



BUSINESS PAPER

ORDINARY MEETING OF COUNCIL

To be held at 6.00 pm on

Monday 26 October 2020

Council Chambers, Level 10,
Council Administration Building, 41 Burelli Street, Wollongong

(Note: Councillors will be able to attend and participate in this meeting via electronic means in accordance with legislation relating to the COVID-19 pandemic)

Order of Business

- 1 Opening Meeting
- 2 Acknowledgement of Traditional Owners
- 3 Civic Prayer
- 4 Apologies and Applications for Leave of Absence by Councillors
- 5 Confirmation of Minutes of Ordinary Council Meeting
- 6 Confirmation of Minutes of Extraordinary Ordinary Council Meeting
- 7 Conflicts of Interest
- 8 Petitions and Presentations
- 9 Confirmation of Minutes of Council Committee Meeting
- 10 Public Access Forum
- 11 Call of the Agenda
- 12 Lord Mayoral Minute
- 13 Urgent Items
- 14 Reports to Council
- 15 Reports of Committees
- 16 Items Laid on the Table
- 17 Notices of Motions(s)/Questions with Notice
- 18 Notice of Rescission Motion
- 19 Confidential Business
- 20 Conclusion of Meeting

Members

Lord Mayor –
Councillor Gordon Bradbery AM (Chair)
Deputy Lord Mayor –
Councillor Tania Brown
Councillor Ann Martin
Councillor Cameron Walters
Councillor Cath Blakey
Councillor David Brown
Councillor Dom Figliomeni
Councillor Janice Kershaw
Councillor Jenelle Rimmer
Councillor John Dorahy
Councillor Leigh Colacino
Councillor Mithra Cox

QUORUM – 7 MEMBERS TO BE PRESENT

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MINUTES

ORDINARY MEETING OF COUNCIL

at 6:00pm

Monday 21 September 2020

Present

Lord Mayor – Councillor Gordon Bradbery AM (in the Chair)
Deputy Lord Mayor – Councillor Tania Brown
Councillor Ann Martin
Councillor Cameron Walters
Councillor Cath Blakey
Councillor David Brown
Councillor Dom Figliomeni
Councillor Janice Kershaw

Councillor Jenelle Rimmer
Councillor John Dorahy
Councillor Leigh Colacino
Councillor Mithra Cox

In Attendance

General Manager
Director Infrastructure + Works, Connectivity Assets + Liveable City
Director Planning + Environment, Future City + Neighbourhoods
Director Corporate Services, Connected + Engaged City
Director Community Services, Creative + Innovative City
Manager Governance + Customer Service (Acting)
Chief Financial Officer
Manager Property + Recreation
Manager City Strategy
Manager City Works (Acting)
Manager Project Delivery
Manager Infrastructure Strategy + Planning (Acting)
Manager Open Space + Environmental Services
Manager Community Cultural + Economic Development
Manager Library + Community Services

Greg Doyle
Andrew Carfield
Linda Davis
Renee Campbell
Kerry Hunt
Michelle Martin
Brian Jenkins
Lucielle Power
Chris Stewart
Corey Stoneham
Glenn Whittaker
Andrew Heaven
Joanne Page
Sue Savage
Jenny Thompson

Note: Due to current government requirements around social distancing obligations due to the COVID-19 pandemic, all Councillors and staff attended the Meeting of Council via Skype. The Lord Mayor and General Manager were present in the Council Chambers for this meeting.

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CLOSED SESSION

- ITEM C1 CONFIDENTIAL: Confidential - Report by Performance Review Committee - General Manager's Annual Performance Review

Reason for Confidentiality

This report recommends that this item be considered in Closed Session to the exclusion of the press and public in accordance with Section 10A(2)(a) of the Local Government Act, 1993, as the report contains personnel matters concerning particular individuals.

CONFIRMATION OF MINUTES OF ORDINARY MEETING OF COUNCIL HELD ON MONDAY, 31 AUGUST 2020

- 433 **COUNCIL'S RESOLUTION** - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor Dorahy that the Minutes of the Ordinary Meeting of Council held on Monday, 31 August 2020 (a copy having been circulated to Councillors) be taken as read and confirmed.

MEETING ADJOURNMENT

Prior to the commencement of the Call of the Agenda, the meeting was adjourned in order to resolve technical issues being experienced with the electronic meeting, the time being 6:04pm.

- 434 **COUNCIL'S RESOLUTION** - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that the meeting be adjourned for five minutes to resolve technical issues being experienced.

CALL OF THE AGENDA

- 435 **COUNCIL'S RESOLUTION** - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that the staff recommendations for Items 2, then 4 to 10 inclusive be adopted as a block.

DEPARTURE OF COUNCILLOR

During debate and prior to voting on Item 1, Councillor Martin joined the meeting, the time being 6:15 pm.

During debate and prior to voting on Item 1, Councillor Kershaw departed and returned to the meeting, the time being from 6:37 pm to 6:39 pm.

ITEM 1 - WOLLONGONG CITY CENTRE PLANNING REVIEW

- 436 **COUNCIL'S RESOLUTION** - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor Colacino that -

- 1 The community and stakeholder feedback on the *Draft Wollongong City Centre Urban Design Framework* (February 2020) and *Wollongong City Centre Planning Review: Economic Analysis* (June 2019) as documented in the *City Centre Planning Review Engagement Report* (Attachment 2), be noted.
- 2 The revised final *Wollongong City Centre Urban Design Framework* (Attachment 3) be adopted as a planning policy to guide development outcomes in the City Centre and the preparation of a Draft Wollongong City Centre Planning Strategy.
- 3 A Draft Wollongong City Centre Planning Strategy, including the Draft Planning Proposal and Draft Wollongong Development Control Plan Chapter, be finalised and reported to Council by no later than 30 June 2021 and after considering and including those suggestions received during the exhibition that will lead to a more consultative and positive outcome for CBD owners, operators, commerce and the community.

Variation The variation moved by Councillor Figliomeni (the addition of the words 'by no later than 30 June 2021 and after considering and including those suggestions received during the exhibition that will lead to a more consultative and positive outcome for CBD owners, operators, commerce and the community' in point 3) was accepted by the mover and seconder.

A PROCEDURAL MOTION was MOVED by Councillor Colacino seconded Councillor Walters

that Councillor Dorahy be granted an additional one and a half minutes to address the meeting in relation to Item 1.

ITEM 2 - PUBLIC EXHIBITION - DRAFT WOLLONGONG COMMUNITY SAFETY PLAN 2021-2025

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that Council endorse the public exhibition of the draft Wollongong Community Safety Plan 2021-2025 from 23 September to 26 October 2020 inclusive.

ITEM 3 - PUBLIC EXHIBITION - DRAFT WOLLONGONG CBD NIGHT TIME ECONOMY COUNCIL POLICY

437 **COUNCIL'S RESOLUTION** - RESOLVED UNANIMOUSLY on the motion of Councillor Bradbery seconded Councillor T Brown that -

- 1 Council endorse the public exhibition of the draft Wollongong CBD Night Time Economy Council Policy for a period of 28 days.
- 2 Council investigate the potential implications and opportunities for Wollongong arising from the Liquor Amendment (24-hour Economy) Bill 2020.

ITEM 4 - 2021 COUNCIL MEETING CYCLE

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that Council adopt the 2021 Council Meeting Cycle as shown in the attachment to this report.

ITEM 5 - COUNCILLOR ATTENDANCE AT THE LOCAL GOVERNMENT NSW (LGNSW) ANNUAL CONFERENCE - 23 NOVEMBER 2020

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that -

- 1 The Lord Mayor, Cr G Bradbery; Deputy Lord Mayor, Cr T Brown; and Councillors D Brown, A Martin, D Figliomeni, and J Kershaw be approved to register to participate in the Local Government NSW Annual Conference to be held online on 23 November 2020.
- 2 Council authorise the Lord Mayor, Cr G Bradbery; Deputy Lord Mayor, Cr T Brown and Councillors D Brown, A Martin, D Figliomeni, and J Kershaw as six voting delegates to the Conference.
- 3 Council delegate to the Lord Mayor authority to approve attendance of any other Councillor requesting to attend the Conference and to nominate up to one further voting delegate if required.

ITEM 6 - PROPOSED GRANT OF EASEMENT TO DRAIN WATER 1.2M WIDE OVER LOT

12 DP 210948, PART GUEST PARK, FAIRY MEADOW

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that -

- 1 Pursuant to section 46(a1) of the *Local Government Act 1993*, Council resolves to grant an easement to drain water 1.2m wide over Council Community land being Part Guest Park, Fairy Meadow known as Lot 12 DP 210948, in favour of Lot 27 DP 26939 No 23 Guest Avenue, Fairy Meadow, as shown crosshatched on Attachment 1.
- 2 Council accept payment in the amount of \$1,200.00 (GST free) from the owner of Lot 27 DP 26939 No 23 Guest Avenue, Fairy Meadow as compensation for the grant of the easement.
- 3 The applicant be responsible for all costs relating to the easement including valuation, survey, plan registration and legal costs, and any other costs incurred in this matter.
- 4 Approval be granted to affix the Common Seal of Council to the survey plan, Section 88B Instrument and any other documentation required to give effect to this resolution.

ITEM 7 - PROPOSED SUPPORT TO ADDRESS GAZETTAL ANOMALY AND TRANSFER OWNERSHIP OF UNFORMED ROAD TO NATIONAL PARKS & WILDLIFE SERVICE

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that -

- 1 Council support the proposal by the National Parks and Wildlife Service to formally reserve the unnamed road reserve that traverses Lot 191 DP 1204076 as shown crosshatched on Attachments 1 and 2, as National Park, effectively transferring ownership of the road reserve to the NSW Minister for Energy and Environment.
- 2 The National Parks and Wildlife Service be responsible for preparation, and if necessary, registration of all necessary documentation to effect the reservation.
- 3 The National Parks and Wildlife Service be responsible for any and all costs in relation to this matter.
- 4 Approval be granted to affix the Common Seal of Council to any documentation required to give effect to this resolution.

ITEM 8 - TENDER T20/27 - RETAINING WALL CONSTRUCTION AT DARCY ROAD, PORT KEMBLA

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that -

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Donnelley Civil Pty Ltd for Darcy Road Port Kembla – Retaining Wall Construction, in the sum of \$124,278.00, excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

ITEM 9 - TENDER T20/30 - INSTALLATION OF SPORTS FIELD LIGHTING AT GUEST

PARK, FAIRY MEADOW

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that -

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of REES Electrical Pty Ltd for the installation of sports field lighting at Guest Park, Fairy Meadow, in the sum of \$236,090.00, excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

ITEM 10 - STATEMENT OF INVESTMENT - AUGUST 2020

The following staff recommendation was adopted as part of the Block Adoption of Items (refer Minute Number 435)

COUNCIL'S RESOLUTION - RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor T Brown that Council receive the Statement of Investment for August 2020.

ITEM 11 - CITY OF WOLLONGONG TRAFFIC COMMITTEE MINUTES OF MEETING HELD ON 26 AUGUST 2020

- 438 **COUNCIL'S RESOLUTION** - RESOLVED UNANIMOUSLY on the motion of Councillor T Brown seconded Councillor D Brown that in accordance with powers delegated to Council, the Minutes and Recommendation related to Regulation of Traffic, of the City of Wollongong Traffic Committee held on 26 August 2020, be adopted.

ITEM 12 - NOTICE OF MOTION - COUNCILLOR TANIA BROWN - REGIONALISATION OF GOVERNMENT SERVICES

- 439 **COUNCIL'S RESOLUTION** - RESOLVED UNANIMOUSLY on the motion of Councillor T Brown seconded Councillor Colacino that -

- 1 Council develop and implement an advocacy program to promote Wollongong as a location for regionalising NSW state government functions and offices due to changes in workforce post COVID.
- 2 The General Manager and Senior Executive be authorised to meet with the NSW Government, including key Ministers and Heads of Departments and relevant agencies such as NSW Property, to promote the regionalisation program.
- 3 The General Manager be authorised to include local peak bodies and representatives to assist this advocacy program, which will help with implementation of the city's adopted Economic Development Strategy and particularly action 1.8 which calls for advocacy to government for relocation of services.
- 4 Council note that the COVID pandemic and rise in technology has shown that workforces no longer need a physical presence in a capital city but can be decentralised, for example away from the Sydney CBD or Parramatta, and delivered effectively from regional centres.
- 5 Council note that Wollongong presents a safe alternative for workforce relocation and has 58,000 sqm of A-grade office space under construction or in the pipeline. This provides an opportunity for the NSW government to deliver tangible and long-term support to regional economies such as the Illawarra and assist in regional economic

development.

- 6 A report be delivered, or Councillor Briefing be held by 30 June 2021 to update on the success of the advocacy program.

CLOSED COUNCIL SESSION

The Lord Mayor called for a motion to close the meeting to consider a Confidential Item which deals with a report to Council on the General Manager's Annual Performance Review in accordance with Section 10A (2) (a) of the Local Government Act.

Prior to putting the above Motion to the vote, the Lord Mayor advised members of the public that Item C1 relates to a report to Council on the General Manager's Annual Performance review and is classified as Confidential for the following reasons -

- 1 Section 10A (2) (a) of the Local Government Act 1993, permits the meeting to be closed to the public, as the report contains personnel matters concerning particular individuals.
- 2 On balance, the public interest in preserving the confidentiality of the information supplied outweighs the public interest in openness and transparency in Council decision-making by discussing the matter in open meeting.

As no representations had been received prior to the meeting, from members of the public objecting to the closure of this section of the meeting, the Lord Mayor advised that the webcast will be paused while Council considers these items.

440 COUNCIL'S RESOLUTION – RESOLVED UNANIMOUSLY on the motion of Councillor T Brown seconded Councillor Kershaw that the meeting move into Closed Session to consider a report to Council on the General Manager's Annual Performance Review in accordance with Section 10a (2) (a) of the Local Government Act 1993 on the basis that -

- 1 The report contains personnel matters concerning particular individuals.
- 2 On balance, the public interest in preserving the confidentiality of the information supplied outweighs the public interest in openness and transparency in Council decision-making by discussing the matter in open meeting.

At this stage, the time being 7:15pm, the meeting moved into Closed Session.

ITEM C1 – REPORT BY PERFORMANCE REVIEW COMMITTEE – GENERAL MANAGER'S ANNUAL PERFORMANCE REVIEW

441 COUNCIL'S RESOLUTION – RESOLVED UNANIMOUSLY on the motion of Councillor Bradbery seconded Councillor Rimmer that -

- 1 Council note the General Manager's Performance Agreement Review 2019-2020.
- 2 Council note the General Manager's Performance Agreement for 2020-2021.
- 3 Council note there is no change proposed to the General Manager's remuneration package.
- 4 Councillors recognise and thank the General Manager for his efforts in difficult and unique circumstances in the last 12 months.

Variation The variation moved by Councillor Kershaw (the addition of point 4) was accepted by the mover and seconder.

442 COUNCIL'S RESOLUTION – RESOLVED UNANIMOUSLY on the motion of Councillor Dorahy seconded Councillor Kershaw that the meeting move out of Closed Session and into Open Council.

Council resumed into Open Session at 7:28pm.

OPEN COUNCIL SESSION

RESOLUTIONS FROM THE CLOSED SESSION OF COUNCIL

The Lord Mayor advised the meeting of Council's Resolution whilst in Closed Session (refer to Minute Number 441).

THE MEETING CONCLUDED AT 7:28PM

Confirmed as a correct record of proceedings at the Ordinary Meeting of the Council of the City of Wollongong held on Monday 26 October 2020.

Chairperson

ITEM A

50TH ANNIVERSARY OF LORD MAYORALTY, NAMING OF THE CITY OF WOLLONGONG AND OPENING OF WOLLONGONG BOTANIC GARDENS

This year marks the 50th anniversary of three significant milestones in the history of Wollongong.

On 30 October 1970 the City of Greater Wollongong was gazetted as the “City of Wollongong”. It came six months after the Queen’s visit to confer a Lord Mayoralty to the City.

The Wollongong Botanic Garden was officially opened on 26 September 1970 and opened daily to the public in January 1971. This wonderful asset is highly valued by the local community and visitors alike. From the original Hoskins gift of land to botanical donations, volunteer support and ongoing Council support, Wollongong Botanic Garden is proudly a people’s garden. During 2021, the 50th anniversary of the Botanic Garden will be recognised in various ways to highlight the horticultural, conservation, environmental education and heritage aspects of the site.

RECOMMENDATION

Wollongong City Council acknowledge and note the 50th anniversary of the -

- 1 Conferring of the Lord Mayoralty to Wollongong by Queen Elizabeth II on 10 April 1970
- 2 Official opening of the Wollongong Botanic Gardens on 26 September 1970
- 3 Adoption of the name "City of Wollongong" on 30 October 1970

ATTACHMENTS

There are no attachments for this report.

ITEM B COUNCIL ROLE IN NSW PLANNING SYSTEM

In New South Wales, the Department of Planning, Industry and Environment set the rules that apply to all councils for land use planning and development. Council is required to prepare a Local Environmental Plan (LEP) and Development Control Plans to guide how development occurs across the LGA. When this is done, it involves a lengthy consultation process over a number of years.

Wollongong City Councillors do not make decisions about individual Development Applications with most being determined by Council staff under delegation. Some DAs, including those that are considered to be regionally significant or attract a lot of community interest, are sent to independent groups for decision. These groups include the Wollongong Local Planning Panel and the Southern Regional Planning Panel.

When Council staff or an independent group assess a DA, they will compare what is proposed to the LEP, DCPs and State policies. As part of the assessment, consideration is given to any objections and feedback that has been received during the exhibition period. Other factors taken into account include the environmental impact of the development, land use constraints and whether or not the development is in the public interest. Community consultation and public participation are an important and valued step in the assessment process however submissions are just one component of an assessment and do not automatically veto a development.

The role of Councillors and community within the NSW planning system is not always understood. There is a need to provide residents with clear information on how planning decisions are made.

RECOMMENDATION

Wollongong City Council -

- 1 Develop a clear, plain English communication strategy to inform and educate residents about the NSW planning system and the role that Councillors and community feedback play in the planning process, more specifically Development Applications and Planning Proposals.
- 2 Write to the Minister for Planning and Public Spaces requesting that the State Government also prepare clear information for the community on the NSW planning system.

ATTACHMENTS

There are no attachments for this report.

ITEM C

LORD MAYORAL MINUTE - TREE VANDALISM - REWARD MECHANISM FOR INFORMATION

The recent vandalism of two coastal tea trees at Belmore Basin has caused outrage in our community. This kind of vandalism unfortunately occurs across our LGA, impacting the amenity and liveability of our City.

One objective of Council's Tree and Vegetation Vandalism Policy is to promote and guide broader community involvement in the prevention of vandalism, and in Council's response to vandalism incidents. Council's ability to successfully investigate vandalism and help prosecute those responsible is heavily reliant on receiving information from the community. Introducing an incentive to provide information may assist in successful enforcement outcomes and reduce the incidence of tree/vegetation vandalism.

RECOMMENDATION

Wollongong City Council review the Tree and Vegetation Vandalism Policy to address continuing instances of vandalism across the LGA and explore further opportunities to deter tree and vegetation vandalism.

The review should include the feasibility of a reward mechanism to encourage a person or persons to provide information to Council or Police that leads to the successful prosecution of offenders responsible for tree and/or vegetation vandalism.

ATTACHMENTS

There are no attachments for this report.

ITEM 1 ANNUAL FINANCIAL STATEMENTS FOR THE YEAR ENDED 30 JUNE 2020

PRESENTER: Audit Office of NSW – Financial Statements

Council officers have prepared the draft 2019-2020 Annual Financial Statements in accordance with the Local Government Act 1993 (as amended), the Regulations, the Australian Accounting Standards and the Local Government Code of Accounting Practice and Financial Reporting.

The draft Financial Statements were presented to the Audit, Risk and Improvement Committee on 6 October 2020. At that meeting, the Audit, Risk and Improvement Committee resolved that the General Purpose Financial Statements and the Special Purpose Financial Statements be recommended to Council for endorsement and the signing of the Statements by the Lord Mayor, Deputy Lord Mayor and Management.

The Statements have been reviewed by Council's Auditors, Audit Office of NSW, and are presented to Council for an opinion to be formed in the prescribed format (as attached). The Audit Office of NSW will address the meeting to provide an overview of their Audit Report for the year ended 30 June 2020.

RECOMMENDATIONS

- 1 The Annual Financial Statements be endorsed and an opinion be formed in the prescribed format (as per Attachments 2 and 3) on the General Purpose Financial Statements and the Special Purpose Financial Statements.
- 2 The audited Financial Statements and the Auditor's report be presented to the public at the Council meeting of 16 November 2020.

REPORT AUTHORISATIONS

Report of: Brian Jenkins, Chief Financial Officer

Authorised by: Renee Campbell, Director Corporate Services - Connected + Engaged City

ATTACHMENTS

- 1 Draft Annual Financial Statements 2019-2020
- 2 Statement by the Lord Mayor, Deputy Lord Mayor and Management on the General Purpose Financial Statements
- 3 Statement by the Lord Mayor, Deputy Lord Mayor and Management on the Special Purpose Financial Statements
- 4 Flowchart of the Annual Financial Statements Approval and Audit Process

BACKGROUND

The Local Government Act 1993 (as amended) requires Council to prepare financial statements for each year and refer them for audit as soon as practicable after the end of that year.

A council's financial statements must include:

- 1 A General Purpose Financial Statement;
- 2 any other matter prescribed by the regulations, and
- 3 a statement in the approved form by the Council as to its opinion on the General Purpose Financial Statements.

The Annual Financial Statements Approval and Audit Process is outlined in Attachment 4 of this report. This process is largely governed by the requirements of the Local Government Act 1993 (as amended) having regard to the timing of Council meetings. In accordance with this process, the Audit, Risk and Improvement Committee recommends to the Council whether the Financial Statements should be signed based on the Committee's assessment of them.

An Executive Summary and Financial Commentary to the Statements are also prepared to provide an overview of the Financial Statements. The Executive Summary and Financial Commentary are not audited.

PROPOSAL

Section 413 of the Local Government Act 1993 (as amended) requires the Council to form an opinion as to whether Council's Annual Financial Statements have been drawn up in accordance with the Local Government Act and associated Codes and Australian Accounting Standards as prescribed by the Regulations. The Statements have been reviewed by Council's Auditors, Audit Office of NSW, and are presented to Council for an opinion to be formed in the prescribed format (see Attachments 2 and 3). Subsequent to these Statements being signed, Council's Auditor will present its Audit Report to Council.

Following the signing of the opinions and receiving the Auditor's Report, a copy of the Audited Financial Statements will be submitted to the Office of Local Government in accordance with Section 417(5) of the Local Government Act 1993 (as amended).

Section 418 of the Local Government Act 1993 (as amended) requires that Council, as soon as practical after receiving a copy of the Auditor's Reports, fix a date for a meeting at which it proposes to present its Audited Financial Statements, together with the Auditor's Reports, to the public. The Council must give public notice of the date of the meeting. Consequently, an advertisement is scheduled to appear in The Advertiser on Wednesday, 4 November 2020.

Section 420 of the Local Government Act 1993 provides that any person may make submissions to the Council with respect to the Financial Statements and/or the Auditor's Reports and those submissions must be made in writing and lodged with the Council within seven days of the public meeting. The date of the Council meeting (public meeting) is the next Council meeting as resolved by Council.

CONSULTATION AND COMMUNICATION

The Audit Office of NSW has been consulted throughout the preparation of the Annual Financial Statements. The audit staff have been helpful in ensuring compliance with all accounting requirements.

A set of draft Financial Statements were presented to the Audit, Risk and Improvement Committee on 6 October 2020. At the meeting, the Audit, Risk and Improvement Committee resolved that:

- 1 The draft Annual Financial Statements for the year ended 30 June 2020 be noted and received.
- 2 The General Purpose Financial Reports be recommended to Council for endorsement and the signing of the Statement by the Lord Mayor, Deputy Lord Mayor and Management, made pursuant to Section 413[2][c] of the Local Government Act 1993 (as amended).
- 3 The Special Purpose Financial Reports be recommended to Council for endorsement and the signing of the Statement by the Lord Mayor, Deputy Lord Mayor and Management, made pursuant to the Local Government Code of Accounting Practice and Financial Reporting.

Through their review of the draft Financial Statements the Audit, Risk and Improvement Committee recommended minor updates to formatting and notes in the Financial Commentary.

Executive, senior managers and significant senior officers were requested to ensure that all relevant information with regards to the 2019-2020 transactions relating to their area/s of responsibility had either been entered into the financial records or disclosed to the Chief Financial Officer.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Wollongong 2022 goal '*We are a connected and engaged community*'. It specifically delivers on the following:

Community Strategic Plan Strategy	Delivery Program 2018-2022 4 Year Action	Operational Plan 2020-2021 Operational Plan Actions
4.3.2 Resources (finance, technology, assets and people) are effectively managed to ensure long term financial sustainability	4.3.2.1 Effective and transparent financial management systems are in place	Monitor and review achievement of Financial Strategy
		Continuous budget management is in place, controlled and reported
		Provide accurate and timely financial reports monthly, quarterly and via the annual statement
		Manage and further develop a compliance program to promote awareness and compliance with Council's procurement policies and procedures and other related policies

RISK MANAGEMENT

The Annual Financial Statements are required to be prepared in accordance with the Local Government Act 1993 (as amended), the Local Government Code of Accounting Practice and Financial Reporting and the Australian Accounting Standards. Council has professionally qualified staff to ensure compliance with the reporting requirements.

Council staff has also undertaken additional training in specialised areas. A higher level of assurance is attained from the Audit Office of NSW review.

Further to this, Council's Executive, senior management and relevant senior officers each signed a document giving the necessary assurances that:

- 1 No matters or occurrences have come to their attention in respect to their areas of responsibility that would materially affect the Financial Statements or disclosures therein, or which are likely to materially affect the future results or operations of the Council; and
- 2 Should any such matters or occurrences come to their attention after the date of signing the document, the Chief Financial Officer be immediately advised.

Consequently, there is considered to be a low risk of any material errors or omissions in reporting.

FINANCIAL IMPLICATIONS

Council is now presented with a set of draft Financial Statements that have been prepared in accordance with the Australian Accounting Standards and other professional pronouncements and the Local Government Code of Accounting Practice and Financial Reporting.

A preliminary pre-audit result was presented to Council on 31 August 2020 as part of the Draft Quarterly Review Statement June 2020. The preliminary pre-audit result reported is consistent with the Annual Financial Statements 2019-2020.

The Financial Statements for 30 June 2020 shows a positive operating result when compared to budget. The Operating Result [pre capital] includes an impact from COVID-19 primarily related to revenue in the order of \$3.1M. The 2019-2020 result has been impacted by a number of significant adjustments related to reclassification of prior year capital works-in-progress (WIP) to operational expense, loss on disposal of assets (assets renewed that had remaining book value, not sold) and revaluation of investment properties partly offset by the revaluation of the asset remediation provision. In addition, the Financial Assistance Grant was paid 52% in advance that is consistent with the prior year and has not had a significant impact year on year.

CONCLUSION

The Financial Statements will be presented to the public at the next Council meeting pending Council forming an opinion that they have been drawn up in accordance with the Local Government Act and associated Codes and Australian Accounting Standards as prescribed by the Regulations.



WOLLONGONG CITY COUNCIL

FINANCIAL REPORT

YEAR ENDED 30 JUNE 2020

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EXECUTIVE SUMMARY

As General Manager of Wollongong City Council, I present the Annual Financial Statements for 2019-2020. These statements continue to reflect Council's transformation in recent years to an organisation which is financially stable having the capacity to manage short, medium and longer term challenges.

The Income Statement shows a net operating surplus of \$35.5 million compared to a surplus of \$52.1 million in 2018-2019. The net operating result before capital grants and contributions was a deficit of \$11.9 million. While this result compares to a deficit of \$7.5 million in 2018-2019, the result was impacted by COVID-19 as well as a number of significant one-off adjustments. We are confident that our underlying financial performance is on track and remains sustainable.

Council's Balance Sheet shows the vast extent of assets managed by Council for the community. The total value of Council's assets at 30 June 2020 was \$2.87 billion. During 2019-2020, Council completed capital works of \$89.8 million including the construction and acquisition of \$31.2 million of new assets and \$58.6 million for the renewal of existing assets. The program included projects such as the continuation of West Dapto Access Works including the extension of Fowlers Road to Fairwater Drive, additional sections of the Grand Pacific Walk, Bellambi Rock Pool Refurbishment, and various upgrades to community centres, amenities and buildings at Council parks, gardens and sports fields across the Local Government Area.

Council maintains a strong position in cash and investments, with holdings of \$157.5 million at 30 June 2020 [\$154.8 million in 2018-2019]. \$138 million of Council's cash [\$139 million in 2018-2019] is restricted in its use to specific purposes by external bodies, legislation and Council resolution.

Council's unrestricted current ratio decreased from 2.70:1 in 2018-2019 to 2.36:1 in 2019-2020. This ratio, which measures Council's liquidity and ability to satisfy short-term obligations, is above the Local Government Benchmark of >1.5:1. Council's strategy is to better utilise cash and target a lean unrestricted cash ratio. The measure is expected to remain above the benchmark until payments are required.

Rates and Annual Charges outstanding increased from 5.09% in 2018-2019 to 6.74% in 2019-2020. Council's assistance package in response to COVID-19 includes an option for ratepayers to defer the 4th instalment for 2019-2020 without penalty. This has resulted in an increased level of outstanding payments.

Council continues to have relatively low levels of borrowing and did not undertake any additional borrowings in 2019-2020. This contrasts to recent years where borrowings under attractive and/or subsidised terms for specific works have been sought. Low debt levels remain a financial strength of Council and adds flexibility in making financial decisions for the future. The 2019-2020 debt service ratio increased slightly to 3.41% compared to 3.28% in 2018-2019. This change is the result of decreased income from continuing operations rather than a shift in borrowing levels. This ratio is within Council's financial strategy to operate within a ratio of <4%.

My thanks to all staff and external auditors who worked on the preparation of these statements.



