment Act, 1979, it is considered that the proposal fails to demonstrate that the likely impacts of the development will not be adverse.
- f Pursuant to the provisions of Section 4.15 (1)(c) of the Environmental Planning and Assessment Act, 1979, it is considered that the proposal fails to demonstrate that the site is suitable for the development.
- g Pursuant to the provisions of Section 4.15 (1) (d) and (e) of the Environmental Planning and Assessment Act, 1979, it is considered that with submissions received and in the circumstances of the case, approval of the development would not be in the public interest.

6 ATTACHMENTS

- 1 Architectural Plans, Traffic Manoeuvring
- 2 DRP & Pre-lodge meeting: notes
- 3 Statements of Variation to development controls – Applicant
- 4 Response from TfNSW



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REF.	DATE	AMENDMENT
Y	04.08.2020	ADDITIONAL INFORMATION

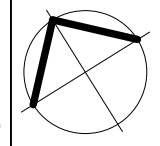
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FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS
BL	BLOCKWORK	D	DOOR
CL01	CLADDING	GD	GARAGE DOOR
CL02	CLADDING	SLD	SLIDING DOOR
RW	RETAINING WALL	BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW	FW	FIXED WINDOW
FW	FIXED WINDOW	OB	OBSCURE WINDOW
AW	AWNING WINDOW	SK	SKYLIGHT
WH	WINDOW HOOD	LV	LOUVRES
RWT	RAINWATER TANK	P	POST
T	TIMBER FLOORS	CT	CERAMIC TILES
CPT	CARPET	PC	POLISHED CONCRETE
SP	FEATURE SCREENING	IWS	INTEGRAL WALL



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Nominated Architect:
Robert Gizzi (Reg. 8286)



CLIENT:	DA MEDIUM DENSITY
ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME:	SITE / ROOF PLAN

DATE:	6/8/2018	PROJECT No.	1815
DRAWN:	TN	DWG No.	DA-AI Y
SCALE:	1 : 200	Rev.	06
QA:	RG		

ADDITIONAL INFORMATION



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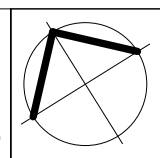
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CL02	CLADDING
RW	RETAINING WALL
R	ROOF
DP	DOWNPIPES
TB	TIMBER BATTENS
D	DOOR
GD	GARAGE DOOR
SLD	SLIDING DOOR
BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW
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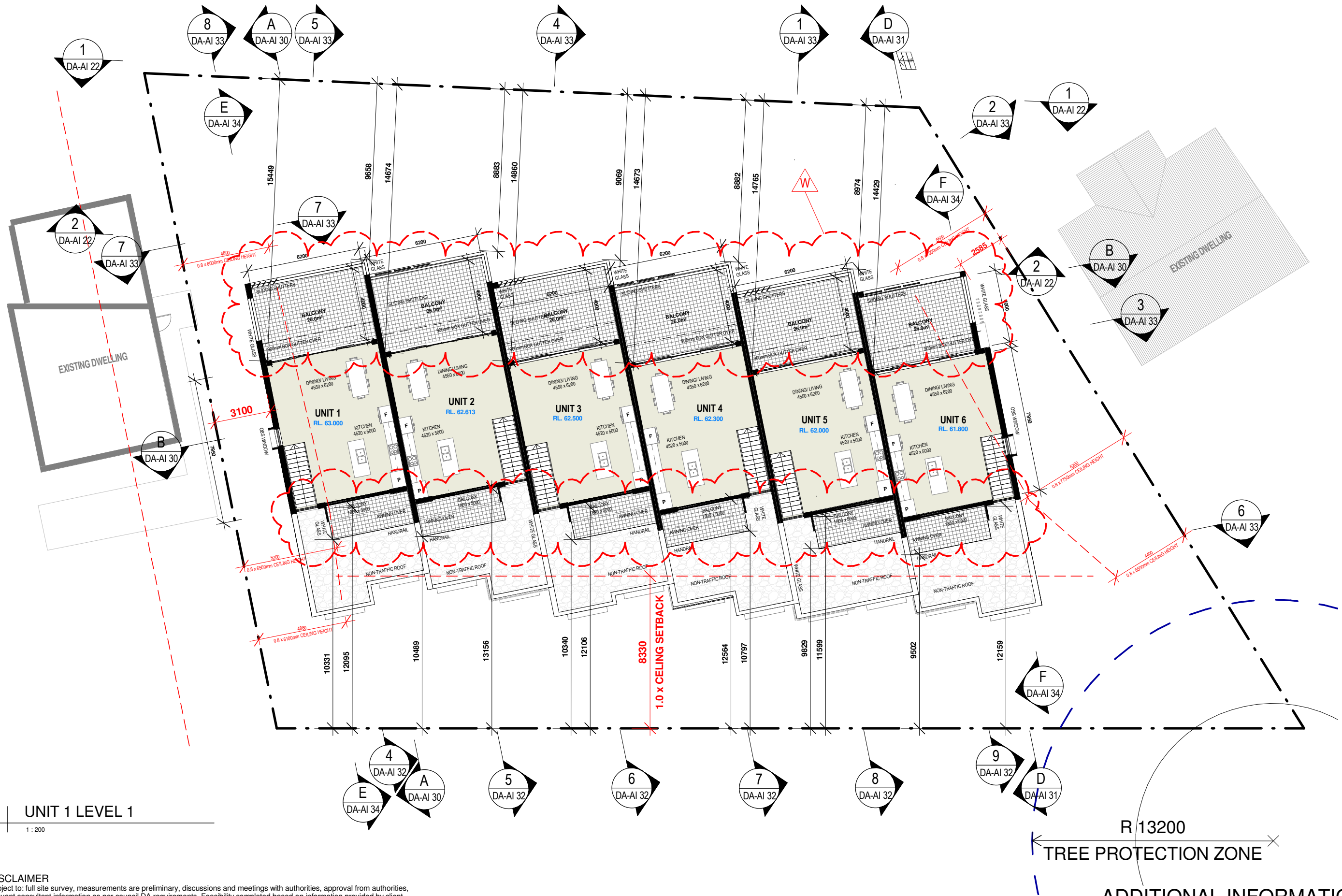
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CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: GROUND FLOOR PLAN

DATE: 6/8/2018
DRAWN: JC / CS / TN
SCALE: 1 : 200
QA: RG
PROJECT No. 1815
DWG No. DA-AI X
Rev. 08



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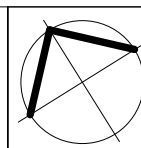
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FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES	FW	FIXED WINDOW	T	TIMBER FLOORS
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS	OB	OBSCURE WINDOW	CT	CERAMIC TILES
BL	BLOCKWORK	D	DOOR	AW	AWNING WINDOW	CPT	CARPET
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CLIENT:	DA MEDIUM DENSITY
ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME:	LEVEL 1

DATE:	6/8/2018	PROJECT No.	1815
DRAWN:	JC / CS / TN	DWG No.	DA-AI Y
SCALE:	1 : 200	Rev.	09
QA:	RG		

ADDITIONAL INFORMATION

PRINCES HIGHWAY

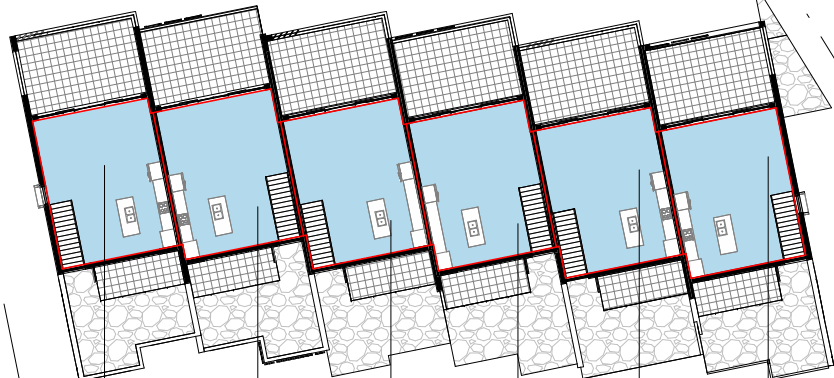


29.4 m² 30.2 m² 30.1 m² 30.2 m² 30.1 m² 29.3 m²

UNIT 1 LOWER GROUND

1 : 400

PRINCES HIGHWAY



50.4 m² 51.3 m² 51.2 m² 51.2 m² 51.2 m² 50.3 m²

UNIT 1 LEVEL 1

1 : 400

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REF. DATE AMENDMENT
W 22.05.2020 ADDITIONAL INFORMATION

Legend:

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FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES	FW	FIXED WINDOW	T	TIMBER FLOORS
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS	OB	OBSOLETE WINDOW	CT	CERAMIC TILES
BL	BLOCKWORK	AW	AWNING WINDOW	SK	SKYLIGHT	CPT	CARPET
CL01	CLADDING	GD	GARAGE DOOR	WH	WINDOW HOOD	PC	POLISHED CONCRETE
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RW	RETAINING WALL	BFD	BI-FOLD DOOR	RWT	RAINWATER TANK	IWS	INTEGRAL WALL

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CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: GFA PLAN

DATE: 6/8/2018
DRAWN: TN
SCALE: 1 : 400
QA: RG
PROJECT No.
1815
DWG No. Rev.
DA-AI W
10

ADDITIONAL INFORMATION

SITE CALCULATIONS

SITE AREA: 1709.1m²
PERMISSIBLE FLOOR AREA: 854.55m²
PERMISSIBLE FLOOR SPACE RATIO: 0.5 : 1

GARAGE CONCESSION: 36.0m²

PROPOSED FLOOR AREA: 853.4m²
PROPOSED FLOOR SPACE RATIO: 0.5 : 1

PARKING REQUIRED: 14 SPACES
PARKING PROVIDED: 14 SPACES

LANDSCAPED REQUIRED: 510.0m² (30%)
LANDSCAPE PROVIDED: 629.4m² (37%)

DEEP SOIL REQUIRED: 256.4m² (15%)
DEEP SOIL PROVIDED: 495.4m² (29%)

UNIT CALCULATIONS TOTAL

UNIT 1: 140.1m²
UNIT 2: 143.7m²
UNIT 3: 143.4m²
UNIT 4: 143.5m²
UNIT 5: 143.3m²
UNIT 6: 139.8m²

STORAGE CALCULATIONS TOTAL

UNIT 1: 6.1m³ (15.1m³)
UNIT 2: 6.1m³ (15.1m³)
UNIT 3: 6.1m³ (15.1m³)
UNIT 4: 6.1m³ (15.1m³)
UNIT 5: 6.1m³ (15.1m³)
UNIT 6: 6.1m³ (15.1m³)

AREA SCHEDULE (Gross Building)

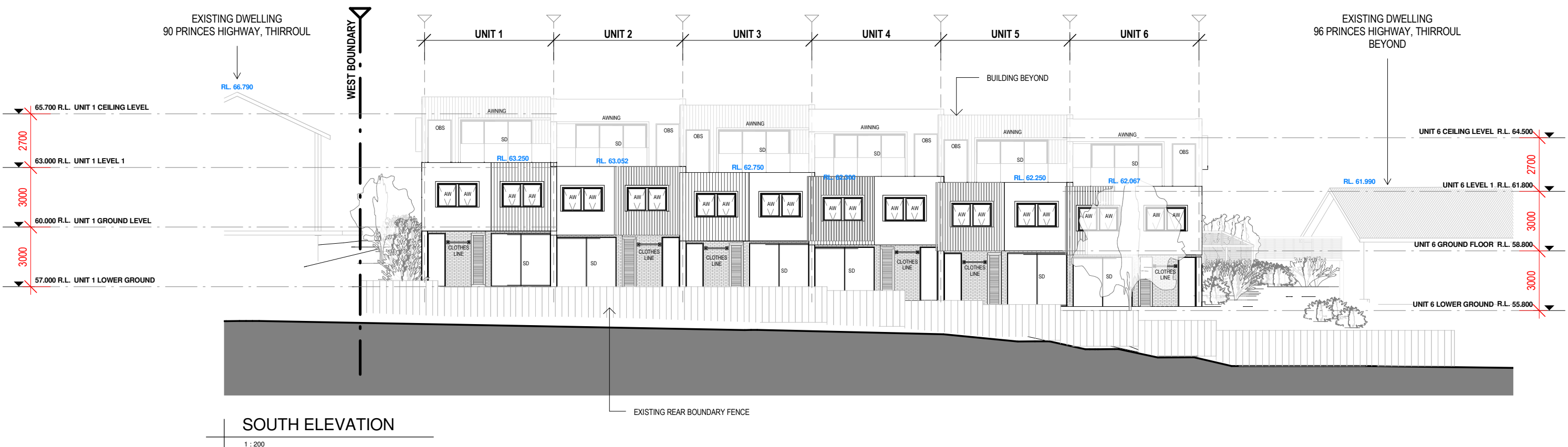
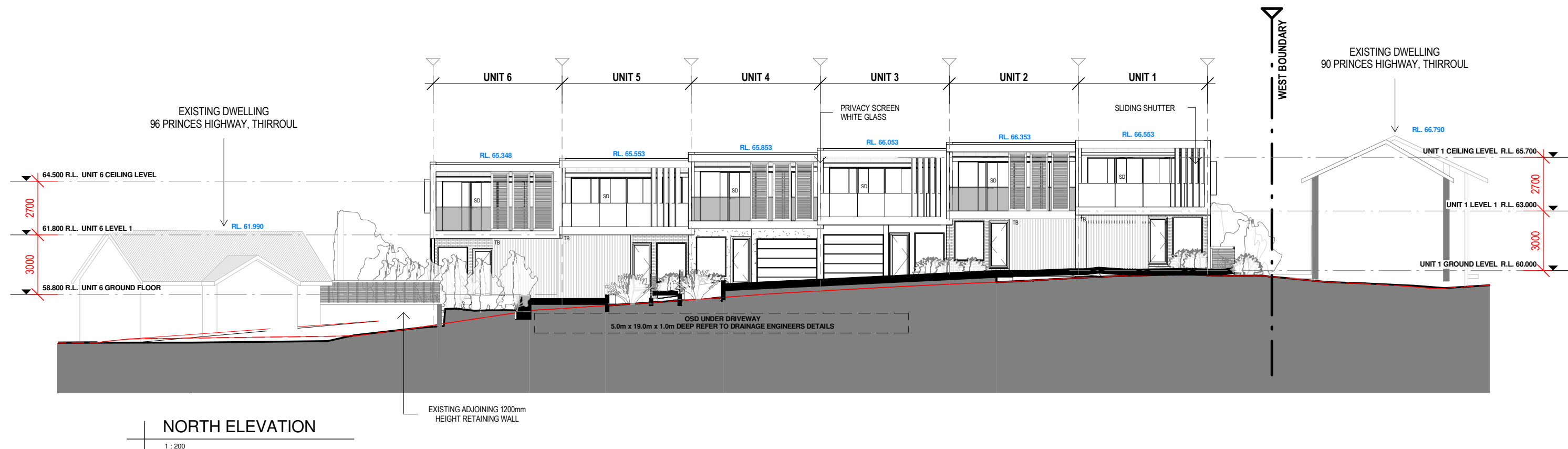
Name	Level	AREA
UNIT 1	UNIT 1 LOWER GROUND	29.4 m ²
UNIT 1	UNIT 1 GROUND LEVEL	60.0 m ²
UNIT 1	UNIT 1 LEVEL 1	50.4 m ²
		139.7 m ²
UNIT 2	UNIT 1 LOWER GROUND	30.2 m ²
UNIT 2	UNIT 1 GROUND LEVEL	62.6 m ²
UNIT 2	UNIT 1 LEVEL 1	51.3 m ²
		144.1 m ²

AREA SCHEDULE (Gross Building)

Name	Level	AREA
UNIT 3	UNIT 1 LOWER GROUND	30.1 m ²
UNIT 3	UNIT 1 GROUND LEVEL	62.0 m ²
UNIT 3	UNIT 1 LEVEL 1	51.2 m ²
		143.3 m ²
UNIT 4	UNIT 1 LOWER GROUND	30.2 m ²
UNIT 4	UNIT 1 GROUND LEVEL	62.2 m ²
UNIT 4	UNIT 1 LEVEL 1	51.2 m ²
		143.6 m ²

AREA SCHEDULE (Gross Building)

Name	Level	AREA
UNIT 5	UNIT 1 LOWER GROUND	30.1 m ²
UNIT 5	UNIT 1 GROUND LEVEL	61.1 m ²
UNIT 5	UNIT 1 LEVEL 1	51.2 m ²
		142.5 m ²
UNIT 6	UNIT 1 LOWER GROUND	29.3 m ²
UNIT 6	UNIT 1 GROUND LEVEL	60.6 m ²
UNIT 6	UNIT 1 LEVEL 1	50.3 m ²
		140.2 m ²
		853.4 m ²



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W	22.05.2020	ADDITIONAL INFORMATION

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Legend:

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				RWT	RAINWATER TANK		

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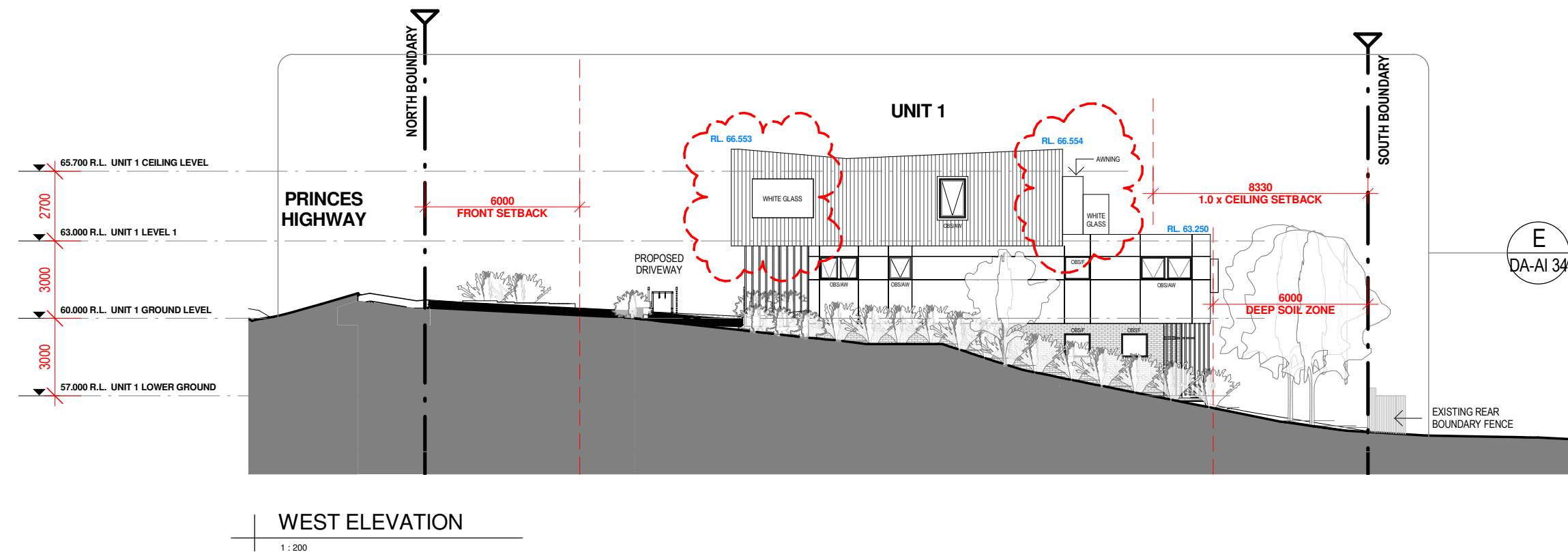
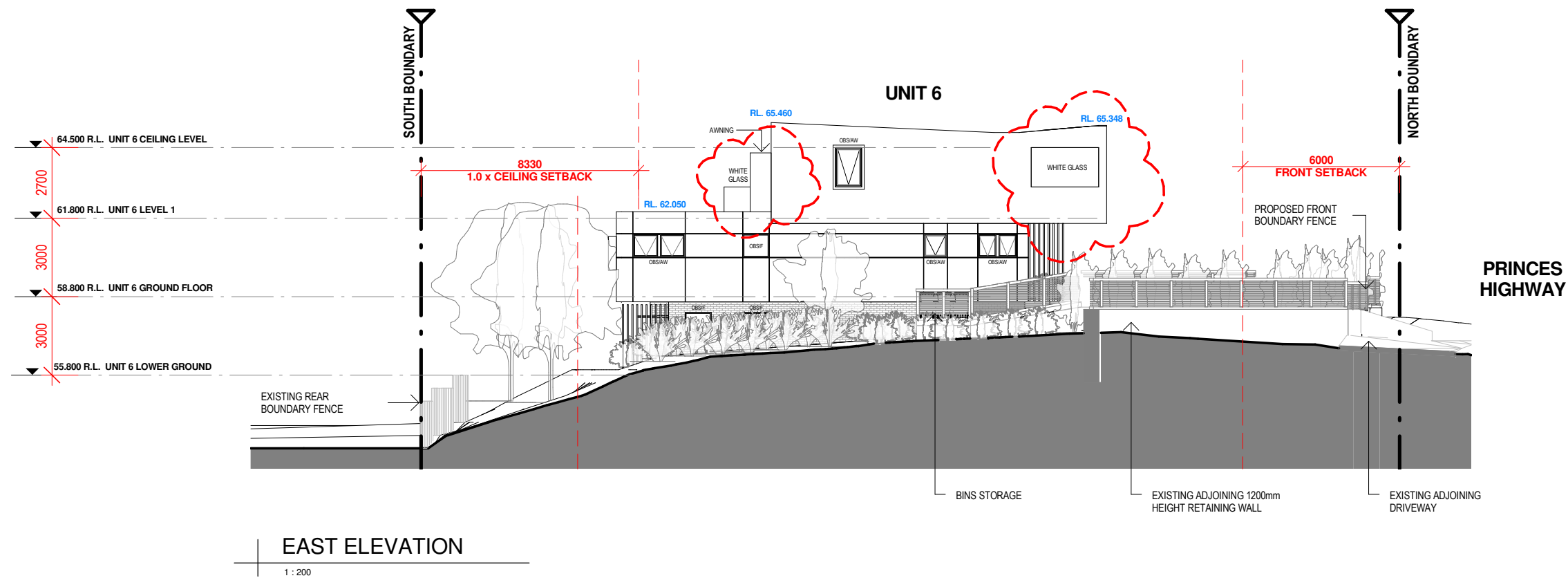
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Nominated Architect:
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CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: NORTH & SOUTH ELEVATIONS