Greg Doyle
General Manager
Wollongong City Council

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2019-2020 FINANCIAL OVERVIEW

This report provides an overview of Council's 2019-2020 Financial Statements. The Financial Statements are prepared by Council to provide information in relation to Council's financial performance and position. The Statements are prepared in accordance with Australian Accounting Standards, the NSW Local Government Act 1993 and the NSW Local Government Code of Accounting Practice and Financial Reporting (Update N° 28). The Statements are independently audited by the Audit Office of NSW, reviewed by the Audit, Risk and Improvement Committee, reported to Council, placed on public exhibition and lodged with the Office of Local Government. The Financial Statements are made up of five key financial reports; Income Statement, Statement of Comprehensive Income, Statement of Financial Position, Statement of Changes in Equity and Statement of Cash Flows.

The 2019-2020 Financial Statements shows a positive Net Operating Result of \$35.5M. This result includes grants and contributions for capital purposes which increases the asset base of Council however is not considered to truly reflect the underlying operating performance of the organisation.

The Net Operating Result before Grants and Contributions for Capital Purposes was a deficit of \$11.9M which is below Council's targeted position of a small surplus result. This included an impact from COVID-19 primarily related to revenue in the order of \$3.1M. The 2019-2020 result has been impacted by a number of significant adjustments related to reclassification of prior year capital works-in-progress (WIP) to operational expense, loss on disposal of assets (assets renewed that had remaining book value, not sold) and revaluation of investment properties partly offset by the revaluation of the asset remediation provision. In addition, the Financial Assistance Grant was paid 52% in advance that is consistent with the prior year and has not had a significant impact year on year. These variations do not change the underlying capacity of the organisation over a period of time, and while there are also some positive one off items reported, Council considers our underlying financial performance to be on track to meet financial strategies and targets in the short, medium and long term.

2019-2020 HIGHLIGHTS

\$2.87B

Total Assets

LY \$2.79B

3.41%

Debt Service Ratio

LY 3.28%

\$31.2M

Capital Works - New

LY \$64.0M

\$58.6M

Capital Works - Renew

LY \$51.8M

\$35.5M Surplus

Net Operating Result

LY \$52.1M Surplus

\$11.9M Deficit

Net Operating Result Before Capital
Grants & Contributions

LY \$7.5M Deficit

\$21.3M

Contributed Assets Recognised

LY \$7.4M

2.36:1

Unrestricted Current Ratio

LY 2.70:1

\$157.5M

Cash Assets & Investments

LY \$154.8M

6.74%

Rates, Annual Charges, Interest &
Extra Charges Outstanding

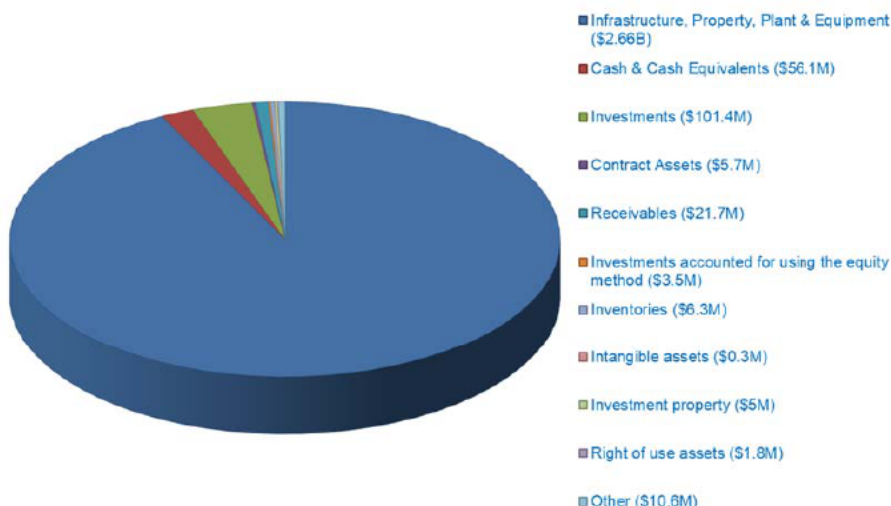
LY 5.09%

*LY = Last Year

ASSETS

Council is the custodian of community assets with a total value of \$2.87B at 30 June 2020. The composition of assets at balance date is shown as follows:

TOTAL ASSET COMPOSITION 2019-2020



Infrastructure, Property, Plant & Equipment (IPPE)

With a carrying value of \$2.66B IPPE is Council's most significant asset group representing 93% of total asset value. This asset group include roads, drains, footpaths, community facilities, recreational facilities, parks and gardens.

During 2019-2020, Council achieved its target and delivered a capital works program of \$89.8M including the construction and purchase of \$31.2M of new assets and renewal of existing assets of \$58.6M. In addition, \$21.2M of assets were contributed to Council during 2019-2020. After accounting for annual depreciation expense, the overall value of IPPE increased by \$145.7M during 2019-2020. This is mainly the result of revaluations to community & crown land. Further financial details of IPPE are shown at Note 11.

Cash and Investments

Council maintained robust levels of cash and investments, with holdings of \$157M at 30 June 2020. Cash and investment positions over recent years are as follows:

	YTD Actual 30 June 2017	YTD Actual 30 June 2018	YTD Actual 30 June 2019	YTD Actual 30 June 2020
TOTAL CASH & INVESTMENTS	167.577	157.768	154.822	157.474
Less Restrictions:				
External	75.788	74.192	77.657	75.470
Internal	57.379	68.129	61.203	62.675
AVAILABLE CASH	34.410	15.447	15.962	19.329
Adjusted for :				
Current payables	(41.617)	(30.010)	(35.020)	(35.163)
Receivables	34.211	37.702	48.499	36.707
AVAILABLE FUNDS	27.004	23.139	29.441	20.873

While Council's cash and investment position is robust, a significant portion of these funds are subject to restriction. Council's true cash position is more accurately depicted by considering available funds which are uncommitted and not subject to restriction. External restrictions are funds held by Council that must be spent for a specific purpose and cannot be used by Council for general operations. Internal restrictions are funds that Council has determined will be used for a specific future purpose.

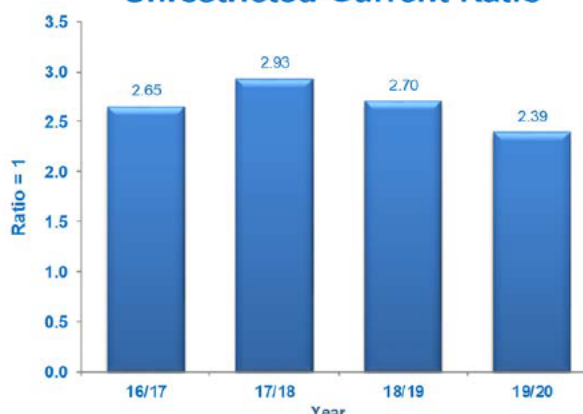
At 30 June 2020, Council achieved an available funds position of \$20.9M, which is significantly higher than the Financial Strategy target range of 3.5% to 5.5% of operational revenue (pre-capital). The result at balance date was uplifted by the early receipt of the first two instalments of the Financial Assistance Grant for 2020-2021 totalling \$9.9M.

Measures: Cash/Liquidity Position or Council's ability to satisfy obligations in the short term from its unrestricted activities.

Strategy: Council's strategy is to maximise the use of available funds for asset renewal by targeting a lean Unrestricted Current Ratio.

Performance: Council's performance is above the Local Government Benchmark of >1.5:1 and is consistent with prior years.

Unrestricted Current Ratio

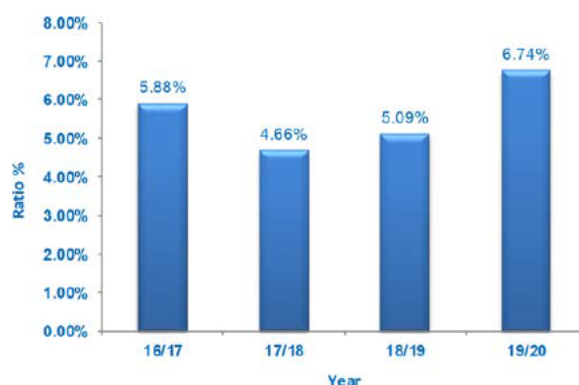


Receivables

Receivables are the amount of money that is owed to Council or funds that Council has paid in advance. At 30 June 2020, receivables totalled \$21.7M, a decrease of \$15.0M compared to the 2018-2019 reporting period. The fluctuation is related to the implementation of new revenue accounting standards

that require the recognition of contract assets in place of receivables for grants and contracts with customers. Full details of receivables are provided in Note 8.

Rates, Annual Charges, Interest & Extra Charges Outstanding



Measures: The impact of uncollected rates and other charges on liquidity and the adequacy of recovery efforts.

Target: Industry standard of <5%.

Performance: In response to COVID-19, Council developed an assistance package that includes an option for ratepayers to defer the 4th instalment for 2019-2020 without penalty. This has resulted in an increased level of outstanding payments.

Contract Assets

AASB 15 Revenue from Contracts with Customers and AASB 1058 Income of not-for-profit entities were effective for the first time for the year ended 30 June 2020. There were \$5.7M in contract assets relating to capital works on grant funded projects completed before the funding has been received recognised for 2019-2020.

LIABILITIES

At 30 June 2020, Council's Total Liabilities were \$154.58M. The composition of Council's Total Liabilities is shown below.

Provisions: Account for 65.2% of Council's Liabilities with the most significant provisions relating to Employee Leave Entitlements [\$59.1M], Waste Depot Remediation Provision [\$33.4M], Workers' Compensation Provision [\$7.7M] and Self-Insurance [\$0.7M].

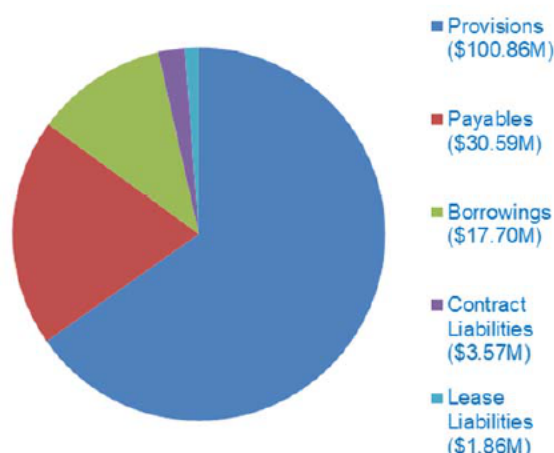
Payables: Account for 19.8% of Council's Liabilities. The most significant payables being goods and services received and capital expenditure incurred but not yet paid for, and accrued expenditure.

Borrowings: Account for 11.4% of Council's Liabilities and relate to the subsidised Local Infrastructure Renewal Scheme Programs (1, 2 and 3) secured over the previous five financial years.

Contract Liabilities: Account for 2.3% of Council's liabilities. Contract liabilities represent unexpended grants and contributions and fees received in advance of the service being delivered.

Lease Liabilities: Account for 1.2% of Council's liabilities. Lease liabilities are recognised for land and buildings that Council leases from other organisations. Further details of Leases are provided in Note 15.

Composition of Council's Liabilities 2019-20



Provisions

Provisions decreased during 2019-2020 by \$4.4M. The decrease is mainly attributable to a reduction in the asset remediation provision (\$9.57M) partially offset by increases in employee leave entitlements (\$4.64M) and the workers' compensation provision (\$0.53M).

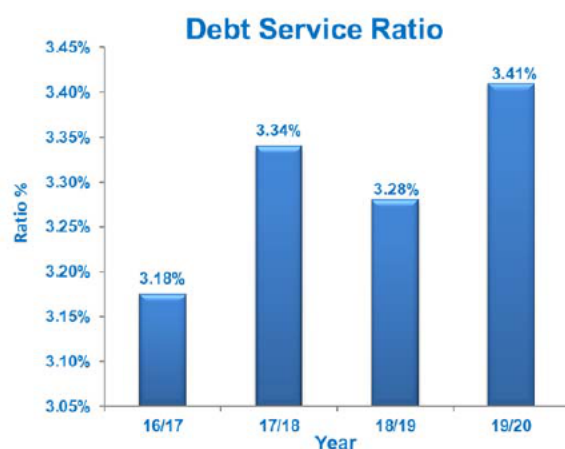
Each financial year the provision for the remediation of Council's waste facility is calculated based on the forecast costs to rehabilitate the site. During 2019-2020, a reduction in the forecast rehabilitation costs caused a significant reduction in the provision.

The movement in the employee leave entitlements are the result of external economic conditions through the application of discount rates to reflect the value of the provision as at 30 June 2020.

Borrowings

Council continues to remain a relatively low debt user. In 2019-2020 Council did not undertake additional borrowings, all activity related to the repayment of previously secured loans.

In prior years, Council took advantage of borrowing opportunities enabling it to accelerate specific capital programs. The loan facilities which Council has in place include subsidised loans under the Local Government Infrastructure Renewal Scheme (LIRS). Funds were secured under LIRS rounds 1, 2 and 3. The projects funded under round 1 (Citywide Footpaths and Shared Path Renewal and Missing Links Construction Program) and round 2 (Upgrade Berkeley and Corrimal Community Centres and Thirroul Pavilion and Kiosk) are complete. The project funded by the round 3 applications, (the West Dapto Access Project to construct Fowlers Road to Fairwater Drive) is still active. Repayment of LIRS loans will continue in accordance with the various payment schedules until 2025.



Measures: The proportion of revenues that is required to meet Council's annual loan repayments.

Target: Council's Financial Strategy allows for a ratio of up to 4%.

Performance: A low level of debt is reflected in Council's Debt Service Ratio of 3.41%. This is exceptionally low in comparison to the Local Government benchmark ratio of <10% and is within Council's own Strategy.

The Debt Service Ratio increased slightly in the 2019-2020 reporting period. This is due to a decrease in Council's income from continuing operations.

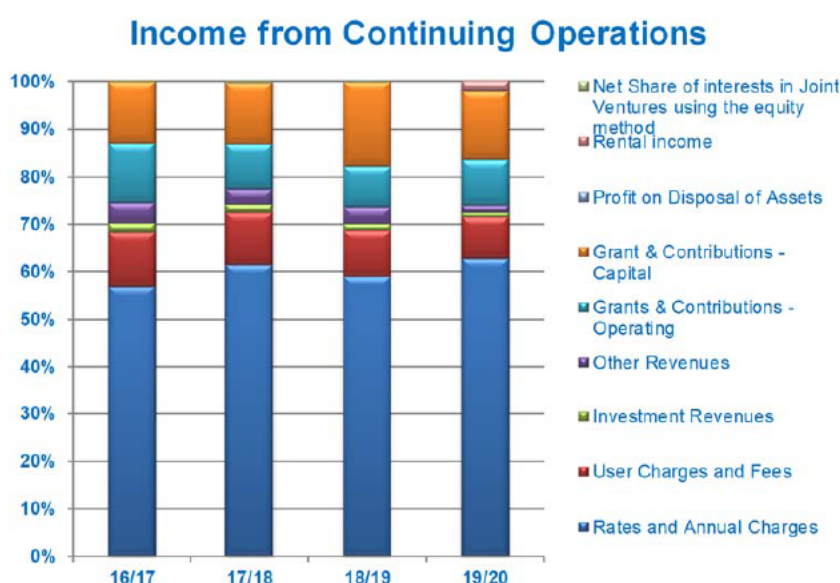
Operational Performance - Income & Expenses

Council achieved a Net Operating Surplus from Continuing Operations of \$35.5M in 2019-2020 in comparison to a surplus of \$52.1M in 2018-2019. This result includes capital grants and contributions that vary significantly from period to period. Council's underlying measure of long term operational performance, the Operational Result before Capital Grants and Contributions, is a deficit of \$11.9M, compared to a deficit of \$7.5M in 2018-2019.

Income

Council's Income from Continuing Operations for 2019-2020 was \$327.5M compared to a prior year result of \$338.5M. This decrease is mainly the result of a reduction in Capital Grants & Contributions during 2019-2020 as a higher level of Capital Grants was received for transport infrastructure associated with the West Dapto Urban Release Area in 2018-2019.

The composition of Council's revenue remained reasonably consistent compared to previous years as depicted in the table below. Exceptions include Rental Income, which has been recategorised from User Fees & Charges and Other Revenues as a result of the new revenue accounting standards effective from 1 July 2019, and Grants and Contributions Capital which decreased in its portion of total income from continuing operations [from 18% to 14%]. It is noted that Council does not control the flow of asset dedications by developers and contributions vary from year to year.

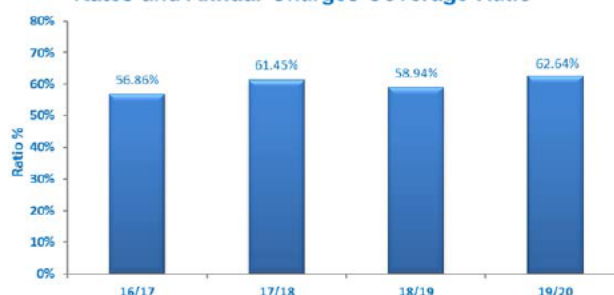


Income from Rates and Annual Charges, Council's largest revenue stream, totalled \$205.1M in 2019-2020, an increase of \$5.6M on the prior year. The increase is due to indexation and rates growth.

Measures: The degree of Council's dependence upon revenue from rates and annual charges and the security of Council's income.

Performance: This ratio indicates an increased level of dependence on rates and annual charges revenue compared to last year.

Rates and Annual Charges Coverage Ratio



Expenses

Expenses from Continuing Operations for 2019-2020 totalled \$291.9M, an increase on prior year expenditure of \$5.5M. This increase can be predominately explained by prior period works in progress being transferred to other expenses (\$8.0M), and the revaluation of the investment property (\$2.0M). This has been partly offset by a reduction in other expenses (\$5.6M) to offset a revaluation of the asset remediation provision.

In terms of composition, the proportion of each expenditure category against total expenses from continuing operations has remained consistent compared to prior years with the exception of the fair value decrement on investment properties. Material budget variations from the 2019-2020 year for income and expenditure items are detailed in Note 24 of the statements.

Expenses from Continuing Operations



Wollongong City Council

Historical Financial Data

Income Statement

		Actual 2016/17 \$'000	Actual 2017/18 \$'000	Actual 2018/19 \$'000	Actual 2019/20 \$'000
	Notes				
Income from Continuing Operations					
Revenue:					
Rates & Annual Charges	3a	184,530	193,429	199,524	205,118
User Charges & Fees	3b	37,491	34,805	32,793	28,695
Interest and Investment Revenue	3c	5,909	5,162	5,069	3,360
Other Revenues	3d	14,418	10,551	12,125	4,794
Grants & Contributions provided for Operating Purposes	3e,f	39,639	29,384	28,856	30,727
Grants & Contributions provided for Capital Purposes	3e,f	42,022	40,637	59,656	47,472
Other Income:					
Fair Value Increment on Investment Property	11	-	5	220	-
Rental income		-	-	-	6,735
Net share of interests in joint ventures using the equity method	17	525	803	291	555
Total Income from Continuing Operations		324,534	314,776	338,534	327,456
Expenses from Continuing Operations					
Employee Benefits & On-Costs					
Employee Benefits & On-Costs	4a	103,139	107,843	122,445	122,912
Borrowing Costs	4b	4,071	3,819	3,337	3,052
Materials & Contracts	4c	47,032	53,709	57,432	60,934
Depreciation & Amortisation	4d	61,271	60,067	61,781	60,845
Other Expenses	4e	35,594	40,211	34,571	36,497
Net Losses from the Disposal of Assets	5	3,960	361	6,856	5,686
Fair value decrement on investment properties		-	-	-	1,998
Loss from interests in Joint Ventures & Associates	19	-	-	-	-
Total Expenses from Continuing Operations		255,067	266,010	286,422	291,924
Operating Result from Continuing Operations		69,467	48,766	52,112	35,532
NET OPERATING RESULT FOR THE YEAR		69,467	48,766	52,112	35,532
Less:					
Grants & Contributions provided for Capital Purposes	3e,f	42,022	40,637	59,656	47,472
Net Operating Result for the year before Grants and Contributions provided for Capital Purposes		27,445	8,129	(7,544)	(11,940)

This Statement is to be read in conjunction with the Notes in the body of the financial statements.



Wollongong City Council

Historical Financial Data

Statement of Financial Position

	Notes	Actual 2016/17 \$'000	Actual 2017/18 \$'000	Actual 2018/19 \$'000	Actual 2019/20 \$'000
ASSETS					
Current assets					
Cash & cash equivalents	6a	23,534	26,491	25,187	56,051
Investments	6b	119,458	109,162	114,579	97,423
Receivables	7	23,531	27,036	36,620	20,466
Inventories	8	298	306	337	381
Contract Assets		-	-	-	5,669
Other	8	10,680	10,666	11,879	10,572
Assets held for sale (previously non-current)	9	6,381	-	-	-
Total current assets		183,882	173,661	188,602	190,562
Non-current assets					
Investments	6b	24,585	22,115	15,056	4,000
Inventories	8	5,791	5,835	5,948	5,972
Infrastructure, property, plant & equipment	10	2,309,628	2,356,306	2,570,098	2,655,487
Investments accounted for using the equity method	17	1,835	2,638	2,929	2,929
Investment property	11	4,775	4,780	5,000	5,000
Intangible assets	12	653	388	440	254
Right of use assets		-	-	-	1,790
Total non-current assets		2,347,267	2,392,062	2,599,471	2,675,432
TOTAL ASSETS		2,531,149	2,565,723	2,788,073	2,865,994
LIABILITIES					
Current liabilities					
Payables	13	38,660	26,754	30,006	29,392
Income received in advance	13	2,957	3,256	5,014	-
Contract Liabilities		-	-	-	3,572
Lease Liabilities		-	-	-	341
Borrowings	13	7,513	7,716	7,934	5,260
Provisions	14	48,853	50,377	58,214	61,084
Total current liabilities		97,983	88,103	101,168	99,649
Non-current liabilities					
Payables	13	-	700	385	-
Lease Liabilities		-	-	-	1,519
Borrowings		32,188	25,039	17,497	12,439
Provisions	14	48,121	44,566	47,054	39,776
Total non-current liabilities		80,309	70,305	64,936	53,734
TOTAL LIABILITIES		178,292	158,408	166,104	153,383
NET ASSETS		\$ 2,352,857	2,407,315	2,621,969	2,712,611
EQUITY					
Retained earnings	15	1,389,905	1,441,990	1,468,848	1,497,753
Revaluation reserves	15	962,952	965,325	1,153,065	1,214,858
	15	-	-	56	-
Council Equity Interest		2,352,857	2,407,315	2,621,969	2,712,611
Minority Equity Interest		-	-	-	-
TOTAL EQUITY		\$ 2,352,857	2,407,315	2,621,969	2,712,611

This Statement is to be read in conjunction with the Notes in the body of the financial statements.

Wollongong City Council

ANNUAL FINANCIAL STATEMENTS for the year ended 30 June 2020

DRAFT

Wollongong City Council

GENERAL PURPOSE FINANCIAL STATEMENTS

for the year ended 30 June 2020

Wollongong City Council

Financial Statements 2020

General Purpose Financial Statements

for the year ended 30 June 2020

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Overview

Wollongong City Council is constituted under the Local Government Act 1993 (NSW) and has its principal place of business at:

41 Burelli Street
Wollongong NSW 2500

Council's guiding principles are detailed in Chapter 3 of the LGA and includes:

- principles applying to the exercise of functions generally by council,
- principles to be applied when making decisions,
- principles of community participation,
- principles of sound financial management, and
- principles for strategic planning relating to the development of an integrated planning and reporting framework.

A description of the nature of Council's operations and its principal activities are provided in Note 2(b).

Through the use of the internet, we have ensured that our reporting is timely, complete and available at minimum cost. All press releases, financial statements and other information are publicly available on our website: www.wollongong.nsw.gov.au.

Wollongong City Council

Financial Statements 2020

General Purpose Financial Statements

for the year ended 30 June 2020

Statement by Councillors and Management

made pursuant to Section 413(2)(c) of the Local Government Act 1993 (NSW) (as amended)

The attached General Purpose Financial Statements have been prepared in accordance with:

- the *Local Government Act 1993* (NSW) (as amended) and the regulations made thereunder,
- the Australian Accounting Standards and other pronouncements of the Australian Accounting Standards Board,
- the Local Government Code of Accounting Practice and Financial Reporting.

To the best of our knowledge and belief, these statements:

- present fairly the Council's operating result and financial position for the year,
- accord with Council's accounting and other records.

We are not aware of any matter that would render these statements false or misleading in any way.

Signed in accordance with a resolution of Council made on 26 October 2020.

Gordon Bradbery
Lord Mayor
26 October 2020

Tania Brown
Deputy Lord Mayor
26 October 2020

Greg Doyle
General Manager
26 October 2020

Brian Jenkins
Responsible Accounting Officer
26 October 2020

Wollongong City Council

Financial Statements 2020

Income Statement

for the year ended 30 June 2020

Original unaudited budget 2020	\$ '000	Notes	Actual 2020	Actual 2019
	Income from continuing operations			
202,991	Rates and annual charges	3a	205,118	199,524
33,665	User charges and fees	3b	28,695	32,793
10,598	Other revenues	3c	4,794	12,125
20,840	Grants and contributions provided for operating purposes	3d,3e	30,727	28,856
42,927	Grants and contributions provided for capital purposes	3d,3e	47,472	59,656
4,882	Interest and investment income	4	3,360	5,069
–	Fair value increment on investment properties	12	–	220
–	Rental income	15e	6,735	–
–	Net share of interests in joint ventures and associates using the equity method	20	555	291
315,903	Total income from continuing operations		327,456	338,534
	Expenses from continuing operations			
117,908	Employee benefits and on-costs	5a	122,912	122,445
2,722	Borrowing costs	5b	3,052	3,337
60,072	Materials and contracts	5c	60,934	57,432
66,276	Depreciation and amortisation	5d	60,845	61,781
35,426	Other expenses	5e	36,497	34,571
–	Net losses from the disposal of assets	6	5,686	6,856
–	Fair value decrement on investment properties	12	1,998	–
282,404	Total expenses from continuing operations		291,924	286,422
33,499	Operating result from continuing operations		35,532	52,112
33,499	Net operating result for the year		35,532	52,112
33,499	Net operating result attributable to council		35,532	52,112
(9,428)	Net operating result for the year before grants and contributions provided for capital purposes		(11,940)	(7,544)

Council has not restated comparatives when initially applying AASB 1058 *Income of Not-for-Profit Entities*, AASB 15 *Revenue from Contracts with Customers* and AASB 16 *Leases*. The comparative information has been prepared under AASB 111 *Construction Contracts*, AASB 118 *Revenue*, AASB 1004 *Contributions*, AASB 117 *Leases* and related Accounting Interpretations.

The above Income Statement should be read in conjunction with the accompanying notes.

Wollongong City Council

Financial Statements 2020

Statement of Comprehensive Income

for the year ended 30 June 2020

\$ '000	Notes	2020	2019
Net operating result for the year (as per Income Statement)		35,532	52,112
Other comprehensive income:			
Amounts which will not be reclassified subsequently to the operating result			
Gain (loss) on revaluation of IPP&E	11	57,580	196,818
Total items which will not be reclassified subsequently to the operating result		57,580	196,818
Amounts which will be reclassified subsequently to the operating result when specific conditions are met			
Gain/(loss) on revaluation of equity instruments at fair value through other comprehensive income		–	(32)
Total items which will be reclassified subsequently to the operating result when specific conditions are met		–	(32)
Total other comprehensive income for the year		57,580	196,786
Total comprehensive income for the year		93,112	248,898
Total comprehensive income attributable to Council		93,112	248,898

Council has not restated comparatives when initially applying AASB 1058 *Income of Not-for-Profit Entities*, AASB 15 *Revenue from Contracts with Customers* and AASB 16 *Leases*. The comparative information has been prepared under AASB 111 *Construction Contracts*, AASB 118 *Revenue*, AASB 1004 *Contributions*, AASB 117 *Leases* and related Accounting Interpretations.

The above Statement of Comprehensive Income should be read in conjunction with the accompanying notes.

Wollongong City Council

Financial Statements 2020

Statement of Financial Position

as at 30 June 2020

\$ '000	Notes	2020	Restated 2019	Restated 1 July 2018
ASSETS				
Current assets				
Cash and cash equivalents	7(a)	56,051	25,187	26,491
Investments	7(b)	97,423	114,579	109,162
Receivables	8	21,667	36,620	26,926
Inventories	9a	381	337	306
Contract assets	14a	5,669	—	—
Other	9b	10,572	11,879	10,666
Total current assets		191,763	188,602	173,551
Non-current assets				
Investments	7(b)	4,000	15,056	22,203
Inventories	9a	5,972	5,948	5,835
Infrastructure, property, plant and equipment	11	2,655,487	2,570,098	2,322,084
Investment property	12	5,000	5,000	4,780
Intangible Assets	13	254	440	388
Right of use assets	15a	1,790	—	—
Investments accounted for using the equity method	20	3,484	2,929	2,638
Total non-current assets		2,675,987	2,599,471	2,357,928
Total assets		2,867,750	2,788,073	2,531,479
LIABILITIES				
Current liabilities				
Payables	16	30,592	30,006	26,754
Income received in advance	16	—	5,014	3,256
Contract liabilities	14b	3,572	—	—
Lease liabilities	15b	341	—	—
Borrowings	16	5,260	7,934	7,716
Provisions	17	61,084	58,214	50,377
Total current liabilities		100,849	101,168	88,103
Non-current liabilities				
Payables	16	—	385	700
Lease liabilities	15b	1,519	—	—
Borrowings	16	12,439	17,497	25,039
Provisions	17	39,776	47,054	44,566
Total non-current liabilities		53,734	64,936	70,305
Total liabilities		154,583	166,104	158,408
Net assets		2,713,167	2,621,969	2,373,071
EQUITY				
Accumulated surplus	18	1,498,309	1,468,848	1,416,231
Revaluation reserves	18	1,214,858	1,153,065	956,752
Other reserves	18	—	56	88
Council equity interest		2,713,167	2,621,969	2,373,071
Total equity		2,713,167	2,621,969	2,373,071

The comparative years have been restated for prior period errors as disclosed in Note 18. Council has not restated comparatives when initially applying AASB 1058 *Income of Not-for-Profit Entities*, AASB 15 *Revenue from Contracts with Customers* and AASB 16 *Leases*. The comparative information has been prepared under AASB 111 *Construction Contracts*, AASB 118 *Revenue*, AASB 1004 *Contributions*, AASB 117 *Leases* and related Accounting Interpretations.