DATE: 6/8/2018
DRAWN: TN
SCALE: 1:200
QA: RG
PROJECT No.
1815
DWG No. Rev.
DA-AI W
20

ADDITIONAL INFORMATION



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		BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW	RWT	RAINWATER TANK
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OB	OBSOLETE WINDOW		
AW	AWNING WINDOW		
SK	SKYLIGHT		
WH	WINDOW HOOD		
SP	FEATURE SCREENING		
P	POST		
T	TIMBER FLOORS		
CT	CERAMIC TILES		
CPT	CARPET		
PC	POLISHED CONCRETE		
SP	FEATURE SCREENING		
IWS	INTEGRAL WALL		

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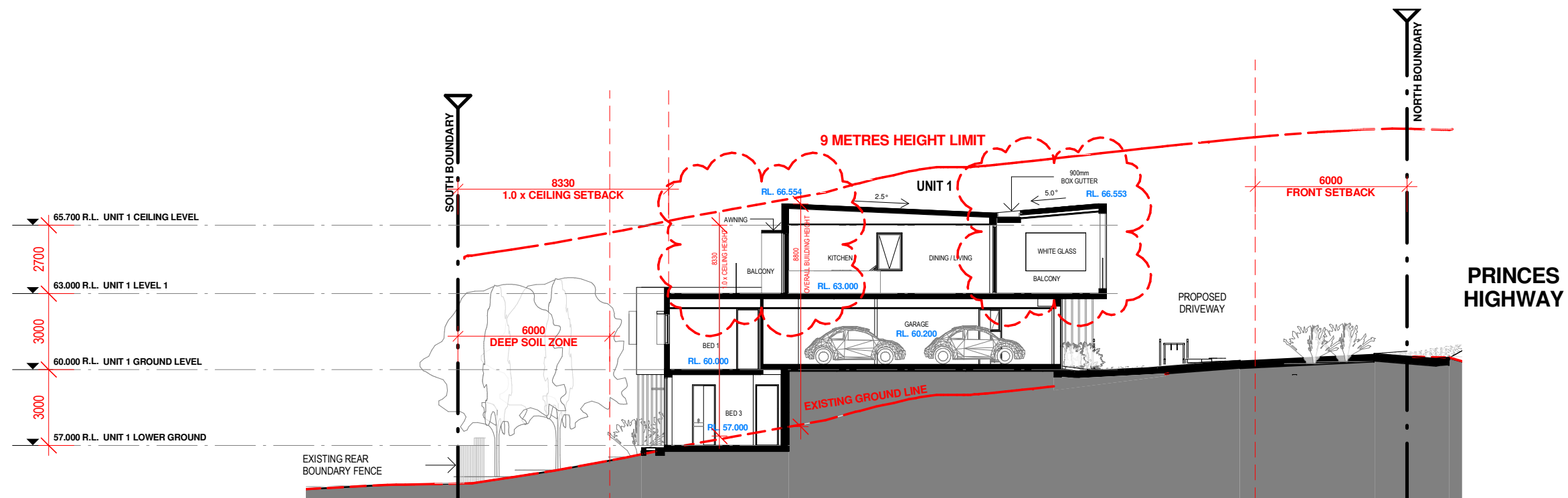
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Sydney
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Wolli Creek NSW 2205
Nominated Architect:
Robert Gizzi (Reg. 8286)

CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: EAST & WEST ELEVATIONS

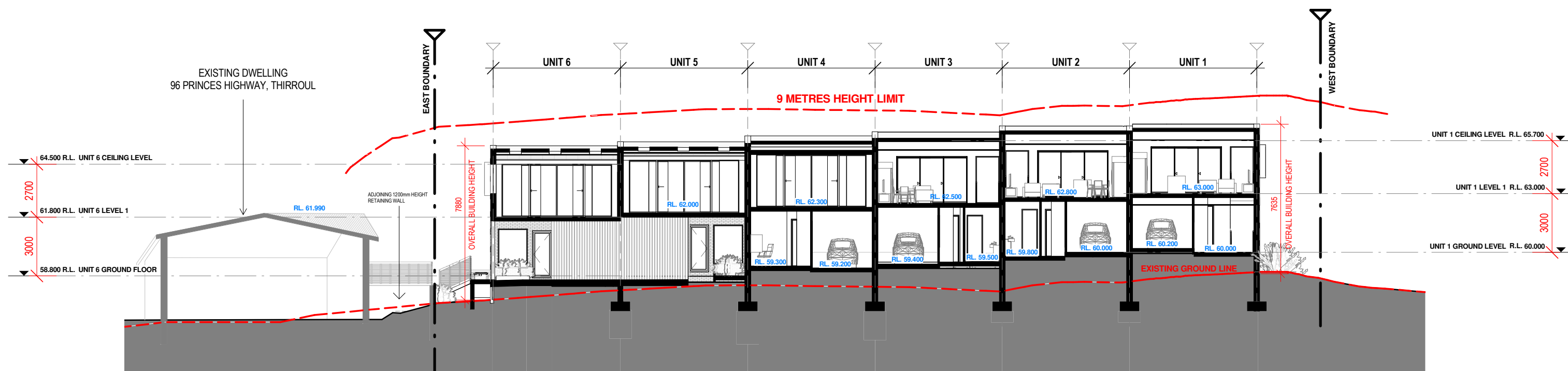
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DRAWN: TN
SCALE: 1 : 200
QA: RG
PROJECT No.
1815
DWG No. Rev.
DA-AI W
21

ADDITIONAL INFORMATION



SECTION A-A

1 : 200



SECTION B-B

1 : 200

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REF. DATE AMENDMENT
W 22.05.2020 ADDITIONAL INFORMATION

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		BFD	BI-FOLD DOOR				

DWA
DESIGN WORKSHOP AUSTRALIA

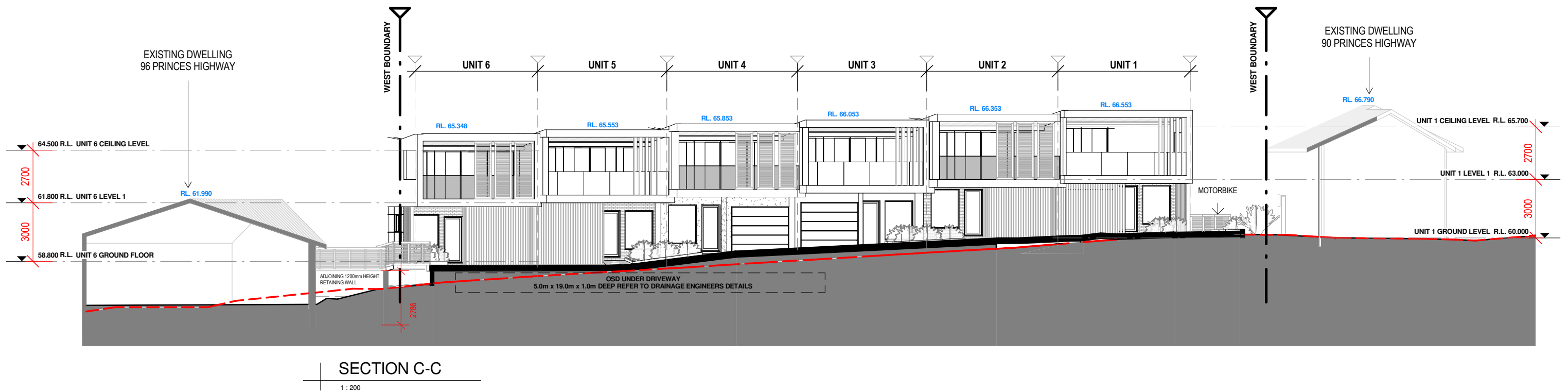
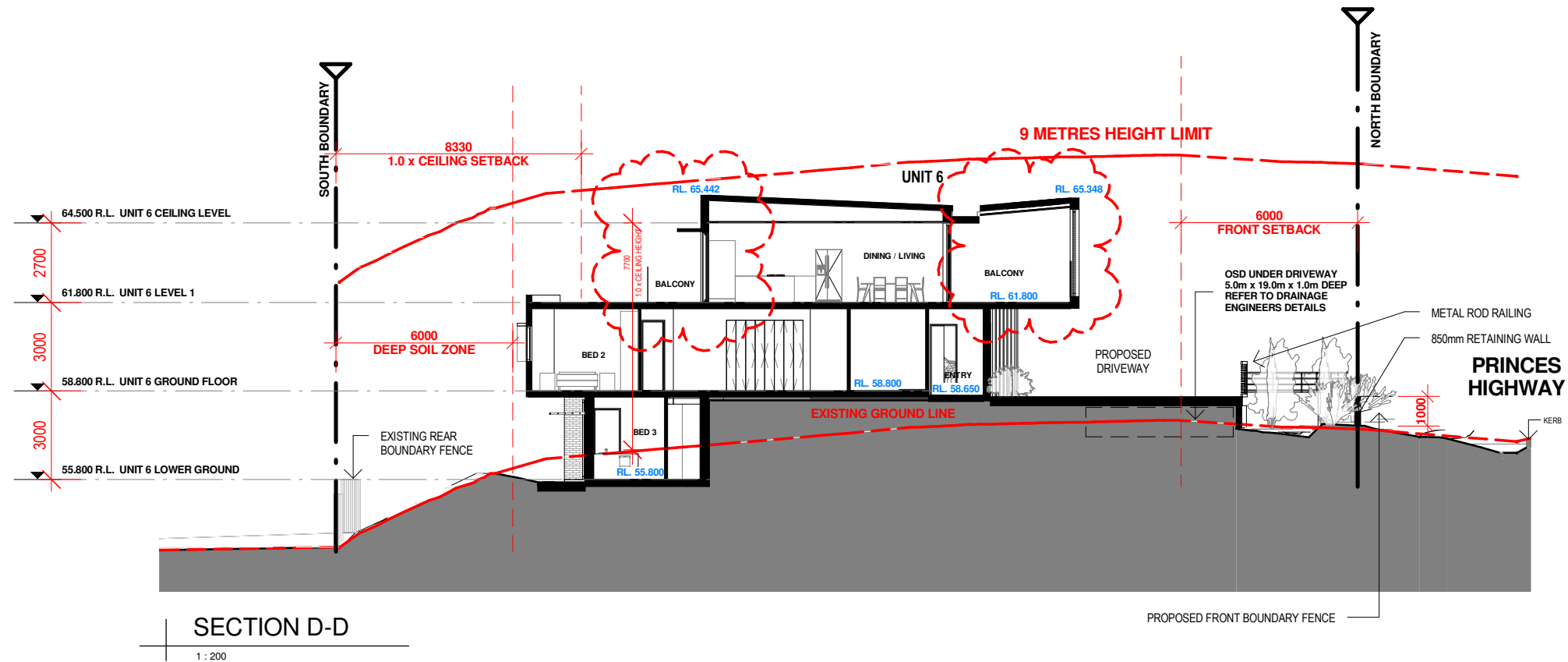
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Wolli Creek NSW 2205
Nominated Architect:
Robert Gizzi (Reg. 8286)

CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: SECTION A-B

DATE: 6/8/2018
DRAWN: JC / CS/
TN
SCALE: 1 : 200
QA: RG
PROJECT No.
1815
DWG No. Rev.
DA-AI W
30

ADDITIONAL INFORMATION



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REF.	DATE	AMENDMENT
W	22.05.2020	ADDITIONAL INFORMATION

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Legend:

FB01	FACE BRICKWORK TYPE 1	R	ROOF	SLW	SLIDING WINDOW	P	POST
FB02	FACE BRICKWORK TYPE 2	DP	DOWNPipes	FW	FIXED WINDOW	T	TIMBER FLOORS
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS	OB	OBSCURE WINDOW	CT	CERAMIC TILES
BL	BLOCKWORK	D	DOOR	AW	AWNING WINDOW	CPT	CARPET
CL01	CLADDING	GD	GARAGE DOOR	SK	SKYLIGHT	PC	POLISHED CONCRETE
CL02	CLADDING	SLD	SLIDING DOOR	WH	WINDOW HOOD	SP	FEATURE SCREENING
RW	RETAINING WALL	BFD	BI-FOLD DOOR	LV	LOUVRES	IWS	INTEGRAL WALL
				RWT	RAINWATER TANK		



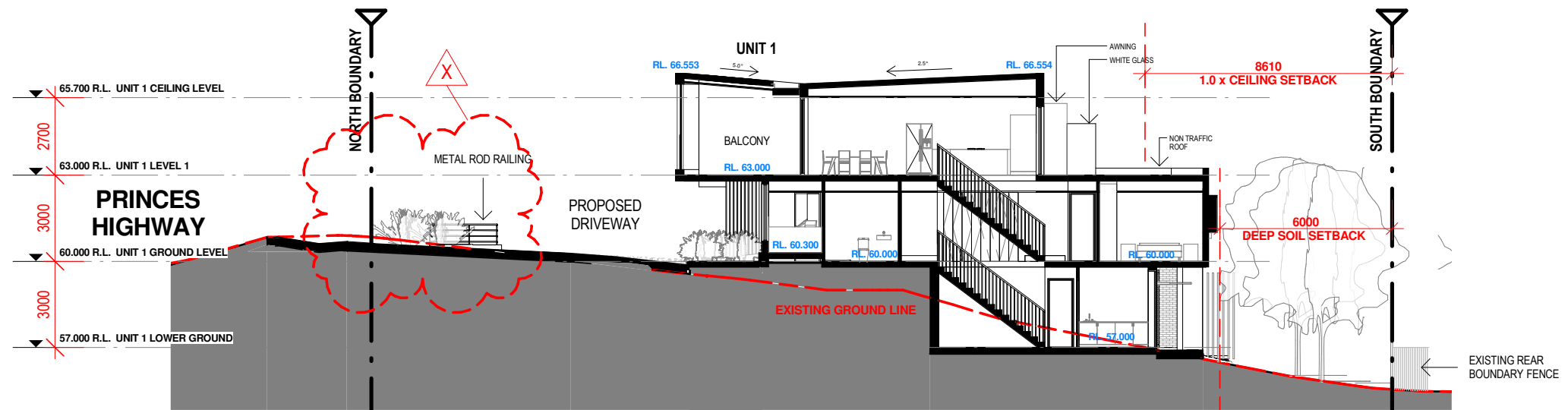
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Level 10, 6 Mount
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Nominated Architect:
Robert Gizzi (Reg. 8286)

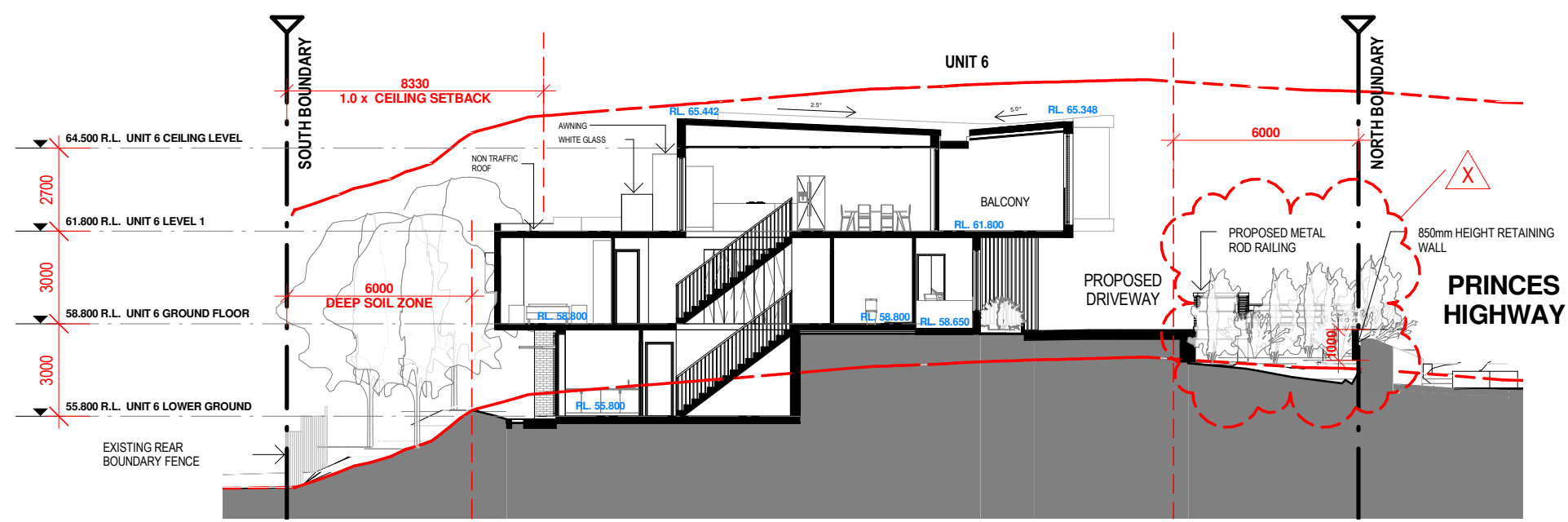
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ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME:	SECTION C-D

DATE:	6/8/2018	PROJECT No.	1815
DRAWN:	TN	DWG No.	DA-AI
SCALE:	1 : 200	Rev.	W
QA:	RG		31

ADDITIONAL INFORMATION



SECTION E-E
1 : 200



SECTION F-F
1 : 200

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Legend:			
FB01	FACE BRICKWORK TYPE 1	R	ROOF
FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS
BL	BLOCKWORK	D	DOOR
CL01	CLADDING	GD	GARAGE DOOR
CL02	CLADDING	SLD	SLIDING DOOR
RW	RETAINING WALL	BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW	FW	FIXED WINDOW
OB	OBSCURE WINDOW	AW	AWNING WINDOW
SK	SKYLIGHT	WH	WINDOW HOOD
LV	LOUVRES	LV	LOUVRES
RWT	RAINWATER TANK		
P	POST	T	TIMBER FLOORS
CT	CERAMIC TILES	CPT	CARPET
PC	POLISHED CONCRETE	SP	FEATURE SCREENING
IWS	INTEGRAL WALL		

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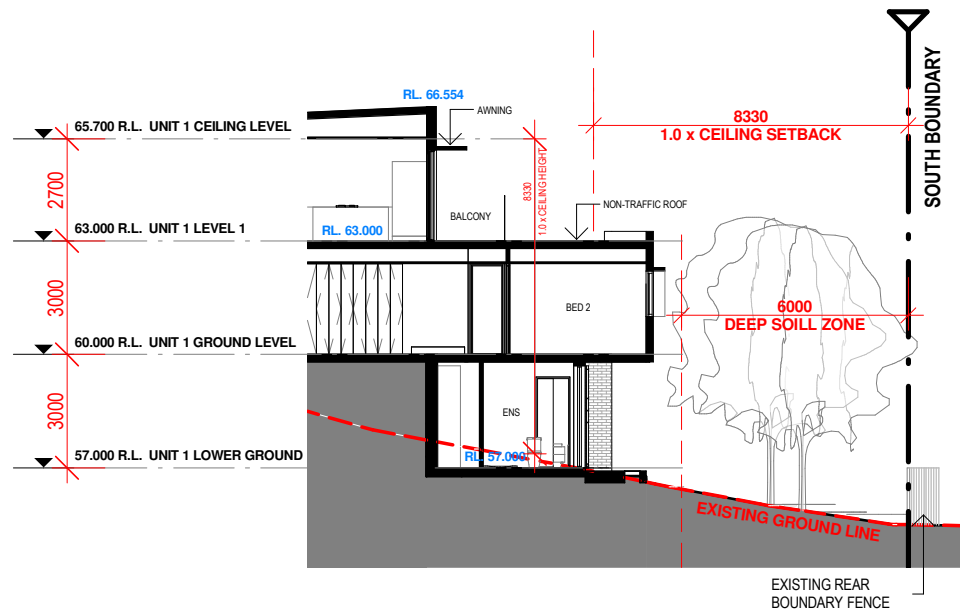
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Robert Gizzi (Reg. 8286)

CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: SECTION E-F

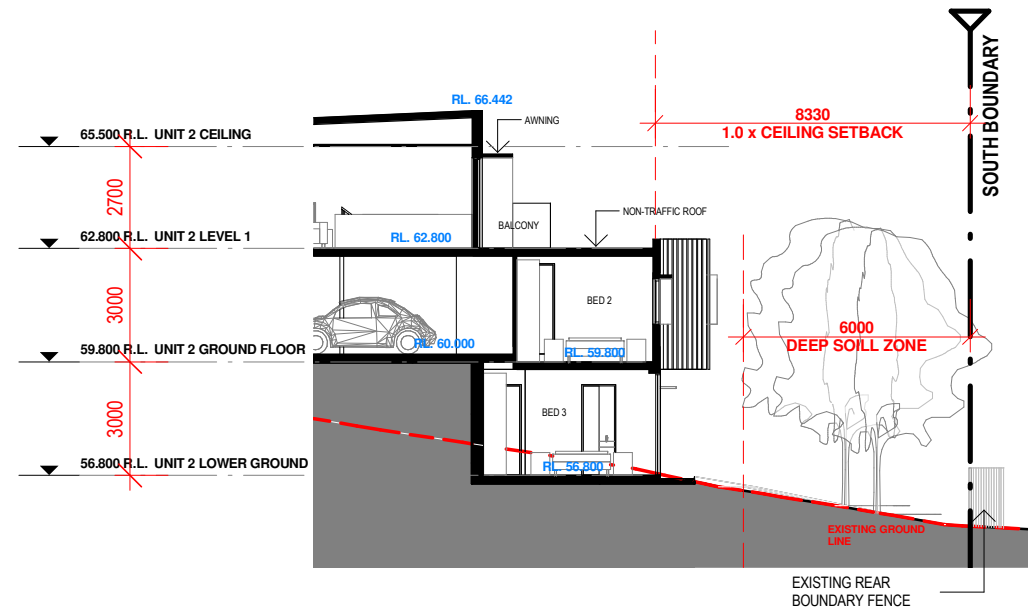
DATE: 6/8/2018
DRAWN: TN
SCALE: 1 : 200
QA: RG
PROJECT No. 1815
DWG No. DA-AI
Rev. X
34

ADDITIONAL INFORMATION



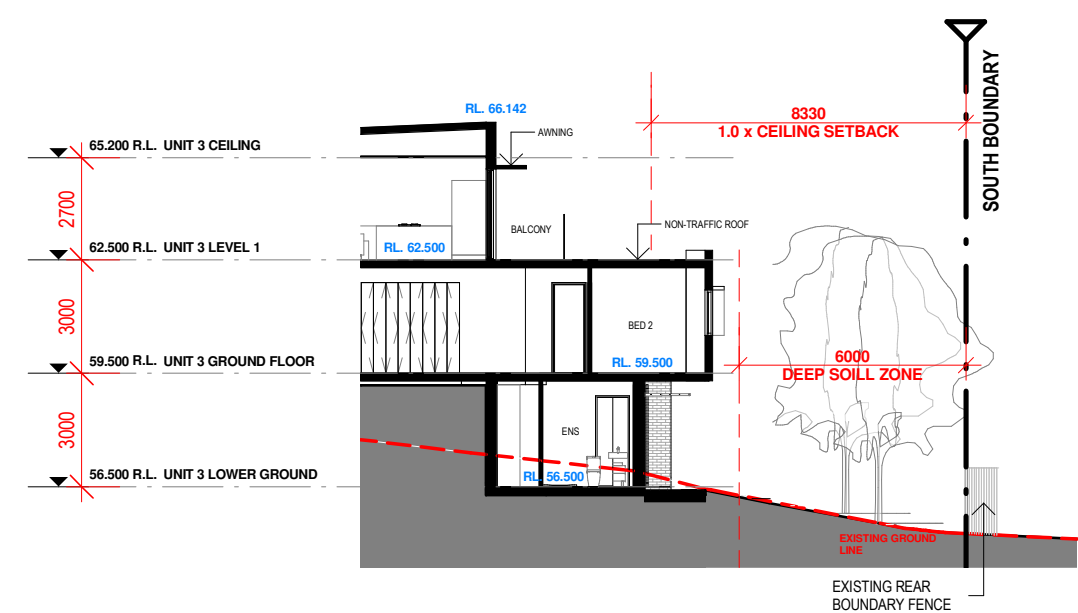
UNIT 1 LOWER GROUND

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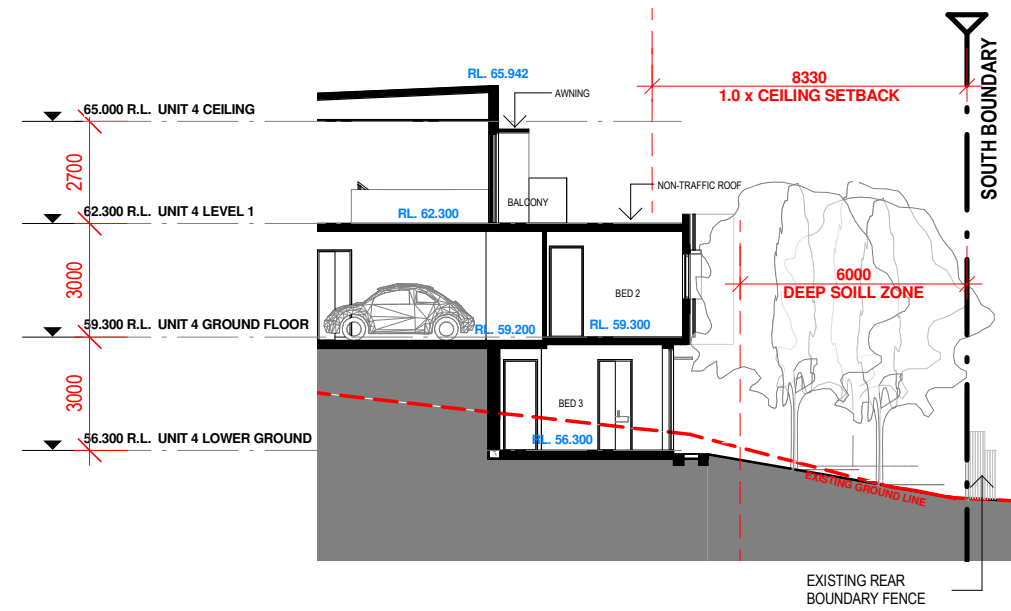
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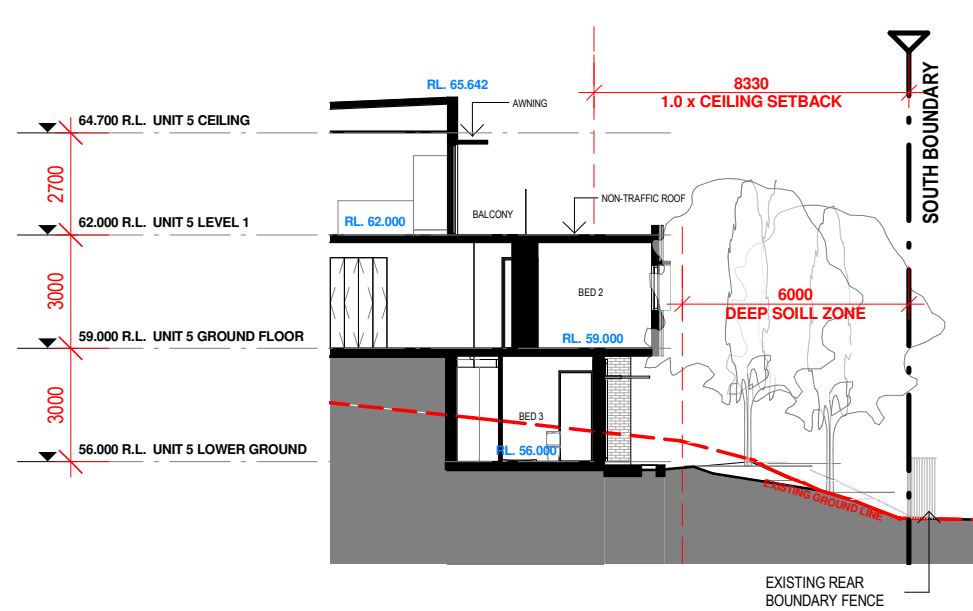
UNIT 3 LOWER GROUND

1 : 200



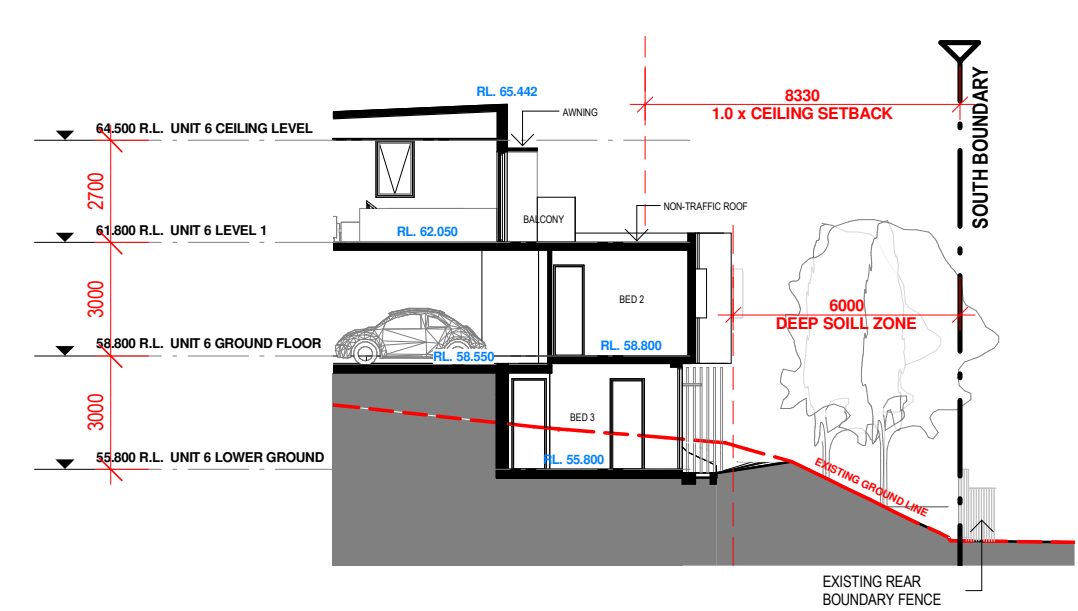
UNIT 4 LOWER GROUND

1 : 200



UNIT 5 LOWER GROUND

1 : 200



UNIT 6 LOWER GROUND

1 : 200

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W	22.05.2020	ADDITIONAL INFORMATION

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Legend:

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FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES	FW	FIXED WINDOW	T	TIMBER FLOORS
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS	OB	OBSCURE WINDOW	CT	CERAMIC TILES
BL	BLOCKWORK	D	DOOR	AW	AWNING WINDOW	CPT	CARPET
CL01	CLADDING	GD	GARAGE DOOR	SK	SKYLIGHT	PC	POLISHED CONCRETE
CL02	CLADDING	SLD	SLIDING DOOR	WH	WINDOW HOOD	SP	FEATURE SCREENING
RW	RETAINING WALL	LV	LOUVRES	LV	LOUVRES	IWS	INTEGRAL WALL
		BFD	BI-FOLD DOOR	RWT	RAINWATER TANK		

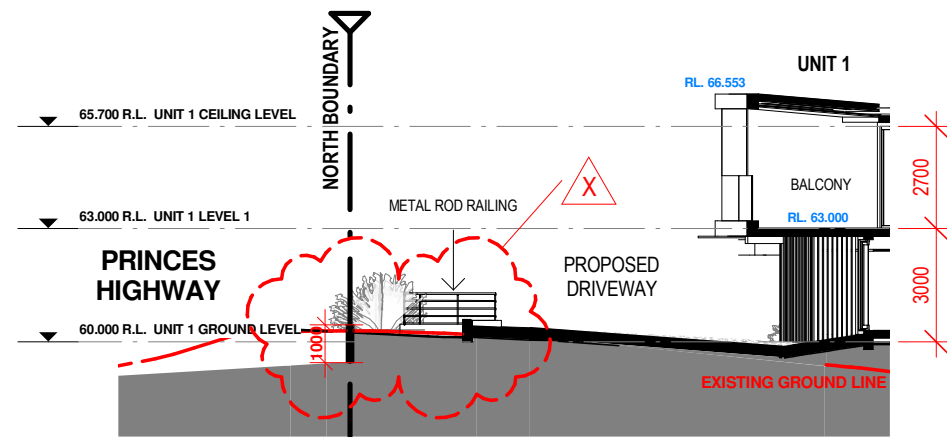
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Nominated Architect:
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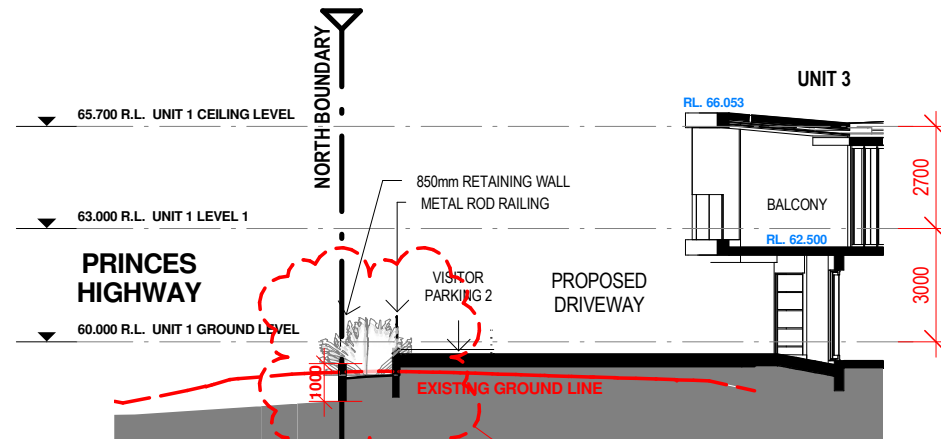
ADDITIONAL INFORMATION

CLIENT:	DA MEDIUM DENSITY	DATE:	6/8/2018	PROJECT No.	1815
ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL	DRAWN:	TN	DWG No.	DA-AI
DRAWING NAME:	UNIT REAR SECTIONS	SCALE:	1 : 200	Rev.	W
		QA:	RG		32



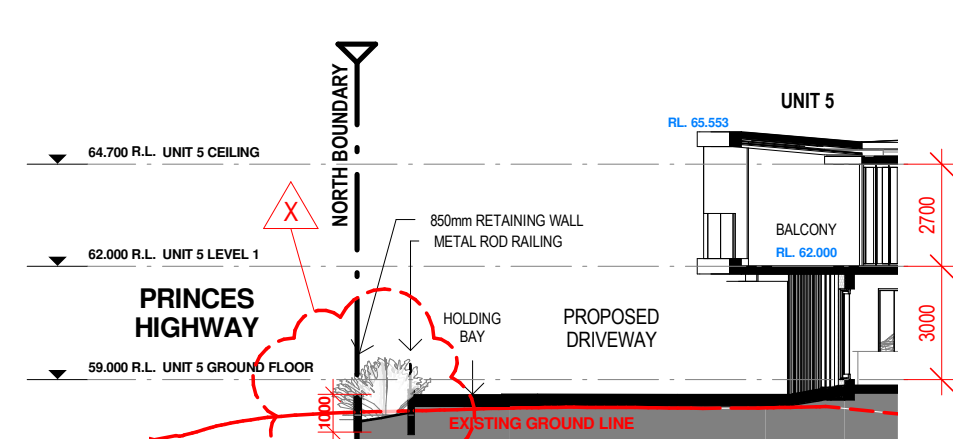
UNIT 1 DRIVEWAY ENTRY

1 : 200



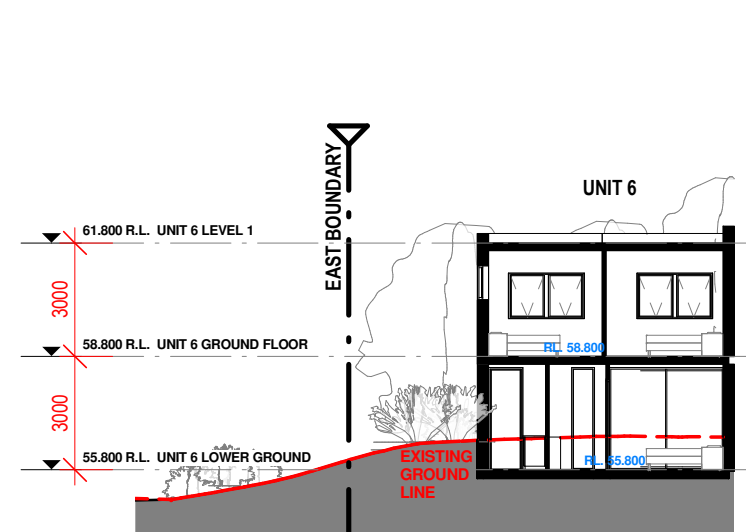
UNIT 3 DRIVEWAY SECTION

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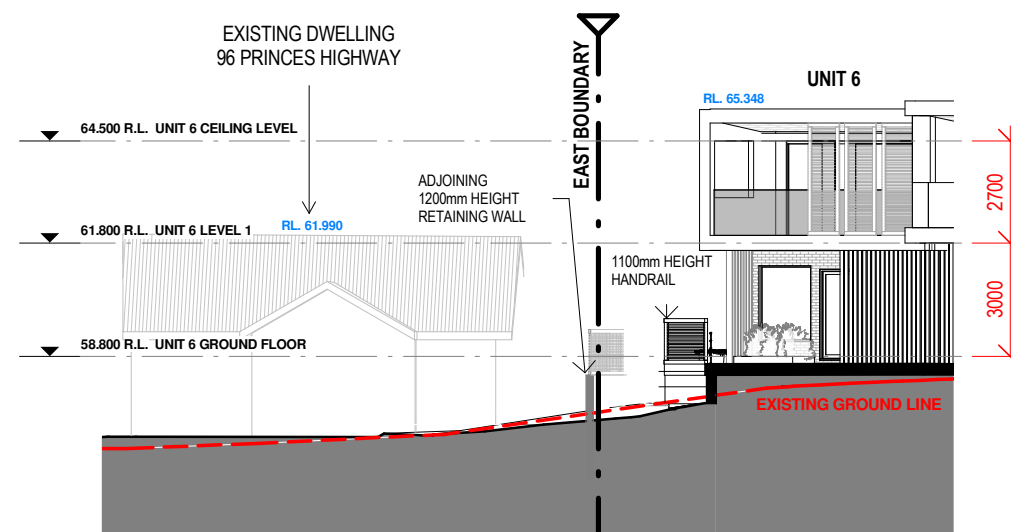
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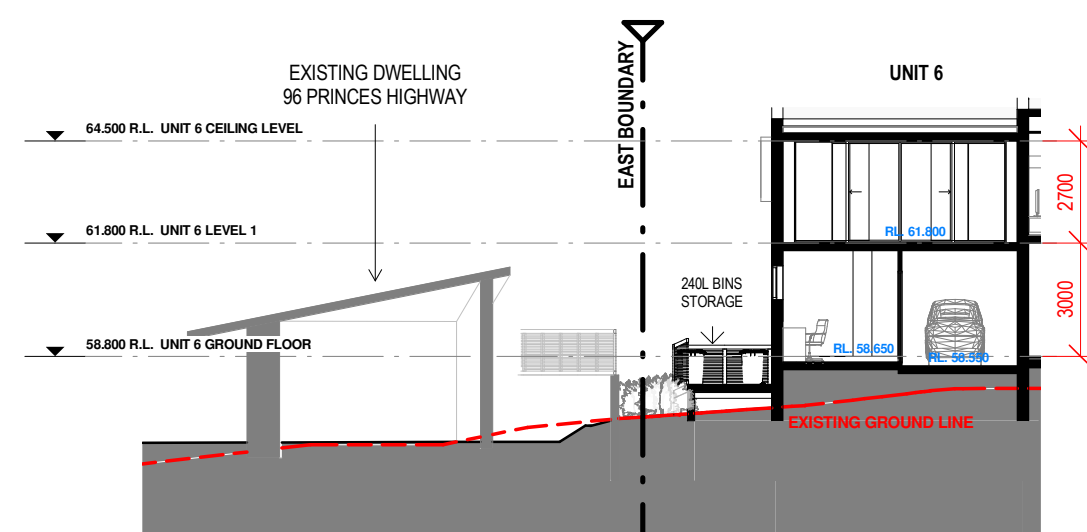
UNIT 6 TO EAST BOUNDARY

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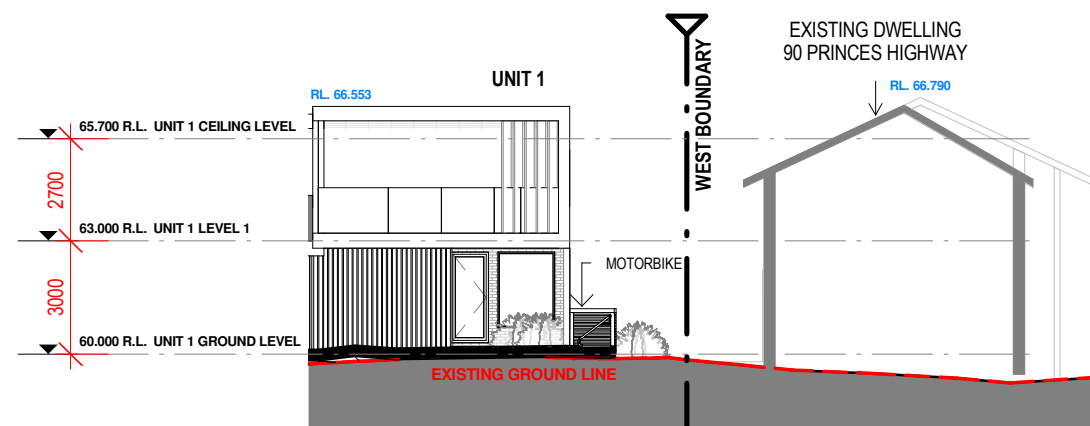
UNIT 6 DRIVEWAY SECTION

1 : 200



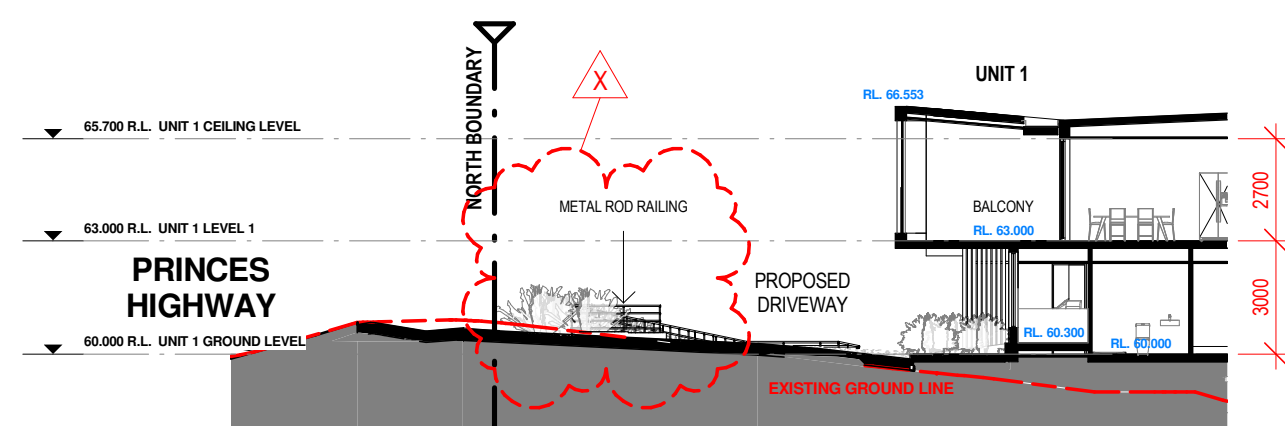
BINS STORAGE SECTION

1 : 200



UNIT 1 TO WEST BOUNDARY

1 : 200



DRIVEWAY SECTION 2

1 : 200

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REF.	DATE	AMENDMENT
X	26.05.2020	ADDITIONAL INFORMATION

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Legend:	
FB01	FACE BRICKWORK TYPE 1
FB02	FACE BRICKWORK TYPE 2
FB03	FACE BRICKWORK TYPE 3
BL	BLOCKWORK
CL01	CLADDING
CL02	CLADDING
RW	RETAINING WALL
R	ROOF
DP	DOWNPIPES
TB	TIMBER BATTENS
D	DOOR
GD	GARAGE DOOR
S	SLIDING DOOR
BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW
FW	FIXED WINDOW
OB	OBSCURE WINDOW
AW	AWNING WINDOW
SK	SKYLIGHT
WH	WINDOW HOOD
LV	LOUVRES
RWT	RAINWATER TANK
P	POST
T	TIMBER FLOORS
CT	CERAMIC TILES
CPT	CARPET
PC	POLISHED CONCRETE
SP	FEATURE SCREENING
IWS	INTEGRAL WALL

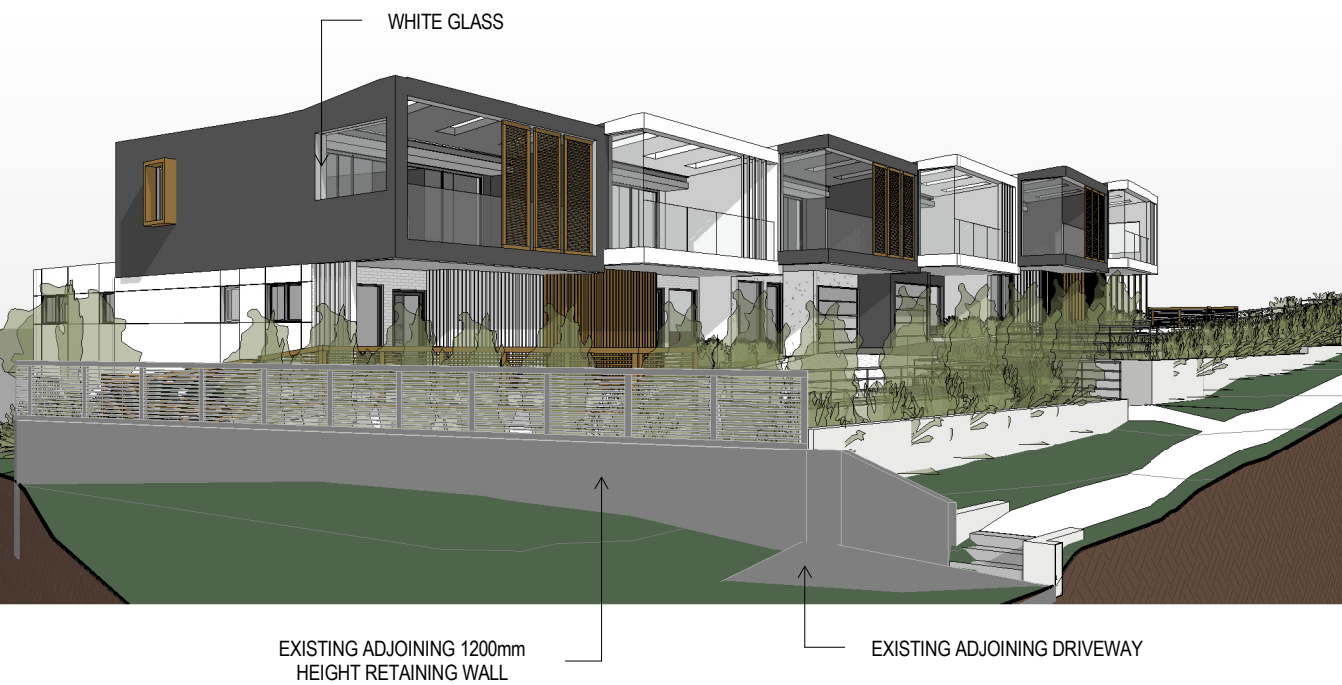
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Nominated Architect:
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CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: DETAIL SECTIONS

DATE: 6/8/2018
DRAWN: TN
SCALE: 1 : 200
QA: RG
PROJECT No.
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DWG No. Rev.
DA-AI X
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BACK COURTYARD VIEWS



FRONT STREET VIEW / ENTRY

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Legend:			
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FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS
BL	BLOCKWORK	D	DOOR
CL01	CLADDING	GD	GARAGE DOOR
CL02	CLADDING	SLD	SLIDING DOOR
RW	RETAINING WALL	BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW	FW	FIXED WINDOW
OB	OBSCURE WINDOW	AW	AWNING WINDOW
SK	SKYLIGHT	WH	WINDOW HOOD
LV	LOUVRES	RWT	RAINWATER TANK
P	POST	T	TIMBER FLOORS
CT	CERAMIC TILES	CPT	CARPET
PC	POLISHED CONCRETE	SP	FEATURE SCREENING
IWS	INTEGRAL WALL		

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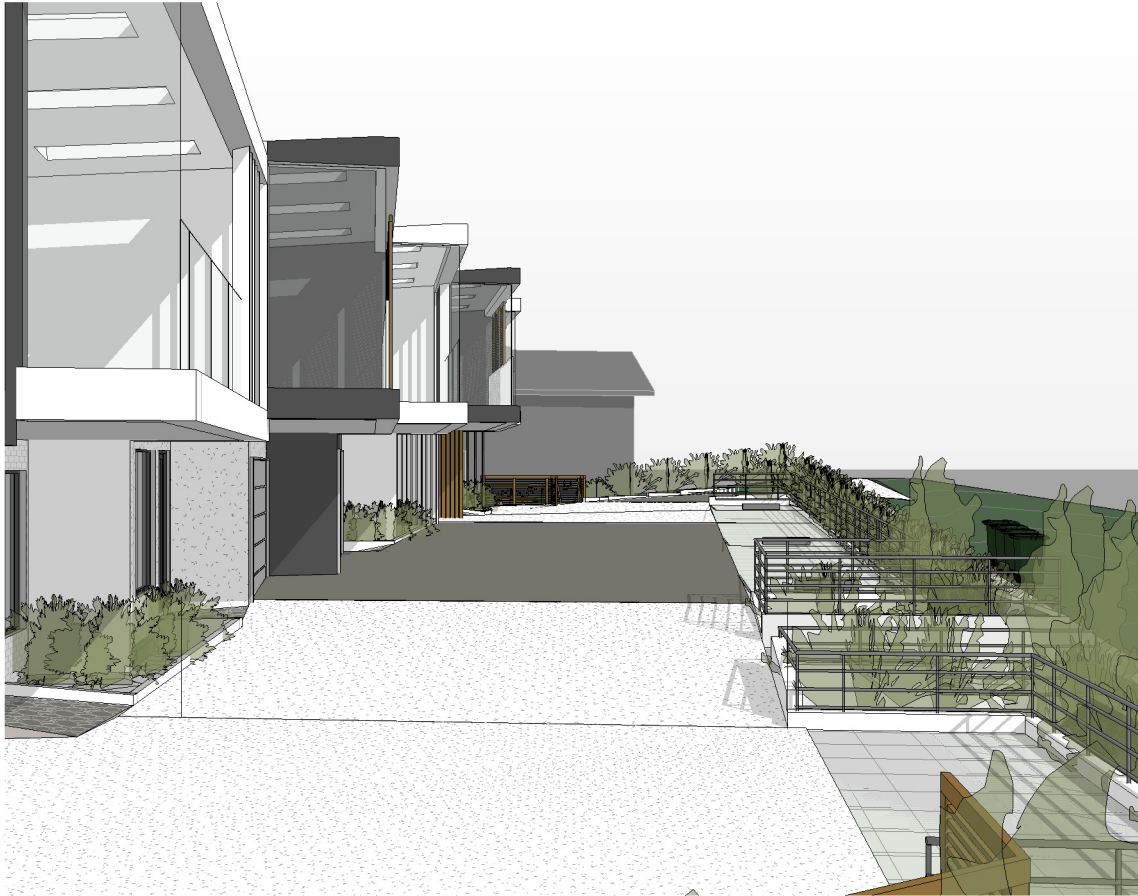
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ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME:	3D VIEWS

ADDITIONAL INFORMATION

DATE:	6/8/2018	PROJECT No.	1815
DRAWN:	TN	DWG No.	DA-AI
SCALE:		Rev.	W
QA:	RG		50



UNIT 6 DRIVEWAY VIEW



PROPOSED DRIVEWAY ENTRY



STREET SITE ENTRY

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Legend:			
FB01	FACE BRICKWORK TYPE 1	R	ROOF
FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS
BL	BLOCKWORK	D	DOOR
CL01	CLADDING	GD	GARAGE DOOR
CL02	CLADDING	SLD	SLIDING DOOR
RW	RETAINING WALL	BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW	FW	FIXED WINDOW
OB	OBSCURE WINDOW	AW	AWNING WINDOW
SK	SKYLIGHT	WH	WINDOW HOOD
LV	LOUVRES	RWT	RAINWATER TANK
P	POST	T	TIMBER FLOORS
CT	CERAMIC TILES	CPT	CARPET
PC	POLISHED CONCRETE	SP	FEATURE SCREENING
IWS	INTEGRAL WALL		

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Nominated Architect:
Robert Gizzi (Reg. 8286)

CLIENT: DA
MEDIUM DENSITY
ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME: 3D VIEWS

DATE: 6/8/2018
DRAWN: TN
SCALE:
QA: RG
PROJECT No. 1815
DWG No. DA-AI
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51

ADDITIONAL INFORMATION



VIEW THROUGH NORTH-WEST BOUNDARY



VIEW FROM SOUTH-EAST BOUNDARY

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ADDITIONAL INFORMATION

REF. W	DATE 22.05.2020	AMENDMENT ADDITIONAL INFORMATION	Legend: <div><div>FB01 FACE BRICKWORK TYPE 1</div><div>FB02 FACE BRICKWORK TYPE 2</div><div>FB03 FACE BRICKWORK TYPE 3</div><div>BL BLOCKWORK</div><div>CL01 CLADDING</div><div>CL02 CLADDING</div><div>RW RETAINING WALL</div></div> <div><div>R ROOF</div><div>DP DOWNPIPES</div><div>TB TIMBER BATTENS</div><div>D DOOR</div><div>GD GARAGE DOOR</div><div>SLD SLIDING DOOR</div><div>BFD BI-FOLD DOOR</div></div> <div><div>SLW SLIDING WINDOW</div><div>FW FIXED WINDOW</div><div>OB OBSCURE WINDOW</div><div>AW AWNING WINDOW</div><div>SK SKYLIGHT</div><div>WH WINDOW HOOD</div><div>LV LOUVRES</div><div>RWT RAINWATER TANK</div></div> <div><div>P POST</div><div>T TIMBER FLOORS</div><div>CT CERAMIC TILES</div><div>CPT CARPET</div><div>PC POLISHED CONCRETE</div><div>SP FEATURE SCREENING</div><div>IWS INTEGRAL WALL</div></div>				DWA DESIGN WORKSHOP AUSTRALIA		Wollongong 81a Princes Highway, Fairy Meadow NSW 2519 Tel: (02) 4227 1661 Email: info@designworkshop.com.au Web: www.designworkshop.com.au	Sydney Level 10, 6 Mount Olympus Boulevard, Wolli Creek NSW 2205 Nominated Architect: Robert Gizzi (Reg. 8286)	CLIENT: DA MEDIUM DENSITY ADDRESS: 92 - 94 PRINCES HIGHWAY, THIRROUL DRAWING NAME: 3D VIEWS	DATE: 6/8/2018 DRAWN: TN SCALE: QA: RG	PROJECT No. 1815 DWG No. Rev. DA-AI W 52
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PRINCES HIGHWAY



JUNE 9 AM SHADOW

1 : 500

PRINCES HIGHWAY



JUNE 10 AM SHADOW

1 : 500

PRINCES HIGHWAY



JUNE 11 AM SHADOW

1 : 500

PRINCES HIGHWAY



JUNE 12 NOON SHADOW

1 : 500

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REF.	DATE	AMENDMENT
V	05.05.2020	ADDITIONAL INFORMATION

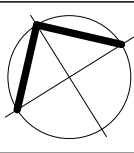
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Legend:			
FB01	FACE BRICKWORK TYPE 1	R	ROOF
FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS
BL	BLOCKWORK	D	DOOR
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FW	FIXED WINDOW	OB	OBSCURE WINDOW
OB	OBSCURE WINDOW	AW	AWNING WINDOW
AW	AWNING WINDOW	SK	SKYLIGHT
SK	SKYLIGHT	WH	WINDOW HOOD
WH	WINDOW HOOD	LV	LOUVRES
LV	LOUVRES	RWT	RAINWATER TANK
RWT	RAINWATER TANK	P	POST
P	POST	T	TIMBER FLOORS
T	TIMBER FLOORS	CT	CERAMIC TILES
CT	CERAMIC TILES	CPT	CARPET
CPT	CARPET	PC	POLISHED CONCRETE
PC	POLISHED CONCRETE	SP	FEATURE SCREENING
SP	FEATURE SCREENING	IWS	INTEGRAL WALL
IWS	INTEGRAL WALL		

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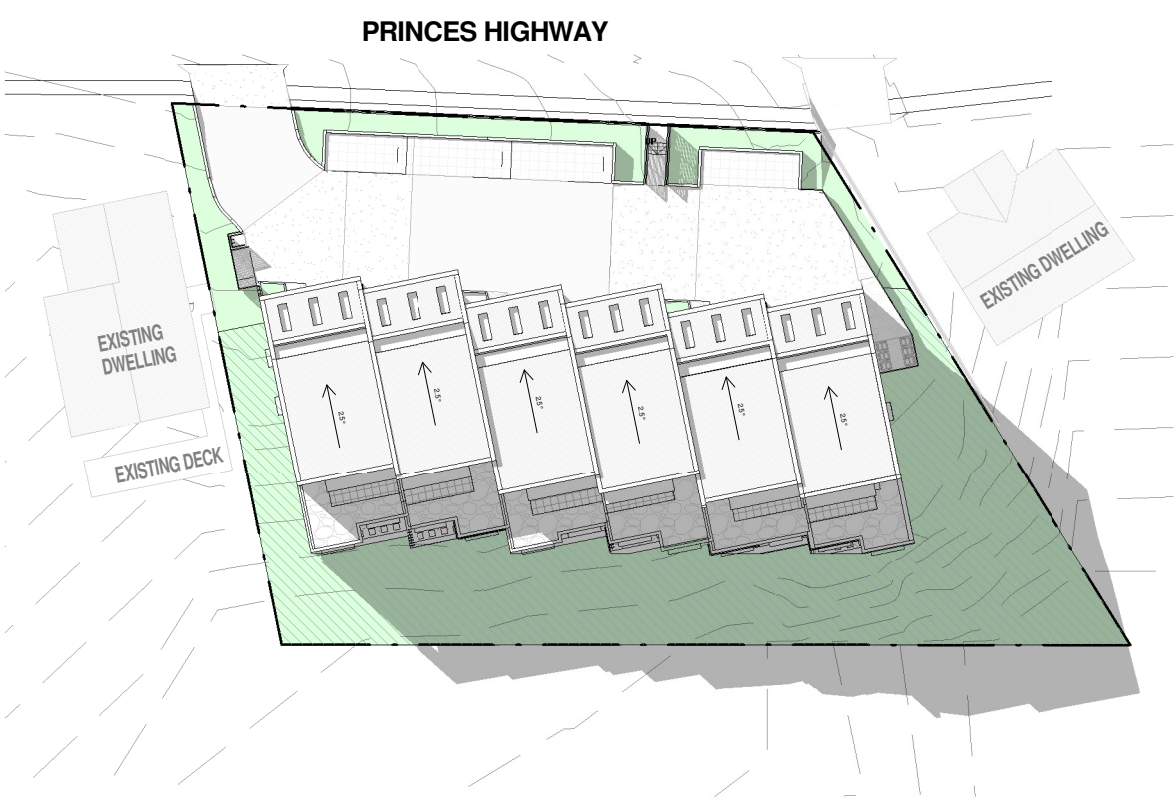
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Nominated Architect:
Robert Gizzi (Reg. 8286)