The above Statement of Financial Position should be read in conjunction with the accompanying notes.

Wollongong City Council

Financial Statements 2020

Statement of Changes in Equity for the year ended 30 June 2020

\$ '000	Notes	as at 30/06/20				as at 30/06/19			
		Accumulated surplus	IPP&E revaluation reserve	Investment Revaluation Reserve	Total equity	Accumulated surplus	IPP&E revaluation reserve	Investment Revaluation Reserve	Total equity
Opening balance		1,468,848	1,153,065	56	2,621,969	1,441,990	965,325	–	2,407,315
Correction of prior period errors	18b	–	–	–	–	(25,649)	(8,573)	–	(34,222)
Adoption of new accounting standards – not retrospective	18c	–	–	–	–	(110)	–	88	(22)
Changes due to AASB 1058 and AASB 15 adoption	18c-ii	(1,914)	–	–	(1,914)	–	–	–	–
Restated opening balance		1,466,934	1,153,065	56	2,620,055	1,416,231	956,752	88	2,373,071
Net operating result for the year		35,532	–	–	35,532	52,112	–	–	52,112
Other comprehensive income									
– Gain (loss) on revaluation of IPP&E	11	–	57,580	–	57,580	–	196,818	–	196,818
– Gain/(loss) on revaluation of equity instruments at fair value through other comprehensive income		–	–	–	–	–	–	(32)	(32)
Other comprehensive income		–	57,580	–	57,580	–	196,818	(32)	196,786
Total comprehensive income		35,532	57,580	–	93,112	52,112	196,818	(32)	248,898
Transfers between equity items		(4,157)	4,213	(56)	–	505	(505)	–	–
Equity – balance at end of the reporting period		1,498,309	1,214,858	–	2,713,167	1,468,848	1,153,065	56	2,621,969

Council has not restated comparatives when initially applying AASB 1058 *Income of Not-for-Profit Entities*, AASB 15 *Revenue from Contracts with Customers* and AASB 16 *Leases*. The comparative information has been prepared under AASB 111 *Construction Contracts*, AASB 118 *Revenue*, AASB 1004 *Contributions*, AASB 117 *Leases* and related Accounting Interpretations.

The above Statement of Changes in Equity should be read in conjunction with the accompanying notes.

Wollongong City Council

Financial Statements 2020

Statement of Cash Flows

for the year ended 30 June 2020

Original unaudited budget 2020	\$ '000	Notes	Actual 2020	Actual 2019
Cash flows from operating activities				
Receipts:				
204,734	Rates and annual charges		203,414	198,536
33,665	User charges and fees		29,485	35,009
4,882	Investment and interest revenue received		4,130	4,859
57,041	Grants and contributions		68,394	74,808
–	Bonds, deposits and retention amounts received		1,701	3,060
10,130	Other		24,185	25,603
Payments:				
(115,745)	Employee benefits and on-costs		(118,396)	(116,018)
(94,943)	Materials and contracts		(72,791)	(76,382)
(870)	Borrowing costs		(910)	(1,096)
–	Bonds, deposits and retention amounts refunded		(1,405)	(907)
–	Other		(35,081)	(33,844)
98,894	Net cash provided (or used in) operating activities	19b	102,726	113,628
Cash flows from investing activities				
Receipts:				
752	Sale of investment securities		69,835	61,106
1,292	Sale of infrastructure, property, plant and equipment		958	1,920
Payments:				
–	Purchase of investment securities		(41,173)	(59,267)
–	Purchase of investment property		(1,998)	–
(99,795)	Purchase of infrastructure, property, plant and equipment		(91,213)	(110,578)
–	Purchase of real estate assets		(24)	(113)
–	Purchase of intangible assets		–	(285)
(97,751)	Net cash provided (or used in) investing activities		(63,615)	(107,217)
Cash flows from financing activities				
Payments:				
(7,913)	Repayment of borrowings and advances		(7,935)	(7,715)
–	Lease liabilities (principal repayments)		(312)	–
(7,913)	Net cash flow provided (used in) financing activities		(8,247)	(7,715)
(6,770)	Net increase/(decrease) in cash and cash equivalents		30,864	(1,304)
123,788	Plus: cash and cash equivalents – beginning of year	19a	25,187	26,491
117,018	Cash and cash equivalents – end of the year	19a	56,051	25,187
13,002	plus: Investments on hand – end of year	7(b)	101,423	129,635
130,020	Total cash, cash equivalents and investments		157,474	154,822

Council has not restated comparatives when initially applying AASB 1058 *Income of Not-for-Profit Entities*, AASB 15 *Revenue from Contracts with Customers* and AASB 16 *Leases*. The comparative information has been prepared under AASB 111 *Construction Contracts*, AASB 118 *Revenue*, AASB 1004 *Contributions*, AASB 117 *Leases* and related Accounting Interpretations.

The above Statement of Cash Flows should be read in conjunction with the accompanying notes.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

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Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 1. Basis of preparation

These financial statements were authorised for issue by Council on 26 October 2020. Council has the power to amend and reissue these financial statements in cases where critical information is received from public submissions or where the OLG directs Council to amend the financial statements.

The principal accounting policies adopted in the preparation of these financial statements are set out below. These policies have been consistently applied to all the years presented, unless otherwise stated.

Basis of preparation

These general purpose financial statements have been prepared in accordance with Australian Accounting Standards and Australian Accounting Interpretations, the *Local Government Act 1993 (NSW)* and Regulations, and the Local Government Code of Accounting Practice and Financial Reporting. Council is a not for-profit entity for the purpose of preparing these financial statements. The financial statements are presented in Australian dollars and are rounded to the nearest thousand dollars.

Unless otherwise indicated, all amounts disclosed in the financial statements are actual amounts.

Specific budgetary amounts have been included for comparative analysis (to actuals) in the following reports and notes:

- Income statement
- Statement of cash flows
- Note 24 – Material budget variations

and are clearly marked.

(a) Historical cost convention

These financial statements have been prepared under the historical cost convention, as modified by the revaluation of certain financial assets and liabilities and certain classes of infrastructure, property, plant and equipment and investment property.

(b) Significant accounting estimates and judgements

The preparation of financial statements requires the use of certain critical accounting estimates. It also requires management to exercise its judgement in the process of applying the Council's accounting policies.

Estimates and judgements are continually evaluated and are based on historical experience and other factors, including expectations of future events that may have a financial impact on the Council and that are believed to be reasonable under the circumstances.

Critical accounting estimates and assumptions

Council makes estimates and assumptions concerning the future. The resulting accounting estimates will, by definition, seldom equal the related actual results. The estimates and assumptions that have a significant risk of causing a material adjustment to the carrying amounts of assets and liabilities within the next financial year include:

- (i) estimated fair values of investment properties – refer Note 12
- (ii) estimated fair values of infrastructure, property, plant and equipment – refer Note 11
- (iii) estimated tip remediation provisions – refer Note 17
- (iv) employee benefit provisions – refer Note 17.

Significant judgements in applying the council's accounting policies

- (i) Impairment of receivables

Council has made a significant judgement about the impairment of a number of its receivables – refer Note 8.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 1. Basis of preparation (continued)

Monies and other assets received by Council

(a) The Consolidated Fund

In accordance with the provisions of Section 409(1) of the Local Government Act 1993 (NSW), all money and property received by Council is held in the Council's Consolidated Fund unless it is required to be held in the Council's Trust Fund.

(b) The Trust Fund

In accordance with the provisions of Section 411 of the *Local Government Act 1993 (NSW)* (as amended), a separate and distinct Trust Fund is maintained to account for all money and property received by the council in trust which must be applied only for the purposes of, or in accordance with, the trusts relating to those monies.

Trust monies and property subject to Council's control have been included in these reports.

The following Trust monies and properties are held by Council but not considered to be under the control of Council and therefore are excluded from these financial statements:

- WCC Trust Account (deposits from sale of land/properties due to unpaid rates)
- Lord Mayor's Relief Fund
- Art Gallery (bequeaths/donations)
- Environment Foundation Gift Trust

A separate statement of monies held in the Trust Fund is available for inspection at the council office by any person free of charge.

Goods and Services Tax (GST)

Revenues, expenses and assets are recognised net of the amount of associated GST, unless the GST incurred is not recoverable from the taxation authority. In this case it is recognised as part of the cost of acquisition of the asset or as part of the expense.

Receivables and payables are stated inclusive of the amount of GST receivable or payable. The net amount of GST recoverable from, or payable to the taxation authority is included with other receivables or payables in the Statement of Financial Position.

Cash flows are presented on a gross basis. The GST components of cash flows arising from investing or financing activities that are recoverable from, or payable to, the taxation authority are presented as operating cash flows.

Volunteer services

Council is supported by a range of volunteers for services including surf lifesaving, bush care, community transport and library programs. Volunteer services are required to be recognised in the financial statements if they can be measured reliably, are material, and would be purchased if not provided by the volunteers. In most instances Council would not purchase the service if it was not provided by volunteers.

New accounting standards and interpretations issued not yet effective

New accounting standards and interpretations issued but not yet effective

Certain new accounting standards and interpretations have been published by the Australian Accounting Standards Board that are not mandatory for the 30 June 2020 reporting period. The following standards are effective for the reporting period ending 30 June 2021:

- AASB 1059 *Service Concession Arrangements: Grantors*
- AASB 2018-5 *Amendments to Australian Accounting Standards - Deferral of AASB 1059*
- AASB 2019-2 *Amendments to Australian Accounting Standards - Implementation of AASB 1059*

Council has elected not to apply any of these pronouncements in these financial statements before their operative dates.

As at the date of authorisation of these financial statements Council does not consider that any of these new (and still to be applied) standards and interpretations are likely to have a material impact on the Council's future financial statements, financial position, financial performance or cash flows.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 1. Basis of preparation (continued)

New accounting standards adopted during the year

During the year Council adopted the following accounting standards and interpretations (as issued by the Australian Accounting Standards Board) which were mandatorily effective from 1 July 2019:

- AASB 16 Leases
- AASB 15 Revenue from contracts with customers and associated amending standards.
- AASB 1058 Income of Not-for-profit entities

Further information on the newly adopted standards which had a material impact on Council's reported financial position, financial performance and/or associated financial statement disclosures can be found at Note 18.

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Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 2(a). Council functions/activities – financial information

Income, expenses and assets have been directly attributed to the following functions or activities.
Details of those functions or activities are provided in Note 2(b).

\$ '000	Income from continuing operations		Expenses from continuing operations		Operating result from continuing operations		Grants included in income from continuing operations		Carrying amount of assets	
	2020	2019	2020	2019	2020	2019	2020	2019	2020	2019
Functions or activities										
Planning and engagement	20,375	27,520	26,651	28,058	(6,276)	(538)	179	273	307,367	258,471
Environment	47,273	49,350	60,192	61,691	(12,919)	(12,341)	1,697	2,941	552,506	470,695
Transport services/infrastructure	13,001	29,491	54,593	50,189	(41,592)	(20,698)	10,179	26,623	959,827	476,782
Community services/facilities	8,226	8,155	34,179	33,661	(25,953)	(25,506)	4,803	4,732	23,509	37,374
Recreation and open space	11,951	13,954	52,905	47,796	(40,954)	(33,842)	188	367	137,192	79,719
Regulatory services and safety	7,749	8,241	20,599	19,284	(12,850)	(11,043)	830	476	5,685	1,527
Governance and internal services	197,581	194,427	42,805	45,743	154,776	148,684	17,702	16,983	881,664	1,463,505
Contributed Assets	21,300	7,396	–	–	21,300	7,396	–	–	–	–
Total functions and activities	327,456	338,534	291,924	286,422	35,532	52,112	35,578	52,395	2,867,750	2,788,073

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 2(b). Council functions/activities - component descriptions

Details relating to the Council's functions/activities as reported in Note 2(a) are as follows:

Planning and engagement

Infrastructure Planning and Support, City Centre Management, Land Use Planning, Public Relations, Economic Development, Strategy and Planning

Environment

Waste Management, Stormwater Services, Natural Area Management, Environmental Planning and Programs

Transport services/infrastructure

Transport Services and Infrastructure works

Community services/facilities

Libraries, Cultural Services, Community Facilities, Age and Disability Services, Memorial Gardens and Cemeteries, Community Programs, Youth Services

Recreation and open space

Parks and Sports fields, Aquatic Services, Tourist Parks, Leisure Centres, Botanic Gardens

Regulatory services and safety

Emergency Management, Development Assessment, Regulatory Control, Public Health

Governance and internal services

Governance and Administration, Human Resources, Financial Services, Customer Service, Property Services, Information Technology, Internal Services

Contributed Assets

Contributed assets relate mainly to the handover of transport and stormwater assets from various subdivisions in the West Dapto area.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations

\$ '000	AASB	2020	2019
(a) Rates and annual charges			
Ordinary rates			
Residential	1058 (1)	119,693	115,655
Farmland	1058 (1)	316	313
Mining	1058 (1)	938	914
Business	1058 (1)	48,263	46,927
Less: pensioner rebates (mandatory)	1058 (1)	(2,845)	(2,902)
Less: pensioner rebates (Council policy)	1058 (1)	(339)	(377)
Abandonments	1058 (1)	(392)	(42)
Rates levied to ratepayers		165,634	160,488
Pensioner rate subsidies received	1058 (1)	1,578	1,596
Total ordinary rates		167,212	162,084
Special rates			
City centre	1058 (1)	410	403
Mall	1058 (1)	1,137	1,107
Total special rates		1,547	1,510
Annual charges			
(pursuant to s.496, s.496A, s.496B, s.501 & s.611)			
Domestic waste management services	1058 (1)	34,954	34,560
Stormwater management services	1058 (1)	1,844	1,831
Less: pensioner rebates (mandatory)	1058 (1)	(766)	(797)
Less: pensioner rebates (Council policy)	1058 (1)	(86)	(97)
Abandonments – annual charges ¹	1058 (1)	(12)	(5)
Pensioner subsidies received:			
– Domestic waste management	1058 (1)	425	438
Total annual charges		36,359	35,930
TOTAL RATES AND ANNUAL CHARGES		205,118	199,524

The **AASB** notation (above) identifies the revenue recognition pattern for material items of Council revenue:

15 (1) indicates income recognised under AASB 15 “at a point in time”,

15 (2) indicates income recognised under AASB 15 “over time”,

1058 (1) indicates income recognised under AASB 1058 “at a point in time”, while

1058 (2) indicates income recognised under AASB 1058 “over time”.

Council has used 2016 year valuations provided by the NSW Valuer General in calculating its rates.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)

Accounting policy for rates and charges

Rates and annual charges are recognised as revenue when the Council obtains control over the assets comprising these receipts which is the beginning of the rating period to which they relate.

Prepaid rates are recognised as a financial liability until the beginning of the rating period.

Pensioner rebates (mandatory) relate to reductions in rates and certain annual charges for eligible pensioners' place of residence in the local government area that are subsidised by the NSW Government.

Pensioner rate subsidies are received from the NSW Government to provide a contribution towards the pensioner rebates (mandatory) and are in substance a rates payment.

2019 accounting policy

Control over assets acquired from rates and annual charges is obtained at the commencement of the rating year as it is an enforceable debt linked to the rateable property or, where earlier, upon receipt of the rates.

⁽¹⁾ Abandonments refer to amounts owed to Council that have been written off due to the property being exempted of rates, pensioner interest being waived, voluntary Council rebates, postponed rates and voluntary conservation agreements as per the OLG Rating and Revenue Raising Manual 2007.

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Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)

\$ '000	AASB	2020	2019
(b) User charges and fees			
Waste management services (non-domestic)	15 (1)	8,186	9,632
Contestable building services	15 (1)	329	386
Inspection services	15 (1)	319	509
Planning and building regulation	15 (1)	2,840	2,790
Registration fees	15 (1)	179	233
Section 10.7 certificates (EP&A Act)	15 (1)	523	517
Section 603 certificates (rating certificate)	15 (1)	294	261
Section 611 charges (occupation of land)	15 (1)	161	192
Additional waste services	15 (1)	131	91
Art gallery	15 (1)	8	11
Car parking	15 (1)	882	852
Crematorium and cemeteries	15 (1)	1,679	1,495
Credit card payment processing fee	15 (1)	128	139
Design review meeting application fees	15 (1)	134	113
Golf course	15 (1)	532	567
Health inspections	15 (1)	291	293
Hire charges (2019 only)		—	601
Leaseback fees – Council vehicles (2019 only)		—	679
Library	15 (1)	38	44
Marketing	15 (1)	29	39
Outdoor dining	15 (1)	75	92
Parking meters	15 (1)	1,134	1,172
Pre-lodgement meeting fees	15 (1)	96	113
Recreation	15 (1)	3,499	4,152
Road opening permits	15 (1)	297	363
Stallholder fees	15 (1)	64	119
Tree management requests	15 (1)	91	85
Tourist parks	15 (2)	6,675	7,135
Other	15	81	118
TOTAL USER CHARGES AND FEES		28,695	32,793

The AASB notation (above) identifies the revenue recognition pattern for material items of Council revenue:

15 (1) indicates income recognised under AASB 15 "at a point in time",

15 (2) indicates income recognised under AASB 15 "over time",

1058 (1) indicates income recognised under AASB 1058 "at a point in time", while

1058 (2) indicates income recognised under AASB 1058 "over time".

Accounting policy for user charges and fees

Revenue arising from user charges and fees is recognised when or as the performance obligation is completed and the customer receives the benefit of the goods / services being provided.

The performance obligation relates to the specific services which are provided to the customers and generally the payment terms are within 30 days of the provision of the service or in some cases the customer is required to pay on arrival. There is no material obligation for Council in relation to refunds or returns.

Licences granted by Council are all either short-term or low value and all revenue from licences is recognised at the time that the licence is granted rather than the term of the licence.

2019 accounting policy

User charges and fees are recognised as revenue when the service has been provided.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)

\$ '000	AASB	2020	2019
(c) Other revenues			
Diesel rebate	1058 (1)	162	197
Fines	1058 (1)	539	645
Fines – parking	1058 (1)	1,967	2,426
Insurance claims recoveries	1058 (1)	108	325
Legal fees recovery – rates and charges (extra charges)	1058 (1)	268	180
Legal settlements	1058 (1)	65	25
Outgoings reimbursements	1058 (1)	119	129
Reimbursements	1058 (1)	272	295
Rental income – investment property (2019 only)		–	389
Rental income – other council properties (2019 only)		–	4,676
Sales – general	15 (1)	943	2,183
Sponsorship and promotional income	1058 (1)	12	50
Other	1058 (1)	339	605
TOTAL OTHER REVENUE		4,794	12,125

The **AASB** notation (above) identifies the revenue recognition pattern for material items of Council revenue:

15 (1) indicates income recognised under AASB 15 “at a point in time”,
15 (2) indicates income recognised under AASB 15 “over time”,
1058 (1) indicates income recognised under AASB 1058 “at a point in time”, while
1058 (2) indicates income recognised under AASB 1058 “over time”.

Accounting policy for other revenue

Where the revenue relates to a contract with customer, the revenue is recognised when or as the performance obligation is completed and the customer receives the benefit of the goods / services being provided.

Where the revenue relates to a contract which is not enforceable or does not contain sufficiently specific performance obligations then revenue is recognised when an unconditional right to a receivable arises or the cash is received, which is earlier.

2019 accounting policy:

Council recognises revenue when the amount of revenue can be reliably measured, it is probable that future economic benefits will flow to the Council and specific criteria have been met for each of the Council's activities as described below. Council bases its estimates on historical results, taking into consideration the type of customer, the type of transaction and the specifics of each arrangement.

Parking fees and fines are recognised as revenue when the service has been provided, or when the penalty has been applied, whichever occurs first.

Rental income is accounted for on a straight-line basis over the lease term.

Sales - general are recognised when physical possession has transferred to the customer which is deemed to be the point of transfer of risks and rewards.

Other income is recorded when the payment is due, the value of the payment is notified, or the payment is received, whichever occurs first.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)

\$ '000	AASB	Operating 2020	Operating 2019	Capital 2020	Capital 2019
(d) Grants					
General purpose (untied)					
Current year allocation					
Financial assistance	1058 (1)	19,236	18,596	–	–
Total general purpose		19,236	18,596	–	–
Specific purpose (tied)					
Arts and culture	1058 (1)	–	79	–	–
Buildings	1058 (1)	22	–	–	–
Community development and support	1058 (1)	277	286	–	–
Economic development	1058 (1)	12	–	–	–
Emergency services	1058 (1)	727	418	–	–
Environmental management and enhancement	1058 (1)	107	2	–	–
Environmental programs	1058 (1)	99	58	–	–
Floodplain and stormwater management	1058	140	173	90	1,072
HACC community transport	1058 (1)	2,700	2,629	–	–
Heritage and cultural	1058	10	19	(15)	–
Information technology	1058	7	–	87	–
Library	1058 (1)	596	450	–	–
LIRS subsidy	1058 (1)	724	835	–	–
Local bus route subsidy	1058 (1)	99	79	–	–
Natural area management	1058 (1)	305	197	–	–
Parks, gardens and sportsfields	1058	151	–	159	545
People and learning	1058 (1)	46	74	–	–
Recreation and culture	1058	115	–	38	36
Regulatory control	1058 (1)	4	–	–	–
Social support programs ¹	1058 (1)	1,227	1,223	–	–
Strategic city planning	1058 (1)	57	95	–	–
Street lighting	1058 (1)	714	700	–	–
Transport (other roads and bridges funding)	1058 (2)	–	99	5,501	21,691
Transport (roads to recovery)	1058 (2)	–	–	1,219	1,433
Voluntary purchase scheme	1058 (2)	–	–	889	1,390
Waste performance improvement	1058 (1)	235	216	–	–
Total specific purpose		8,374	7,632	7,968	26,167
Total grants		27,610	26,228	7,968	26,167
Grant revenue is attributable to:					
– Commonwealth funding		23,093	18,679	1,064	8,266
– State funding		4,517	7,549	6,050	17,901
– Other funding		–	–	854	–
		27,610	26,228	7,968	26,167

The AASB notation (above) identifies the revenue recognition pattern for material items of Council revenue:

- 15 (1) indicates income recognised under AASB 15 “at a point in time”,
- 15 (2) indicates income recognised under AASB 15 “over time”,
- 1058 (1) indicates income recognised under AASB 1058 “at a point in time”, while
- 1058 (2) indicates income recognised under AASB 1058 “over time”.

(1) The description on this item has been updated from Wollongong multi service outlet to more accurately reflect the purpose of this funding.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)

\$ '000	Notes	AASB	Operating 2020	Operating 2019	Capital 2020	Capital 2019
(e) Contributions						
Developer contributions: (s7.4 & s7.11 - EP&A Act, s64 of the LGA):						
Cash contributions						
S 7.11 – contributions towards amenities/services	1058 (1)	–	–	–	13,305	17,848
S 7.12 – fixed development consent levies	1058 (1)	–	–	–	4,806	5,805
Total cash contributions			–	–	18,111	23,653
Non-cash contributions						
Planning agreements	1058 (1)	–	–	–	1,272	–
Total non-cash contributions			–	–	1,272	–
Total developer contributions	28		–	–	19,383	23,653
Other contributions:						
Cash contributions						
Buildings	1058 (2)	–	–	–	32	1,003
Community development and support	1058 (1)	76	89	–	–	–
Community services and facilities	1058 (1)	418	561	–	–	–
Floodplain and stormwater management	1058 (1)	(70)	299	–	–	–
Heritage/cultural		–	–	–	–	5
Parks, gardens and sportsfields	1058	10	17	205	1,306	–
People and learning	1058 (1)	160	74	–	–	–
Pollution minimisation	1058 (1)	288	118	–	–	–
RMS contributions (regional roads, block grant)	1058 (1)	1,600	1,431	–	–	–
Roads and bridges	1058 (1)	36	39	–	125	–
Total cash contributions			2,518	2,628	237	2,439
Non-cash contributions						
Bushfire services	1058 (1)	–	–	–	122	748
Dedications – subdivisions (other than by s7.11)	1058 (1)	–	–	–	19,566	6,403
Wollongong City Gallery collection	1058 (1)	–	–	–	196	246
Volunteer Services ¹	1058 (2)	599	–	–	–	–
Total non-cash contributions			599	–	19,884	7,397
Total other contributions			3,117	2,628	20,121	9,836
Total contributions			3,117	2,628	39,504	33,489
TOTAL GRANTS AND CONTRIBUTIONS			30,727	28,856	47,472	59,656

The AASB notation (above) identifies the revenue recognition pattern for material items of Council revenue:

15 (1) indicates income recognised under AASB 15 “at a point in time”,

15 (2) indicates income recognised under AASB 15 “over time”,

1058 (1) indicates income recognised under AASB 1058 “at a point in time”, while

1058 (2) indicates income recognised under AASB 1058 “over time”.

Wollongong City Council**Financial Statements 2020****Notes to the Financial Statements**

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)**Accounting policy for grants and contributions****Accounting policy from 1 July 2019****Grant income under AASB 15**

Where grant income arises from an agreement which is enforceable and contains sufficiently specific performance obligations then the revenue are recognised when control of each performance obligations is satisfied.

The performance obligations are varied based on the agreement. Payment terms vary depending on the terms of the grant, cash is received upfront for some grants and on the achievement of certain payment milestones for others.

Each performance obligation is considered to ensure that the revenue recognition reflects the transfer of control and within grant agreements there may be some performance obligations where control transfers at a point in time and others which have continuous transfer of control over the life of the contract.

Where control is transferred over time, generally the input methods being either costs or time incurred are deemed to be the most appropriate methods to reflect the transfer of benefit.

Grant income

Assets arising from grants in the scope of AASB 1058 is recognised at the assets fair value when the asset is received. Councils considers whether there are any related liability or equity items associated with the asset which are recognised in accordance with the relevant accounting standard.

Once the assets and liabilities have been recognised then income is recognised for any remaining asset value at the time that the asset is received

Capital grants

Capital grants received to enable Council to acquire or construct an item of infrastructure, property, plant and equipment to identified specifications which will be under Council's control and which is enforceable are recognised as revenue as and when the obligation to construct or purchase is completed.

For construction projects, this is generally as the construction progresses in accordance with costs incurred since this is deemed to be the most appropriate measure of the completeness of the construction project as there is no profit margin.

For acquisitions of assets, the revenue is recognised when the asset is acquired and controlled by the Council.

Contributions

Council has obligations to provide facilities from contribution revenues levied on developers under the provisions of sections 7.4, 7.11 and 7.12 of the *Environmental Planning and Assessment Act 1979*.

While Council generally incorporates these amounts as part of a Development Consents Order, such developer contributions are only recognised as income upon receipt by Council, due to the possibility that individual development consents may not be acted upon by the applicant and, accordingly, would not be payable to Council.

Developer contributions may only be expended for the purposes for which the contributions were required, but the Council may apply contributions according to the priorities established in work schedules

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)

Accounting policy prior to 1 July 2019

Control over grants and contributions is normally obtained upon their receipt (or acquittal) and revenue is recognised at this time and is valued at the fair value of the granted or contributed asset at the date of transfer.

Where grants or contributions recognised as revenues during the financial year were obtained on condition that they be expended in a particular manner, or used over a particular period, and those conditions were un-discharged at reporting date, the unused grant or contribution is disclosed below.

A liability is recognised in respect of revenue that is reciprocal in nature to the extent that the requisite service has not been provided at reporting date.

(1) In prior financial years, Council has not recognised the support provided through volunteers as they would not be purchased. During 2019-2020, events relating to the COVID-19 pandemic have required Council to reassess some of the support provided to them by volunteers.

\$ '000	2020	2019
(f) Unspent grants and contributions – external restrictions		
Certain grants and contributions are obtained by Council on condition that they be spent in a specified manner due to externally imposed restrictions.		
Operating grants		
Unexpended at the close of the previous reporting period	3,294	2,637
Reclassification between capital and operating grants	(508)	–
Add: operating grants recognised as income in the current period but not yet spent (2019 only)	–	1,232
Add: operating grants received for the provision of goods and services in a future period	864	–
Less: operating grants recognised in a previous reporting period now spent (2019 only)	–	(575)
Less: operating grants received in a previous reporting period now spent and recognised as income	(528)	–
Unexpended and held as externally restricted assets (operating grants)	3,122	3,294
Capital grants		
Unexpended at the close of the previous reporting period	60	78
Reclassification between capital and operating grants	508	–
Add: capital grants recognised as income in the current period but not yet spent (2019 only)	–	8
Add: capital grants received for the provision of goods and services in a future period	173	–
Less: capital grants recognised in a previous reporting period now spent (2019 only)	–	(26)
Less: capital grants received in a previous reporting period now spent and recognised as income	(295)	–
Unexpended and held as externally restricted assets (capital grants)	446	60

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 3. Revenue from continuing operations (continued)

\$ '000	2020	2019
Contributions		
Unexpended at the close of the previous reporting period	36,814	18,990
Add: contributions recognised as income in the current period but not yet spent	–	18,180
Add: contributions received for the provision of goods and services in a future period	8,844	–
Add: contributions recognised as income in the current period obtained in respect of a future rating identified by Council for the purpose of establishing a rate (2019 only)	–	–
Less: contributions recognised in a previous reporting period now spent	(10,209)	(356)
Unexpended and held as externally restricted assets (contributions)	35,449	36,814

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Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 4. Interest and investment income

\$ '000	2020	2019
Interest on financial assets measured at amortised cost		
– Overdue rates and annual charges (incl. special purpose rates)	379	606
– Cash and investments	3,122	4,227
Dividend income relating to investments held at fair value through other comprehensive income	139	95
Fair value adjustments		
– Movements in investments at fair value through profit and loss	(280)	141
Finance income on the net investment in the lease	–	–
Total Interest and investment income	3,360	5,069

Accounting policy for interest and investment revenue

Interest income is recognised using the effective interest rate at the date that interest is earned.