CLIENT:	DA MEDIUM DENSITY
ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME:	JUNE 9 AM - 12 NOON SHADOWS

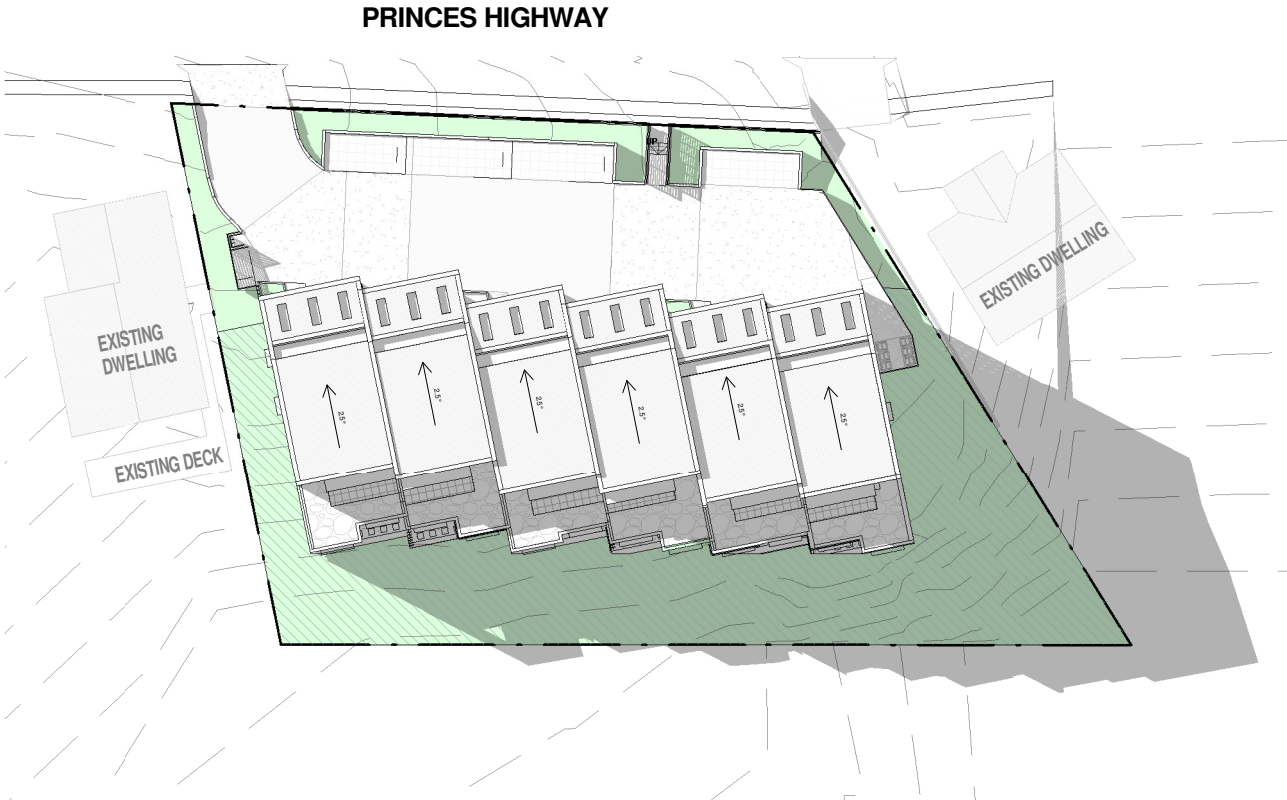
DATE:	6/8/2018	PROJECT No.	1815
DRAWN:	TN	DWG No.	DA-AI
SCALE:	1 : 500	Rev.	V
QA:	RG		40

ADDITIONAL INFORMATION



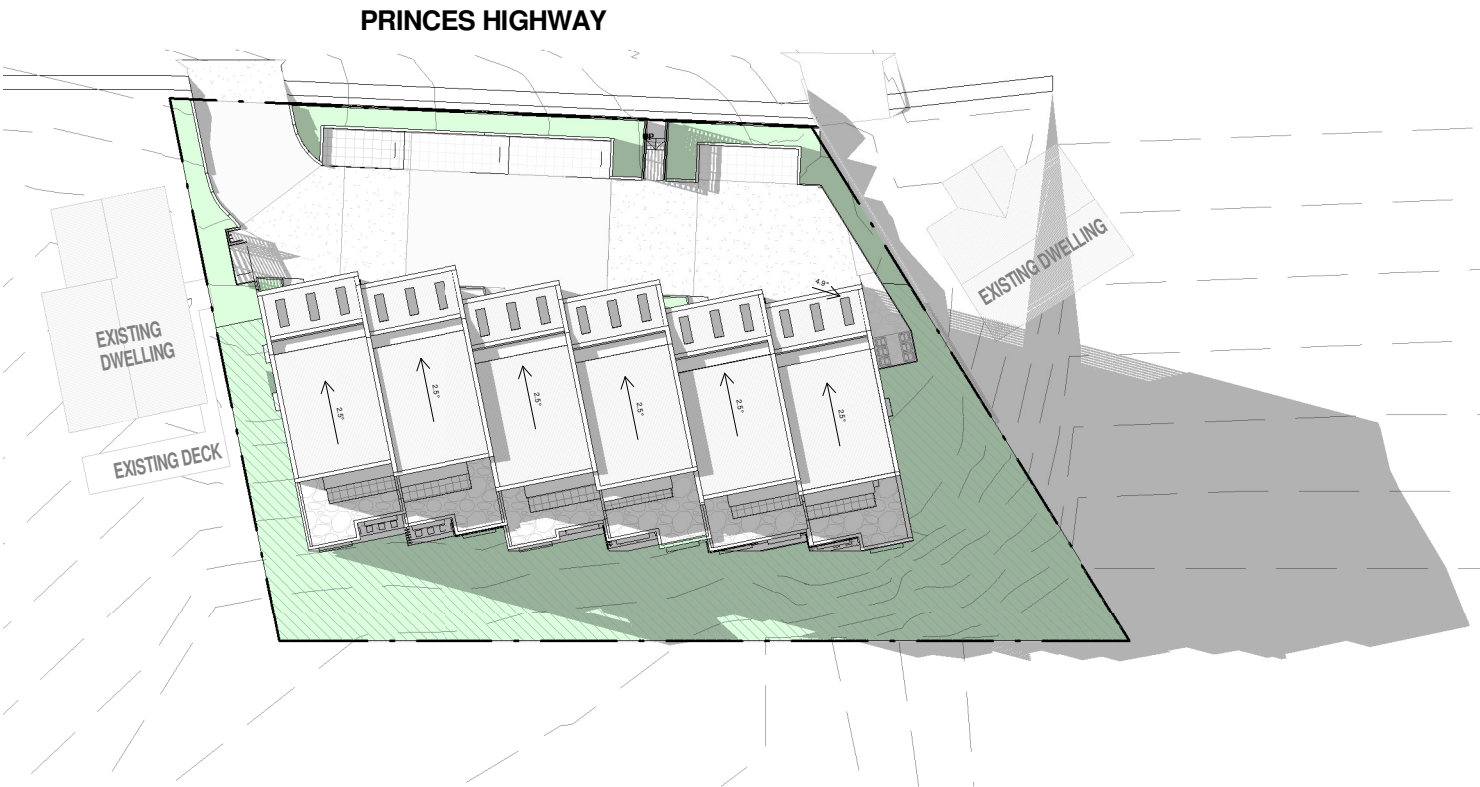
JUNE 1 PM SHADOW

1 : 500



JUNE 2 PM SHADOW

1 : 500



JUNE 3 PM SHADOW

1 : 500

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REF.	DATE	AMENDMENT
V	05.05.2020	ADDITIONAL INFORMATION

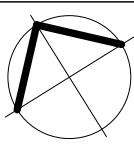
DISCLAIMER
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Legend:			
FB01	FACE BRICKWORK TYPE 1	R	ROOF
FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS
BL	BLOCKWORK	D	DOOR
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CL02	CLADDING	SLD	SLIDING DOOR
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SK	SKYLIGHT	WH	WINDOW HOOD
LV	LOUVRES	RWT	RAINWATER TANK
P	POST	T	TIMBER FLOORS
CT	CERAMIC TILES	CPT	CARPET
PC	POLISHED CONCRETE	SP	FEATURE SCREENING
IWS	INTEGRAL WALL		



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Nominated Architect:
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CLIENT:	DA MEDIUM DENSITY
ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME:	JUNE 1 PM - 3 PM SHADOWS

DATE:	6/8/2018	PROJECT No.	1815
DRAWN:	TN	DWG No.	DA-AI
SCALE:	1 : 500	Rev.	V
QA:	RG		41

ADDITIONAL INFORMATION

PRINCES HIGHWAY



DECEMBER 9 AM SHADOW

1 : 500

PRINCES HIGHWAY



DECEMBER 12 NOON SHADOW

1 : 500

PRINCES HIGHWAY



DECEMBER 3 PM SHADOW

1 : 500

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REF.	DATE	AMENDMENT
V	05.05.2020	ADDITIONAL INFORMATION

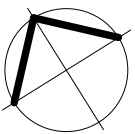
DISCLAIMER
All dimensions are in millimeters. Verify all dimensions on site prior to commencement of any work. Copyright of DWA.

Legend:			
FB01	FACE BRICKWORK TYPE 1	R	ROOF
FB02	FACE BRICKWORK TYPE 2	DP	DOWNPIPES
FB03	FACE BRICKWORK TYPE 3	TB	TIMBER BATTENS
BL	BLOCKWORK	D	DOOR
CL01	CLADDING	GD	GARAGE DOOR
CL02	CLADDING	SLD	SLIDING DOOR
RW	RETAINING WALL	BFD	BI-FOLD DOOR
SLW	SLIDING WINDOW	FW	FIXED WINDOW
FW	FIXED WINDOW	OB	OBSCURE WINDOW
OB	OBSCURE WINDOW	AW	AWNING WINDOW
AW	AWNING WINDOW	SK	SKYLIGHT
SK	SKYLIGHT	WH	WINDOW HOOD
WH	WINDOW HOOD	LV	LOUVRES
LV	LOUVRES	RWT	RAINWATER TANK
RWT	RAINWATER TANK	P	POST
P	POST	T	TIMBER FLOORS
T	TIMBER FLOORS	CT	CERAMIC TILES
CT	CERAMIC TILES	CPT	CARPET
CPT	CARPET	PC	POLISHED CONCRETE
PC	POLISHED CONCRETE	SP	FEATURE SCREENING
SP	FEATURE SCREENING	IWS	INTEGRAL WALL
IWS	INTEGRAL WALL		



Wollongong
81a Princes Highway,
Fairy Meadow NSW 2519
Tel: (02) 4227 1661
Email: info@designworkshop.com.au
Web: www.designworkshop.com.au

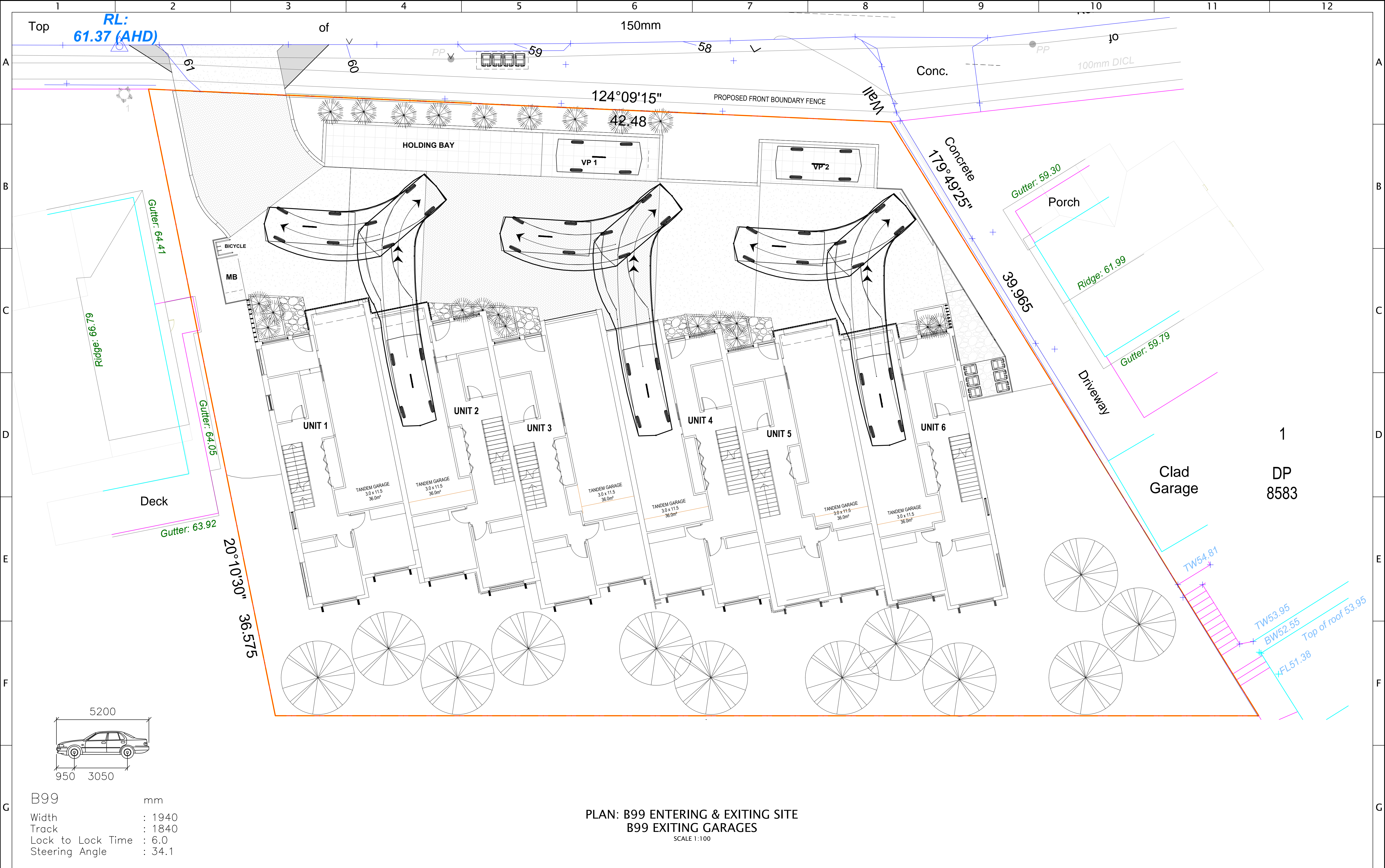
Sydney
Level 10, 6 Mount
Olympus Boulevard,
Wolli Creek NSW 2205
Nominated Architect:
Robert Gizzi (Reg. 8286)



CLIENT:	DA MEDIUM DENSITY
ADDRESS:	92 - 94 PRINCES HIGHWAY, THIRROUL
DRAWING NAME:	DECEMBER 9 AM - 3 PM SHADOWS

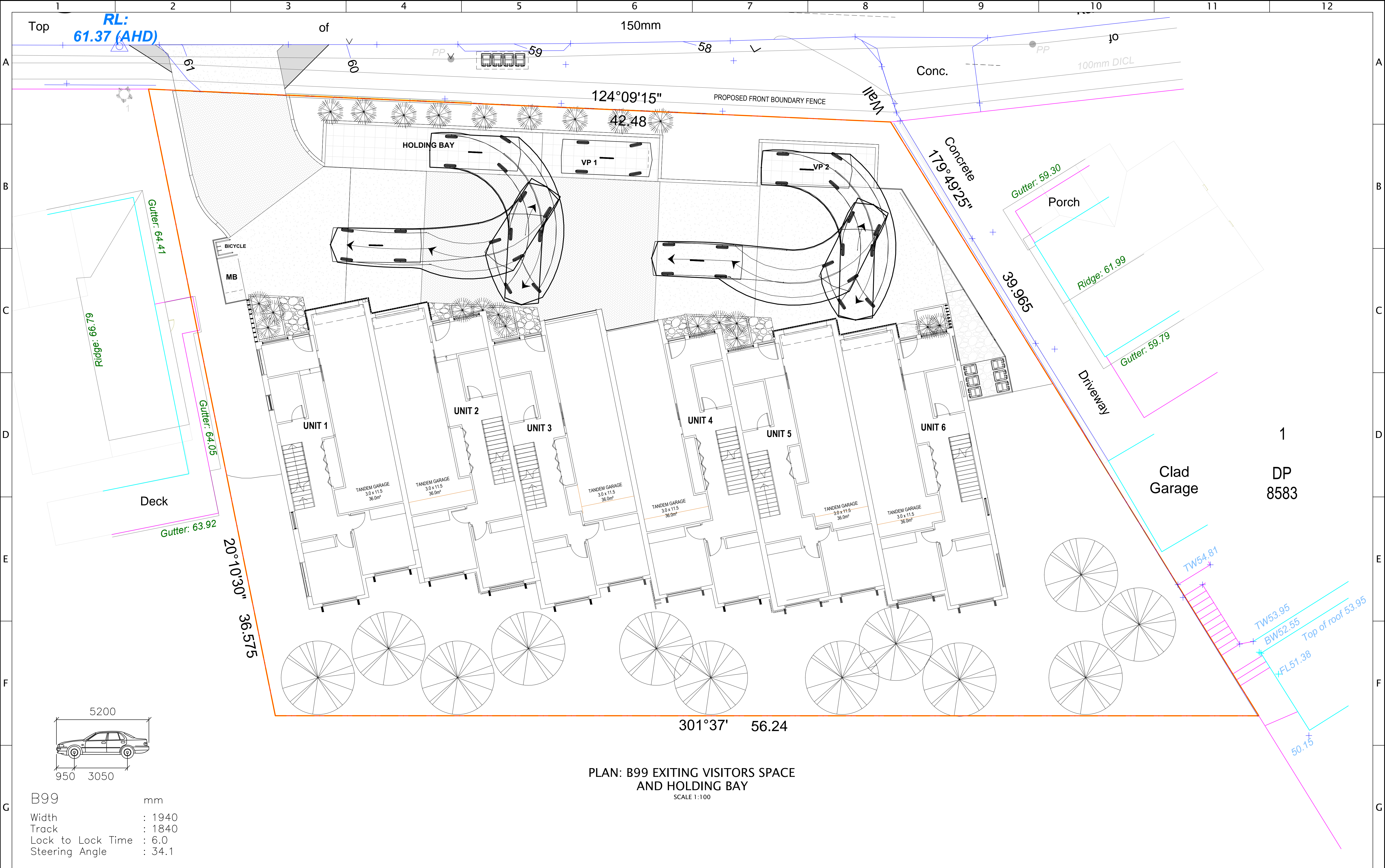
DATE:	6/8/2018	PROJECT No.	1815
DRAWN:	TN	DWG No.	DA-AI
SCALE:	1 : 500	Rev.	V
QA:	RG		42

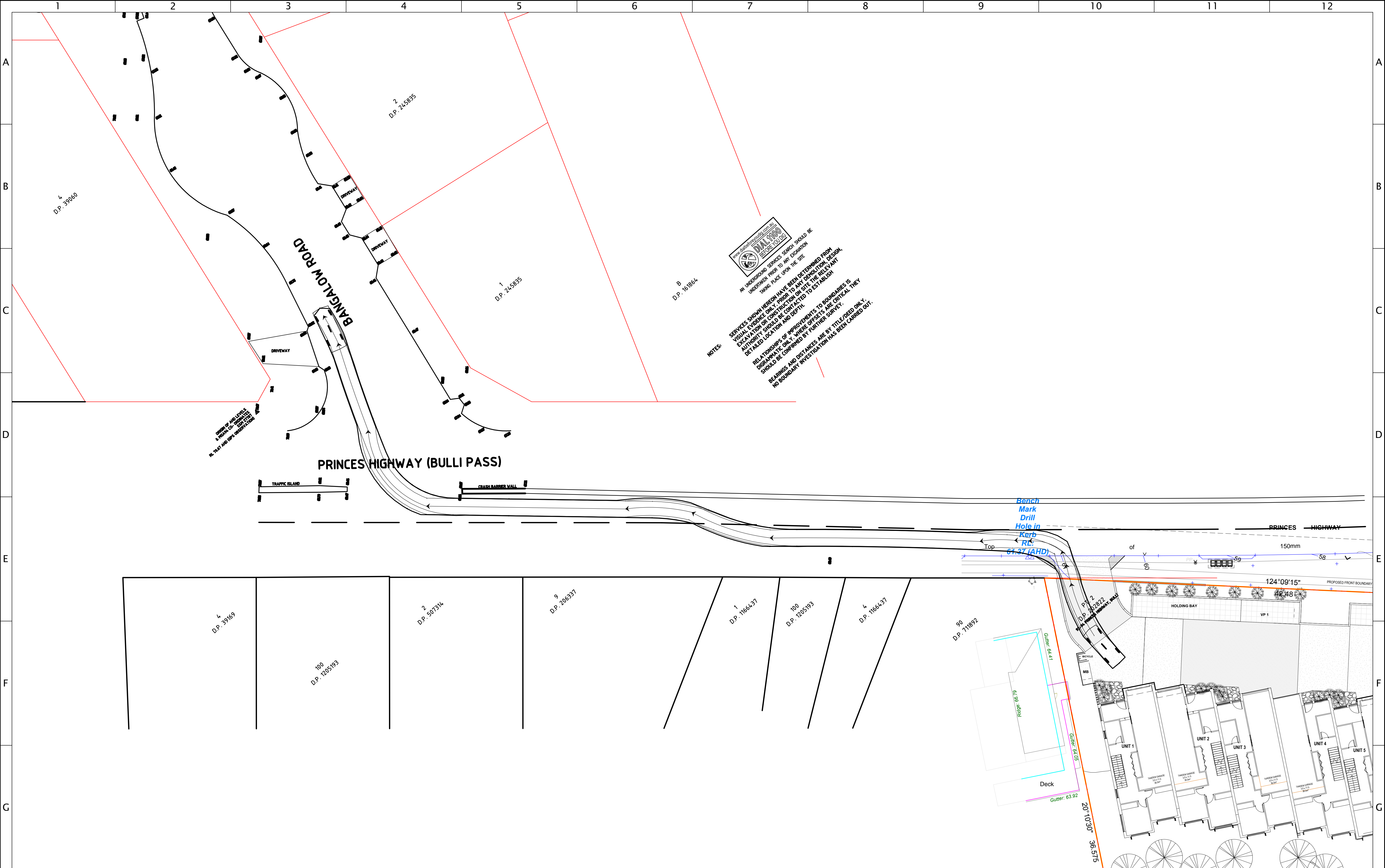
ADDITIONAL INFORMATION



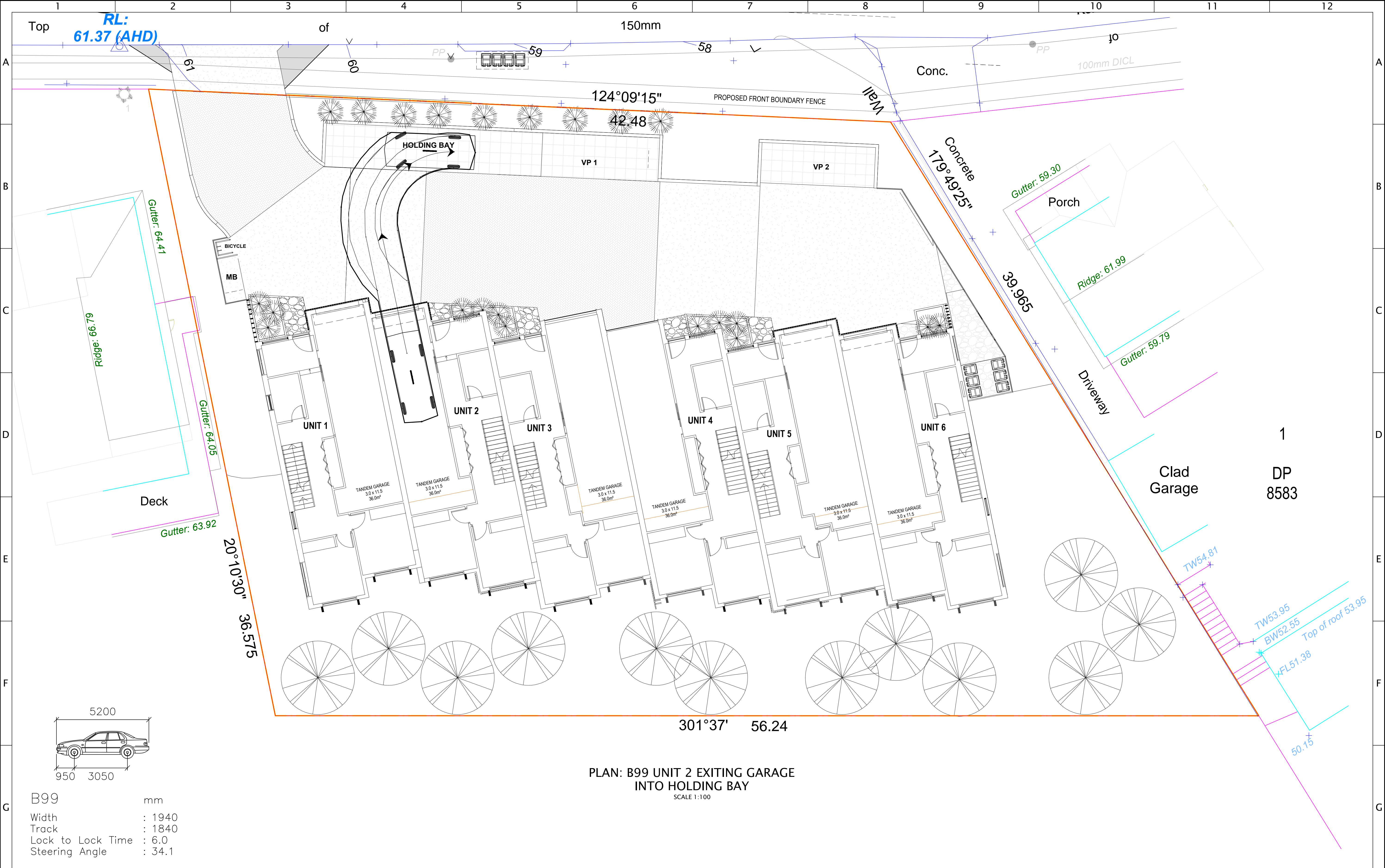
PLAN: B99 ENTERING & EXITING SITE
B99 EXITING GARAGES
SCALE 1:100

ISSUE	AMENDMENT	DATE	PLANS	010002000300040005000	ATB	11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au	ACEA	The Association of Consulting Engineers Australia	Title	PLAN: TRAFFIC ASSESSMENT SHEET 4	SCALES	AS SHOWN	DATE PLOTTED	22/02/19
A	ISSUE FOR DA APPROVAL	17/09/19	DO NOT SCALE IF IN DOUBT ASK THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS COPYRIGHT: Concepts and information contained in these engineering drawings and related documents are the copyright of ATB Consulting Engineers Pty. Ltd. Unauthorised copying of part or whole of the document/s is a breach of copyright.	A1	ATB	11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au	ACEA	The Association of Consulting Engineers Australia	Project	TRAFFIC ASSESSMENT	DRAWN	D.K.	DATUM	A.H.D.
B	MINOR AMENDMENTS	28/01/20							At	92 PRINCES HIGHWAY THIRROUL	DESIGNED	G.U.	DATE CHK'D	22/02/19
C	MINOR AMENDMENTS	11/03/20							Client	AKPINAR	CHECKED	G.U.	PROJECT No	18172
D	HOLDING BAY ADDED, BUILDING LOCATION REVISED	30/04/20											DWG	T4
E	ISSUE FOR ADDITIONAL INFORMATION	13/07/20											REVISION	G
F	ISSUE FOR ADDITIONAL INFORMATION	06/08/20												
G	ISSUE FOR ADDITIONAL INFORMATION	24/08/20												





ISSUE		AMENDMENT	DATE	PLANS				<div>11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au</div>		Title		PLAN: TRAFFIC ASSESSMENT SHEET 9		SCALES		AS SHOWN		DATE PLOTTED	
A		ISSUE FOR DA APPROVAL		23/04/20						Project		TRAFFIC ASSESSMENT		DRAWN		D.K.		22/02/19	
B		ISSUE FOR ADDITIONAL INFORMATION		13/07/20						At		92 PRINCES HIGHWAY THIRROUL		DESIGNED		G.U.		DATUM A.H.D.	
C		ISSUE FOR ADDITIONAL INFORMATION		06/08/20						Client		AKPINAR		CHECKED		G.U.		DATE CHK'D	
D		ISSUE FOR ADDITIONAL INFORMATION		24/08/20						A1								22/02/19	
				DO NOT SCALE IF IN DOUBT ASK															
				THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS															
				COPYRIGHT: Concepts and information contained in these engineering drawings and related documents are the copyright of ATB Consulting Engineers Pty. Ltd. Unauthorised copying of part or whole of the document/s is a breach of copyright.															
																PROJECT No		18172	
																DWG		T9	
																REVISION		D	



ISSUE		AMENDMENT	DATE	PLANS	010002000300040005000			Title		PLAN: TRAFFIC ASSESSMENT SHEET 13	SCALES AS SHOWN		DATE PLOTTED		
A B	ISSUE FOR ADDITIONAL INFORMATION ISSUE FOR ADDITIONAL INFORMATION		06/08/20 24/08/20	<div>DO NOT SCALE IF IN DOUBT ASK</div> <div>THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS</div> <div>COPYRIGHT: Concepts and information contained in these engineering drawings and related documents are the copyright of ATB Consulting Engineers Pty. Ltd. Unauthorised copying of part or whole of the document/s is a breach of copyright.</div>				Project		TRAFFIC ASSESSMENT	DRAWN	D.K.	22/02/19		
								At		92 PRINCES HIGHWAY THIRROUL	DESIGNED		G.U.	DATUM A.H.D.	
								Client		AKPINAR	CHECKED		G.U.	DATE CHK'D 22/02/19	
										PROJECT No	18172	DWG	T13	REVISION	B

Wollongong Design Review Panel
Meeting minutes and recommendations DE-2018/214

Date	29 January 2019
Meeting location	Wollongong City Council Administration Offices
Panel members	David Jarvis
	Tony Quinn
	Sue Hobley
Apologies	
Council staff	John Wood - City Wide Development Manager Brigit Mathai – Development Project Officer
Guests/ representatives of the applicant	Robert Gizzi - Design Workshop Australia Luke Rollinson – MMJ Wollongong Goran Ugrinovski – ATB Consulting
Declarations of Interest	Nil
Item number	1
DE number	DE-2018/214
Determination pathway	SEPP 65
Property address	92-94 Princes Highway Thirroul
Proposal	Multi Dwelling Housing
Applicant or applicant's representative address to the design review panel	
Background	The site was Inspected by the Panel on 29 January 2019
Design quality principals SEPP65	
Context and Neighbourhood Character	<p>The proposal is accessed from a very busy road to the north and has a highly desirable outlook across open space to the south. The site falls significantly from front to rear.</p> <p>The proposal provides a driveway that runs adjacent to the road along the site's wide (42.48m) frontage. This is not a strategy that would typically be endorsed by the panel as it can result in a site frontage that is dominated by hard paving and garage doors. However, given the context of the site and its steep topography it is acknowledged that this is a very efficient strategy that could be developed to provide an acceptable outcome, if the following issues are addressed:</p> <ul style="list-style-type: none"> - Functional vehicle manoeuvring must be demonstrated. All cars must be capable of exiting the site in a forward direction. - Adequate deep soil landscaping must be provided to screen the driveway from the road. - Consideration is to be given to the quality of driveway materials. - Garage doors must not dominate the street frontage. Doors should utilise high quality materials and be recessive. The strategy to vary the door finish to each unit as currently shown on perspectives is endorsed by the Panel. - The level of the proposed driveway is currently unclear, further information is required. The driveway must be kept as close as possible to natural ground level if its impact is to be minimised. The position of the driveway entry may need to be reconsidered if an elevated driveway is to be

	avoided.
Built Form and Scale	<p>The proposed site planning is considered acceptable, pending resolution of the issues outlined above (see Context and Neighbourhood Character).</p> <p>The building's set-backs to side boundaries do not comply with council controls. However, it is acknowledged that:</p> <ul style="list-style-type: none"> - Set-backs are in excess of 3m, which will allow for landscaping to provide a buffer with neighbours. - There appears to be minimal impact from overshadowing. - Potential privacy issues can be minimized with further detail refinement. <p>Further development of the proposal and documentation package, should seek to demonstrate how the potential impacts of the non-compliant side set-backs have been mitigated.</p> <p>The living and dining area of each unit is located on the upper level, is detached from the rear garden and does not capture the desirable southern outlook. It is recommended that the section of each unit is developed to step down a full level to the rear of the site. This will create the opportunity to provide a south facing terrace to each living room (located above the bed rooms). Each living area will then have a direct connection to a more generous area of private open space, take advantage of the southern outlook and creating good potential for cross ventilation.</p> <p>This strategy will also provide a more direct connection to the rear gardens at the lower ground level and reduce the bulk of the building in the rear of the site, which may also help to justify the non-compliant side boundary set-backs.</p> <p>Care must be taken when developing the rear terraces to minimize potential privacy issues with neighbours. The building must be analyzed in its context and screening / planters developed to minimize potential privacy issues. Consideration should be given to the privacy of neighbours adjoining both the side and rear boundaries of the site.</p>
Density	<p>The proposal appears compliant with council's FSR control. Pending further resolution of the building's relationship with the street, appropriate landscaping and development of the building section the proposal will not read as an over-development of the site.</p>
Sustainability	<p>Opportunities to harvest rainwater for use in maintaining any plantings established on the building or the site should be explored. Other water minimization measures (reuse of rainwater for toilet flushing and washing machines) should also be considered. The use of photovoltaic cells and solar panels is also</p>

	<p>encouraged.</p> <p>Species selection for any plantings should aim to support council's commitment to maintaining local biodiversity and natural landscapes, water sensitive design and preventing future weed problems.</p>
Landscape	<p>The landscape plan fails to relate to its ecological context and responds mundanely/inappropriately to its environmental context.</p> <p>The following concerns are raised by the panel:</p> <ul style="list-style-type: none"> - the locality is characterised by large, indigenous trees dotted throughout domestic landscapes that link the suburban development to the vegetation of the forested escarpment, and the site itself adjoins E3 zoning, but the indicative species list does not include any of the large local tree species - a large (exotic) canopy tree is to be removed from the front of the site but only small trees are proposed to replace it, altering the character of the streetscape and diminishing amenity (particularly in terms of shade and wind protection) to the site and its neighbours - expansive views across the rural and natural landscape to the south are available from the rear of the development but the proposal is to provide "screen planting to curtilage" - Ground level views to the coast will be largely obstructed by screen plantings - screen plantings along the western boundary may potentially impact on neighbour's views (no analysis is provided to demonstrate whether or not this will be the case) - the rear of the site appears to have been substantially filled creating a split level landscape with almost 2m height difference between the upper and lower levels but the landscape plan does not appear to address this - the rear landscapes of the units are poorly resolved in terms of topography, plantings (which will crowd out the limited open space for recreation), functionality and links to indoor spaces - the species list includes numerous species that have the potential to cause weed management problems in local bushland. <p>It is noted that stormwater management is an important concern on this property. The landscape plan should address the proposed stormwater management scheme to ensure no conflicts between plantings and stormwater infrastructure and to deal with stormwater in the southern landscape of the site that will not be connected to the stormwater infrastructure.</p>
Amenity	<p>Further development of the building section is recommended to improve outlook, cross ventilation and connections to private open space (as outlined above, Built Form). This will require a reconfiguration of the stairs and development of the living room, dining room and kitchen layout.</p>

	<p>The ground level study / entry will function better as a generous entry space than a study. With this in mind, it may be better to reallocate some of the space currently used as a long study desk to storage space. Some of which could be accessed from the garage and some of which could provide a coat cupboard within the entry.</p> <p>The landscape plan needs to be revisited to take better advantage of and respond sensitively to its context. The front of the development will require a treatment that enables enjoyment of views, summer shade, winter sun, and a buffer between the carparking and the street. The rear of the site will require a treatment that resolves the topographical problems, takes full advantage of the fine prospect, and provides functional outdoor spaces for play and recreation. Adverse impacts on neighbours must and local ecological assets must be avoided through appropriate planting plans and species selection.</p>
Safety	The applicant is encouraged to seek further clarification from RMS that multi dwelling developments are acceptable on this very busy road.
Housing Diversity and Social Interaction	This is an appropriate typology of housing for this location, that will provide a more affordable housing option for this neighbourhood.
Aesthetics	<p>Initial perspectives show a reasonable contemporary aesthetic applied to the expression of the buildings. However, the level of information provided is limited. Far more development is essential if this design intent is to be realised.</p> <p>The strategy to vary finishes to provide variety and some identity to each unit is endorsed.</p> <p>The extent of screens to the street façade should be developed to respond to the rooms they serve. There must be a balanced approach to providing privacy, outlook, shade and solar access.</p> <p>Servicing of the building should be considered at this stage of the design process. The location of service risers, AC condensers and down pipes should be accommodated.</p>
Key issues, further Comments & Recommendations	<p>It appears that a reasonable strategy has been proposed for the development of this site. However, more detailed information, analysis and testing of this strategy is required. In particular, further consideration is to given to the following issues:</p> <ul style="list-style-type: none"> - Further information / development is necessary to demonstrate that the proposed location of the driveway /site planning are acceptable. - Further information / development is necessary to demonstrate that the proposed non-compliant side set backs are acceptable.

	<ul style="list-style-type: none">- Further development of the building section to provided improved amenity to each unit.- Further development of a landscape plan to address the opportunities and constraints of the site- Implementation of environmental initiatives.- Further development of building aesthetic
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PRE-LODGEMENT NOTES – PL-2019/33

26-Mar-2019

Property:	92-94 Princes Highway, THIRROUL NSW 2515 Lot 2 DP 202822	
Proposal:	Residential - multi dwelling housing	
Attendees:	<i>Council:</i>	Charlie Bevan- Traffic Engineer John Madry – Landscape Architect Mathew Carden – Stormwater Engineer Brigit Mathai – Development Project Officer
	<i>Proponent:</i>	Luke Rollinson- MMJ Goran Urganovski – ATB Rob Gizzi - DWA

Proposal/Project Overview:	Multi dwelling development comprising six(6) 2 storey split level Units
Meeting Outcomes Summary:	<p>Development is considered excessive for site presenting variations to development standard in relation to the FSR and other development controls.</p> <p>Any future development application submission should respond to the issues and comments as detailed within the notes in particular the stacked parking and manoeuvring, floor space ratio and the DCP non-compliances.</p> <p>Design advice from the Design Review Panel (DE- 2018/214) must be considered in the preparation of the plans prior to the lodgement of application.</p>

Main Issues:

- Floor space ratio
- Parking, Access and manoeuvring
- Stormwater drainage
- Side setbacks
- Development considered excessive for site presenting variations to development standard in relation to the FSR and other development controls.

Planning:

Relevant Environmental Planning Instruments

The provisions of all relevant Environmental Planning Instruments and Development Control Plan(s) must be addressed within the Statement of Environmental Effects (SEE).

The relevant Environmental Planning Instruments and Development Control Plans are:

- Wollongong Local Environmental Plan 2009 (WLEP 2009)
- Wollongong Development Control Plan 2009 (WDCP 2009)
- Wollongong City-Wide Development Contributions Plan 2018

Wollongong Local Environmental Plan 2009 (WLEP 2009)

A comprehensive Statement of Environmental Effects (SEE) addressing all relevant clauses of WLEP 2009 including but not limited to:

Clause 2.3 Zone objectives and land use table

Clause 4.3 Height of buildings

Clause 4.4 Floor space ratio.

Wollongong Development Control Plan 2009 WDCP 2009)

A comprehensive Statement of Environmental Effects (SEE) including all relevant site constraint reports is to be submitted. The SEE must address all relevant chapters as relates to the development proposal and provide full justification for any variation requests in accordance with Cl. 8 of Chapter A1.

Wollongong City-Wide Development Contributions Plan 2018:

A development contributions levy will apply to the proposed development if approved. A detailed cost estimate report is required to be provided in conjunction with the Development Application. (Please note: Council uses the Cordell's Ecosting Guide to confirm the accuracy of construction cost estimates).

General planning issues:

- Compliance tables for both instruments (WLEP 2009 & WDCP 2009) should be detailed within the documentation and submitted for assessment with any application lodged. Any variation to a control contained within the WDCP 2009 should be accompanied by a variation statement. Impacts to the neighbouring dwelling to the western side in terms of view loss and visual amenity are to be assessed and submitted with any variation to side setback on that side.
- A comprehensive Site and Context Analysis Plan prepared in accordance with the minimum standards outlined within Chapter A1, Section 9 of Wollongong Development Control Plan 2009 is to be submitted with the development application.
- Chapter B1: Residential Development applies and all controls are to be addressed in the statement.
- Siting of the Private Open Space for the units need to be carefully considered so as to avoid overlooking to the neighbouring properties.
- Consideration of Chapter E2 Crime Prevention through Environmental Design is required in the design of the development. The proposal is to address all the requirements contained in this chapter which includes lighting, landscaping and fencing for the development.
- Given the site locality and scale of the development proposed, it is likely that the proposal would require referral to the Wollongong Local Planning Panel.

Site information/constraints:

A Section 10.7 Certificate should be obtained to clarify details on any constraints affecting the proposed development site. All relevant site constraint reports should be included within the Statement of Environmental Effects.

- **10.7 (2) Certificate** - Provides information about the zoning of the property, the relevant state, regional and local planning controls and other planning affectations such as heritage, land contamination and road widening; and
- **10.7 (2) and (5) Certificate** - Provides additional advice regarding demolition, foreshore building lines, other heritage considerations and general advice.

Stormwater/Flooding:

- The development is subject to the controls in Chapter E14 (Stormwater Management) of the Wollongong Development Control Plan (WDCP) 2009.
- A stormwater concept plan including on-site stormwater detention (OSD) will need to be prepared by a suitably qualified civil engineer in accordance with Chapter E14 of the Wollongong DCP2009 and submitted with the development application.
- Stormwater disposal will need to comply with Section 11.3.6 and 11.3.3 of Chapter E14 of the Wollongong DCP2009.
- The watercourse to which it is proposed to discharge stormwater must be well defined, having defined bed and banks. This will need to be confirmed by site investigations and survey.
- The submission of documentary evidence will be at the lodgement of the development application, confirming that the owner(s) of the downstream land owner agree to the provision of a drainage easement through their property.
- Details of the proposed stormwater disposal pipeline and outlet to the watercourse will need to be provided in conjunction with the stormwater concept plan, along with surveyed details of any trees within the vicinity of the proposed pipeline.
- An independent detailed survey plan of the site by a registered surveyor to Australian Height Datum (AHD), including lot boundaries, contours/spot levels, easements, services, existing drainage infrastructure etc. will need to be submitted with the development application.
- The landscape and stormwater plans for the development will need to be compatible.