Dividends are recognised as income in profit or loss unless the dividend clearly represents a recovery of part of the cost of the investment.

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Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 5. Expenses from continuing operations

\$ '000	2020	2019
(a) Employee benefits and on-costs		
Employee leave entitlements (ELE) ¹	17,540	20,101
Salaries and wages ²	104,019	99,408
Superannuation	12,186	11,869
Change in workers compensation provision	534	2,209
Workers compensation – self insurance	1,876	1,791
Fringe benefit tax (FBT)	175	145
Payroll tax	25	33
Training costs (other than salaries and wages)	586	859
Protective clothing	426	349
Labour hire	273	475
Other	247	364
Total employee costs	137,887	137,603
Less: capitalised costs	(14,975)	(15,158)
TOTAL EMPLOYEE COSTS EXPENSED	122,912	122,445

Accounting policy for employee benefits and on-costs

Employee benefit expenses are recorded when the service has been provided by the employee.

Retirement benefit obligations

All employees of the Council are entitled to benefits on retirement, disability or death. Council contributes to various defined benefit plans and defined contribution plans on behalf of its employees.

Superannuation plans

Contributions to defined contribution plans are recognised as an expense as they become payable. Prepaid contributions are recognised as an asset to the extent that a cash refund or a reduction in the future payments is available.

Council participates in a Defined Benefit Plan under the Local Government Superannuation Scheme, however, sufficient information to account for the plan as a defined benefit is not available and therefore Council accounts for its obligations to defined benefit plans on the same basis as its obligations to defined contribution plans, i.e. as an expense when it becomes payable – refer to Note 22 for more information.

(1) Employee leave entitlements reflect the current value of the future payments which are discounted based on published long term government bond rates. The average discount factor reduced from 2.6% in June 2018 to 1.2% in June 2019 resulting in a larger than anticipated movement in the provision during 2018-2019. This trend has continued into 2019-2020 as the average discount rate reduced to 0.6% in June 2020.

(2) The salary and wages increase in 2019/20 compared to 2018/19 is due to indexation of salaries in accordance with the enterprise agreement, a number of fixed duration projects that required additional specialised labour resources and a reduction in employee leave takings attributed to COVID-19 impacts. Leave takings are charged to the balance sheet, and a reduction in these will reflect as an increase in current year expenses.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 5. Expenses from continuing operations (continued)

\$ '000	Notes	2020	2019
(b) Borrowing costs			
(i) Interest bearing liability costs			
Interest on leases		65	–
Interest on loans		845	1,096
Total interest bearing liability costs		910	1,096
Total interest bearing liability costs expensed		910	1,096
(ii) Other borrowing costs			
Fair value adjustments on recognition of advances and deferred debtors			
– Remediation liabilities	17	1,939	1,850
Amortisation of discount on interest free (& favourable) loans to council		203	391
Total other borrowing costs		2,142	2,241
TOTAL BORROWING COSTS EXPENSED		3,052	3,337

Accounting policy for borrowing costs

Borrowing costs incurred for the construction of any qualifying asset are capitalised during the period of time that is required to complete and prepare the asset for its intended use or sale. Other borrowing costs are expensed as incurred.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 5. Expenses from continuing operations (continued)

\$ '000	2020	2019
(c) Materials and contracts		
Raw materials and consumables	27,560	17,208
Contractor and consultancy costs	67,922	94,996
– Waste Contractor	18,407	17,546
– Road Works Contractor	2,699	3,468
– Building Contractor	2,129	2,360
– Parks Contractor	2,315	2,065
– Cleaning Contractor	1,154	960
– Security Contractor	1,085	1,120
– Software Maintenance and Support Contractor	2,953	4,229
– Internal audit	76	144
Auditors remuneration ¹	190	162
Legal expenses:		
– Planning and development	112	–
– Other	831	866
Operating leases expense (2019 only):		
– Minimum lease payments	–	327
Total materials and contracts	127,433	145,451
Less: capitalised costs	(66,499)	(88,019)
TOTAL MATERIALS AND CONTRACTS	60,934	57,432

Accounting policy for materials and contracts

Expenses are recorded on an accruals basis as the council receives the goods or services.

Operating leases (2019 only)

Leases in which a significant portion of the risks and rewards of ownership are not transferred to Council as lessee are classified as operating leases. Payments made under operating leases (net of any incentives received from the lessor) are charged to the income statement on a straight-line basis over the period of the lease.

1. Auditor remuneration

During the year, the following fees were incurred for services provided by the auditor of Council, related practices and non-related audit firms

Auditors of the Council - NSW Auditor-General:

(i) Audit and other assurance services

Audit and review of financial statements	150	154
Remuneration for audit and other assurance services	150	154
Total Auditor-General remuneration	150	154

Non NSW Auditor-General audit firms

(i) Audit and other assurance services

Other audit and assurance services	40	8
Remuneration for audit and other assurance services	40	8
Total remuneration of non NSW Auditor-General audit firms	40	8
Total Auditor remuneration	190	162

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Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 5. Expenses from continuing operations (continued)

\$ '000	Notes	2020	2019
(d) Depreciation, amortisation and impairment of non-financial assets			
Depreciation and amortisation			
Plant and equipment		4,586	4,334
Office equipment		1,057	988
Furniture and fittings		298	269
Infrastructure:	11		
– Bridges		1,951	1,941
– Buildings – non-specialised ¹		3,757	5,186
– Buildings – specialised ¹		5,507	6,535
– Footpaths		4,172	4,021
– Other open space/recreational assets		3,690	3,668
– Other structures		584	582
– Roads		20,289	20,460
– Stormwater drainage		10,562	10,246
– Swimming pools		499	475
Right of use assets	15	382	–
Other assets:			
– Library books		1,164	1,162
– Other		2,043	1,541
Reinstatement, rehabilitation and restoration assets:			
– Tip assets	17,11	118	141
Intangible assets	13	186	232
Total gross depreciation and amortisation costs		60,845	61,781
Total depreciation and amortisation costs		60,845	61,781
TOTAL DEPRECIATION, AMORTISATION AND IMPAIRMENT FOR NON-FINANCIAL ASSETS		60,845	61,781

Accounting policy for depreciation, amortisation and impairment expenses of non-financial assets

Depreciation and amortisation

Depreciation and amortisation are calculated using the straight line method to allocate their cost, net of their residual values, over their estimated useful lives. Useful lives are included in Note 11 for IPPE assets, Note 13 for intangible assets and Note 15 for right-of-use assets.

Impairment of non-financial assets

Council assets held at fair value that are not held primarily for their ability to generate net cash flow, and that are deemed to be specialised, are no longer required to be tested for impairment under AASB 136. This is because these assets are assessed on an annual basis to ensure that the carrying amount is not materially different from fair value and therefore an impairment loss would be captured during this assessment.

Intangible assets that have an indefinite useful life, or are not yet available for use, are tested annually for impairment, or more frequently if events or changes in circumstances indicate that they might be impaired. Other assets that do not meet the criteria above are tested for impairment whenever events or changes in circumstances indicate that the carrying amount may not be recoverable. An impairment loss is recognised for the amount by which the asset's carrying amount exceeds its recoverable amount. The recoverable amount is the higher of an asset's fair value less costs to sell and value in use.

For the purposes of assessing impairment, assets are grouped at the lowest levels for which there are separately identifiable cash inflows that are largely independent of the cash inflows from other assets or groups of assets (cash-generating units). Non-financial assets that suffered an impairment are reviewed for possible reversal of the impairment at each reporting date.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 5. Expenses from continuing operations (continued)

Impairment losses for revalued assets are firstly offset against the amount in the revaluation surplus for the class of asset, with only the excess to be recognised in the Income Statement.

(1) The buildings asset classes were revalued as at 30 June 2019, causing a reduction in the depreciation expense resulting from the extension of the asset useful lives.

\$ '000	2020	2019
(e) Other expenses		
Advertising	279	286
Bad and doubtful debts	471	138
Bank charges	646	612
Contributions/levies to other levels of government		
– Emergency services levy (includes FRNSW, SES, and RFS levies)	336	287
– NSW fire brigade levy	3,053	2,769
– NSW rural fire service levy	603	480
– Waste and environment levy	10,983	11,890
Councillor expenses – mayoral fee	100	97
Councillor expenses – councillors' fees	406	401
Councillors' expenses (incl. mayor) – other (excluding fees above)	54	128
Donations, contributions and assistance to other organisations (Section 356)		
– City Centre management	20	27
– Illawarra Institute of Sport	37	35
– Illawarra Joint Project Organisation	55	61
– Illawarra Performing Arts Centre	718	999
– Illawarra Regional Information Service	88	88
– Illawarra Surf Life Saving	56	55
– Neighbourhood youth program	–	115
– Sponsorship fund	61	53
– Tourism	1,436	1,410
– Wollongong Shuttle Service	350	350
– Other	861	690
Insurance	2,885	2,737
Light, electricity and heating	2,253	1,987
Membership fees	167	213
Prior year works in progress 'write offs' ¹	8,048	–
Postage	527	446
Provision for asset remediation ²	(5,558)	–
Provision for self insurance claims	(4)	(26)
Rental agreements	4	57
Royalty payments	315	343
Sewerage charges	264	185
Street lighting	3,345	3,500
Telephone and communications	856	627
Valuation fees	408	447
Volunteer reimbursements	232	280
Water rates	1,225	735
Other	1,107	2,689
Total other expenses	36,687	35,191
Less: capitalised costs	(190)	(620)
TOTAL OTHER EXPENSES	36,497	34,571

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Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 5. Expenses from continuing operations (continued)

Accounting policy for other expenses

Other expenses are recorded on an accruals basis as the Council receives the goods or services.

- (1) During 2019-2020, capital expenditure previously included in Works in Progress was transferred to operating expenses. This includes capital works to deliver assets not under the control of Council such as traffic facilities and street lighting.
- (2) Each financial year a provision for the remediation of Council's waste facility is calculated based on the forecast costs to rehabilitate the site. The movements in the provision are generally recognised against the value of the corresponding tip asset (refer to Note 11). During 2019-2020, a reduction in the forecast rehabilitation costs caused a reduction in the provision beyond the carrying value of the asset. The balance of this adjustment and all future adjustments to the provision will be recognised through other expenses.

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Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 6. Gain or loss from disposal of assets

\$ '000	Notes	2020	2019
Property (excl. investment property)			
Proceeds from disposal – property		12	30
Less: carrying amount of property assets sold/written off		(26)	(10)
Net gain/(loss) on disposal		(14)	20
Plant and equipment			
	11		
Proceeds from disposal – plant and equipment		946	1,890
Less: carrying amount of plant and equipment assets sold/written off		(1,029)	(870)
Net gain/(loss) on disposal		(83)	1,020
Infrastructure			
	11		
Proceeds from disposal – infrastructure		–	–
Less: carrying amount of infrastructure assets sold/written off ¹		(6,319)	(7,895)
Net gain/(loss) on disposal		(6,319)	(7,895)
Investments			
	7(b)		
Proceeds from disposal/redemptions/maturities – investments ²		69,835	61,106
Less: carrying amount of investments sold/redeemed/matured		(69,105)	(61,106)
Net gain/(loss) on disposal		730	–
Intangible assets			
	13		
Proceeds from disposal – intangible assets		–	–
Less: carrying amount of intangible assets sold/written off		–	(1)
Net gain/(loss) on disposal		–	(1)
NET GAIN/(LOSS) ON DISPOSAL OF ASSETS		(5,686)	(6,856)

Accounting policy for disposal of assets

Gains and losses on disposals are determined by comparing proceeds with carrying amount. These are included in the Income Statement.

The gain or loss on sale of an asset is determined when control of the asset has irrevocably passed to the buyer and the asset is de-recognised.

(1) The increased expense is the result of replacing and disposing of asset that have not been fully depreciated and have a book value at the time of disposal. During 2019-2020, the disposal of the Whytes Gully MRF Warehouse and replacement of stormwater assets has had a significant impact on the losses recognised.

(2) During 2019-2020, Council sold their shares in Southern Phones resulting in a gain on the disposal of investments.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 7(a). Cash and cash equivalents

\$ '000	2020	2019
Cash and cash equivalents ¹		
Cash on hand and at bank	2,005	1,968
Cash-equivalent assets		
– Deposits at call	13,932	10,218
– Managed funds	40,114	10,001
– Short-term deposits	–	3,000
Total cash and cash equivalents	56,051	25,187

Accounting policy for cash and cash equivalents

For Statement of Cash Flow presentation purposes, cash and cash equivalents include: cash on hand; deposits held at call with financial institutions; other short-term, highly liquid investments with original maturities of three months or less that are readily convertible to known amounts of cash and which are subject to an insignificant risk of changes in value; and bank overdrafts. Bank overdrafts are shown within borrowings in current liabilities on the Statement of Financial Position.

(1) In response to the COVID-19 pandemic to ensure Council had adequate funds to meet cash flow requirements, Council did not enter into any new investments in the last quarter of the financial year. This has resulted in a higher level of cash and cash equivalents in comparison to prior financial years.

Note 7(b). Investments

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
Investments				
a. 'Financial assets at fair value through profit and loss'				
– 'Designated at fair value on initial recognition'	46,393	–	47,549	–
b. 'Financial assets at amortised cost'	51,030	4,000	67,030	15,000
d. 'Financial assets at fair value through other comprehensive income' ¹	–	–	–	56
Total Investments	97,423	4,000	114,579	15,056
TOTAL CASH ASSETS, CASH EQUIVALENTS AND INVESTMENTS	153,474	4,000	139,766	15,056
Financial assets at fair value through the profit and loss				
Managed funds	2,307	–	2,304	–
NCD's, FRN's (with maturities > 3 months)	42,464	–	43,582	–
Mortgage backed securities	1,622	–	1,663	–
Total	46,393	–	47,549	–
Financial assets at amortised cost				
Long term deposits	51,030	4,000	67,030	15,000
Total	51,030	4,000	67,030	15,000
Financial assets at fair value through other comprehensive income				
Unlisted equity securities	–	–	–	56
Total	–	–	–	56

(1) This investment was sold during 2019-2020.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 7(b). Investments (continued)

Accounting policy for investments

Financial instruments are recognised initially on the date that the Council becomes party to the contractual provisions of the instrument.

On initial recognition, all financial instruments are measured at fair value plus transaction costs (except for instruments measured at fair value through profit or loss where transaction costs are expensed as incurred).

Financial assets

All recognised financial assets are subsequently measured in their entirety at either amortised cost or fair value, depending on the classification of the financial assets.

Classification

On initial recognition, Council classifies its financial assets into the following categories – those measured at:

- amortised cost
- fair value through profit and loss (FVTPL)
- fair value through other comprehensive income – equity instrument (FVOCI-equity)

Financial assets are not reclassified subsequent to their initial recognition.

Amortised cost

Assets measured at amortised cost are financial assets where:

- the business model is to hold assets to collect contractual cash flows, and
- the contractual terms give rise on specified dates to cash flows that are solely payments of principal and interest on the principal amount outstanding.

Council's financial assets measured at amortised cost comprise trade and other receivables, term deposits and cash and cash equivalents in the Statement of Financial Position.

Subsequent to initial recognition, these assets are carried at amortised cost using the effective interest rate method less provision for impairment.

Interest income, impairment and gains or loss on de-recognition are recognised in profit or loss.

Fair value through other comprehensive income – equity instruments

Council had strategic investment in an entity over which they did not have significant influence nor control. Council made an irrevocable election to classify these equity investments as fair value through other comprehensive income as they were not held for trading purposes.

These investments were carried at fair value with changes in fair value recognised in other comprehensive income (financial asset reserve). This investment was sold during the 2019-2020 financial year. On disposal the balance in the financial asset reserve was transferred to accumulated surplus and not reclassified to profit and loss.

Other net gains and losses excluding dividends and profit on sale are recognised in OCI.

Financial assets through profit or loss

All financial assets not classified as measured at amortised cost or fair value through other comprehensive income as described above are measured at fair value through profit or loss.

Net gains or losses, including any interest or dividend income, are recognised in profit or loss.

Council's financial assets measured at fair value through profit or loss comprise investments in FRNs in the Statement of Financial Position.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 7(c). Restricted cash, cash equivalents and investments

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
Total cash, cash equivalents and investments	153,474	4,000	139,766	15,056
attributable to:				
External restrictions	71,470	4,000	62,601	15,056
Internal restrictions	62,675	–	61,203	–
Unrestricted	19,329	–	15,962	–
	153,474	4,000	139,766	15,056

\$ '000	2020	2019
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Details of restrictions

External restrictions – included in liabilities

Specific purpose unexpended grants – general fund (2020 only)	399	–
RMS contributions	350	–
Private contributions	339	–

External restrictions – included in liabilities	1,088	–
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External restrictions – other

Developer contributions – general ^A	35,099	36,768
RMS contributions ^B	–	46
Specific purpose unexpended grants (recognised as revenue) – general fund ^C	3,169	3,354
Stormwater management ^D	1,860	1,668
Unexpended loan ^E	2,686	3,470
Private contributions	5,828	5,761
Special rates levy – Wollongong mall	214	135
Domestic waste management ^D	14,216	13,763
Special rates levy – city centre	49	40
Local infrastructure renewal scheme round 3 ^F	274	1,867
West Dapto home deposit assistance program	10,987	10,785

External restrictions – other	74,382	77,657
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Total external restrictions	75,470	77,657
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Internal restrictions

Car parking strategy	2,195	1,629
Darcy Wentworth Park	171	171
Maccabe Park development	1,440	1,290
Sports Priority Program	938	644
Waste Disposal Facility	561	(638)
West Dapto Development	6,067	5,076
Strategic Projects	42,900	44,015
Property Investment Fund	7,889	8,416
Southern Phone natural areas	173	267
Lake Illawarra Estuary Management Fund	341	333

Total internal restrictions	62,675	61,203
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TOTAL RESTRICTIONS	138,145	138,860
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(A) Development contributions which are not yet expended for the provision of services and amenities in accordance with contributions plans.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 7(c). Restricted cash, cash equivalents and investments (continued)

- (B) RMS contributions which are not yet expended for the provision of services and amenities in accordance with those contributions.
- (C) Grants which are not yet expended for the purposes for which the grants were obtained.
- (D) Domestic Waste Management and other special rates/levies/charges are externally restricted assets and must be applied for the purposes for which they were raised.
- (E) State Government interest free loan to be administered on infrastructure as part of the West Dapto development.
- (F) State Government subsidised loans to be administered on infrastructure projects over the Local Government Area.

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Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 8. Receivables

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
Purpose				
Rates and annual charges	13,104	–	9,023	–
Interest and extra charges	1,477	–	1,543	–
User charges and fees	3,656	–	5,942	–
Accrued revenues				
– Interest on investments	447	–	871	–
– Other income accruals	1,162	–	1,744	–
Government grants and subsidies (2019 only)	–	–	16,517	–
Net GST receivable	2,652	–	1,457	–
Total	22,498	–	37,097	–
Less: provision of impairment				
User charges and fees ¹	(831)	–	(477)	–
Total provision for impairment – receivables	(831)	–	(477)	–
TOTAL NET RECEIVABLES	21,667	–	36,620	–

(1) Council has adopted a policy in line with the National Code of Conduct for leasing arrangements during COVID-19. The provision has been increased to allow for potential rental waivers as a result of this policy position.

Restrictions applicable to receivables

There are no restrictions applicable to the above assets.

\$ '000	2020	2019
Movement in provision for impairment of receivables		
Amount restated through opening retained earnings on adoption of AASB 9	–	110
Balance at the beginning of the year (calculated in accordance with AASB 139)	477	229
+ new provisions recognised during the year	489	170
– amounts already provided for and written off this year	(117)	–
– previous impairment losses reversed	(18)	(32)
Balance at the end of the year	831	477

Accounting policy for receivables

Recognition and measurement

Receivables are included in current assets, except for those with maturities greater than 12 months after the reporting date which are classified as non-current assets.

Receivables are recognised initially at fair value and subsequently measured at amortised cost using the effective interest method, less provision for impairment. Receivables are generally due for settlement within 30 days.

Cash flows relating to short-term receivables are not discounted if the effect of discounting is immaterial.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 8. Receivables (continued)

Impairment

Impairment of financial assets measured at amortised cost is recognised on an expected credit loss (ECL) basis.

When determining whether the credit risk of a financial asset has increased significantly since initial recognition, and when estimating ECL, Council considers reasonable and supportable information that is relevant and available without undue cost or effort. This includes both quantitative and qualitative information and analysis based on Council's historical experience and informed credit assessment, and including forward-looking information.

When considering the ECL for rates debtors, Council takes into account that unpaid rates represent a charge against the rateable property that will be recovered when the property is next sold. For non-rates debtors, Council uses the presumption that an asset which is more than 30 days past due has seen a significant increase in credit risk.

Council uses the presentation that a financial asset is in default when:

- the other party is unlikely to pay its credit obligations to the Council in full, without recourse by Council to actions such as realising security (if any is held)

Credit losses are measured as the present value of the difference between the cash flows due to the entity in accordance with the contract, and the cash flows expected to be received. This is applied using a probability weighted approach.

On initial recognition of the asset, an estimate of the expected credit losses for the next 12 months is recognised. Where the asset has experienced significant increase in credit risk then the lifetime losses are estimated and recognised.

Council uses the simplified approach for trade receivables where the expected lifetime credit losses are recognised on day 1.

There has been no change in the estimation techniques or significant assumptions made during the current reporting period.

Council writes off a trade receivable when there is information indicating that the debtor is in severe financial difficulty and there is no realistic prospect of recovery.

None of the receivables that have been written off are subject to enforcement activity.

Where Council renegotiates the terms of receivables due from certain customers, the new expected cash flows are discounted at the original effective interest rate and any resulting difference to the carrying value is recognised in profit or loss.

Rates and annual charges outstanding are secured against the property.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 9. Inventories and other assets

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
(a) Inventories				
(i) Inventories at cost				
Real estate for resale	–	5,972	–	5,948
Stores and materials	381	–	337	–
Total inventories at cost	381	5,972	337	5,948
TOTAL INVENTORIES	381	5,972	337	5,948

(b) Other assets

Prepayments	10,572	–	11,879	–
TOTAL OTHER ASSETS	10,572	–	11,879	–

(i) Other disclosures

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
(a) Details for real estate development				
Residential	–	5,972	–	5,948
Total real estate for resale	–	5,972	–	5,948
(Valued at the lower of cost and net realisable value)				
Represented by:				
Acquisition costs	–	5,948	–	5,835
Development costs	–	24	–	113
Total costs	–	5,972	–	5,948
Total real estate for resale	–	5,972	–	5,948
Movements:				
Real estate assets at beginning of the year	–	5,948	–	5,835
– Purchases and other costs	–	24	–	113
Total real estate for resale	–	5,972	–	5,948

Accounting policy for inventories and other assets

Raw materials and stores, work in progress and finished goods

Raw materials and stores, work in progress and finished goods are stated at the lower of cost and net realisable value. Costs are assigned to individual items of inventory on the basis of weighted average costs. Costs of purchased inventory are determined after deducting rebates and discounts. Net realisable value is the estimated selling price in the ordinary course of business less the estimated costs of completion and the estimated costs necessary to make the sale.

Land held for resale/capitalisation of borrowing costs

Land held for resale is stated at the lower of cost and net realisable value. Cost is assigned by specific identification and includes the cost of acquisition, and development and borrowing costs during development. When development is completed, borrowing costs and other holding charges are expensed as incurred.

Wollongong City Council

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Notes to the Financial Statements for the year ended 30 June 2020

Note 9. Inventories and other assets (continued)

Borrowing costs included in the cost of land held for resale are those costs that would have been avoided if the expenditure on the acquisition and development of the land had not been made. Borrowing costs incurred while active development is interrupted for extended periods are recognised as expenses.

Note 10. Non-current assets classified as held for sale

Council did not classify any non-current assets as held for sale during the 2019/20 financial year.

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Wollongong City Council

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Notes to the Financial Statements for the year ended 30 June 2020

Note 11. Infrastructure, property, plant and equipment

	as at 30/06/19			Asset movements during the reporting period											as at 30/06/20		
	Gross carrying amount	Accumulated depreciation	Net carrying amount	Additions renewals	Additions new assets	Carrying value of disposals	Depreciation expense	WIP transfers	Adjustments and transfers	Other movements - Transfer to Expense	Other movements - Waste Remediation Reassessment	Revaluation decrements to equity (ARR)	Revaluation increments to equity (ARR)	Gross carrying amount	Accumulated depreciation	Net carrying amount	
Capital work in progress	119,950	—	119,950	51,948	29,146	—	—	(138,979)	—	(8,048)	—	—	—	54,017	—	54,017	
Plant and equipment	42,040	(19,349)	22,691	4,327	—	(1,029)	(4,586)	—	—	—	—	—	—	42,203	(20,800)	21,403	
Office equipment	8,118	(4,742)	3,376	934	—	—	(1,057)	29	—	—	—	—	—	7,497	(4,215)	3,282	
Furniture and fittings	2,904	(807)	2,097	132	—	—	(298)	—	—	—	—	—	—	3,035	(1,104)	1,931	
Land:																	
— Operational land	249,843	—	249,843	—	4	(26)	—	—	—	—	—	—	—	249,821	—	249,821	
— Community land	229,910	(760)	229,150	—	1,923	—	—	5	—	—	—	—	63,189	295,027	(760)	294,267	
— Crown land	108,721	—	108,721	—	—	—	—	—	—	—	—	(5,664)	—	103,057	—	103,057	
— Land under roads (post 30/6/08)	5,684	—	5,684	—	—	—	—	286	—	—	—	—	—	5,970	—	5,970	
Infrastructure:																	
— Buildings – non-specialised	213,464	(73,592)	139,872	—	—	(1,402)	(3,757)	5,575	(5,051)	—	—	—	—	210,898	(75,661)	135,237	
— Buildings – specialised	326,555	(163,263)	163,292	—	—	(503)	(5,507)	3,497	5,019	—	—	—	—	332,125	(166,327)	165,798	
— Other structures	13,971	(6,489)	7,482	—	—	(37)	(584)	2,324	61	—	—	—	—	16,204	(6,958)	9,246	
— Roads	1,426,090	(856,091)	569,999	—	9,657	(2,022)	(20,289)	52,913	—	—	—	—	—	1,479,575	(869,317)	610,258	
— Bridges	152,318	(53,579)	98,739	—	—	(18)	(1,951)	22,052	—	—	—	—	—	174,322	(55,500)	118,822	
— Footpaths	291,660	(144,470)	147,190	—	3,343	(633)	(4,172)	12,507	—	—	—	—	—	305,485	(147,250)	158,235	
— Stormwater drainage	947,790	(422,151)	525,639	—	7,838	(1,364)	(10,562)	8,542	—	—	—	—	55	961,129	(430,981)	530,148	
— Swimming pools	29,738	(23,407)	6,331	—	—	(104)	(499)	3,635	—	—	—	—	—	32,093	(22,730)	9,363	
— Other open space/recreational assets	158,109	(47,686)	110,423	137	—	(236)	(3,690)	7,714	(528)	—	—	—	—	163,782	(49,962)	113,820	
Other assets:																	
— Heritage collections	15,121	—	15,121	—	294	—	—	—	—	—	—	—	—	15,415	—	15,415	
— Library books	8,162	(3,666)	4,496	1,230	—	—	(1,164)	—	—	—	—	—	—	8,159	(3,597)	4,562	
— Other	49,066	(14,589)	34,477	—	—	—	(2,043)	17,902	499	—	—	—	—	67,702	(16,867)	50,835	
Reinstatement, rehabilitation and restoration assets (refer Note 17):																	
— Tip assets	14,817	(9,292)	5,525	—	—	—	(118)	—	—	—	(5,407)	—	—	9,410	(9,410)	—	
Investment Property (refer to Note 12):																	
Investment Property	—	—	—	—	—	—	—	1,998	—	—	—	—	—	—	—	—	
Total Infrastructure, property, plant and equipment	4,414,031	(1,843,933)	2,570,098	58,708	52,205	(7,374)	(60,277)	—	—	(8,048)	(5,407)	(5,664)	63,244	4,536,926	(1,881,439)	2,655,487	

(1) Renewals are defined as the replacement of existing assets (as opposed to the acquisition of new assets).