Contact Details: The applicant's Consulting Engineer may contact Council's Stormwater Engineer through Council's Customer Service Centre on the telephone number (02) 4227 7111 if any issues arise through the design phase prior to the lodgement of the development application.

Traffic:

General

- The applicant should refer to Chapter E3 – Car Parking, Access, Servicing/ Loading Facilities and Traffic Management of the Wollongong Development Control Plan 2009.

- The applicant must provide all internal access dimensions on the site plan, including grades, access widths, parking aisle widths which comply with AS2890.1.
- A Traffic Assessment will need to be prepared by a suitably qualified consultant in accordance with the RTA Guide to Traffic Generating Development. The assessment must quantify the trip generation, impacts on the local road network and certifies the suitability of the proposed access, internal road design and car parking arrangement.
- The Traffic Impact Assessment must address any potential impacts from additional vehicular travel as vehicles make their way to and from the site in a northbound and southbound direction.
- The applicant must ensure that the development complies with State Government Guidance – Development near Rail Corridors and Busy Roads: Interim Guideline 2008.

Access and Manoeuvring

- The first 6 metres of the access driveway from the property boundary needs to be a minimum of 5.5 metres wide and with a maximum gradient which complies with Clause 3.3 of AS2890.1.
- The access design should ensure that adequate pedestrian and vehicle sight distance is provided as per AS2890.1.
- Swept paths need to be provided showing a B99 vehicle passing a B85 vehicle within the site.

Manoeuvring Concerns relating to this Application

- Encroachment on the proposed visitor spaces occurs when vehicles are manoeuvring out of the garages.
- Not all garage exit swept paths have been undertaken.
- Turning for visitors is problematic and encroaches on the proposed 'holding/waiting' bay.

Parking

- 1 car space per dwellings (<70m²) or 1.5 car spaces per dwelling (70-110m²) or 2 car spaces per dwelling (>110m²), plus 0.2 car parking spaces per dwelling for visitors.
- 1 bicycle space per 3 dwellings (residents) and 1 bicycle space per 12 dwellings (visitors).
- 1 motorcycle space per 15 dwellings.

Stacked Car Parking

- Stacked car parking spaces can only be accepted if they meet the points under Clause 7.7 of Chapter E3 of the DCP.
 - The applicant must demonstrate that there is a need for stacked parking and that the provision of stacked parking will not adversely affect the safe, efficient and effective use of the site;

- No more than two cars are parked in a stacked arrangement, so that no more than one vehicle has to move to allow egress of another;
- Provision shall be made on site for shifting cars without the movement of vehicles onto public streets; and
- Stacked spaces are only permitted where both spaces are utilised by the same dwelling and such spaces do not interfere with common manoeuvring areas.

Concerns relating to Stacked Parking Proposed under this Application

- There is a concern that the provision of stacked parking will adversely affect the safe, efficient and effective use of the site which is contrary to Clause 7.7 (2) (b) (i) of Chapter E3 of the DCP.
- The applicant has not demonstrated how vehicles will be shifted without moving onto public streets. This is contrary to Clause 7.7 (2) (b) (iii) of Chapter E3 of the DCP.
- Compliance with the above controls needs to be demonstrated by the applicant with swept paths showing a vehicle exiting all stacked spaces and parking in a temporary location within the site (not obstructing other parking spaces or garage entrances), while the rear vehicle is being moved out. Without this type of facility (waiting bay) the shifting of vehicles would result in internal congestion, loss of amenity and safety concerns.
- The Traffic Section is concerned that the proposed layout would be inconvenient for residents, and result in difficulties while moving stacked vehicles.

Residential Bicycle Security

- The applicant should show the location of residential bicycle parking which provides the appropriate level of security (User Class B) as required by AS2890.3 and Austroads. This should either be provided individually within the dwelling (indicated on plans and not encroaching on garage space) or in a secure communal compound and protected from the weather.

Visitor Bicycle Security

- The applicant should provide any required visitor bicycle spaces in an accessible area within the site. These spaces have lower security requirements (Class C) and can be rails which are protected from the weather.

Waste Servicing and Deliveries

- Waste collection details are to be provided, such as the location of the bins for storage and collection, method of collection, and size of collection vehicle.
- It should be noted that street collection can be accepted where the total number of waste and recycling bins for all uses can be accommodated within 50% of the site frontage on collection day (Chapter E7, Clause 5.4.3). The applicant will need to show the bins on the road reserve fronting the site to allow assessment of the impact on the street frontage.

- There is a crash barrier located along the frontage of the property which could obstruct bin collection. In view of this, the applicant will need to obtain written agreement from Council's waste contractor (currently Remondis) who will need to agree to the collection of bins from behind this barrier in order for street collection to be accepted.
- If Council's waste contractor is unable to accept the collection of bins from behind the barrier, waste will need to be collected from within the site. Turning for waste collection vehicles (no more than 3 turning movements) should be demonstrated using swept paths. Overhead clearances must also be observed. The operating clearances for garbage trucks can be found within Chapter E7 of the DCP.
- AS2890.2 provides a maximum grade of 15.4% for service vehicles which should also be taken into consideration.

Contact Details: The applicant's Traffic Engineer may contact Council's Traffic Engineer through Council's Customer Service Centre on the telephone number (02) 4227 7111 if any issues arise through the design phase prior to the lodgement of the development application.

Landscape:

- Streetscape grades need to be taken into consideration with steep grass verge which is not ideal with a retaining wall at eastern end making it difficult for pedestrians, bin etc. A 2.5% crossfall should be achieved from property boundary to back of kerb.
- Large trees are noted on site, no objection to removal of exempt species and trees within proposed building footprint.
- Submitted Landscape Plan needs to take into consideration planning for bushfire protection.
- The developer is required to submit a Landscape Concept Plan (scale 1:100 or 1:200) as part of the Development Application in accordance with the requirement of Chapter E6 – Landscape of Wollongong Development Control Plan 2009. The landscape plan must identify all proposed retaining walls, driveways, fences existing/proposed and existing vegetation to be removed/or retained and any proposed car parking area.
- The landscape concept plan must show all existing trees on site accurately plotted, species identified and numbered to correspond with the arborist report. Clearly show which trees are to be retained or removed.
- Landscape Plan to be prepared by Registered Landscape Architect or person eligible for registration with the Australian Institute of Landscape Architects in accordance with the requirement of Chapter E6 – Landscape of Wollongong Development Control Plan 2009. Plan submitted is noted which will require major modification to comply with site requirements.
- Turfed areas within Deep Soil Zone contrary to DCP requirements is noted, however proposal may offer additional amenity, long term maintenance advantages and comply with Bushfire requirements. The SE corner of site may offer opportunities for alternative additional Deep Soil Zone location.
- Site and Context Analysis plan to be provided in accordance with the requirement of Chapter E6 – Landscape of Wollongong Development Control Plan 2009 and Chapter A1 Clause 9 Site and Context Analysis. Site Analysis plan submitted noted. Proximity to public transport and pedestrian movement should also be added and included in SEE. Steep footpath and challenging parking arrangements and access noted.

- The protection and enhancement of surrounding suburb character is required as outlined in Wollongong Development Control Plan 2009 – Chapter D1: Character Statements and Chapter E6: Landscaping Section 5 Neighbourhood Amenity and Character.
- An Arborist Report is required in relation to any tree on the subject site (other than an exempt tree) and trees on neighboring properties that will be affected by the development, if it is 3 metres or more in height, or has a trunk diameter of 200mm or more at a height of 1 metre from the ground, or has a branch spread of 3 metres or more.
- This report must be completed by a qualified Arborist who is eligible for membership as a 'Consulting Arborist' with Arboriculture Australia (www.arboriculture.org.au), or the Institute of Australian Consulting Arboriculturists (www.iaca.org.au), and who has attained a Level 5 Certificate of Horticulture / Arboriculture or equivalent.
- The report must identify trees on the site survey plan by number and provide details of the following:
 - (a) Species and accurate location (by registered surveyor) of each tree;
 - (b) Health, amenity value and S.U.L.E. (Safe Useful Life Expectancy) rating of each tree;
 - (c) Impact of the development on each tree;
 - (d) Impact of retaining trees on the proposed development;
 - (e) The Tree Protection Zone (TPZ) required for each tree proposed to be retained;
 - (f) Any root barriers necessary, type and their location; and
 - (g) Any branch or root pruning which may be required for the trees.
- Arboricultural Impact Assessment (AIA). Where the proposal impacts upon the existing trees, an AIA is to include but not limited to the following:
 - a) the accurate location, genus and species of each tree;
 - b) health, amenity value and ULE (Useful Life Expectancy) rating of each tree;
 - c) any pests or diseases that may be present on each tree;
 - d) any site changes and surrounding structures which may affect the health or vitality of the tree
 - e) impact of the development on each tree, including encroachment on the TPZ and SRZ
 - f) the Tree Protection Zone (TPZ) and Structural Root Zone (SRZ) required for each tree proposed to be retained;
 - g) tree Protection Plan (TPP) outlining the specifications for tree protection to be in place during the construction phase including any pruning requirements;
 - h) any root barriers necessary, type and their location;
 - i) any branch or root pruning which may be required for the trees;
 - j) any supporting evidence such as photographs, if relevant; and
 - k) recommendations which minimise the impact of the development on the tree on the preferred option and an explanation why this option is preferred. Where appropriate, recommend a VTA Level 3 assessment and recommendations.
- Arboricultural Assessment of trees is only required for trees likely to be impacted by proposal - construction of structures, grading, road construction or installation of services. All vegetation on site to be noted. Arborist Report to provide information for preliminary planning. A level 1 Visual Tree Assessment (VTA) may be beneficial.
- Arboricultural Assessment of trees is only required for trees likely to be impacted by proposal - construction of structures, grading, road construction or installation of services. All vegetation on site to be noted. Arborist Report to provide information for preliminary planning.
- Attention is drawn to large *Bradyton acerifolius* Illawarra Flame Tree on site.

- Any trees impacted by proposed easements on adjacent properties will also need to be assessed.
- A greater degree of screening will be required along site frontage. Proposed parking and vehicle turning shall not diminish area required for adequate planting.
- Several palms on site may lend themselves for transplanting.
- No objection of removal of trees within 3m of existing dwelling or structure.
- Fencing, planting and material selection to be sympathetic with adjoining areas and compatible with stormwater drainage requirements.
- The site is quite steep. Plan shows a crib ladder ? stairs? for access. Retaining walls to be constructed in accordance with policy. Refer B1 Section 4.16 page 17 and in addition 4.8.2.3 and 4.8.2.10. Retaining walls over 600mm high are to be designed and certified by a qualified and experienced Engineer. A retaining wall or embankment should be restricted to a maximum height above or depth below natural ground level of no more than:
 - (a) 600mm at any distance up to 900mm setback from any side or rear boundary; or
 - (b) 1 metre, if the toe of the retaining wall or embankment is setback greater than 900mm from any side or rear boundary.

Note: Council may consider a variation to the abovementioned maximum height / depth of a retaining wall, in cases where the subject site is steeply sloping and the proposed retaining wall is setback more than 1 metre from any side or rear common property boundary. Additionally, appropriate structural design details will be required and in some cases appropriate landscape buffer screen planting may be required, where necessary.

- Retaining walls should to be constructed with materials consistent with the building style and adjacent properties. All retaining walls should include levels at top and bottoms of walls.
- Implementation of plant recovery program could be considered on site to enable some of existing vegetation to be reused and relocated.
- The establishment of trees and hedges should be considered to ameliorate any potential overlooking from adjoining residences and the use of deciduous trees for solar access.
- Planting is recommended for amenity, shade, ecology, diminish soil erosion and water pollution, conserve energy, provide oxygen, increase property values, build communities, provide habitat for wildlife, screening and marking the seasons.
- Integration with the stormwater management and overall development is required: Site landscaping must be integrated with the stormwater management (drainage) controls. In particular, the location and nature of on-site stormwater detention should not conflict with landscaping areas and objectives. The construction of a pipe through rear of property may be required to connect to proposed easements.
- Consideration is to be given to maintenance access along sides of property and throughout site. Access along western boundary appears difficult and would likely not reflect bushfire recommendations.
- Landscaping to the site is to comply with the principles in Appendix 5 of Planning for Bush Fire Protection 2006 and *Standards for Asset Protection Zones* (NSW Rural Fire Service) and take into consideration PBP 2018.
- Any fill material should not cover topsoil. Topsoil shall be removed, stockpiled, ameliorated and replaced over any fill material to a minimum depth of 100mm.
- The developer must install mailboxes along street frontage of the property boundary in accordance with Australia Post Guidelines. Prominent house numbers are to be displayed, with a minimum

number size of 300 mm in height for each number and letter in the alphabet considering steepness of road and potential confusion for emergency services/ deliveries/ visitors.

- The developer must install minimum two (2 No.) reflective paint house number on face of kerb along street frontage of the property to assist emergency services/ deliveries/ visitors.
- Safer By Design (CPTED) principles to be adhered to.
- Extent of lighting to be addressed in SEE.

Contact Details: The applicant's Landscape Architect may contact Council's Landscape Architect/Design Officer, through Council's Customer Service Centre on the telephone number (02) 4227 7111 if any issues arise through the design phase prior to the lodgement of the development application.

Please note: If construction cost estimate is known you may obtain a lodgement fee quote by contacting Council's Customer Service Centre on the telephone number (02) 4227 7111

WHAT INFORMATION IS NEEDED WITH A DEVELOPMENT APPLICATION?

In order for Council to conduct a proper and informed assessment of your application, the following information must be submitted.

(* Required information)

- | | |
|--------------------------------------|--|
| * Owner's Consent | * Traffic Generation Impact Assessment |
| * Survey Plan | * Plan of On-Site Traffic Movement/Parking |
| * Site Context Analysis Plan | * Stormwater Concept Plan |
| * Demolition Plan and Photographs | * Threatened Species (7 part test/AoS) |
| * BASIX assessment/certificate | * Landscape Concept Plan |
| * Statement of Environmental Effects | * Arborist Report |
| * Site Plan | * Site Management Plan/Staging Plan |
| | * Waste Management Plan |
| | * Integrated Development |
| * Floor Plans | |
| * Building Elevations Plans | |
| * Building Sections Plan(s) | |
| * Shadow Profiles and Plans | |
| * Schedule of External Finishes | |
| * Geotechnical Report | |
| * Bushfire Hazard Report | |

OUR AGREEMENT:

This pre-lodgement information does not constitute an approval.

This meeting note represents an agreement for the submission of information considered necessary for the timely determination of an application.

The notes are provided in good faith to assist applicants in the preparation of a development application. Relevant legislation and Council's requirements can vary from the time of this meeting to lodgement of the application. Public exhibition of the application can also raise unexpected relevant issues requiring lodgement of new or amended information.

Accordingly Council's final position on the proposal can only be made once a development application has been lodged and assessed.

Development Project Officer: Brigit Mathai

Attachment 3

Council Comment: Setbacks

The minimum side setbacks required (as calculated from the existing ground levels on southern side) on the north-west side is 4.8m for ground floor and 8m to first floor level (1.0 x ceiling height as per Cl. 5.4.2(1) in Chapter B1 of DCP).

The variation requested has not been satisfactorily addressed to meet the objectives set out for the development control. Amendments are required to improve and soften the bulk of the side facades incorporating methods like increased setbacks for balconies on the upper level, avoiding blank walls providing high level window to the ground floor side wall to the sides, lower roof form to the rear balcony etc.

It is assessed that the reduced side setback could pose visual impacts to the neighbouring dwelling to the western side boundary. Please provide a view impact assessment report in this regard.

Response:

In consideration of Council's comments relative to the side and rear setbacks, as aforementioned, the extent of balcony areas to the Level 1 rear facades have been significantly reduced. In addition, parapet dividing walls between these balconies have been replaced with white glass screens and pergolas have been removed and replaced with small

awnings only. The upper Level 1 building line setback has been increased from the rear boundary. Further to these changes to reduce building bulk and scale, the side elevations have been further articulated with further introduction of obscure and white glass window elements and material changes to these facades.

Clause 5.4.2.1 – Side and Rear Setbacks requires that the minimum side and rear setback distance be calculated as 0.8 x ceiling height for essentially non-habitable interfaces, and 1.0 x ceiling height for habitable interfaces (such as balconies or windows of living areas that are directly oriented towards a particular boundary). The proposed building interfaces to the side boundaries are deemed non-habitable (by virtue of only white glass/obscure openings) and, as such, the guideline setback distance is to be calculated as 0.8 x ceiling height. The building interface to the rear is deemed as habitable, with balconies and living area windows oriented towards this boundary. Therefore, the guideline setback distance is to be calculated as 1.0 x ceiling height for this rear setback. In this regard, the following table shows the guideline as calculated from the existing ground levels versus proposed:

Table 1: Building Line Side and Rear Setbacks

Boundary	Guideline Calculated Setback	Proposed Setback
Western Side (0.8 x ceiling height)	Variable from: Ground Level: 2.64m to 4.8m Level 1: 4.8m to 5.2m	Ground Level: Consistent 3.1m Level 1: Consistent 3.1m
Eastern Side (0.8 x ceiling height)	Variable from: Ground Level: 3.2m to 4m Level 1: 5.6m to 6.2m	Ground Level: Variable 3.1 to 9.0m Level 1: Variable 2.1 to 6.9m
Southern Rear (1.0 x ceiling height)	Variable from: Ground Level: 4.48m to 4.96m Level 1: 7.7m to 8.3m	Ground Level: Minimum 6.0m Level 1: Variable Minimum 9.5m

As detailed above, the proposed development encroaches partly into the required calculable setbacks at the ground level for both side boundaries (west and east), and for the most part is also non-compliant with the calculable side setback lines for the upper Level 1 also. However, the proposed rear building line is well in excess of the minimum required rear setbacks at the southern boundary.

Due to site slope of the land at these sections where the building lines are sited, these setback calculations become exacerbated (as ceiling height is measured from the outer edge of the

façade to the natural ground level below), and as such, the proposed development cannot achieve the required side setbacks for part thereof.

The proposed developments have been purposefully designed to provide an appropriate environmental and development outcome for the subject site and surrounds as a whole. In doing so, a minor variation to the side setback development guidelines contained within *Wollongong Development Control Plan (DCP) 2009* have been adopted. In relation to these matters and given the site specific characteristics in place, it is considered unreasonable and unnecessary to have to strictly accord with the numerical guidelines within the *Chapter B1* of the *DCP* in this instance. Therefore, this *Variations Justification Statement* is prepared for Council's consideration to identify the development control variations sought, and to provide appropriate justification as required under *Section 8 – Chapter A1* of the *DCP*.

In considering these variations proposed, it is important to understand the site's context and deliberate design response to the intent of Council's guidelines. Both *Wollongong LEP 2009* and *Wollongong DCP 2009* identify guidelines and controls to shape development outcomes. These Plans apply to the City of Wollongong as a whole, which has significant physical land characteristic variations throughout, together with the need to apply varying adaptations of the many merit-based planning considerations for development. Therefore, the stated aims/objectives of these Plans allow flexibility in the application of such development controls, where strict compliance with the controls is not warranted.

Whilst the above design matters do not directly comply with Council's numerical standards in this instance, the development outcome does meet the intent of those objectives identified and provides an appropriate urban design outcome in this locality as if strict compliance was achieved nonetheless. In considering the variations proposed, it is understood that the intent of these provisions are to ensure and allow for good quality urban design outcomes, in order to respond to achieving appropriate residential amenity for both future occupants and their neighbours. We strongly believe this has still been achieved with the proposed development.

In this regard, the following is noted in relation to the objectives of this *Clause 5.4 – Side and Rear Setbacks*:

"(a) To provide adequate setbacks from boundaries and adjoining dwellings to retain privacy

levels, views, sunlight and daylight access and to minimise overlooking."