Wollongong City Council

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Notes to the Financial Statements for the year ended 30 June 2020

Note 11. Infrastructure, property, plant and equipment (continued)

	as at 30/06/18			Asset movements during the reporting period											as at 30/06/19		
	Gross carrying amount	Accumulated depreciation	Net carrying amount	Restatement of Net Carrying Amount at 1/7/2018	Additions renewals	Additions new assets	Carrying value of disposals	Depreciation expense	WIP transfers	Adjustments and transfers	Waste Remediation reclassification	Transfer to expense	Revaluation decrements to equity (A/R)	Revaluation increments to equity (A/R)	Gross carrying amount	Accumulated depreciation	Net carrying amount
Capital work in progress	99,074	–	99,074	–	35,231	38,340	–	–	(51,044)	–	(1,851)	–	–	–	119,950	–	119,950
Plant and equipment	43,048	(20,898)	22,350	(278)	5,822	–	(869)	(4,334)	–	–	–	–	–	–	42,040	(19,349)	22,691
Office equipment	7,500	(3,755)	3,745	–	619	–	–	(988)	–	–	–	–	–	–	8,118	(4,742)	3,376
Furniture and fittings	2,404	(537)	1,867	–	499	–	–	(269)	–	–	–	–	–	–	2,904	(807)	2,097
Land:																	
– Operational land	170,106	–	170,106	(7,214)	–	2,467	(10)	–	–	(159)	–	–	–	84,853	249,843	–	249,843
– Community land	230,990	(760)	230,230	(4,726)	–	3,564	–	–	–	82	–	–	–	–	229,910	(760)	229,150
– Crown land	108,721	–	108,721	–	–	–	–	–	–	–	–	–	–	–	108,721	–	108,721
– Land under roads (post 30/6/08)	30,165	–	30,165	(26,182)	–	150	–	–	–	77	–	–	–	1,474	5,684	–	5,684
Infrastructure:																	
– Buildings – non-specialised	203,416	(123,640)	79,776	(174)	39	5	(212)	(5,186)	2,958	–	–	–	–	62,666	213,464	(73,592)	139,872
– Buildings – specialised	319,580	(202,440)	117,140	(101)	1,095	147	(1,580)	(6,535)	8,316	(149)	–	–	–	44,959	328,555	(163,263)	165,292
– Other structures	15,544	(8,935)	6,609	–	69	99	–	(582)	149	(9)	–	–	–	1,147	13,971	(6,489)	7,482
– Roads	1,401,805	(849,463)	552,342	215	9,021	12,707	(4,549)	(20,460)	19,810	913	–	–	–	–	1,426,090	(856,091)	569,999
– Bridges	150,163	(51,837)	98,326	–	933	1,314	(355)	(1,941)	262	–	–	–	–	–	152,318	(53,579)	98,739
– Footpaths	279,783	(143,942)	135,841	(368)	1,593	661	(572)	(4,021)	15,022	(966)	–	–	–	–	291,660	(144,470)	147,190
– Stormwater drainage	931,671	(407,815)	523,856	4,617	2,776	3,119	(518)	(10,246)	1,989	46	–	–	–	–	947,790	(422,151)	525,639
– Swimming pools	30,218	(22,933)	7,285	–	–	–	–	(475)	–	(479)	–	–	–	–	29,738	(23,407)	6,331
– Other open space/recreational assets	157,008	(45,553)	111,455	(11)	286	810	(110)	(3,668)	1,033	644	–	–	(16)	–	158,109	(47,686)	110,423
Other assets:																	
– Heritage collections	12,842	–	12,842	–	–	344	–	–	–	–	–	–	–	1,935	15,121	–	15,121
– Library books	8,158	(3,707)	4,451	–	1,207	–	–	(1,162)	–	–	–	–	–	–	8,162	(3,666)	4,496
– Other	47,249	(13,048)	34,201	–	47	265	–	(1,541)	1,505	–	–	–	–	–	49,066	(14,589)	34,477
Reinstatement, rehabilitation and restoration assets (refer Note 17):																	
– Tip assets	14,875	(9,151)	5,724	–	–	–	–	(141)	–	–	–	(58)	–	–	14,817	(9,292)	5,525
Investment Property (refer to Note 12):																	
Investment Property (refer to Note 12):																	
Total Infrastructure, property, plant and equipment	4,264,320	(1,908,014)	2,356,306	(34,222)	59,237	63,992	(8,775)	(61,549)	–	–	(1,651)	(58)	(16)	196,834	4,414,031	(1,843,933)	2,570,098

(1) Renewals are defined as the replacement of existing assets (as opposed to the acquisition of new assets).

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 11. Infrastructure, property, plant and equipment (continued)

Accounting policy for infrastructure, property, plant and equipment

Infrastructure, property, plant and equipment are held at fair value. Independent comprehensive valuations are performed at least every five years, however the carrying amount of assets is assessed by Council at each reporting date to confirm that it is not materially different from current fair value.

Increases in the carrying amounts arising on revaluation are credited to the revaluation reserve. To the extent that the increase reverses a decrease previously recognising profit or loss relating to that asset class, the increase is first recognised as profit or loss. Decreases that reverse previous increases of assets in the same class are first charged against revaluation reserves directly in equity to the extent of the remaining reserve attributable to the class; all other decreases are charged to the Income Statement.

Subsequent costs are included in the asset's carrying amount or recognised as a separate asset, as appropriate, only when it is probable that future economic benefits associated with the item will flow to Council and the cost of the item can be measured reliably. All other repairs and maintenance are charged to the Income Statement during the financial period in which they are incurred.

When infrastructure, property, plant and equipment are acquired by Council at significantly below fair value, the assets are initially recognised at their fair value at acquisition date.

Land is not depreciated. Depreciation on other assets is calculated using the straight-line method to allocate their cost, net of their residual values, over their estimated useful lives as follows:

Plant and equipment	Years	Other equipment	Years
Office equipment	3 to 25	Playground equipment	10 to 15
Office furniture	5 to 34	Benches, seats etc.	10 to 20
Computer equipment	2 to 10		
Vehicles	3 to 20	Buildings	
Heavy plant/road making equipment	8 to 10	Buildings: masonry	50 to 196
Other plant and equipment	2 to 34	Buildings: other	2 to 50
Stormwater assets			
Drains	30 to 130		
Culverts	30 to 130		
Flood control structures	30 to 130		
Transportation assets		Other infrastructure assets	
Roads: seal	8 to 95	Bulk earthworks	Infinite
Roads: base	15 to 145	Swimming pools	40 to 100
Roads: sub-base	15 to 145	Other open space/recreational assets	3 to 115
Bridge: concrete	80	Other infrastructure	10 to 100
Bridge: other	20 to 80		
Kerb, gutter and footpaths	20 to 104		

The assets' residual values and useful lives are reviewed, and adjusted if appropriate, at each reporting date. Gains and losses on disposal are determined by comparing proceeds with carrying amount. These are included in the Income Statement.

Land under roads

Land under roads is land under roadways and road reserves including land under footpaths, nature strips and median strips.

Council has elected not to recognise land under roads acquired before 1 July 2008 in accordance with AASB 1051 Land Under Roads.

Land under roads acquired after 1 July 2008 is recognised in accordance with AASB 116 Property, Plant and Equipment.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 11. Infrastructure, property, plant and equipment (continued)

Crown reserves

Crown reserves under Council's care and control are recognised as assets of the Council. While ownership of the reserves remains with the Crown, Council retains operational control of the reserves and is responsible for their maintenance and use in accordance with the specific purposes to which the reserves are dedicated. Improvements on Crown reserves are also recorded as assets, while maintenance costs incurred by Council and revenues relating to the reserves are recognised within Council's Income Statement.

Rural Fire Service assets

Under Section 119 of the *Rural Fire Services Act 1997 (NSW)*, "all firefighting equipment purchased or constructed wholly or from money to the credit of the Fund is to be vested in the council of the area for or on behalf of which the firefighting equipment has been purchased or constructed".

These Rural Fire Service assets are recognised as assets of the Council in these financial statements.

Note 12. Investment properties

	2020	2019
Owned investment property		
Investment property on hand at fair value	5,000	5,000
Total owned investment property	5,000	5,000
(a) Reconciliation – owned investment property		
Reconciliation of annual movement:		
Opening balance	5,000	4,780
– Capitalised expenditure	1,998	–
– Net gain/(loss) from fair value adjustments	(1,998)	220
CLOSING BALANCE – OWNED INVESTMENT PROPERTY	5,000	5,000

(b) Valuation basis (2019 only)

The basis of valuation of investment properties is fair value, being the amounts for which the properties could be exchanged between willing parties in arms length transaction, based on current prices in an active market for similar properties in the same location and condition and subject to similar leases.

The 2019 revaluations were based on independent assessments made by: Brendan Carr Certified Practising Valuer.

(c) Contractual obligations at reporting date (2019 only)

Refer to Note 18 for disclosures relating to any capital and service obligations that have been contracted.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 12. Investment properties (continued)

\$ '000	2020	2019
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(d) Leasing arrangements – Council as lessor (2019 only)

The investment properties are leased to tenants under long-term operating leases with rentals payable monthly.

Future minimum lease payments receivable under non-cancellable investment property operating leases not recognised in the financial statements are receivable as follows:

Within 1 year	–	371
Later than 1 year but less than 5 years	–	720
Total minimum lease payments receivable	–	1,091

Leases for Council's investment property are generally of a 5 year term with an option for a further 5 years. Rent is paid monthly in advance with tenants paying 100% of outgoings. Rent reviews are a mix of C.P.I. and market reviews. There is a requirement on tenants to hold \$20m public liability insurance.

\$ '000	2020	2019
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(e) Investment property income and expenditure – summary (2019 only)

Rental income from investment property:

– Minimum lease payments	–	389
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Direct operating expenses on investment property:

– that generated rental income	–	(140)
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Net revenue contribution from investment property	–	249
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plus:

Fair value movement for year	–	220
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Total income attributable to investment property	–	469
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Accounting policy for investment property

Investment property, principally comprising freehold office buildings, is held for long-term rental yields and is not occupied by the Council. Changes in fair values are recorded in the Income Statement as a separate line item.

Properties that are under construction for future use as investment properties are regarded as investment property. These are also carried at fair value unless the fair value cannot yet be reliably determined. Where that is the case, the property will be accounted for at cost until either the fair value becomes reliably determinable or construction is complete.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 13. Intangible assets

Intangible assets are as follows:

\$ '000	2020	2019
Software		
Opening values at 1 July		
Gross book value	2,506	2,333
Accumulated amortisation	(2,066)	(1,945)
Net book value – opening balance	440	388
Movements for the year		
– Purchases	–	285
– Amortisation charges	(186)	(232)
– Gross book value written off	–	(113)
– Accumulated amortisation charges written off	–	112
Closing values at 30 June		
Gross book value	2,345	2,506
Accumulated amortisation	(2,091)	(2,066)
Total software – net book value	254	440
<u>TOTAL INTANGIBLE ASSETS – NET BOOK VALUE</u>	<u>254</u>	<u>440</u>

Accounting policy for intangible assets

IT development and software

Costs incurred in developing products or systems and costs incurred in acquiring software and licenses that will contribute to future period financial benefits through revenue generation and/or cost reduction are capitalised to software and systems.

Costs capitalised include external direct costs of materials and service, direct payroll, and payroll related costs of employees' time spent on the project. Amortisation is calculated on a straight line basis over periods generally ranging from three to five years. IT development costs include only those costs directly attributable to the development phase and are only recognised following completion of technical feasibility, and where Council has an intention and ability to use the asset.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 14. Contract assets and liabilities

\$ '000	2020 Current	2020 Non-current
(a) Contract assets		
Other	5,669	–
Total Contract assets	5,669	–

Impairment

Council recognises contract assets when performance obligations relating to a contract with a customer have been met before the funds are received. Any impairment of these assets is assessed annually taking into consideration that contract assets are only recognised when an enforceable contract is in place.

\$ '000	Notes	2020 Current	2020 Non-current
(b) Contract liabilities			
Grants and contributions received in advance:			
Unexpended capital grants (to construct Council controlled assets)	(i)	707	–
Unexpended operating grants (received prior to performance obligation being satisfied)	(ii)	43	–
Unexpended capital contributions (to construct Council controlled assets)	(i)	339	–
Total grants received in advance		1,089	–
User fees and charges received in advance:			
Upfront fees	(iii)	2,483	–
Total user fees and charges received in advance		2,483	–
Total contract liabilities		3,572	–

Notes

(i) Council has received funding to construct assets. The funds received are under an enforceable contract which require Council to construct an identified asset which will be under Council's control on completion. The revenue is recognised as Council constructs the asset and the contract liability reflects the funding received which cannot yet be recognised as revenue. The revenue is expected to be recognised in the next 12 months.

(ii) The contract liability relates to operating grants received prior to the revenue recognition criteria in AASB 15 being satisfied since the performance obligations are ongoing.

(iii) Fees paid upfront for the delivery of specific Council services are recorded as a contract liability on receipt and recognised as revenue when the performance obligations are met.

\$ '000	2020
(i) Revenue recognised (during the financial year) from opening contract liability balances	
Capital grants (to construct Council controlled assets)	302
Capital contributions (to construct Council controlled assets)	42
Upfront fees	2,598
Total Revenue recognised during the financial year that was included in the contract liability balance at the beginning of the period	2,942

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Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 14. Contract assets and liabilities (continued)

Significant changes in contract assets and liabilities

The contract liabilities have arisen on adoption of AASB 15 and AASB 1058. Previously income received in advance was recognised for reciprocal contracts. The increase in a contract liability is primarily due to grants in the scope of AASB 15 and capital grants received by Council to acquire or construct assets which will be under Council's control. Previously, revenue was recognised on receipt of the funds.

Accounting policy for contract assets and liabilities

Where the amounts billed to customers are based on the achievement of various milestones established in the contract, the amounts recognised as revenue in a given period do not necessarily coincide with the amounts billed to or certified by the customer.

When a performance obligation is satisfied by transferring a promised good or service to the customer before the customer pays consideration or before the payment is due, Council presents the work in progress as a contract asset, unless the rights to that amount of consideration are unconditional, in which case Council recognises a receivable.

When an amount of consideration is received from a customer / fund provider prior to Council transferring a good or service to the customer, Council presents the funds which exceed revenue recognised as a contract liability.

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Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 15. Leases

Council has applied AASB 16 using the modified retrospective (cumulative catch-up) method and therefore the comparative information has not been restated and continues to be reported under AASB 117 and related Interpretations.

(i) Council as a lessee

Council has leases over land and buildings. Information relating to the leases in place and associated balances and transactions is provided below.

Land & Buildings

Council leases land and building for libraries and other operations; these leases are between 5 and 30 years and some include a renewal option to allow Council to renew the lease term. These leases contain an annual pricing mechanism based on either fixed increases or CPI movements at each anniversary of the lease inception.

Extension options

Council includes extension options in some of their leases to provide flexibility and certainty to Council operations and reduce costs of moving premises. These extension options are at Council's discretion.

At commencement date and each subsequent reporting date, Council assesses where it is reasonably certain that the extension options will be exercised.

There are \$6.2M in potential future lease payments which are not included in lease liabilities as Council has assessed that the exercise of the option is not reasonably certain.

\$ '000	Land & Buildings	Total
(a) Right of use assets		
Adoption of AASB 16 at 1 July 2019 – first time lease recognition	2,172	2,172
Depreciation charge	(382)	(382)
<u>BALANCE AT 30 JUNE 2020</u>	<u>1,790</u>	<u>1,790</u>

\$ '000	2020 Current	2020 Non-current
(b) Lease liabilities		
Lease liabilities	341	1,519
<u>TOTAL LEASE LIABILITIES</u>	<u>341</u>	<u>1,519</u>

(i) The maturity analysis

The maturity analysis of lease liabilities based on contractual undiscounted cash flows is shown in the table below:

\$ '000	< 1 year	1 – 5 years	> 5 years	Total	Total per Statement of Financial Position
Cash flows	341	972	547	1,860	1,860

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 15. Leases (continued)

\$ '000	2020
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(c) Income Statement

The amounts recognised in the Income Statement relating to leases where Council is a lessee are shown below:

Interest on lease liabilities	65
Depreciation of right of use assets	382
	447

(d) Statement of Cash Flows

Total cash outflow for leases	377
	377

Leases at significantly below market value – concessionary / peppercorn leases

Council has a number of leases at significantly below market for land and infrastructure which are used for:

- pedestrian crossings and bridges
- boat ramp

The leases are generally for an extended period of time and require payments of a maximum amount of \$1,000 per year. Council does not believe that any of the leases in place are individually material from a statement of financial position or performance perspective.

Accounting policy

Accounting policies under AASB 16 – applicable from 1 July 2019

At inception of a contract, Council assesses whether a lease exists – i.e. does the contract convey the right to control the use of an identified asset for a period of time in exchange for consideration?

Council has elected not to separate non-lease components from lease components for any class of asset and has accounted for payments as a single component.

At the lease commencement, Council recognises a right-of-use asset and associated lease liability for the lease term. The lease term includes extension periods where Council believes it is reasonably certain that the option will be exercised.

The right-of-use asset is measured using the cost model where cost on initial recognition comprises: the lease liability, initial direct costs, prepaid lease payments, estimated cost of removal and restoration, less any lease incentives. The right-of-use asset is depreciated over the lease term on a straight-line basis and assessed for impairment in accordance with the impairment of asset accounting policy.

The lease liability is initially recognised at the present value of the remaining lease payments at the commencement of the lease. The discount rate is the rate implicit in the lease, however where this cannot be readily determined then the Council's incremental borrowing rate for a similar term with similar security is used.

Subsequent to initial recognition, the lease liability is measured at amortised cost using the effective interest rate method. The lease liability is re-measured when there is a lease modification, or change in estimate of the lease term or index upon which the lease payments are based (e.g. CPI).

Where the lease liability is re-measured, the right-of-use asset is adjusted to reflect the re-measurement.

Exceptions to lease accounting

Council has applied the exceptions to lease accounting for both short-term leases (i.e. leases with a term of less than or equal to 12 months) and leases of low-value assets. Council recognises the payments associated with these leases as an expense on a straight-line basis over the lease term.

Leases at significantly below market value / concessionary leases

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Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 15. Leases (continued)

Council has elected to measure the right of use asset arising from the concessionary leases at cost which is based on the associated lease liability at initial recognition.

Accounting policy under AASB 117 and associated Accounting Interpretations (2019 only)

Refer to Note 5c and Note 16.

(ii) Council as a lessor

(e) Operating leases

Council leases out a number of properties; these leases have been classified as operating leases for financial reporting purposes and the assets are included as investment property (refer note 12) and/or IPP&E in the Statement of Financial Position.

The amounts recognised in the Income Statement relating to operating leases where Council is a lessor are shown below:

\$ '000	2020
(i) Operating lease income	
Investment properties	
Lease income (excluding variable lease payments not dependent on an index or rate)	392
Other lease income	
Room/Facility Hire	374
Leaseback fees - council vehicles	702
Other Council Properties	5,267
Total income relating to operating leases	6,735
(ii) Operating lease expenses	
Investment properties	
Direct operating expenses that generated rental income	142
Total expenses relating to operating leases	142

(iii) Repairs and maintenance: investment property

Refer to Note 21 Commitments for disclosures relating to any capital and service obligations that have been contracted.

(iv) Maturity analysis of contractual lease income: investment property

Maturity analysis of future lease income receivable showing the undiscounted lease payments to be received after reporting date for operating leases:

< 1 year	428
1-2 years	287
2-3 years	156
3-4 years	67
Total undiscounted contractual lease income receivable	938

Accounting policy

When Council is a lessor, the lease is classified as either an operating or finance lease at inception date, based on whether substantially all of the risks and rewards incidental to ownership of the asset have been transferred to the lessee. If the risks and rewards have been transferred then the lease is classified as a finance lease, otherwise it is an operating lease.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 15. Leases (continued)

When Council has a sub-lease over an asset and is the intermediate lessor then the head lease and sub-lease are accounted for separately. The classification of the sub-lease is based on the right-of-use asset which arises from the head lease rather than the useful life of the underlying asset.

If the lease contains lease and non-lease components then the non-lease components are accounted for in accordance with AASB 15 *Revenue from Contracts with Customers*.

The lease income is recognised on a straight-line basis over the lease term.

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Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 16. Payables and borrowings

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
Payables				
Prepaid rates	2,377	–	–	–
Goods and services – operating expenditure	3,633	–	3,302	–
Goods and services – capital expenditure	2,964	–	3,627	–
Accrued expenses:				
– Other expenditure accruals	16,504	–	18,299	–
Security bonds, deposits and retentions	4,054	–	3,758	–
Agency Funds	385	–	385	385
Other	675	–	635	–
Total payables	30,592	–	30,006	385
Income received in advance (2019 only)				
Payments received in advance	–	–	5,014	–
Total income received in advance	–	–	5,014	–
Borrowings				
Loans – secured ¹	5,260	12,439	7,934	17,497
Total borrowings	5,260	12,439	7,934	17,497
TOTAL PAYABLES AND BORROWINGS	35,852	12,439	42,954	17,882

(1) Loans are secured over the general rating income of Council.

Disclosures on liability interest rate risk exposures, fair value disclosures and security can be found in Note 20.

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
(a) Payables and borrowings relating to restricted assets				
Externally restricted assets				
Loans	274	–	7,934	17,497
Total payables and borrowings relating to restricted assets	274	–	7,934	17,497
Total payables and borrowings relating to unrestricted assets	35,578	12,439	35,020	385
TOTAL PAYABLES AND BORROWINGS	35,852	12,439	42,954	17,882

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 16. Payables and borrowings (continued)

\$ '000	2020	2019
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(b) Current payables and borrowings not anticipated to be settled within the next twelve months

The following liabilities, even though classified as current, are not expected to be settled in the next 12 months.

Payables – security bonds, deposits and retentions	3,202	3,088
Total payables and borrowings	3,202	3,088

(c) Changes in liabilities arising from financing activities

	as at 30/06/19		Non-cash changes				as at 30/06/20
	Opening Balance	Cash flows	Acquisition	Fair value changes	Acquisition due to change in accounting policy	Other non-cash movement	Closing balance
\$ '000							
Loans – secured	25,431	(7,934)	–	–	–	202	17,699
Lease liabilities	–	(377)	–	–	2,172	65	1,860
TOTAL	25,431	(8,311)	–	–	2,172	267	19,559

	as at 30/06/18		Non-cash changes				as at 30/06/19
	Opening Balance	Cash flows	Acquisition	Fair value changes	Other non-cash movement		Closing balance
\$ '000							
Loans – secured	32,755	(7,716)	–	–	392		25,431
TOTAL	32,755	(7,716)	–	–	392		25,431

\$ '000	2020	2019
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(d) Financing arrangements

(i) Unrestricted access was available at balance date to the following lines of credit:

Bank overdraft facilities ¹	300	300
Credit cards/purchase cards	835	835
Total financing arrangements	1,135	1,135

Drawn facilities as at balance date:

– Credit cards/purchase cards	59	100
Total drawn financing arrangements	59	100

Undrawn facilities as at balance date:

– Bank overdraft facilities	300	300
– Credit cards/purchase cards	776	735
Total undrawn financing arrangements	1,076	1,035

Additional financing arrangements information

Breaches and defaults

During the current and prior year, there were no defaults or breaches on any of the loans.

(1) The bank overdraft facility may be drawn at any time and may be terminated by the bank without notice.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 16. Payables and borrowings (continued)

Accounting policy for payables and borrowings

Council measures all financial liabilities initially at fair value less transaction costs, subsequently financial liabilities are measured at amortised cost using the effective interest rate method.

The financial liabilities of the Council comprise trade payables and loans.

Payables

These amounts represent liabilities for goods and services provided to the council prior to the end of financial year that are unpaid. The amounts are unsecured and are usually paid within 30 days of recognition.

Borrowings

Borrowings are initially recognised at fair value, net of transaction costs incurred. Borrowings are subsequently measured at amortised cost. Any difference between the proceeds (net of transaction costs) and the redemption amount is recognised in the Income Statement over the period of the borrowings using the effective-interest method. Fees paid on the establishment of loan facilities are recognised as transaction costs of the loan to the extent that it is probable that some or all of the facility will be drawn down. In this case, the fee is deferred until the drawdown occurs. To the extent that there is no evidence that it is probable that some or all of the facility will be drawn down, the fee is capitalised as a prepayment for liquidity services and amortised over the period of the facility to which it relates.

Borrowings are removed from the Statement of Financial Position when the obligation specified in the contract is discharged, cancelled or expired. The difference between the carrying amount of a financial liability that has been extinguished or transferred to another party and the consideration paid, including any non-cash assets transferred or liabilities assumed, is recognised in other income or finance cost.

Borrowings are classified as current liabilities unless Council has an unconditional right to defer settlement of the liability for at least 12 months after the reporting date.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 17. Provisions

\$ '000	2020 Current	2020 Non-current	2019 Current	2019 Non-current
Provisions				
Employee benefits				
Annual leave	11,404	–	10,025	–
Sick leave	179	–	167	–
Long service leave	45,412	905	42,505	680
Other leave	1,209	–	1,097	–
Sub-total – aggregate employee benefits	58,204	905	53,794	680
Asset remediation/restoration:				
Asset remediation/restoration (future works)	500	32,867	2,292	40,647
Sub-total – asset remediation/restoration	500	32,867	2,292	40,647
Other provisions				
Self insurance – workers compensation	2,075	5,579	1,758	5,363
Self insurance – claims incurred	305	425	370	364
Sub-total – other provisions	2,380	6,004	2,128	5,727
TOTAL PROVISIONS	61,084	39,776	58,214	47,054

\$ '000	2020	2019
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(a) Current provisions not anticipated to be settled within the next twelve months

The following provisions, even though classified as current, are not expected to be settled in the next 12 months.

Provisions – employees benefits	47,811	43,517
	47,811	43,517

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 17. Provisions (continued)

(b) Description of and movements in provisions

\$ '000	Other provisions			Total
	Self Insurance: Claims incurred	Self Insurance: Workers compensation	Asset remediation	
2020				
At beginning of year	734	7,121	42,939	50,794
Additional provisions	—	2,503	—	2,503
Amounts used (payments)	—	(2,434)	—	(2,434)
Remeasurement effects	—	464	(10,965)	(10,501)
Unwinding of discount	—	—	1,939	1,939
Unused amounts reversed	(4)	—	—	(4)
Other	—	—	—	—
Expenditure incurred attributable to provisions	—	—	(546)	(546)
Total other provisions at end of year	730	7,654	33,367	41,751
2019				
At beginning of year	760	4,912	41,334	47,006
Additional provisions	—	2,108	—	2,108
Amounts used (payments)	—	(2,523)	—	(2,523)
Remeasurement effects	—	2,624	—	2,624
Unwinding of discount	—	—	1,850	1,850
Unused amounts reversed	(26)	—	(57)	(83)
Other	—	—	—	—
Expenditure incurred attributable to provisions	—	—	(188)	(188)
Total other provisions at end of year	734	7,121	42,939	50,794

Nature and purpose of non-employee benefit provisions

Asset remediation

Council has a legal/public obligation to make, restore, rehabilitate and reinstate the open Whytes Gully Waste Disposal Depot and closed Helensburgh Waste Dapto Rehabilitation.

Self-insurance- workers compensation

To recognise liabilities for outstanding claims (uninsured losses) arising from Council's decision to undertake self-insurance for excesses up to \$750,000 on any individual claim. Claims beyond this are supported by an external insurance policy.

Self-insurance - claims incurred

To recognise liabilities for both (i) claims expected to be incurred but not reported and (ii) claims reported and estimated as a result of Council being self insurer up to an excess of \$100,000 on any individual claim.

Accounting policy for provisions

Provisions are recognised when Council has a present legal or constructive obligation as a result of past events, it is probable that an outflow of resources will be required to settle the obligation, and the amount has been reliably estimated.

Where there are a number of similar obligations, the likelihood that an outflow will be required in settlement is determined by considering the class of obligations as a whole. A provision is recognised even if the likelihood of an outflow with respect to any one item included in the same class of obligations may be small.

Provisions are measured at the present value of management's best estimate of the expenditure required to settle the present obligation at the reporting date. The discount rate used to determine the present value reflects current market assessments of the time value of money and the risks specific to the liability. The increase in the provision due to the passage of time is recognised as interest expense.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 17. Provisions (continued)

Employee benefits

Short-term obligations

Liabilities for wages and salaries (including non-monetary benefits, annual leave and accumulating sick leave expected to be wholly settled within 12 months after the end of the period in which the employees render the related service) are recognised in respect of employees' services up to the end of the reporting period and are measured at the amounts expected to be paid when the liabilities are settled. The liability for annual leave and accumulating sick leave is recognised in the provision for employee benefits. All other short-term employee benefit obligations are presented as payables.

Other long-term employee benefit obligations

The liability for long-service leave and annual leave that is not expected to be wholly settled within 12 months after the end of the period in which the employees render the related service is recognised in the provision for employee benefits and measured as the present value of expected future payments to be made in respect of services provided by employees up to the end of the reporting period using the projected unit credit method. Consideration is given to expected future wage and salary levels, experience of employee departures, and periods of service. Expected future payments are discounted using market yields at the end of the reporting period on national government bonds with terms to maturity and currency that match, as closely as possible, the estimated future cash outflows.

On-costs

The employee benefit provisions include the aggregate on-cost liabilities that will arise when payment of current employee benefits is made in future periods. These amounts include superannuation which will be payable upon the future payment of certain leave liabilities which employees are entitled to at the reporting period. The obligations are presented as current liabilities in the Statement of Financial Position if the Council does not have an unconditional right to defer settlement for at least 12 months after the reporting date, regardless of when the actual settlement is expected to occur.

Asset Remediation/Restoration

Restoration

Close-down and restoration costs include the dismantling and demolition of infrastructure, and the removal of residual materials and remediation of disturbed areas. Estimated close-down and restoration costs are provided for in the accounting period when the obligation arising from the related disturbance occurs, whether this occurs during the development or during the operation phase, based on the net present value of estimated future costs.

Provisions for close-down and restoration costs do not include any additional obligations which are expected to arise from future disturbance. The costs are estimated on the basis of a closure plan. The cost estimates are calculated annually during the life of the operation to reflect known developments, e.g. updated cost estimates and revisions to the estimated lives of operations, and are subject to formal review at regular intervals.

Rehabilitation

Where rehabilitation is conducted systematically over the life of the operation, rather than at the time of closure, provision is made for the estimated outstanding continuous rehabilitation work at each reporting date, and the cost is charged to the Income Statement.

Provision is made for the estimated present value of the costs of environmental clean-up obligations outstanding at the reporting date. These costs are charged to the Income Statement. Movements in the environmental clean-up provisions are presented as an operating cost, except for the unwinding of the discount which is shown as a borrowing cost.

Remediation procedures generally commence soon after the time the damage, remediation process, and estimated remediation costs become known, but may continue for many years depending on the nature of the disturbance and the remediation techniques.

As noted above, the ultimate cost of environmental remediation is uncertain and cost estimates can vary in response to many factors, including changes to the relevant legal requirements, the emergence of new restoration techniques, or experience at other locations. The expected timing of expenditure can also change, for example in response to changes in quarry reserves or production rates. As a result, there could be significant adjustments to the provision for close down and restoration and environmental clean-up, which would affect future financial results.

Other movements in the provisions for close-down and restoration costs, including those resulting from new disturbance, updated cost estimates, changes to the estimated lives of operations, and revisions to discount rates, are capitalised within property, plant and equipment. These costs are then depreciated over the lives of the assets to which they relate.

Close-down and restoration costs are a normal consequence of tip and quarry operations, and the majority of close-down and restoration expenditure is incurred at the end of the life of the operations. Although the ultimate cost to be incurred is uncertain,

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Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 17. Provisions (continued)

Council estimates the respective costs based on feasibility and engineering studies using current restoration standards and techniques.

Self-insurance - workers compensation

Council holds a level of self-insurance in the form of an excess layer of \$750,000 on any individual claim for workers compensation. A provision for self-insurance has been made to recognise outstanding claims. Council also maintains a bank guarantee to meet expected future claims; refer to Note 22.

Self-insurance - claims incurred

Council holds a level of self-insurance in the form of an excess layer of \$100,000 on any individual claim for public liability and professional indemnity. A provision for self-insurance has been made to recognise outstanding claims.

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Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 18. Accumulated surplus, revaluation reserves, changes in accounting policies, changes in accounting estimates and errors

(a) Nature and purpose of reserves

Infrastructure, property, plant and equipment revaluation reserve

The infrastructure, property, plant and equipment revaluation reserve is used to record increments / decrements of non-current asset values due to their revaluation.

Investment revaluation reserve

Changes in the fair value of financial assets are taken through the investment revaluation reserve. The accumulated changes in fair value are transferred to profit or loss when the financial asset is derecognised or impaired.

(b) Correction of errors relating to a previous reporting period

Nature of prior-period error

As part of an ongoing inspection program and data cleansing process it was found that IPPE that Council owned had not been recognised (\$5M).

The errors identified above have been corrected by restating the balances at the beginning of the earliest period presented (1 July 2018) and taking the adjustment through to accumulated surplus at that date.

Comparatives have been changed to reflect the correction of errors. The impact on each line item is shown in the tables below.

\$ '000	Original Balance 1 July, 2018	Impact Increase/ (decrease)	Restated Balance 1 July, 2018
Stormwater Drainage	523,470	5,003	528,473
Other Assets without variation	2,003,006	–	2,003,006
Total assets	2,526,476	5,003	2,531,479
Total liabilities	158,408	–	158,408
Reserves	956,840	–	956,840
Accumulated Surplus	1,411,228	5,003	1,416,231
Total equity	2,368,068	5,003	2,373,071

(c) Changes in accounting policies due to adoption of new accounting standards (not-retrospective)

During the year ended 30 June 2020, the Council has adopted AASB 15 *Revenue from Contracts with Customers*, AASB 1058 *Income of Not-for-profit Entities* and AASB 16 *Leases* using the modified retrospective (cumulative catch-up) method and therefore the comparative information for the year ended 30 June 2019 has not been restated and continues to comply with AASB 111 *Construction Contracts*, AASB 117 *Leases*, AASB 118 *Revenue*, AASB 1004 *Contributions* and associated Accounting Interpretations.

All adjustments on adoption of AASB 15 and AASB 1058 have been taken to retained earnings at 1 July 2019.

The impacts of adopting these standards and associated transition disclosures are provided below:

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 18. Accumulated surplus, revaluation reserves, changes in accounting policies, changes in accounting estimates and errors (continued)

(i) AASB 15 and AASB 1058

The following approach has been applied on transition to AASB 15 and AASB 1058:

- Council has not adopted the completed contract expedient and therefore has not excluded revenue which was fully recognised in previous years in accordance with the former accounting standards and pronouncements
- Council has retrospectively restated contracts for modifications that occurred before 1 July 2019 unless such contract modification were minor.

Costs incurred in fulfilling customer contracts

Prior to adopting AASB 15 Council would have recognised direct costs associated with fulfilling customer contracts as expenses when incurred, as they did not qualify for recognition as assets under any other accounting standards. Under AASB 15, as these costs relate directly to the contracts, generate resources used in satisfying the contracts, and are expected to be recovered, they would be capitalised as 'costs to fulfil a contract' assets and released through profit and loss on the same basis as the revenue is recognised. Council has not assessed any of their costs to fall within this category.

Upfront fees – Council leisure centre

Prior to adopting AASB 15, Council recognised membership fees and multi-trip passes on receipt. Under AASB 15, they are combined with other goods and services transferred to the customer and therefore they would be spread over the expected life of the contract with the customer (i.e. the membership life). Council has assessed the impact of changing the revenue recognition timing of these fees as immaterial and will therefore continue to recognise revenue on receipt.

Upfront fees - Development assessment

Prior to adopting AASB 15, Council recognised development assessment related fees on receipt. Under AASB 15, as the performance obligation is not met until the application has been assessed, the revenue is recognised following a determination being provided to the customer.

Revenue recognition from contract modifications

In relation to contract modifications, AASB 15 requires customer approval, which is a more prudent criteria than the probability requirement in the previous standards and would result in deferral of revenue where unapproved works have been performed.

Transfer of control to a customer – over time or at a point in time

AASB 15 has specific criteria regarding whether control is transferred over time or at a point in time. The entity has reviewed its contracts and concluded that the criteria for recognition over time is not met in some circumstances. In such cases, revenue and related production costs will be recognised at the delivery of each separate performance obligation instead of over the contract using a single margin.

Principal v agent

Under AASB 15, the indicators of a principal have changed. If Council was considered a principal this would result in Council only recognising the "commission" to which they are entitled rather than the gross revenue and expenses. There would be no change to reported profit.

Licences

Council has reviewed the licences it grants and considers that all licences are either short-term or low value and elects to recognise all revenue from licences up-front rather than spreading them over the life of the licence.

Prepaid rates

Under AASB 1004, rates were recorded as revenue at the earliest of receipt of the funds from the ratepayer and the beginning of the rating period. Under AASB 1058, prepaid rates are recognised as a financial liability until the beginning of the rating period.

Grants & Contributions – operating

Under AASB 1004, most grant income was recognised as revenue on receipt. Under AASB 15, where an agreement is enforceable and contains sufficiently specific performance obligations, the revenue is either recognised over time as the work is performed, or recognised at the point in time that the control of the services passes to the customer.

Grants & Contributions – capital

Under AASB 1004, most grant monies were recorded as revenue on receipt. Under AASB 1058, where Council has received assets (including cash) to acquire or construct a non-financial asset, the asset is to be controlled by Council and the contract

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 18. Accumulated surplus, revaluation reserves, changes in accounting policies, changes in accounting estimates and errors (continued)

is enforceable, then the asset is recognised as a contract liability on receipt and recorded as revenue as the performance obligation to acquire or construct the asset is completed.

Developer contributions will continue to be recognised on receipt. Although developer contributions may only be expended for the purposes for which the contributions were required, Council may apply contributions according to priorities established in work schedules meaning they can be recognised on receipt under the new accounting standards.

Changes in presentation

In addition to the above changes in accounting policies, the Council has also amended the presentation of certain items to align them with the requirements of AASB 15 and AASB 1058:

- Movement of balances between receivables and contract assets.
- Movement of balances between payables, income received in advance and contract liabilities.
- Additional line items and disclosure notes for contract assets and contract liabilities have been created.
- Line items not applicable for the period ended 30 June 2020 due to the implementation of new accounting standards have been labelled as "2019 only."

\$ '000	Balance at 1 July 2019
Opening contract balances at 1 July 2019	
Contract assets	
– Under AASB 1058	16,336
Total Contract assets	16,336
Contract liabilities	
– Under AASB 15	3,412
– Under AASB 1058	910
Total Contract liabilities	4,322

Comparison of financial statement line items under AASB 15 compared to previous standards for the current year

The following tables show the impact of adopting AASB 15 and AASB 1058 on the Council's financial statements for the year ended 30 June 2020.

Statement of Financial Position

\$ '000	Carrying amount per Statement of Financial Position under AASB 15 and AASB 1058	Reclassific- ation	Remeasur- ement	Carrying amount under previous revenue standards	Notes
Current assets					
Cash and cash equivalents	56,051	–	–	56,051	
Investments	97,423	–	–	97,423	
Receivables	21,667	6,428	–	28,095	
Inventories	381	–	–	381	
Contract assets	5,669	(6,428)	759	–	
Other	10,572	–	–	10,572	
Total current assets	191,763	–	759	192,522	

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 18. Accumulated surplus, revaluation reserves, changes in accounting policies, changes in accounting estimates and errors (continued)

\$ '000	Carrying amount per Statement of Financial Position under AASB 15 and AASB 1058	Reclassification	Remeasurement	Carrying amount under previous revenue standards	Notes
Current liabilities					
Payables	30,592	(2,377)	–	28,215	
Income received in advance	–	4,226	–	4,226	
Contract liabilities	3,572	(1,849)	(1,723)	–	
Lease liabilities	341	–	–	341	
Borrowings	5,260	–	–	5,260	
Provisions	61,084	–	–	61,084	
Total current liabilities	100,849	–	(1,723)	99,126	
Non-current assets					
Investments	4,000	–	–	4,000	
Inventories	5,972	–	–	5,972	
Infrastructure, property, plant and equipment	2,655,487	–	–	2,655,487	
Investment property	5,000	–	–	5,000	
Intangible assets	254	–	–	254	
Right of use assets	1,790	–	–	1,790	
Investments accounted for using equity method	3,484	–	–	3,484	
Total non-current assets	2,675,987	–	–	2,675,987	
Non-current liabilities					
Lease liabilities	1,519	–	–	1,519	
Borrowings	12,439	–	–	12,439	
Provisions	39,776	–	–	39,776	
Total Non-current liabilities	53,734	–	–	53,734	
Net assets	2,713,167	–	2,482	2,715,649	
Equity					
Accumulated surplus	1,498,309	–	2,482	1,500,791	
Revaluation reserves	1,214,858	–	–	1,214,858	
Council equity interest	2,713,167	–	2,482	2,715,649	
Total equity	2,713,167	–	2,482	2,715,649	

Through the implementation of AASB15 & 1058 the following adjustments have resulted in a variation to the statement of financial position:

- Reclassification of income received in advance to contract liabilities and payables (prepaid rates)
- Changes in the timing of income recognition for grants, contributions and user fees & charges has resulted in an increase in contract liabilities
- Recognition of contract assets for grants and contributions income in place of a receivable.

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 18. Accumulated surplus, revaluation reserves, changes in accounting policies, changes in accounting estimates and errors (continued)

Income Statement

\$ '000	Income Statement and comprehensive income under AASB 15 and AASB 1058	Reclassification	Remeasurement	Income Statement and comprehensive income under previous revenue standards	Notes
<u>Income from continuing operations</u>					
Rates and annual charges	205,118	–	–	205,118	
User charges and fees	28,695	1,076	67	29,838	
Other revenues	4,794	5,659	–	10,453	
Grants and contributions provided for operating purposes	30,727	–	(664)	30,063	
Grants and contributions provided for capital purposes	47,472	–	566	48,038	
Interest and investment income	3,360	–	–	3,360	
Rental income	6,735	(6,735)	–	–	
Net share of interests in joint ventures and associates using the equity method	555	–	–	555	
Total Income from continuing operations	327,456	–	(31)	327,425	
<u>Expenses from continuing operations</u>					
Employee benefits and on-costs	122,912	–	–	122,912	
Borrowing costs	3,052	–	–	3,052	
Materials and contracts	60,934	–	(599)	60,335	
Depreciation and amortisation	60,845	–	–	60,845	
Other expenses	36,497	–	–	36,497	
Net losses from the disposal of assets	5,686	–	–	5,686	
Fair value decrement on investment properties	1,998	–	–	1,998	
Total Expenses from continuing operations	291,924	–	(599)	291,325	
Total Operating result from continuing operations	35,532	–	568	36,100	
Net operating result for the year	35,532	–	568	36,100	
Total comprehensive income	93,112	–	–	93,112	

Through the implementation of AASB15 & 1058 the following adjustments have resulted in a variation to the operating result:

- Recognition of volunteer services income offset by an increase in materials and contracts
- Reclassification of income from user fees & charges and other revenues to rental income
- Changes to the timing of recognition of grants & contributions income and user fees & charges income.

The adoption of AASB 15 and AASB 1058 has not materially changes the Statement of Cash Flows for the year ended 30 June 2020.

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 18. Accumulated surplus, revaluation reserves, changes in accounting policies, changes in accounting estimates and errors (continued)

Adjustments to the current year figures for the year ended 30 June 2020

\$ '000	Original Balance 1 July, 2019	Impact Increase/ (decrease)	Restated Balance 1 July, 2019
Contract assets	–	16,336	16,336
Receivables	36,620	(16,773)	19,847
Total assets	–	(437)	(437)
Contract liabilities	–	4,322	4,322
Current Payables	35,020	(2,845)	32,175
Total liabilities	–	1,477	1,477
Accumulated surplus	–	(1,914)	(1,914)
Total equity	–	(1,914)	(1,914)

(ii) AASB 16 Leases

Council as a lessee

Under AASB 117, Council assessed whether leases were operating or finance leases, based on its assessment of whether the significant risks and rewards of ownership had been transferred to Council or remained with the lessor. Under AASB 16, there is no differentiation between finance and operating leases for the lessee and therefore all leases which meet the definition of a lease are recognised on the statement of financial position (except for short-term leases and leases of low-value assets).

Council has used the exception to lease accounting for short-term leases and leases of low-value assets, and the lease expense relating to these leases is recognised in the Income Statement on a straight-line basis.

Practical expedients used on transition

AASB 16 includes a number of practical expedients which can be used on transition. Council has used the following expedients:

- Contracts which had previously been assessed as not containing leases under AASB 117 were not re-assessed on transition to AASB 16.
- Lease liabilities have been discounted using the Council's incremental borrowing rate at 1 July 2019.
- Right-of-use assets at 1 July 2019 have been measured at an amount equal to the lease liability adjustment by the any prepaid or accrued lease payments.
- A single discount rate was applied to all leases with similar characteristics.
- The right-of-use asset was adjusted by the existing onerous lease provision (where relevant) at 30 June 2019 rather than perform impairment testing of the right-of-use asset.
- Excluded leases with an expiry date prior to 30 June 2020 from the Statement of Financial Position, and lease expenses for these leases have been recorded on a straight-line basis over the remaining term.
- Used hindsight when determining the lease term if the contract contains options to extend or terminate the lease.

Financial statement impact of adoption of AASB 16

Council has recognised right-of-use assets and lease liabilities of \$2.17M at 1 July 2019 for leases previously classified as operating leases, or leases that are significantly below market value which were previously off balance sheet.

The weighted average lessee's incremental borrowing rate applied to lease liabilities at 1 July 2019 was 3.00%.

Operating lease commitments at 30 June 2019 per Council financial statements

1,273

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 18. Accumulated surplus, revaluation reserves, changes in accounting policies, changes in accounting estimates and errors (continued)

\$ '000	Balance at 1 July 2019
Reconciliation of lease liabilities recognised on adoption of AASB 16 Leases	
Operating lease commitments discounted using the incremental borrowing rate at 1 July 2019	1,273
Add:	
Contracts not accounted for as operating lease commitments last year	103
Other	796
Lease liabilities recognised at 1 July 2019	2,172

Council as a lessor

For the arrangements where Council is a lessor, there are no significant accounting policy changes on adoption of AASB 16 except for sub-leases, which have now been classified in relation to the right-of-use asset under the head lease rather than the underlying asset.

Statement of Financial Position

\$ '000	Original Balance 1 July, 2019	Impact Increase/ (decrease)	Restated Balance 1 July, 2019
Rights-of-use assets	–	2,172	2,172
Total assets	–	2,172	2,172
Leases	–	2,172	2,172
Total liabilities	–	2,172	2,172
Accumulated surplus	–	–	–
Total equity	–	–	–

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 19. Statement of cash flow information

\$ '000	Notes	2020	2019
(a) Reconciliation of cash and cash equivalents			
Total cash and cash equivalents per Statement of Financial Position	7(a)	56,051	25,187
Balance as per the Statement of Cash Flows		56,051	25,187

(b) Reconciliation of net operating result to cash provided from operating activities

Net operating result from Income Statement	35,532	52,112
Adjust for non-cash items:		
Depreciation and amortisation	60,845	61,781
Net losses/(gains) on disposal of assets	5,686	6,856
Non-cash capital grants and contributions	(21,156)	(7,397)
Adoption of AASB 15/1058	(1,914)	–
Prior period WIP written off during year	8,048	–
IPP&E restoration write offs	5,407	–
Losses/(gains) recognised on fair value re-measurements through the P&L:		
– Investments classified as 'at fair value' or 'held for trading'	280	(141)
– Investment property	1,998	(220)
Amortisation of premiums, discounts and prior period fair valuations		
– Interest exp. on interest-free loans received by Council (previously fair valued)	203	391
Unwinding of discount rates on reinstatement provisions	1,939	1,850
Share of net (profits)/losses of associates/joint ventures using the equity method	(555)	(291)
+/- Movement in operating assets and liabilities and other cash items:		
Decrease/(increase) in receivables	14,599	(9,832)
Increase/(decrease) in provision for impairment of receivables	354	138
Decrease/(increase) in inventories	(44)	(31)
Decrease/(increase) in other current assets	1,307	(1,213)
Decrease/(increase) in contract assets	(5,669)	–
Increase/(decrease) in payables	331	(4,366)
Increase/(decrease) in other accrued expenses payable	996	1,861
Increase/(decrease) in other liabilities	(2,686)	3,655
Increase/(decrease) in contract liabilities	3,572	–
Increase/(decrease) in provision for employee benefits	4,635	6,537
Increase/(decrease) in other provisions	(10,982)	1,938
Net cash provided from/(used in) operating activities from the Statement of Cash Flows	102,726	113,628

(c) Non-cash investing and financing activities

Other dedications	19,566	6,403
Contributed Art Works	196	246
Contributed Bush Fire Assets	122	748
Planning Agreement - Non-cash contribution	1,272	–
Total non-cash investing and financing activities	21,156	7,397

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 20. Interests in other entities

\$ '000	Council's share of net income		Council's share of net assets	
	2020	2019	2020	2019
Joint ventures	555	291	3,484	2,929
Total	555	291	3,484	2,929

(a) Joint arrangements

(i) Joint ventures

Council has incorporated the following joint ventures into its consolidated financial statements. Included are the total amounts as per the joint venture financial statements, adjusted for fair-value adjustments at acquisition date and differences in accounting policies, rather than the Council's share.

(a) Net carrying amounts – Council's share

\$ '000	Nature of relationship	Measurement method	2020	2019
Civic West	Joint venture	Equity	2,561	2,060
Civic Risk	Joint venture	Equity	923	869
Total carrying amounts – material joint ventures			3,484	2,929

(b) Details

	Principal activity
Civic West	Insurance
Civic Risk	Insurance

(c) Relevant interests and fair values

\$ '000	Quoted fair value		Interest in outputs		Interest in ownership		Proportion of voting power	
	2020	2019	2020	2019	2020	2019	2020	2019
Civic West	2,561	2,062	6.1%	4.4%	6.1%	4.4%	7.7%	9.1%
Civic Risk	923	869	12.9%	12.5%	12.9%	12.5%	5.3%	5.9%

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 20. Interests in other entities (continued)

(d) Summarised financial information for joint ventures

\$ '000	Civic West 2020	2019	Civic Risk 2020	2019
Statement of financial position				
Current assets				
Cash and cash equivalents	14,665	2,961	6,177	1,813
Other current assets	15,584	27,370	5,924	7,216
Non-current assets	33,048	36,522	5,102	5,368
Current liabilities				
Current financial liabilities (excluding trade and other payables and provisions)	4,561	6,657	5,923	4,827
Other current liabilities	8,617	740	490	346
Non-current liabilities				
Non-current financial liabilities (excluding trade and other payables and provisions)	7,999	12,116	3,635	2,279
Net assets	42,120	47,340	7,155	6,945
Reconciliation of the carrying amount				
Opening net assets (1 July)	47,340	41,761	6,944	8,468
Profit/(loss) for the period	3,181	5,579	211	(1,524)
Closing net assets	50,521	47,340	7,155	6,944
Council's share of net assets (%)	6.1%	4.4%	12.9%	12.5%
Council's share of net assets (\$)	2,561	2,060	923	869
Statement of comprehensive income				
Income	6,648	6,295	14,474	11,320
Interest income	1,321	3,672	205	447
Interest expense	(18)	(17)	—	—
Other expenses	(4,770)	(4,371)	(14,468)	(13,291)
Profit/(loss) from continuing operations	3,181	5,579	211	(1,524)
Profit/(loss) for the period	3,181	5,579	211	(1,524)
Total comprehensive income	3,181	5,579	211	(1,524)
Share of income – Council (%)	15.8%	8.5%	25.1%	12.1%
Profit/(loss) – Council (\$)	502	476	53	(185)
Total comprehensive income – Council (\$)	502	476	53	(185)
Summarised Statement of cash flows				
Cash flows from operating activities	(1,887)	2,439	1,614	2,904
Cash flows from investing activities	12,681	(2,500)	2,750	(2,250)
Net increase (decrease) in cash and cash equivalents	10,794	(61)	4,364	654

Accounting policy for joint arrangements

Council has determined that it has only joint ventures.

Joint Ventures

Interests in joint ventures are accounted for using the equity method in accordance with AASB128 *Investments in Associates and Joint Ventures*. Under this method, the investment is initially recognised as a cost and the carrying amount is increased or decreased to recognise the Council's share of the profit or loss and other comprehensive income of the joint venture after the date of acquisition.

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 20. Interests in other entities (continued)

If the Council's share of losses of a joint venture equals or exceeds its interest in the joint venture, the Council discontinues recognising its share of further losses.

The Council's share in the joint venture's gains or losses arising from transactions between itself and its joint ventures are eliminated.

Adjustments are made to the joint venture's accounting policies where they are different from those of the Council for purpose of the consolidated financial statements.

Refer to Note 27 for events occurring after the reporting date to be read in conjunction with the joint venture disclosure.

(b) Subsidiaries, joint arrangements and associates not recognised

The following subsidiaries, joint arrangements and associates have not been recognised in this financial report.

Name of entity/operation	Principal activity/type of entity	2020 Net profit (\$'000s)	2020 Net assets (\$'000s)
Illawarra Shoalhaven Joint Organisation	Inter-Governmental Collaboration Joint Venture	7	892

Reasons for non-recognition

Council has assessed this operation as not material for recognition in these statements.

Name of entity/operation	Principal activity/type of entity	2020 Net profit (\$'000s)	2020 Net assets (\$'000s)
Illawarra District Noxious Weed Authority	Administer Noxious Weeds Act Joint Venture	(11)	790

Reasons for non-recognition

Council has assessed this operation as not material for recognition in these statements.

Name of entity/operation	Principal activity/type of entity	2020 Net profit (\$'000s)	2020 Net assets (\$'000s)
Destination Wollongong	Tourism Development & Promotion Associate	104	31

Reasons for non-recognition

Council has assessed this operation as not material for recognition in these statements.

Name of entity/operation	Principal activity/type of entity	2020 Net profit (\$'000s)	2020 Net assets (\$'000s)
Illawarra Performing Arts Centre	Theatre & Town Hall Management Associate	120	479

Reasons for non-recognition

Council has assessed this operation as not material for recognition in these statements.

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 21. Commitments

\$ '000	2020	2019
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(a) Capital commitments (exclusive of GST)

Capital expenditure committed for at the reporting date but not recognised in the financial statements as liabilities:

Property, plant and equipment

Buildings	2,880	387
Infrastructure	2,526	17,975
Land	1,668	110
Total commitments	7,074	18,472

These expenditures are payable as follows:

Within the next year	5,890	18,289
Later than one year and not later than 5 years	1,184	183
Total payable	7,074	18,472

Sources for funding of capital commitments:

Unrestricted general funds	3,077	6,269
Section 94 Funds	3,562	7,172
Future grants and contributions	435	5,031
Total sources of funding	7,074	18,472

Details of capital commitments

Buildings includes Warrawong Community Centre & Library Design, Bulli Tourist Park Amenities Refurbishment, Towradgi SLSC Amenities Refurbishment, Wollongong Heliport Roof Replacement & Austinmer Beach Amenities Refurbishment.

Land includes Wongawilli Road Wongawilli and Fowlers Road Dapto.

Infrastructure includes Wongawilli Road Watermain Installation, Wollongong Waste Recovery Park Stabilisation & Drainage, Uralba Street Bridge Structural Remediation, Towradgi Pool Concourse Slab, Port Kembla Beach Viewing Platform, Kurraba Road Woonona Embankment Rehabilitation, Hayes Lane Bridge, Stephen Drive Woonona Reconstruction, & Whytes Gully Leachate Treatment System.

\$ '000	2020	2019
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(b) Non-cancellable operating lease commitments (2019 only)

a. Commitments under non-cancellable operating leases at the reporting date, but not recognised as liabilities are payable:

Within the next year	–	335
Later than one year and not later than 5 years	–	428
Later than 5 years	–	510
Total non-cancellable operating lease commitments	–	1,273

b. Non-cancellable operating leases include the following assets:

Lease of land for Dapto Ribbonwood Centre and lease of Warrawong Library.

Conditions relating to finance and operating leases:

- All operating lease agreements are secured only against the leased asset.
- No lease agreements impose any financial restrictions on Council regarding future debt etc.

Refer to Note 15 for information relating to leases for 2020.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 22. Contingencies

The following assets and liabilities do not qualify for recognition in the Statement of Financial Position, but their knowledge and disclosure is considered relevant to the users of Council's financial report.

ASSETS NOT RECOGNISED

1. Land under roads

As permitted under AASB 1051, Council has elected not to bring to account land under roads that it owned or controlled up to and including 30/6/08.

2. Various Insurances - HIH & CIC

Council placed or was a party to various liability, property and workers compensation insurance policies with HIH & CIC. During 2000/2001 HIH & CIC went into liquidation. Both HIH & CIC remain responsible for payment of their portion of each Council claim incurred which exceeded Council's insurance excess. Council has recovered \$1,178,421.50 to date. The total of Council's unrecovered claims is currently \$1,119,080.31. At this time, based on the liquidator's estimated final dividend rates, Council expects to recover a further \$52,774.42 from the remaining assets of HIH & CIC.

LIABILITIES NOT RECOGNISED

1. Bank Guarantees

Council has provided Bank Guarantees totalling \$760,547 as security over damages for work that may impact a third party.

Council has provided security to Work Cover for outstanding workers compensation claims liability in the form of a bank guarantee to the sum of \$7,711,000.

Council is also Guarantor on a mortgage for a third party of \$180,000.

2. Defined benefit superannuation contribution plans

Council participates in an employer sponsored Defined Benefit Superannuation Scheme, and makes contributions as determined by the Superannuation Scheme's Trustees.

Member Councils bear responsibility of ensuring there are sufficient funds available to pay out the required benefits as they fall due.

The Schemes most recent full actuarial review indicated that the Net Assets of the Scheme were not sufficient to meet the accrued benefits of the Schemes Defined Benefit member category with member Councils required to make a higher contribution during the reporting period. This amounted to \$1.828M for Council in 2019/20. During January 2020, Local Government Super confirmed that Council's contribution for 2020/21 for past service will be \$1.846M and this has been included in Council's financial forecasts for 2020/21. At this stage there has not been any advice regarding any future extension of the top up payment.

The Local Government Superannuation Scheme however is unable to provide Council with an accurate estimate of its share of the net deficit and accordingly Council has not recorded any net liability from its Defined Benefit Scheme obligations in accordance with AASB 119.

Future contributions made to the defined benefit scheme to rectify the net deficit position will be recognised as an expense when they become payable - similar to the accounting for Defined Contributions Plans.

3. Third party claims

The Council is involved from time to time in various claims incidental to the ordinary course of business including claims for damages relating to its services.

Council believes that it is appropriately covered for all claims through its insurance coverage and does not expect any material liabilities to eventuate.

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 22. Contingencies (continued)

4. Development Contributions

Council levies Development Contributions upon various development across the Council area through the required Contributions Plans. As part of these Plans, Council has received funds for which it will be required to expend the monies in accordance with those Plans.

As well, these Plans indicate proposed future expenditure to be undertaken by Council, which will be funded by making levies and receiving funds in future years or where a shortfall exists by the use of Council's General Funds.

These future expenses do not yet qualify as liabilities as of the Reporting Date, but represent Council's intention to spend funds in the manner and timing set out in those plans.

5. Greenhouse Park

Council owns and manages a former landfill site at Greenhouse Park. The landfill was constructed prior to contemporary environmental regulations and used as both a putrescible and builders waste landfill. Following the closure of the site as a landfill, remediation of the site has been progressively occurring to transform the site into a natural area.

Council is also working with the EPA and specialised consultants to manage the landfill waste which was placed on the site. Total remediation costs at this stage are unknown and will be dependent on the remediation strategies implemented. Council is currently working with the EPA and specialised consultants to determine the remediation actions required.

6. Helensburgh Landfill Site

Council manages a former landfill site at Helensburgh located off Halls Road/Nixon Place. The landfill was commenced prior to contemporary environmental regulations and was used at various times for both putrescible and builders construction waste. Following the closure of the site as a landfill, Council is required to remediate the site in accordance with EPA requirements and licence conditions. Council is currently working with EPA and specialised consultants to prepare the final design documentation for this project. Final future remediation costs at this stage are in the order of \$7.4M to \$11.8M depending on the source of capping materials and the final design solution adopted. Council's current provision is approximately \$7.4M. If a shortfall eventuates this will impact the income statement.

7. Native Title

In January 2018, the National Native Title Tribunal accepted registration of a native title claim that included the Wollongong LGA. The claim is now before the Federal Court, and Council is one of a number of defendants to those proceedings. Private freehold land, and certain other land owned by Council is not affected. The claim will take some time to determine before the Federal Court.

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 23. Financial risk management

Risk management

Council's activities expose it to a variety of financial risks including (1) price risk, (2) credit risk, (3) liquidity risk and (4) interest rate risk.

The Council's overall risk management program focuses on the unpredictability of financial markets and seeks to minimise potential adverse effects on the financial performance of the Council.

Council does not engage in transactions expressed in foreign currencies and is therefore not subject to foreign currency risk.

Financial risk management is carried out by Council's finance section under policies approved by the Council.

The fair value of Council's financial assets and financial liabilities approximates their carrying amount.

Council's objective is to maximise its return on cash and investments whilst maintaining an adequate level of liquidity and preserving capital.

Council's finance area manages the cash and Investments portfolio with the assistance of independent advisors.

Council has an investment policy which complies with the Local Government Act 1993 and Minister's investment order 625. This policy is regularly reviewed by Council and its staff and an investment report is tabled before Council on a monthly basis setting out the portfolio breakup and its performance as required by Local Government regulations.