- The setback variations proposed at these parts of the site are considered very minor and unlikely to adversely impact on the amenity of the adjoining properties. Both Units 1 and 6 are setback from the street frontage so they are well behind the front building lines on the dwellings immediately east and west. Not being forward of the adjoining dwellings will ensure that daylight and sunlight access to these neighbouring buildings are maximised, due to the front available northern orientation to solar access.
- The corresponding side building facades are all treated as non-habitable elevations, with a solid wall interface and limited window openings (at either high level or with white glass/obscure material). Therefore, the impacts associated with privacy and overlooking as a result of reduce side setbacks will be nil (that is, no greater than if compliance was achieved).
- The main building line variations into the side setbacks occur at the upper Level 1 of Units 1 and 6. The façade length of Unit 1 at the upper level is some 12 metres, and when measured against the length of the corresponding western boundary of 36.575 metres, only represents some 32.8% of total boundary length. Similarly, the non-compliant section of façade length of Unit 6 at the upper level is approx. 10 metres, and when measured against the length of the corresponding western boundary of 39.965 metres, only represents some 25% of total boundary length. These limited lengths of façade interfaces with the corresponding side boundaries will ensure view lines across the property will still be available from varying directions, and visual impacts of building mass towards these neighbours are limited.
- The design of the proposed development means that the side boundaries and neighbouring dwellings will only interface directly with one dwelling within the complex. Thus, considering a minimum 2.1m setback for only one dwelling is provided to the east and a 3.1m setback for only one dwelling is still provided to the west, the impact of these penetrations into the building line setbacks are very minor. Particularly compared to say that of a permitted setback for a new house or dual occupancy on this site, whereby a façade up to 7 metres high could be setback as

little as 900mm from the side boundaries, for a much longer length than proposed.

- In terms of view loss for neighbouring properties, both 90 Princes Highway (west) and 96 Princes Highway (east) have rear decks/balconies oriented towards the south to take advantage of the vegetated outlook across the Woodlands Creek gully environmentally zoned lands. This outlook will not be impacted whatsoever by the proposed development. From these rear decks/balconies, it is anticipated that partial Escarpment and Coastal views may also be enjoyed. In consideration of these view aspects, the Escarpment view from 90 Princes Highway (west) will not be impacted at all, however, partial view loss may be incurred towards the south-east coastal aspect. Conversely, the Escarpment view from 96 Princes Highway (east) may be impacted, however, no view loss will be incurred towards the south-east coastal aspect. In consideration of any view impact, it is important to note that the rear setback lines of the proposed development completely comply with the required controls. In fact, the rear balconies and upper level living spaces are setback well beyond the required minimum setback. Therefore, view lines across the rear of the property are being maintained in accordance with the required calculable setbacks, thus not reducing lines of sight any more than otherwise permitted. Additionally, the south-west corner of Unit 6 is compliant with the side setbacks, so no additional view loss is created as a result of the variation to this boundary. In terms of the setback variation to the western boundary, to push the Unit 1 building line a few metres additionally back from this edge is unlikely to drastically change the view lines across the rear boundary and towards the coastal aspect. This is because these views are simply distant views, as the closest foreshore area to the south east is a little over 2km away to Sandon Point Beach (refer aerial image below). Therefore, the impacts on view will be negligible compared to if strict compliance was achieved for the side boundary setbacks.

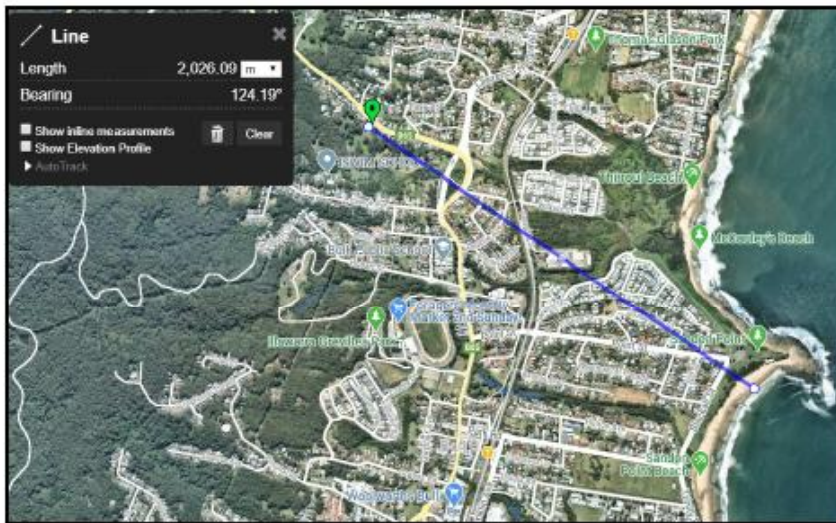


Figure 1: View Line Distance to Ocean Foreshore (south east)

"(b) To provide appropriate separation between buildings to achieve the desired urban form."

- The building separations to be achieved between the proposed development and the corresponding eastern/western neighbours is approx. 5.1 to 5.5 metres at either side at a minimum. For a low density residential zoned area such as this, the proposed building separation is considered to be quite generous in terms of the current day housing market and the types of products erected and sited on properties. The proposed dwellings are of a modest and contemporary design that complements the surrounding bulk and scale of the immediate area, whilst not detracting from the existing buildings in close proximity.

"(c) To optimise the use of land at the rear of the property and surveillance of the street at the front of the property."

- The proposed development is setback well beyond the street frontage and any other existing dwellings within the Princes Highway frontage, with the frontage to be modestly planted out and paved to provide good surveillance throughout. All units have their front doors and living spaces directly facing the street frontage, for convenient/consistent line of sight and to accord with passive surveillance principles.

At the rear of the site, a 6m wide deep soil zone and private drying court areas are provided so as to provide an adequate buffer between the proposed development and the rear properties.

"(d) To minimise overshadowing of adjacent properties and private or shared open space."

- As shown within the Shadow Diagrams prepared by Design Workshop Australia (attached to the application), the proposed development will have limited impacts on sunlight and daylight access to properties either side. The main impacts on overshadowing are to the property immediately south, although impacts are limited from midday onwards during the winter solstice. The appropriate daylight/sunlight access will still be achieved to the adjoining sites in accordance with Council's requirements.

In general, this development outcome will provide an adequate amenity for both future occupants of the building and adjoining land uses, and the immediate streetscape outlook and characteristics will not be compromised by these side setback variations. In fact, it is considered that these variations will allow the built form to assimilate with the existing location a little better and provide a positive architectural and land use outcome as a result.

Council Comment: 3 Storeys

The statement of variation submitted fails to address the 3 storey appearance when viewed from the north west and south-eastern sides which is considered more critical in this case. Please provide satisfactory justification in this regard in order for Council to consider the variation to this development control.

Response:

Additional perspective images have been provided by DWA to demonstrate however the building will be viewed from the north west and south-eastern sides as requested. See extracted images below.



Figure 2: North western perspective



Figure 2: South eastern perspective

As explained in the SEE Report submitted with the DA, *WDCP 2009 Chapter B1 Clause 5.2.2.1* requires the maximum number of storeys for multi dwelling developments within an R2 zone to be two storeys. In this regard, the proposed dwellings are all of 2 storey construction in height/scale (relatively to ground level), and technically in cross-section comply with two-storey definition parameters. That is, no part of unit at the rear is three storeys due to the no part of the lower ground bedroom retreat spaces being located directly below part of the upper floor levels. Refer to typical cross-section by DWA extracted below to show 2 storey technical compliance.



Figure 3: Typical Cross-Section showing two-storey compliance

Whilst the proposed development will present as two storeys from the sides and north of the property, it is acknowledged that in elevational form views from the south may present a three storey elevation in part due to the topographic profile of the site and split level design.

In relation to these matters and given the site specific characteristics in place, it is considered the proposed development still accords with the 2 storey numerical guidelines within the *Chapter B1* of the *DCP* in this instance. Notwithstanding, with regards to elevational profile from the rear, this *Variation Justification Statement* is prepared for Council's consideration to identify the development control objectives, and to provide appropriate justification as required under *Section 8 - Chapter A1* of the *DCP*.

In this regard, the following is noted in relation to the objectives of this *Clause 5.2 - Number*

of Storeys:

"(a) To encourage buildings which integrate within the existing streetscape and the desired future character for the area."

- The proposed development design in this instance encourages a split level and stepped building solution, on moderately sloping site. It is considered that the proposed 'cascading' design response is a direct reflection of the intent of this objective and, being similar to constructed dwellings nearby (by way of this stepped building solution), is sympathetic to addressing these site constraints and integrating with the natural landscape.
- As explained, this integration with the landform has allowed for a development outcome that falls well below the maximum height limited permitted, and primarily presents as two storey construction, apart from an elevational perspective from the south (rear) due to this site slope. In considering this, it must be recognised that the front building line has been setback well beyond the other dwelling forms fronting the Princes Highway immediately to the west and east. Therefore, visual presence within the streetscape will be very limited. In addition, the proposed development also allows for landscaped gardens and tree planting within the front building line setback and throughout the property. Thus, dramatically softening the visual connection of built forms to the streetscape.
- In terms of desired future character of the area, the Princes Highway itself is dominated by large lots and large homes, which suggests that the proposed multi-dwelling development will be compatible with surrounding land uses and built forms. *WDCP 2009 Chapter D1* states that *for properties within the treed upper slopes of Thirroul, moderately pitched rooflines are preferred. New dwellings on sloping sites should be stepped down the slope with suspended floors and decks, to minimise the disturbance of the natural contours of the site and any such new dwelling should be designed to fit below the tree canopy line.* The proposed stepped building forms are reflective of these other two and three storey housing products seen throughout the steeper sites of Thirroul. Accordingly, it is considered that the proposed development will accord with both the existing and desired future character of the Thirroul area.

"(b) To minimise the potential impacts of overshadowing and overlooking on adjacent

dwellings and open space areas."

- The subject site has a north-south axis in terms of orientation and its street frontage is due north. As such, the potential impacts of overshadowing on any adjacent dwellings and private open space areas west or east are limited altogether. The main impacts on overshadowing are to the property immediately south, although impacts are limited from midday onwards during the winter solstice. This is a result of the upper level being set back from the rear boundary well in excess of the minimum requirements by Council's DCP. The appropriate daylight/sunlight access will still be achieved to the adjoining sites in accordance with Council's requirements.
- The side building facades are all treated as non-habitable elevations, with a solid wall interface and limited window openings (at either high level or with white glass/obscure material). Therefore, the impacts associated with privacy and overlooking as a result of reduce side setbacks will be nil (that is, no greater than if compliance was achieved). In terms of overlooking to the property to the south, it is considered that this proposal will have minimal impact upon the issues of privacy or visual impacts. A 6 metre wide deep soil zone is provided to this rear boundary and the building lines proposed comply with the required setbacks for habitable edges. Again, the rear upper level balconies and internal living spaces are setback well beyond the minimum requirements under Council's DCP. There is no dwelling immediately adjacent to the south at this time that will be impacted by this rear elevation.
- Again, in order to reduce upper level building mass at the rear elevation, the extent of balcony areas to the Level 1 rear facades have been significantly reduced. In addition, parapet dividing walls have been replaced with white glass screens and pergolas have been removed and replaced with small awnings only. This has further improved the receding nature of the upper level when viewed from the south, so as to soften and enhance the visual quality and presentation of this 3 storey appearance.

The proposed development's architectural design by DWA Architects in this instance is a deliberate attempt to respond to the site's context and client's brief (balance between development outcomes and respecting site features such as slope), but at the same time accord with the objectives and guidelines/standards adopted within both the *Wollongong DCP 2009*. It is considered that the proposed 2 storey technical compliance and in part 3 storey elevation perceptions can be justified in this instance.



Our ref: STH18/00084/11
Contact: Hayley Sarvanandan 4221 2423
Your ref: DA 2019/1124 (CNR-3283)

8 September 2020

Brigit Mathai
Wollongong City Council
BY EMAIL: bmathai@wollongong.nsw.gov.au
CC: council@wollongong.nsw.gov.au

DEVELOPMENT APPLICATION 2019/1124 – LOT 2 DP 202822, 92-94 PRINCES HIGHWAY, THIRROUL, DEMOLITION OF EXISTING DWELLING AND CONSTRUCTION OF MULTI DWELLING HOUSING

Dear Brigit

Transport for NSW (TfNSW, formerly Roads and Maritime Services) refers to your correspondence dated 26th August 2020 regarding the subject development application.

TfNSW has completed an assessment of the DA, based on the information provided and focussing on the impact to the state road network. TfNSW notes for this DA:

- The key state road is Princes Highway;
- Council is seeking advice from TfNSW to assist in its assessment under Clause 101 of State Environmental Planning Policy (Infrastructure) 2007; and
- The proposed access arrangements onto the Princes Highway and U-turn facility on Bangalow Road shown in Attachment 1.

TfNSW supports the proposed U-turn facility on Bangalow Road as it will improve the safety of the existing layout for vehicles wishing to turn around to travel east on Princes Highway. However TfNSW has concerns with the stacked parking arrangements, particularly within proposed Units 1 and 2 as this has the potential to create queuing at the entrance of the development. Although a holding bay is proposed, TfNSW believes this may be used as parking.

TfNSW also notes concerns with the proposed garbage collection arrangements and proposed bin capacity provision. If the bins are not collected from the roadside collection point in a timely manner this may cause a road safety hazard. TfNSW believes occupants would be encouraged to request additional bins due to the limited capacity proposed.

Having regard for the above, if Council is satisfied with the proposed arrangements TfNSW will not object to the DA subject to the conditions outlined in Attachment 2 being included in the conditions of development consent.

TfNSW highlights that in determining the DA under Part 4 of the *Environmental Planning and Assessment Act, 1979*, it is the consent authority's responsibility to consider the environmental impacts of any road works

which are ancillary to the development. This includes any works which form part of the proposal and/or any works which are deemed necessary to include as requirements in the conditions of development consent. Depending on the level of environmental assessment undertaken to date and nature of the works, the consent authority may require the developer to undertake further environmental assessment for any ancillary road works.

Upon determination of this matter, it would be appreciated if Council could send a copy of the Notice of Determination to development.southern@rms.nsw.gov.au.

Yours faithfully

A handwritten signature in black ink, appearing to read 'Fiona', with a stylized flourish extending to the right.

Fiona McLauchlan
A/Senior Manager, Regional Customer Services
Community and Place | South Region

See attached concept design titled Attachment 1

Prior to the issuing of the Construction Certificate, the developer must:

1. Apply for Section 138 consent under the Roads Act, 1993 from Council for all works on Princes Highway.
2. Demonstrate to the satisfaction of Council the post development storm water discharge from the subject site, if going into the Princes Highway drainage system, does not exceed the pre-development application discharge.

Prior to commencing works within the road reserve, the developer must:

1. Obtain Section 138 consent under the Roads Act, 1993 for the works on Princes Highway from Council.

Notes:

- Provided Council is satisfied the works have been designed in accordance with the relevant Council standard, TfNSW issues its concurrence under Section 138 of the Roads Act, 1993.

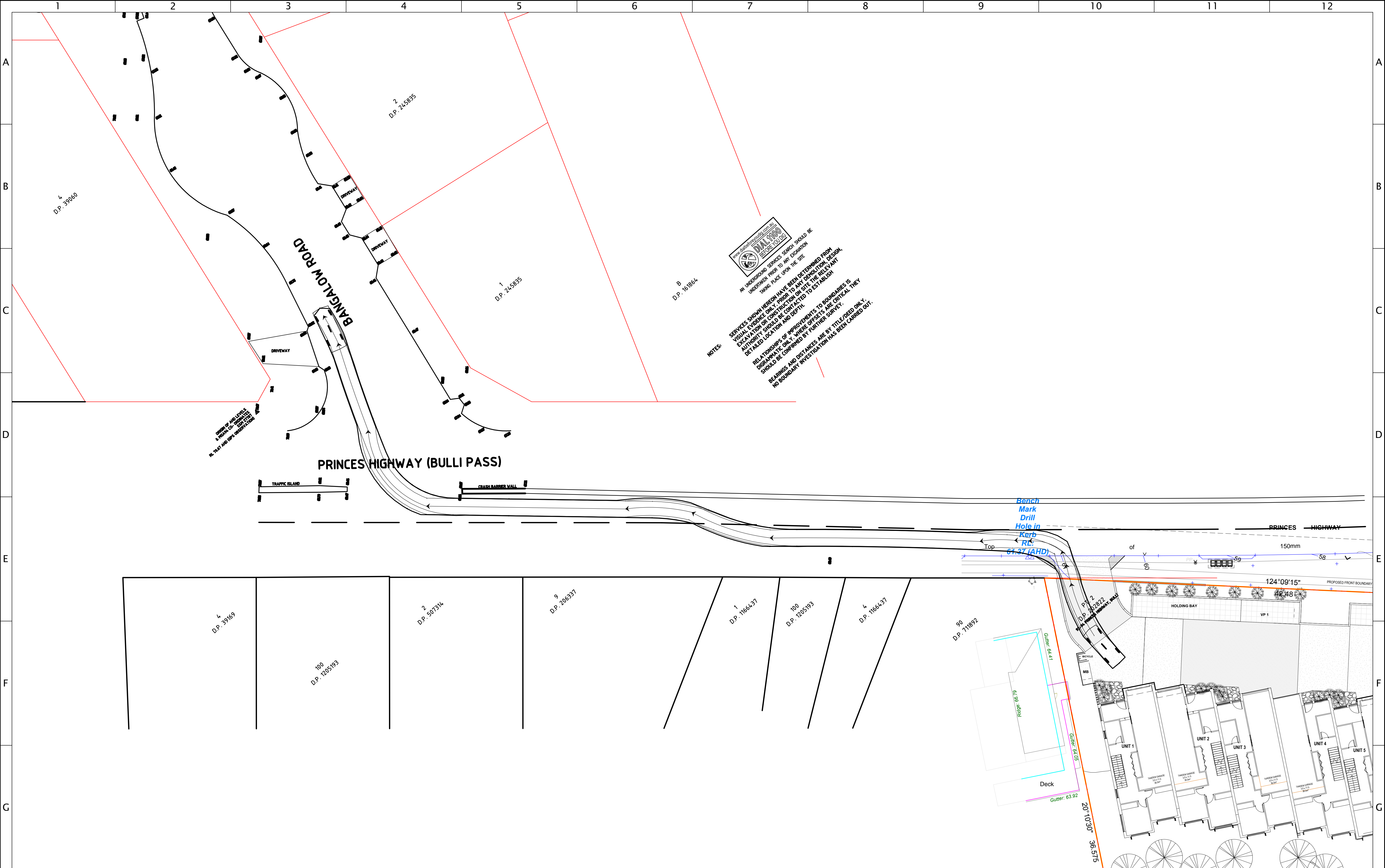
2. Apply for, and obtain a Road Occupancy Licence (ROL) from the TfNSW Traffic Operations Unit (TOU) prior to commencing roadworks on a State road or any other works that impact a travel lane of a State road or impact the operation of traffic signals on any road.

Notes:

- For information on the ROL process and to lodge an ROL application, please visit <https://myrta.com/oplinc2/pages/security/oplincLogin.jsf>
- The applicant will need to create an account (this may take a few days to register), prior to submitting the ROL application. The applicant must submit the ROL application 10 business days prior to commencing work. It should be noted that receiving an approval for the ROL within this 10 business day period is dependent upon TfNSW receiving an accurate and compliant TMP.
- The application will require a Traffic Management Plan (TMP) to be prepared by a person who is certified to prepare Traffic Control Plans. Should the TMP require a reduction of the speed limit, a Speed Zone Authorisation will also be required from the TOU.
- An approved ROL does not constitute an approval to commence works until an authorisation letter for the works has been issued by TfNSW Project Manager.

Prior to the issuing of the Occupation Certificate (interim or final), the developer must:

1. Design and construct the driveway on Princes Highway to the satisfaction of Council, generally in accordance with Attachment 1 and Council standards.
2. Design and construct the roundabout on Bangalow Road to the satisfaction of Council, generally in accordance with Attachment 1 and Council standards.
3. The developer must provide no U-turn signage on the Princes Highway as shown in Attachment 1 to prohibit U-turn movements within the entrance of Bangalow Road. To initiate the installation of the signage the developer should contact Network and Safety Officer, Danny Benedetti via email at danny.benedetti@transport.nsw.gov.au.



ISSUE		AMENDMENT	DATE	PLANS				11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au		Title		PLAN: TRAFFIC ASSESSMENT SHEET 9		SCALES		AS SHOWN		DATE PLOTTED			
A		ISSUE FOR DA APPROVAL		23/04/20						0 1000 2000 3000 4000 5000		Project		TRAFFIC ASSESSMENT		DRAWN		D.K.		22/02/19	
B		ISSUE FOR ADDITIONAL INFORMATION		13/07/20								At		92 PRINCES HIGHWAY THIRROUL		DESIGNED		G.U.		DATUM A.H.D.	
C		ISSUE FOR ADDITIONAL INFORMATION		06/08/20								Client		AKPINAR		CHECKED		G.U.		DATE CHK'D	
D		ISSUE FOR ADDITIONAL INFORMATION		24/08/20								PROJECT No		18172		DWG		T9		REVISION	
						DO NOT SCALE IF IN DOUBT ASK		A1													
						THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS															
						COPYRIGHT: Concepts and information contained in these engineering drawings and related documents are the copyright of ATB Consulting Engineers Pty. Ltd. Unauthorised copying of part or whole of the document/s is a breach of copyright.															