The risks associated with the instruments held are:

- **Price risk** – the risk that the capital value of Investments may fluctuate due to changes in market prices, whether there changes are caused by factors specific to individual financial instruments or their issuers or are caused by factors affecting similar instruments traded in a market.
- **Interest rate risk** – the risk that movements in interest rates could affect returns and income.
- **Liquidity risk** – the risk that Council will not be able to pay its debts as and when they fall due.
- **Credit risk** – the risk that the investment counterparty will not complete their obligations particular to a financial instrument, resulting in a financial loss to Council – be it of a capital or income nature.

Council manages these risks (amongst other measures) by diversifying its portfolio and only purchasing investments with high credit ratings or capital guarantees.

Council also seeks advice from independent advisers before placing any funds in cash equivalents and investments.

(a) Market risk – price risk and interest rate risk

The following represents a summary of the sensitivity of Council's Income Statement and accumulated surplus (for the reporting period) due to a change in either the price of a financial asset or the interest rates applicable.

It is assumed that the change in interest rates would have been constant throughout the reporting period.

\$ '000	Increase of values/rates		Decrease of values/rates	
	Profit	Equity	Profit	Equity
2020				
Possible impact of a 10% movement in price of investments	4,639	4,639	(4,639)	(4,639)
Possible impact of a 1% movement in interest rates	1,111	1,111	(1,111)	(1,111)
2019				
Possible impact of a 10% movement in price of investments	4,755	4,755	(4,755)	(4,755)
Possible impact of a 1% movement in interest rates	1,072	1,072	(1,072)	(1,072)

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Note 23. Financial risk management (continued)

(b) Credit risk

Council's major receivables comprise (i) rates and annual charges and (ii) user charges and fees.

Council manages the credit risk associated with these receivables by monitoring outstanding debt and employing stringent debt recovery procedures.

There are no significant concentrations of credit risk, whether through exposure to individual customers, specific industry sectors and/or regions.

Council makes suitable provision for doubtful receivables as required and carries out credit checks on most non-rate debtors.

There are no material receivables that have been subjected to a re-negotiation of repayment terms.

Credit risk profile

Receivables – rates and annual charges

Credit risk on rates and annual charges is minimised by the ability of Council to secure a charge over the land relating to the debts – that is, the land can be sold to recover the debt. Council is also able to charge interest on overdue rates and annual charges at higher than market rates which further encourages the payment of debt.

\$ '000	Not yet overdue	< 1 year overdue	1 - 2 years overdue	2 - 5 years overdue	> 5 years overdue	Total
2020						
Gross carrying amount	–	9,989	1,313	864	2,415	14,581
2019						
Gross carrying amount	–	5,975	1,415	939	2,237	10,566

Receivables - non-rates and annual charges

Council applies the simplified approach for non-rates and annual charges debtors to provide for expected credit losses prescribed by AASB 9, which permits the use of the lifetime expected loss provision. To measure the expected credit losses, non-rates and annual charges debtors have been grouped based on shared credit risk characteristics and the days past due.

The loss allowance provision as at 30 June 2020 is determined as follows. The expected credit losses incorporate forward-looking information.

\$ '000	Not yet overdue	0 - 30 days overdue	31 - 60 days overdue	61 - 90 days overdue	> 91 days overdue	Total
2020						
Gross carrying amount	5,577	374	354	431	1,181	7,917
Expected loss rate (%) ¹	2.60%	22.74%	27.44%	21.11%	34.96%	10.50%
ECL provision	145	85	97	91	413	831
2019						
Gross carrying amount	23,982	812	173	157	1,407	26,531
Expected loss rate (%) ¹	0.23%	2.09%	2.31%	7.01%	27.71%	1.80%
ECL provision	55	17	4	11	390	477

⁽¹⁾ Council has adopted a policy in line with the National Code of Conduct for leasing arrangements during COVID-19. The provision has been increased to allow for potential rental waivers as a result of this policy position.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 23. Financial risk management (continued)

(c) Liquidity risk

Payables and borrowings are both subject to liquidity risk – the risk that insufficient funds may be on hand to meet payment obligations as and when they fall due.

Council manages this risk by monitoring its cash flow requirements and liquidity levels and maintaining an adequate cash buffer.

Payment terms can (in extenuating circumstances) also be extended and overdraft facilities utilised as required.

Borrowings are also subject to interest rate risk – the risk that movements in interest rates could adversely affect funding costs and debt servicing requirements. Council manages this risk through diversification of borrowing types, maturities and interest rate structures. The finance team regularly reviews interest rate movements to determine if it would be advantageous to refinance or renegotiate part or all of the loan portfolio.

The timing of cash flows presented in the table below to settle financial liabilities reflects the earliest contractual settlement dates. The timing of expected outflows is not expected to be materially different from contracted cashflows.

\$ '000	Weighted average interest rate	Without defined maturity	≤ 1 Year	payable in: 1 - 5 Years	> 5 Years	Total cash outflows	Actual carrying values
2020							
Trade/other payables	0.00%	4,054	24,161	–	–	28,215	28,215
Loans and advances	1.50%	–	8,814	13,134	–	21,948	17,699
Total financial liabilities		4,054	32,975	13,134	–	50,163	45,914
2019							
Trade/other payables	0.00%	3,758	26,175	385	–	30,318	30,391
Loans and advances	1.50%	–	8,814	21,947	–	30,761	25,431
Total financial liabilities		3,758	34,989	22,332	–	61,079	55,822

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 24. Material budget variations

Council's original financial budget for 2019/20 was adopted by the Council on 24/06/2019 and is unaudited.

While the Income Statement included in this General Purpose Financial Statements must disclose the original budget adopted by Council, the Local Government Act 1993 requires Council to review its financial budget on a quarterly basis, so that it is able to manage the various variations between actuals versus budget that invariably occur throughout the year.

This note sets out the details of **material variations** between Council's original budget and its actual results for the year as per the Income Statement – even though such variations may have been adjusted for during each quarterly budget review.

Material variations represent those variances between the original budget figure and the actual result that amount to **10%** or more.

Variation Key: **F** = Favourable budget variation, **U** = Unfavourable budget variation.

\$ '000	2020 Budget	2020 Actual	2020 ----- Variance -----	
REVENUES				
Rates and annual charges	202,991	205,118	2,127	1% F
User charges and fees	33,665	28,695	(4,970)	(15)% U
User Charges & Fees is lower due to lower Commercial Tipping Income (\$1.8M), and impacts from the COVID-19 pandemic closures on operational areas including Leisure Centres (\$0.6M), Tourist Parks (\$0.5M), Park and Sportsfield income (\$0.3M), Car Parking fees (\$0.1) and other operational areas. In addition there has been the reclassification of vehicle leaseback income (\$0.8M) and Community Facility hire income (\$0.6M) as Rental Income.				
Other revenues	10,598	4,794	(5,804)	(55)% U
Other Revenues have a variance due to reclassification of Property Leasing Income as Rental Income (\$5.0M), lower levels of parking infringements issued (\$0.7M) and other more minor offsetting variations impacted by the impact of the COVID-19 pandemic.				
Operating grants and contributions	20,840	30,727	9,887	47% F
Operating grants is affected by the early receipt of two instalments of the Financial Assistance Grant relating to the 2020/21 financial year (\$9.5M) and additional grants received during the year.				
Capital grants and contributions	42,927	47,472	4,545	11% F
Capital grants and contributions are impacted by lower West Dapto developer contributions (\$25.7M), partially offset by contributed assets (\$14.7M), additional developer contributions in other areas (\$3.8M), additional grants including the National Stronger Regions (\$2.0M) and other more minor amounts relating to civil assets.				
Interest and investment revenue	4,882	3,360	(1,522)	(31)% U
Interest & investment income is lower due to the financial market impact of lower interest rates and the revaluation of fair value investment movements impacted from the COVID-19 pandemic.				
Fair value increment on investment property	–	–	–	∞ F
Rental income	–	6,735	6,735	∞ F
The reclassification of rental income under the new accounting standards was not included in the original budget. It is offset largely in Other Revenue (\$5.0M) and User Charges and Fees (\$1.4M) as well as more minor variations.				
Joint ventures and associates – net profits	–	555	555	∞ F
This variance is due to the positive revaluation of the holding in the CivicRisk insurance joint venture.				

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 24. Material budget variations

\$ '000	2020 Budget	2020 Actual	2020 ----- Variance -----	
EXPENSES				
Employee benefits and on-costs	117,908	122,912	(5,004)	(4)% U
Borrowing costs	2,722	3,052	(330)	(12)% U
Borrowing Costs were underestimated for the remediation of the Whytes Gully Waste Facility.				
Materials and contracts	60,072	60,934	(862)	(1)% U
Depreciation and amortisation	66,276	60,845	5,431	8% F
Other expenses	35,426	36,497	(1,071)	(3)% U
Net losses from disposal of assets	–	5,686	(5,686)	∞ U
This budget variation is largely a result of the disposal of civil assets that have residual values. These predominantly include transport (\$2.7M), buildings (\$2.1M), stormwater (\$1.4M) and other more minor items partially offset by the gain on disposal of the Southern Phone shareholding (\$0.7M).				
Fair value decrement on investment property	–	1,998	(1,998)	∞ U
During the 2019/20 financial year, the cost of capital works were added to the value of the investment property. The valuation of the property did not change from the prior financial year as the valuation is based on the market value of the property including future expected cashflow.				
STATEMENT OF CASH FLOWS				
Cash flows from operating activities	98,894	102,726	3,832	4% F
Cash flows from investing activities	(97,751)	(63,615)	34,136	(35)% F
In response to the COVID-19 pandemic to ensure Council had adequate funds to meet cashflow requirements, Council did not enter into any new investments in the last quarter of the financial year. This has resulted in a lower than anticipated value for the purchase of investment securities offset by an increase in the value of the cash and cash equivalents that is not captured within the cash flows from investing activities.				
Cash flows from financing activities	(7,913)	(8,247)	(334)	4% U

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement

Council measures the following asset and liability classes at fair value on a recurring basis:

- Infrastructure, property, plant and equipment
- Investment property
- Financial assets and liabilities

The fair value of assets and liabilities must be estimated in accordance with various accounting standards for either recognition and measurement requirements or for disclosure purposes.

AASB 13 *Fair Value Measurement* requires all assets and liabilities measured at fair value to be assigned to a 'level' in the fair value hierarchy as follows:

Level 1: Unadjusted quoted prices in active markets for identical assets or liabilities that the entity can access at the measurement date.

Level 2: Inputs other than quoted prices included within level 1 that are observable for the asset or liability, either directly or indirectly.

Level 3: Inputs for the asset or liability that are not based on observable market data (unobservable inputs).

(1) Assets and liabilities that have been measured and recognised at fair values

2020	Date of latest valuation	Fair value measurement hierarchy			Total
		Level 1 Quoted prices in active mkt	Level 2 Significant observable inputs	Level 3 Significant unobservable inputs	
Recurring fair value measurements					
Financial assets					
Investments					
– ‘Designated at fair value on initial recognition’	30/6/20	–	–	46,393	46,393
– ‘Financial assets at fair value through other comprehensive income’	N/A	–	–	–	–
Total financial assets		–	–	46,393	46,393
Investment property					
Commercial building	30/6/20	–	–	5,000	5,000
Total investment property		–	–	5,000	5,000
Infrastructure, property, plant and equipment					
Roads	30/6/17	–	–	610,258	610,258
Bridges	30/6/17	–	–	118,822	118,822
Footpaths	30/6/17	–	–	158,235	158,235
Stormwater	30/6/17	–	–	530,148	530,148
Plant and equipment	N/A	–	–	21,403	21,403
Office equipment	N/A	–	–	3,282	3,282
Furniture and fittings	N/A	–	–	1,931	1,931
Operational land	30/6/19	–	–	249,821	249,821
Community land	30/6/20	–	–	294,267	294,267
Crown Land	30/6/20	–	–	103,057	103,057
Land under roads	30/6/19	–	–	5,970	5,970
Buildings	30/6/19	–	–	301,035	301,035
Other structures	30/6/19	–	–	9,246	9,246
Swimming pools	30/6/16	–	–	9,363	9,363
Library books	N/A	–	–	4,562	4,562
Other open space/recreational assets	30/6/16	–	–	113,820	113,820
Tip asset	30/6/20	–	–	–	–
Works in progress	N/A	–	–	54,017	54,017
Artworks	30/6/19	–	–	15,415	15,415
Other	30/6/16	–	–	50,835	50,835
Total infrastructure, property, plant and equipment		–	–	2 655 487	2 655 487

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

2019	Date of latest valuation	Fair value measurement hierarchy			Total
		Level 1 Quoted prices in active mkts	Level 2 Significant observable inputs	Level 3 Significant unobservable inputs	
Recurring fair value measurements					
Financial assets					
Investments					
– ‘Designated at fair value on initial recognition’	30/06/19	–	–	47,549	47,549
– ‘Financial assets at fair value through other comprehensive income’	30/06/19	–	–	56	56
Total financial assets		–	–	47,605	47,605
Investment property					
Commercial building	30/06/19	–	–	5,000	5,000
Total investment property		–	–	5,000	5,000
Infrastructure, property, plant and equipment					
Roads	30/06/17	–	–	569,999	569,999
Bridges	30/06/17	–	–	98,739	98,739
Footpaths	30/06/17	–	–	147,190	147,190
Stormwater	30/06/17	–	–	520,636	520,636
Plant and equipment	N/A	–	–	22,691	22,691
Office equipment	N/A	–	–	3,376	3,376
Furniture and fittings	N/A	–	–	2,097	2,097
Operational land	30/06/19	–	–	249,843	249,843
Community land	30/06/16	–	–	337,871	337,871
Crown Land		–	–	–	–
Land under roads	30/06/19	–	–	5,684	5,684
Buildings	30/06/19	–	–	303,164	303,164
Other structures	30/06/19	–	–	7,482	7,482
Swimming pools	30/06/16	–	–	6,331	6,331
Library books	N/A	–	–	4,496	4,496
Other open space/recreational assets	30/06/16	–	–	110,423	110,423
Tip asset	30/06/19	–	–	5,525	5,525
Works in progress	N/A	–	–	119,950	119,950
Artworks	30/06/19	–	–	15,121	15,121
Other	30/06/16	–	–	34,477	34,477
Total infrastructure, property, plant and equipment		–	–	2,565,095	2,565,095

Note that capital WIP is not included above since it is carried at cost.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

(2) Valuation techniques used to derive level 2 and level 3 fair values

Where Council is unable to derive Fair Valuations using quoted market prices of identical assets (i.e. Level 1 inputs), Council instead utilises a spread of both observable inputs (Level 2 inputs) and unobservable inputs (Level 3 inputs).

Level 2 inputs include;

- quoted prices for similar assets in active markets,
- quoted prices for identical or similar assets in markets that are not active,
- inputs other than quoted prices that are observable (e.g. interest rates, credit spreads etc.) and
- market corroborated inputs.

Level 3 inputs are unobservable inputs. If an observable input (Level 2) requires an adjustment using an unobservable input and that adjustment results in a significantly higher or lower fair value measurement, the resulting measurement is categorised within Level 3 of the fair value hierarchy. Council uses unobservable inputs to the extent relevant observable inputs are not available. But the objective remains the same; i.e. an exit price from the perspective of market participants. Therefore, unobservable inputs reflect the assumptions market participants would use when pricing, including assumptions about risk. Assumptions about risk include risk inherent in a particular valuation technique and risk inherent in inputs to the technique. Such an adjustment may be necessary if there is a significant measurement uncertainty.

Unobservable inputs have been developed using the best information available, which includes Council's own data. In some cases, Council adjusts its own data if reasonable available information indicates other market participants would use different data or if there is an entity specific synergy (i.e. not available to other market participants).

Level 3 inputs include;

- Unit Rates,
- Unit Price,
- Asset Condition,
- Remaining Useful Life,
- Future Demands,
- Borrowing Rates

The Fair Valuation techniques Council has employed while utilising Level 2 and Level 3 inputs are as follows:

Financial assets

Valuation Technique – A portion of Council's investment portfolio is measured at fair value (i.e. market approach).

Fair Value Hierarchy – the fair value has been derived from the current price in an active market for similar assets. Emerald Reverse Mortgage investment securities form part of this portion of Council's portfolio. The market for Australian mortgage backed securities, regardless of the robustness of the structure, is highly illiquid as a direct consequence of the global financial crisis. This has caused difficulties in valuing the security as there is limited "price discovery" in the market. As such the level of valuation input for Council's fair valued investments was considered a Level 3.

Investment property

Valuation Technique – Council's Investment Property is measured using sales direct comparison based on a market selling approach (i.e. market approach).

Fair Value Hierarchy - the fair value has been derived from the sales prices of comparable properties after adjusting for differences in key attributes such as property size.
The most significant inputs into this valuation approach are rental yields and price per square metre.

The level of evidence to support the critical assumptions of Council's investment property valuation was considered to be significant due to high levels of variability in the market for rental yields and future demands. As such the level of valuation input for these properties was considered level 3.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

Infrastructure, property, plant and equipment (IPP&E)

Infrastructure – Council's Infrastructure incorporates:

- Roads - Surface and bases, Car Parks, Kerb and Guttering and Traffic Facilities (speed humps, bollards and signs),
- Bridges – Road, Pedestrian and Jetties,
- Footpaths including shared pathways and
- Stormwater Drainage

Valuation Technique – Infrastructure assets are recognised using the cost method, which equates to the current replacement cost of a modern equivalent asset. The cost to replace the asset is to equal the amount that a market participant buyer of that asset would pay to acquire it.

Fair Value Hierarchy - the general valuation approach to determine the fair value of Council's infrastructure inventory is to determine a unit rate based on square metres or an appropriate unit corroborated by market evidence (Level 2 input). A process is then undertaken to compare these rates with internal unit rates derived by Council as a result of specific projects that have been undertaken. Further to this other input such as asset condition and useful life require a significant level of professional judgement and can impact significantly on the fair value. As such the level of valuation input for these assets was considered level 3.

Property Plant & Equipment, Office Equipment & Furniture & Fittings incorporate:

- Property, Plant & Equipment – Trucks, Tractors, Graders, Rollers, Buses, Vans, Passenger Vehicles, Mobile Equipment (i.e. generators, hand mowers, tools), Fluid storage units (i.e. septic tanks, water tanks),
- Office Equipment – Electronic Whiteboards, Printing Equipment, Safes and I.T. equipment such as computers, printers and scanners,
- Furniture & Fittings – Chairs, Tables, Filing Cabinets, Bookshelves, Compactuses,

Valuation Technique – These assets are recognised at depreciated historical cost as an acceptable substitute for fair value because any difference between fair value and depreciated historical cost is unlikely to be material.

Fair Value Hierarchy – The key unobservable unit to the valuation of this category is asset condition and useful life. The condition of assets is reviewed on an annual basis and an assessment of remaining life undertaken based on these results.

Operational Land

Valuation Technique – Council's Operational Land is measured using a comparative market selling approach (i.e. market approach).

Fair Value Hierarchy – the fair value has been derived from the sales prices of comparable properties after adjusting for differences in key attributes such as property size.

The most significant inputs into this valuation approach is price per square metre. The level of evidence to support the critical assumptions of Council's operational land valuation was considered to be significant due to high levels of variability in the market for similar properties and future demands. As such the level of valuation input for these properties was considered level 3.

Community & Crown Land

Valuation Technique – Council's Community & Crown Land is measured using comparative Land Values (LV) provided by the Valuer General (VG) or an average unit rate based on a comparable LV for similar properties (i.e. market approach).

Fair Value Hierarchy – the fair value has been derived from the LV's provided by the Valuer General or an average unit rate based on the LV for similar properties where the Valuer General did not provide a LV. The most significant input into this valuation approach is price per square metre. Valuations provided by the Valuer General are not in the public domain and the application of an average rate requires a level of professional judgement. As such the level of valuation input for these properties was considered level 3.

Wollongong City Council

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Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

Land Under Roads

Valuation Technique – Land is generally valued using comparative Land Values (LV) provided by the Valuer General (VG) or an average unit rate based on a comparable LV for similar properties (i.e. market approach).

Fair Value Hierarchy – The existing use fair value of land under roads is best expressed as undeveloped or en globo land (pre-subdivision). However, as sufficient sales evidence of en globo land with similar features to the land being valued is generally not available, it is appropriate to use a proxy to estimate the en globo value. Community land value is used as a reasonable proxy to value land under roads, as such land generally has no feasible alternative use, and it is undeveloped and is publicly accessible.

As such the level of valuation input for these properties was considered level 3.

Buildings – Non-Specialised and Specialised

Valuation Technique – Buildings are recognised using the cost method, which equates to the current replacement cost of a modern equivalent asset. The cost to replace the asset is equal to the amount that a market participant buyer of that asset would pay to acquire it.

Fair Value Hierarchy – Specialised and Non-Specialised buildings are generally assessed at level 3 of the fair value hierarchy due to lack of market evidence. Key inputs are unit rates and remaining useful life. The exception is non-specialised residential properties which have been valued using sale prices of comparable properties (level 2).

The most significant input into this valuation approach is price per square metre. The level of evidence to support the critical assumptions of Council's residential property valuation was considered to be significant due to high levels of variability in the market for rental yields and future demands. As such the level of valuation input for all buildings was considered level 3.

Intangible Assets

Valuation Technique – These assets are recognised at depreciated historical cost as an acceptable substitute for fair value because any difference between fair value and depreciated historical cost is unlikely to be material.

Fair Value Hierarchy – The key unobservable unit to the valuation of this category is asset condition and useful life. The condition of assets is reviewed on an annual basis and an assessment of remaining life undertaken based on these results.

Other Structures

Other Structures incorporates Bus Shelters, Shade Structures, Picnic Shelters and BBQ Shelters.

Valuation Technique – Other Structures are recognised at depreciated historical cost as an acceptable substitute for fair value because any difference between fair value and depreciated historical cost is unlikely to be material.

Fair Value Hierarchy – The key unobservable unit to the valuation of this category is asset condition and useful life. The condition of assets is reviewed on an annual basis and an assessment of remaining life undertaken based on these results.

Other Open Space / Recreational Assets

Other Open Space/Recreational Assets incorporate Off-road Footpaths, Park Assets including Playgrounds, Skateboard Facilities, Tennis Courts, Furniture and Landscaping and Power Poles.

Valuation Technique – Other Open Space/Recreational Assets are recognised using the cost method (e.g. Off-Road Footpaths).

Fair Value Hierarchy – while some elements of the cost method can be supported by market evidence (Level 2) other factors require professional judgement such as asset condition and useful life. As these inputs can have a significant impact on the fair value the valuation input all Other Open Space / Recreational Assets were considered level 3.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

Swimming Pools – Structures

Valuation Technique – Swimming Pools and Rock Pools are valued using the cost approach, which equates to the current replacement cost of a modern equivalent asset.

The cost to replace the asset is to equal the amount that a market participant buyer of that asset would pay to acquire it. External Valuer Nannes Fong undertook the original valuation in 2009. An internal valuation was undertaken during the 2016 financial year.

Fair Value Hierarchy - the general valuation approach to determine the fair value of Council's swimming pool inventory is to determine a unit rate based on square metres corroborated by market evidence (Level 2 input). A process is then undertaken to compare these rates with internal unit rates derived by Council as a result of specific work that has been undertaken. Further to this other input such as asset condition and useful life require a significant level of professional judgement and can impact significantly on the fair value. As such the level of valuation input for these properties was considered level 3.

Artworks

Valuation Technique – Art Works are valued using the cost approach, which equates to the current replacement cost of a modern equivalent asset. The cost to replace the asset is to equal the amount that a market participant buyer of that asset would pay to acquire it.

Fair Value Hierarchy - the general valuation approach to determine the fair value of Council's Artworks is to use the market price or purchase price of the original transaction or if the work is in the form of a donation an external valuation is undertaken corroborated by market evidence (Level 2 input). It is noted that the valuation process requires a significant level of professional judgement and this can impact significantly on the fair value. As such the level of valuation input for artworks was considered level 3.

Library Books

Valuation Technique – These assets are recognised at depreciated historical cost as an acceptable substitute for fair value because any difference between fair value and depreciated historical cost is unlikely to be material.

Fair Value Hierarchy – The key unobservable unit to the valuation of this category is asset condition and useful life. The condition of assets is reviewed on an annual basis and an assessment of remaining life undertaken based on these results.

Other Assets

Other Assets is a catch all for the remaining assets held by Council and includes Waste Assets such as Cell Development and Liners, Public Art, Power Poles and Crematorium and Cemetery Beams and Walls.

Valuation Technique - Other Assets are recognised using the cost method.

Fair Value Hierarchy – While some elements of the cost method can be supported by market evidence (Level 2) others factors require professional judgement such as asset condition and useful life. As these inputs can have a significant impact on the fair value the valuation input all Other Assets were considered level 3.

Tip Remediation Asset

Valuation Technique – Council's Tip Remediation Asset is measured using the cost method.

Fair Value Hierarchy – Whytes Gully Waste Disposal Depot will require remediation and restoration works to be carried out during and at the end of its useful life. The cash outflows relating to these remediation and restoration works have been modelled and recognised as an asset in Note 11 of Council's statements. Key unobservable inputs were the discount rate, cost escalation rate, timing of costs and future environmental management requirements. As such the level of valuation input for Council's tip asset was considered a Level 3.

The tip remediation asset was adjusted in line with changes to the remediation provision. During 2019-2020, the remediation provision was reduced to by an amount greater than the carrying value of the tip remediation and as a result this asset now has a carrying value of zero.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

A summary of the Fair Valuation techniques Council has employed while utilising Level 2 and Level 3 inputs are as follows:

Asset Category	Valuation Frequency	Description of Process	Valuer*	Responsibility
Financial Assets	Monthly	Monthly valuation using the current price in an active market for similar assets	External	Finance Division
Investment Properties	Annually	Assessed each year for material change and adjusted accordingly	External	Property Division
Infrastructure	5 years	Full valuation using current unit rates and comparable work. Assessed each year for material change and adjusted accordingly	Internal	Infrastructure & Strategic Planning Division
Property Plant & Equipment	Annually	Assessment of remaining useful life undertaken with adjustments to consumption patterns that may impact fair value	Internal	Finance Division
Office Equipment & Furniture & Fittings	Annually	Assessment of remaining useful life undertaken with adjustments to consumption patterns that may impact fair value	Internal	Finance Division
Operational Land	5 Years	Full valuation every 5 years or index applied annually if material	External	Property Division
Community & Crown Land	5 Years	Valuer General Land Values or Average Unit Rate for similar properties if not available	Valuer General / Internal	Property / Finance Division
Land Under Roads	5 Years	Valuer General Land Values or Average Unit Rate used as proxy to derive en globo rate	Valuer General / Internal	Finance Division
Buildings – Non Specialised & Specialised	5 Years	Full valuation every 5 years or index applied annually if material	External / Internal	Infrastructure & Strategic Planning Division / Property Division
Intangibles	5 Years	Assessment of remaining useful life undertaken with adjustments to consumption patterns that may impact fair value	Internal	Finance Division
Other Structures	5 Years	Assessment of remaining useful life undertaken with adjustments to consumption patterns that may impact fair value	Internal	Infrastructure & Strategic Planning Division

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Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

Asset Category	Valuation Frequency	Description of Process	Valuer*	Responsibility
Other Open Space / Recreational Assets	5 Years	Full valuation every 5 years or index applied annually if material	Internal	Infrastructure & Strategic Planning Division
Swimming Pools - Structures	5 Years	Full valuation every 5 years or index applied annually if material	External / Internal	Infrastructure & Strategic Planning Division
Library Books	Annually	Assessment of remaining useful life undertaken with adjustments to consumption patterns that may impact fair value	Internal	Finance Division
Other Assets	5 years	Full valuation every 5 years or index applied annually if material	Internal	Various
Tip Remediation Asset	Annually	Reassessment of discount rate and application to discounted cash flows if material	Internal	Finance Division

*Internal Valuation refers to the utilisation of in-house expertise to value Council's assets.

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements for the year ended 30 June 2020

Note 25. Fair Value Measurement (continued)

(3) Fair value measurements using significant unobservable inputs (level 3)

a. The following tables present the changes in level 3 fair value asset classes.

\$ '000	Financial assets	Investment property	IPP&E	Total
2019				
Opening balance	43,162	4,780	2,356,306	2,404,248
Purchases (GBV)	14,299	–	123,229	137,528
Disposals (WDV)	(10,021)	–	(8,775)	(18,796)
Depreciation and impairment	–	–	(61,549)	(61,549)
FV gains – other comprehensive income	–	–	196,818	196,818
FV gains – Income Statement ⁽¹⁾	165	220	–	385
Prior Period Error	–	–	(39,225)	(39,225)
Waste remediation reassessment	–	–	(58)	(58)
Transfer to expense	–	–	(1,651)	(1,651)
Closing balance	47,605	5,000	2,565,095	2,617,700
2020				
Opening balance	47,605	5,000	2,565,095	2,617,700
Purchases (GBV)	9,177	1,998	108,915	120,090
Disposals (WDV)	(10,106)	–	(7,374)	(17,480)
Depreciation and impairment	–	–	(60,277)	(60,277)
FV gains – other comprehensive income	–	–	57,580	57,580
FV gains – Income Statement ⁽¹⁾	(283)	(1,998)	–	(2,281)
Prior Period Error	–	–	5,003	5,003
Waste remediation reassessment	–	–	(5,407)	(5,407)
Transfer to expense	–	–	(8,048)	(8,048)
Closing balance	46,393	5,000	2,655,487	2,706,880

(1) FV gains recognised in the Income Statement relating to assets still on hand at year end total

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 26. Related Party Transactions

(a) Key management personnel

Key management personnel (KMP) of the council are those persons having the authority and responsibility for planning, directing and controlling the activities of the council, directly or indirectly.

The aggregate amount of KMP compensation included in the Income Statement is:

\$'000s	2020	2019
Compensation:		
Short-term benefits	2,221	2,239
Termination benefits	-	42
Total	2,221	2,281

(b) Other transactions with KMP and their related parties

Council has assessed other transactions with KMP and their related parties as not material for recognition in these statements.

(c) Other related party transactions

\$'000s	Value of transactions during the year	Outstanding balance	Terms and conditions
2020			
Associate			
Marketing, events, business and investment in LGA	1,436	-	Amounts provided under a funding agreement.
Event sponsorship & support	97	-	Based on specific events.
Advertisement	4	-	
Partner program	2	-	
Associate			
Performing Arts Centre Management	718	-	Amounts provided under a funding agreement.
Town Hall management	395	-	Amounts provided under a funding agreement.
Asset Maintenance	41	-	
Community Subsidy	20	-	Amounts provided under a funding agreement.
Venue Hire	13	-	

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 26. Related Party Transactions (continued)

\$'000s	Value of transactions during the year	Outstanding balance	Terms and conditions
2019			
Associate			
Marketing, events, business and investment in LGA	1,419	-	Amounts provided under a funding agreement.
Event sponsorship & support	71	-	Based on specific events.
LGA Major Events	10	-	Amounts provided under a funding agreement.
Partner program	6	-	
Merchandise	1	-	
Associate			
Performing Arts Centre Management	1,006	-	Amounts provided under a funding agreement.
Town Hall management	388	-	Amounts provided under a funding agreement.
Community Subsidy	20	-	Amounts provided under a funding agreement.
Venue hire	11	-	
Asset Maintenance	9	-	
Catering	2	-	

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 27. Events occurring after the reporting date

Council is aware of the following 'non-adjusting events' that merit disclosure:

Amalgamation of Civic Risk

From 1st July 2020, the Civic Risk entities currently disclosed as joint ventures will legally combine into CivicRisk Mutual Ltd, a company limited by guarantee. Through this process the three joint ventures will novate their assets, liabilities and members surpluses into CivicRisk Mutual Ltd. The new entity will be recognised as a passive interest financial asset from 1st July 2020.

Note 28. Statement of developer contributions

Under the *Environmental Planning and Assessment Act 1979*, Council has significant obligations to provide Section 7.11 (contributions towards provision or improvement of amenities or services) infrastructure in new release areas.

It is possible that the funds contributed may be less than the cost of this infrastructure, requiring Council to borrow or use general revenue to fund the difference.

Summary of contributions and levies

\$ '000	as at 30/06/19			Interest earned in year	Expenditure during year	Internal borrowing (to)/from	as at 30/06/20	
	Opening Balance	Contributions received during the year Cash	Non-cash				Held as restricted asset	Cumulative internal borrowings due/(payable)
Drainage	6,694	1,392	–	132	–	–	8,218	–
Roads	(647)	10,671	–	(47)	(17,472)	–	(7,495)	–
Open space	7,271	973	–	141	–	–	8,385	–
Community facilities	1,220	21	–	23	–	–	1,264	–
Administration	(1,176)	248	–	(24)	(255)	–	(1,207)	–
Public transport	625	–	–	12	–	–	637	–
S7.11 contributions – under a plan	13,987	13,305	–	237	(17,727)	–	9,802	–
S7.12 levies – under a plan	22,281	4,806	–	429	(2,731)	–	24,785	–
Total S7.11 and S7.12 revenue under plans	36,268	18,111	–	666	(20,458)	–	34,587	–
S7.11 not under plans	347	–	–	6	–	–	353	–
S7.4 planning agreements	153	–	1,272	6	–	–	159	–
Total contributions	36,768	18,111	1,272	678	(20,458)	–	35,099	–

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Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 28. Statement of developer contributions (continued)

	as at 30/06/19						as at 30/06/20	
	Opening Balance	Contributions received during the year		Interest earned in year	Expenditure during year	Internal borrowing (to)/from	Held as restricted asset	Cumulative internal borrowings due/(payable)
\$ '000		Cash	Non-cash					
S7.11 Contributions – under a plan								
CONTRIBUTION PLAN – WEST DAPTO								
Drainage	6,694	1,392	–	132	–	–	8,218	–
Roads	(647)	10,671	–	(47)	(17,472)	–	(7,495)	–
Open space	7,271	973	–	141	–	–	8,385	–
Community facilities	1,220	21	–	23	–	–	1,264	–
Administration	(1,176)	248	–	(24)	(255)	–	(1,207)	–
Public transport	625	–	–	12	–	–	637	–
Total	13,987	13,305	–	237	(17,727)	–	9,802	–
S7.12 Levies – under a plan								
CONTRIBUTION PLAN – WOLLONGONG ^{A, B}								
City Wide	21,094	2,878	–	395	(2,731)	–	21,636	–
City Centre	1,187	1,928	–	34	–	–	3,149	–
Total	22,281	4,806	–	429	(2,731)	–	24,785	–

(A) The Wollongong City-Wide Development Contributions Plan is a levy based plan that reflects development activity in the Local Government Area excluding areas covered by the West Dapto Section 94 Plan.

(B) Figures provided include amounts collected under the Wollongong City-Wide Development Plan as well as contributions received from relevant development consents approved prior to 2006 that contained conditions for contributions to be made, under now repealed Development Contribution plans. These are transferred and applied towards items within the Section 7.12 Plan works schedule as the Section 7.12 Plan is the replacement for the plans repealed in June 2006.

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Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 28. Statement of developer contributions (continued)

	as at 30/06/19			as at 30/06/20				
	Opening Balance	Contributions received during the year		Interest earned in year	Expenditure during year	Internal borrowing (to)/from	Held as restricted asset	Cumulative internal borrowings due/(payable)
\$ '000		Cash	Non-cash					
S7.11 Contributions – not under a plan								
CONTRIBUTION PLAN – CALDERWOOD ^{A, B, C}								
Roads	347	–	–	6	–	–	353	–
Total	347	–	–	6	–	–	353	–

(A) The Calderwood Urban Release Area was historically in the Shellharbour City Local Government Area. However, the Urban Release Area was later expanded during the State Government's major project approval process to include land that straddles the local government boundary, which comprises 107 hectares of land in the Wollongong Local Government Area at Marshall Mount.

(B) In 2013, the Land and Environment Court imposed a development contribution condition on Stage 1 of the Calderwood development within Shellharbour City Council Local Government Area, requiring the payment of a contribution of \$1,320 per lot to Wollongong City Council to be used towards upgrades of Marshall Mount Road and Yallah Road. The payments reflected in the above relate to these court proceedings.

(C) Wollongong City Council and Lendlease Communities (Australia) Limited have entered into a Planning Agreement for the remainder of the contributions relating to the Calderwood development. No contributions have been received as a result of the Planning Agreement.

S7.4 planning agreements

S7.4 planning agreements

Roads	153	–	1,272	6	–	–	159	–
Total	153	–	1,272	6	–	–	159	–

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 29. Result by fund

Council utilises only a general fund for its operations.

Note 30(a). Statement of performance measures – consolidated results

\$ '000	Amounts 2020	Indicator 2020	Prior periods 2019 2018		Benchmark
1. Operating performance ratio					
Total continuing operating revenue excluding capital grants and contributions less operating expenses ^{1,2}	(4,531)	(1.62)%	(0.48)%	2.76%	>0.00%
Total continuing operating revenue excluding capital grants and contributions ¹	279,709				
2. Own source operating revenue ratio					
Total continuing operating revenue excluding all grants and contributions ¹	248,982	76.10%	73.20%	77.06%	>60.00%
Total continuing operating revenue ¹	327,181				
3. Unrestricted current ratio					
Current assets less all external restrictions ³	114,624	2.36x	2.70x	2.93x	>1.50x
Current liabilities less specific purpose liabilities ⁴	48,474				
4. Debt service cover ratio					
Operating result before capital excluding interest and depreciation/impairment/amortisation ¹	59,366	5.25x	5.77x	6.30x	>2.00x
Principal repayments (Statement of Cash Flows) plus borrowing costs (Income Statement)	11,299				
5. Rates, annual charges, interest and extra charges outstanding percentage					
Rates, annual and extra charges outstanding	14,581	6.74%	5.09%	4.66%	<5.00%
Rates, annual and extra charges collectible	216,331				
6. Cash expense cover ratio					
Current year's cash and cash equivalents plus all term deposits	111,081	5.63 mths	5.45 mths	5.83 mths	>3.00 mths
Monthly payments from cash flow of operating and financing activities	19,736				

(1) Excludes fair value adjustments, reversal of revaluation decrements, net gain/(loss) on sale of assets, and net loss of interests in joint ventures and associates.

(2) Excludes impairment/revaluation decrements, net loss on sale of assets, and net loss on share of interests in joint ventures and associates

(3) Refer to Notes 7-9 inclusive. Excludes any real estate and land for resale not expected to be sold in next 12 months.

(4) Refer to Notes 16 and 17. Excludes all payables and provisions not expected to be paid in the next 12 months (incl. ELE).

Wollongong City Council

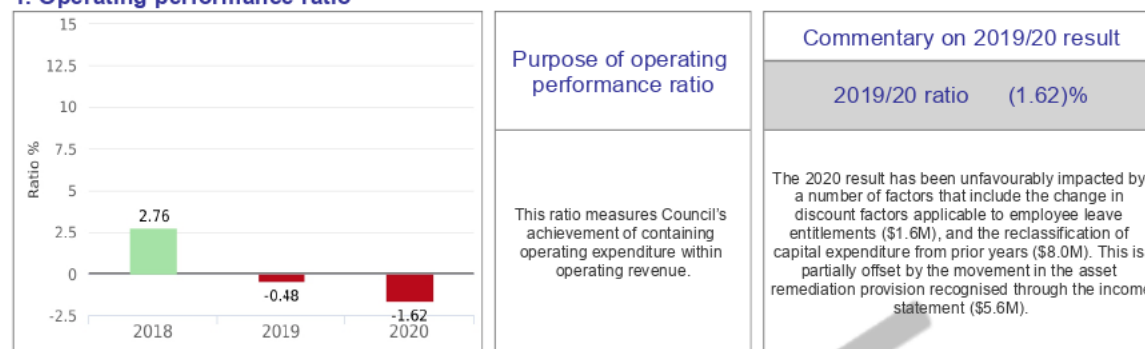
Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 30(b). Statement of performance measures – consolidated results (graphs)

1. Operating performance ratio

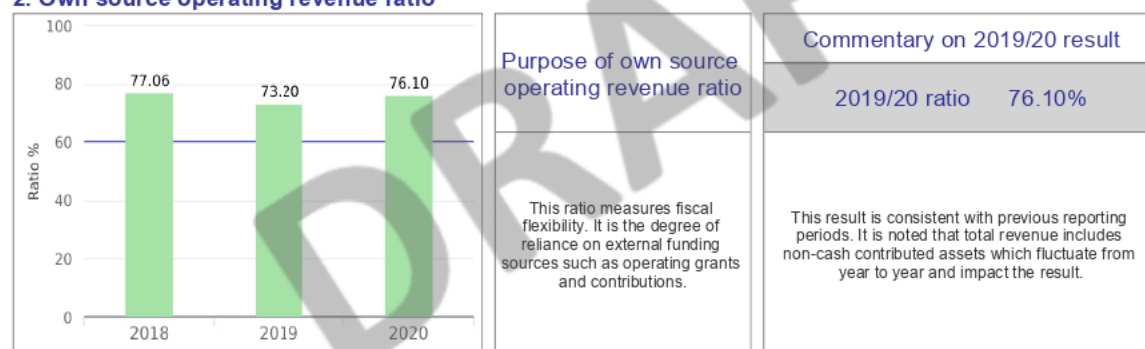


Benchmark: — > 0.00%

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Ratio achieves benchmark
Ratio is outside benchmark

2. Own source operating revenue ratio

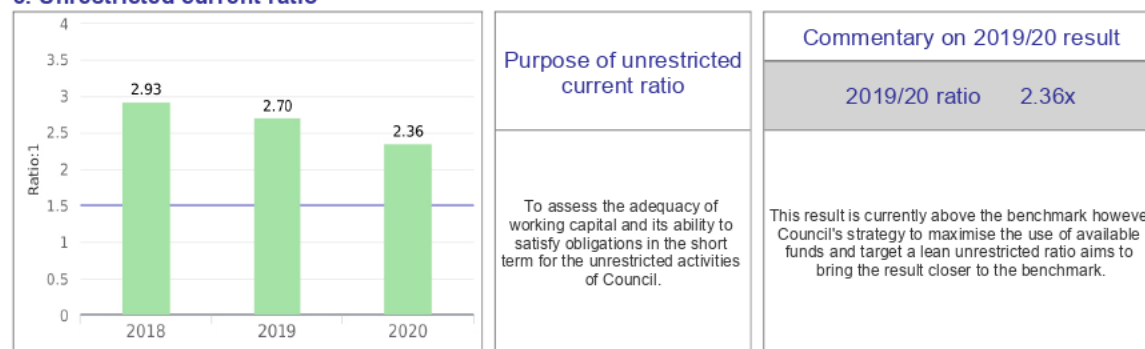


Benchmark: — > 60.00%

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Ratio achieves benchmark
Ratio is outside benchmark

3. Unrestricted current ratio



Benchmark: — > 1.50x

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Ratio achieves benchmark
Ratio is outside benchmark

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Wollongong City Council

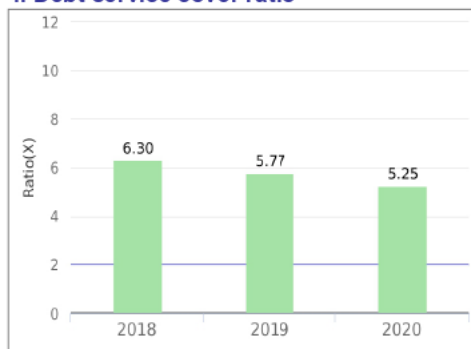
Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 30(b). Statement of performance measures – consolidated results (graphs)

4. Debt service cover ratio



Benchmark: — > 2.00x

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Purpose of debt service cover ratio

This ratio measures the availability of operating cash to service debt including interest, principal and lease payments

Commentary on 2019/20 result

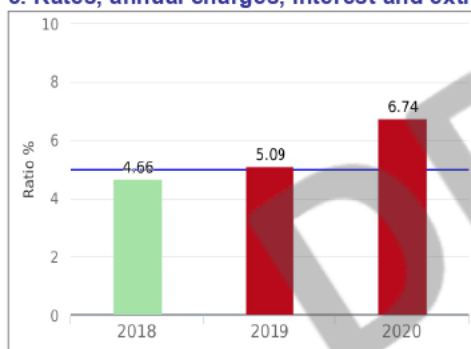
2019/20 ratio 5.25x

The result indicates that Council can adequately service its outstanding debt.

Ratio achieves benchmark

Ratio is outside benchmark

5. Rates, annual charges, interest and extra charges outstanding percentage



Benchmark: — < 5.00%

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Purpose of rates, annual charges, interest and extra charges outstanding

To assess the impact of uncollected rates and annual charges on Council's liquidity and the adequacy of recovery efforts.

Commentary on 2019/20 result

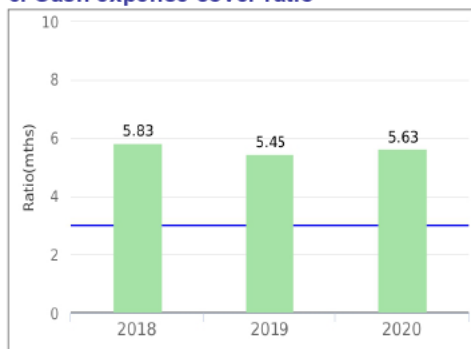
2019/20 ratio 6.74%

Council's assistance package in response to COVID-19 includes an option for ratepayers to defer the 4th instalment for 2019/20 without penalty. This has resulted in an increased level of outstanding payments.

Ratio achieves benchmark

Ratio is outside benchmark

6. Cash expense cover ratio



Benchmark: — > 3.00mths

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Purpose of cash expense cover ratio

This liquidity ratio indicates the number of months a Council can continue paying for its immediate expenses without additional cash inflow.

Commentary on 2019/20 result

2019/20 ratio 5.63 mths

The result of this measure reflects a high level of liquidity in Council's investment portfolio at the end of 2019/20.

Ratio achieves benchmark

Ratio is outside benchmark

Wollongong City Council

Financial Statements 2020

Notes to the Financial Statements

for the year ended 30 June 2020

Note 31. Council information and contact details

Principal place of business:

41 Burelli Street
Wollongong NSW 2500

Contact details

Mailing Address:

Locked Bag 8821
Wollongong NSW 2500

Telephone: (02) 4227 7111

Facsimile: (02) 4227 7277

Opening hours:

Administration Building: 8:30am - 5:00pm

Internet: www.wollongong.nsw.gov.au

Email: council@wollongong.nsw.gov.au

Officers

GENERAL MANAGER

Greg Doyle

RESPONSIBLE ACCOUNTING OFFICER

Brian Jenkins

PUBLIC OFFICER

Todd Hopwood

AUDITORS

Audit Office of New South Wales
Level 19, Darling Park Tower 2, 201 Sussex Street,
Sydney, NSW, 2000

Elected members

LORD MAYOR

Gordon Bradbery

COUNCILLORS

Ward 1

Leigh Colacino
Mithra Cox
Janice Kershaw
Jenelle Rimmer

Ward 2

Cath Blakey
David Brown
Tania Brown
John Dorahy

Ward 3

Dom Figliomeni
The Late Vicky King
Ann Martin
Cameron Walters

Other information

ABN: 63 139 525 939

DRAFT

Wollongong City Council

SPECIAL PURPOSE FINANCIAL STATEMENTS
for the year ended 30 June 2020

Wollongong City Council

Special Purpose Financial Statements 2020

Special Purpose Financial Statements

for the year ended 30 June 2020

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Background

- i. These Special Purpose Financial Statements have been prepared for the use by both Council and the Office of Local Government in fulfilling their requirements under National Competition Policy.
- ii. The principle of competitive neutrality is based on the concept of a 'level playing field' between persons/entities competing in a market place, particularly between private and public sector competitors.

Essentially, the principle is that government businesses, whether Commonwealth, state or local, should operate without net competitive advantages over other businesses as a result of their public ownership.

- iii. For Council, the principle of competitive neutrality and public reporting applies only to declared business activities.

These include **(a)** those activities classified by the Australian Bureau of Statistics as business activities being water supply, sewerage services, abattoirs, gas production and reticulation, and **(b)** those activities with a turnover of more than \$2 million that Council has formally declared as a business activity (defined as Category 1 activities).

- iv. In preparing these financial statements for Council's self-classified Category 1 businesses and ABS-defined activities, councils must **(a)** adopt a corporatisation model and **(b)** apply full cost attribution including tax-equivalent regime payments and debt guarantee fees (where the business benefits from Council's borrowing position by comparison with commercial rates).

Wollongong City Council

Special Purpose Financial Statements 2020

Special Purpose Financial Statements

for the year ended 30 June 2020

Statement by Councillors and Management made pursuant to the Local Government Code of Accounting Practice and Financial Reporting

The attached Special Purpose Financial Statements have been prepared in accordance with:

- the NSW Government Policy Statement 'Application of National Competition Policy to Local Government',
- the Division of Local Government Guidelines 'Pricing and Costing for Council Businesses – A Guide to Competitive Neutrality',
- the Local Government Code of Accounting Practice and Financial Reporting,

To the best of our knowledge and belief, these statements:

- present fairly the operating result and financial position for each of Council's declared business activities for the year, and
- accord with Council's accounting and other records.

We are not aware of any matter that would render these statements false or misleading in any way.

Signed in accordance with a resolution of Council made on 26 October 2020.

Gordon Bradbery
Lord Mayor
26 October 2020

Tania Brown
Deputy Lord Mayor
26 October 2020

Greg Doyle
General Manager
26 October 2020

Brian Jenkins
Responsible Accounting Officer
26 October 2020

Wollongong City Council

Special Purpose Financial Statements 2020

Income Statement – Waste Disposal

for the year ended 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
Income from continuing operations		
User charges	26,285	27,821
Rental Income	215	–
Grants and contributions provided for non-capital purposes	20	–
Other income	270	516
Total income from continuing operations	26,790	28,337
Expenses from continuing operations		
Employee benefits and on-costs	2,176	1,905
Borrowing costs	1,939	1,849
Materials and contracts	3,016	2,466
Depreciation, amortisation and impairment	1,663	1,221
Calculated taxation equivalents	522	529
EPA levy	10,958	11,764
Other expenses ¹	(3,651)	1,903
Total expenses from continuing operations	16,623	21,637
Surplus (deficit) from continuing operations before capital amounts	10,167	6,700
Surplus (deficit) from continuing operations after capital amounts	10,167	6,700
Surplus (deficit) from all operations before tax	10,167	6,700
Less: corporate taxation equivalent (27.5%) [based on result before capital]	(2,796)	(1,843)
SURPLUS (DEFICIT) AFTER TAX	7,371	4,857
Plus accumulated surplus	52,729	45,500
Plus adjustments for amounts unpaid:		
– Taxation equivalent payments	522	529
– Corporate taxation equivalent	2,796	1,843
Closing accumulated surplus	63,418	52,729
Return on capital %	22.1%	21.6%

(1) Each financial year the provision for the remediation of Council's waste facility is calculated based on the forecast costs to rehabilitate the site. The movements in the provision are generally recognised against the value of the corresponding tip asset. During 2019-2020, a reduction in the forecast rehabilitation costs caused a reduction in the provision beyond the carrying value of the asset. The balance of this adjustment and all future adjustments to the provisions will be recognised through other expenses.

Wollongong City Council

Special Purpose Financial Statements 2020

Income Statement – Tourist parks

for the year ended 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
Income from continuing operations		
User charges	6,717	7,153
Rental Income	101	–
Other income	46	144
Total income from continuing operations	6,864	7,297
Expenses from continuing operations		
Employee benefits and on-costs	2,817	2,813
Materials and contracts	850	1,157
Depreciation, amortisation and impairment	1,305	1,095
Loss on sale of assets	86	53
Calculated taxation equivalents	552	535
Other expenses	1,260	1,192
Total expenses from continuing operations	6,870	6,845
Surplus (deficit) from continuing operations before capital amounts	(6)	452
Surplus (deficit) from continuing operations after capital amounts	(6)	452
Surplus (deficit) from all operations before tax	(6)	452
Less: corporate taxation equivalent (27.5%) [based on result before capital]	–	(124)
SURPLUS (DEFICIT) AFTER TAX	(6)	328
Plus accumulated surplus	13,005	12,018
Plus adjustments for amounts unpaid:		
– Taxation equivalent payments	552	535
– Corporate taxation equivalent	–	124
Closing accumulated surplus	13,551	13,005
Return on capital %	0.0%	1.7%
Subsidy from Council	255	–

Wollongong City Council

Special Purpose Financial Statements 2020

Income Statement – Health & Fitness

for the year ended 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
Income from continuing operations		
User charges	2,331	2,896
Rental Income	1	–
Other income	78	95
Total income from continuing operations	2,410	2,991
Expenses from continuing operations		
Employee benefits and on-costs	2,295	2,521
Materials and contracts	252	332
Depreciation, amortisation and impairment	42	54
Calculated taxation equivalents	226	176
Other expenses	1,044	977
Total expenses from continuing operations	3,859	4,060
Surplus (deficit) from continuing operations before capital amounts	(1,449)	(1,069)
Surplus (deficit) from continuing operations after capital amounts	(1,449)	(1,069)
Surplus (deficit) from all operations before tax	(1,449)	(1,069)
SURPLUS (DEFICIT) AFTER TAX	(1,449)	(1,069)
Plus accumulated surplus	(12,948)	(12,055)
Plus adjustments for amounts unpaid:		
– Taxation equivalent payments	226	176
Closing accumulated surplus	(14,171)	(12,948)
Return on capital %	(25.5)%	(24.3)%
Subsidy from Council	1,499	1,127

Wollongong City Council

Special Purpose Financial Statements 2020

Income Statement – Crematorium & Cemeteries

for the year ended 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
Income from continuing operations		
User charges	–	1,550
Other income	–	9
Total income from continuing operations	–	1,559
Expenses from continuing operations		
Employee benefits and on-costs	–	1,431
Materials and contracts	–	269
Depreciation, amortisation and impairment	–	9
Loss on sale of assets	–	7
Calculated taxation equivalents	–	173
Other expenses	–	560
Total expenses from continuing operations	–	2,449
Surplus (deficit) from continuing operations before capital amounts	–	(890)
Surplus (deficit) from continuing operations after capital amounts	–	(890)
Surplus (deficit) from all operations before tax	–	(890)
SURPLUS (DEFICIT) AFTER TAX	–	(890)
Plus accumulated surplus	–	(827)
Plus adjustments for amounts unpaid:		
– Taxation equivalent payments	–	173
Closing accumulated surplus	–	(1,544)
Return on capital %	0.0%	(13.6)%
Subsidy from Council	–	976

Wollongong City Council

Special Purpose Financial Statements 2020

Statement of Financial Position – Waste Disposal

as at 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
ASSETS		
Current assets		
Investments	561	–
Total current assets	561	–
Non-current assets		
Receivables	54,637	70,160
Infrastructure, property, plant and equipment	54,704	39,543
Total non-current assets	109,341	109,703
TOTAL ASSETS	109,902	109,703
LIABILITIES		
Current liabilities		
Payables	–	638
Total current liabilities	–	638
Non-current liabilities		
Provisions	33,367	42,939
Total non-current liabilities	33,367	42,939
TOTAL LIABILITIES	33,367	43,577
NET ASSETS	76,535	66,126
EQUITY		
Accumulated surplus	63,418	52,729
Revaluation reserves	13,117	13,397
TOTAL EQUITY	76,535	66,126

Wollongong City Council

Special Purpose Financial Statements 2020

Statement of Financial Position – Tourist parks

as at 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
ASSETS		
Non-current assets		
Receivables	3,507	4,127
Infrastructure, property, plant and equipment	28,262	26,480
Total non-current assets	31,769	30,607
TOTAL ASSETS	31,769	30,607
NET ASSETS	31,769	30,607
EQUITY		
Accumulated surplus	13,552	13,005
Revaluation reserves	18,217	17,602
TOTAL EQUITY	31,769	30,607

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Wollongong City Council

Special Purpose Financial Statements 2020

Statement of Financial Position – Health & Fitness

as at 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
ASSETS		
Non-current assets		
Infrastructure, property, plant and equipment	5,692	4,398
Total non-current assets	<u>5,692</u>	<u>4,398</u>
TOTAL ASSETS	<u>5,692</u>	<u>4,398</u>
LIABILITIES		
Non-current liabilities		
Payables	15,558	13,901
Total non-current liabilities	<u>15,558</u>	<u>13,901</u>
TOTAL LIABILITIES	<u>15,558</u>	<u>13,901</u>
NET ASSETS	<u>(9,866)</u>	<u>(9,503)</u>
EQUITY		
Accumulated surplus	(14,171)	(12,946)
Revaluation reserves	4,303	3,443
TOTAL EQUITY	<u>(9,868)</u>	<u>(9,503)</u>

Wollongong City Council

Special Purpose Financial Statements 2020

Statement of Financial Position – Crematorium & Cemeteries

as at 30 June 2020

\$ '000	2020 Category 1	2019 Category 1
ASSETS		
Non-current assets		
Infrastructure, property, plant and equipment	–	6,534
Total non-current assets	–	6,534
TOTAL ASSETS	–	6,534
LIABILITIES		
Non-current liabilities		
Payables	–	4,585
Total non-current liabilities	–	4,585
TOTAL LIABILITIES	–	4,585
NET ASSETS	–	1,949
EQUITY		
Accumulated surplus	–	(1,535)
Revaluation reserves	–	3,484
TOTAL EQUITY	–	1,949

Wollongong City Council

Special Purpose Financial Statements 2020

Notes to the Special Purpose Financial Statements

for the year ended 30 June 2020

Note 1. Significant Accounting Policies

A statement summarising the supplemental accounting policies adopted in the preparation of the Special Purpose Financial Statements (SPFS) for National Competition Policy (NCP) reporting purposes follows.

These financial statements are SPFS prepared for use by Council and the Office of Local Government. For the purposes of these statements, the Council is a non-reporting not-for-profit entity.

The figures presented in these Special Purpose Financial Statements have been prepared in accordance with the recognition and measurement criteria of relevant Australian Accounting Standards, other authoritative pronouncements of the Australian Accounting Standards Board (AASB) and Australian Accounting Interpretations.

The disclosures in these Special Purpose Financial Statements have been prepared in accordance with the Local Government Act 2093 (NSW), the *Local Government (General) Regulation 2005*, and the Local Government Code of Accounting Practice and Financial Reporting.

The statements are prepared on an accruals basis. They are based on historic costs and do not take into account changing money values or, except where specifically stated, current values of non-current assets. Certain taxes and other costs, appropriately described, have been imputed for the purposes of the National Competition Policy.

The Statement of Financial Position includes notional assets/liabilities receivable from/payable to Council's general fund. These balances reflect a notional intra-entity funding arrangement with the declared business activities.

National Competition Policy

Council has adopted the principle of 'competitive neutrality' in its business activities as part of the National Competition Policy which is being applied throughout Australia at all levels of government.

The framework for its application is set out in the June 1996 NSW government policy statement titled 'Application of National Competition Policy to Local Government'.

The *Pricing and Costing for Council Businesses, A Guide to Competitive Neutrality* issued by the Office of Local Government in July 1997 has also been adopted.

The pricing and costing guidelines outline the process for identifying and allocating costs to activities and provide a standard for disclosure requirements.

These disclosures are reflected in Council's pricing and/or financial reporting systems and include taxation equivalents, Council subsidies, return on investments (rate of return), and dividends paid.

Declared business activities

In accordance with Pricing and Costing for Council Businesses – A Guide to Competitive Neutrality, Council has declared that the following are to be considered as business activities:

Category 1

(where gross operating turnover is over \$2 million)

- a. Waste Disposal:** Manages the disposal of solid waste generated within the city.
- b. Tourist Parks:** Operation, management & development of tourist parks at Bulli, Corrimal & Windang.
- c. Health & Fitness:** Responsible for the management and upkeep of Council's Leisure Centres.
- d. Crematorium & Cemeteries:** Provision of quality crematorium, cemetery & memorial facilities & services (2019 only).

Category 2

(where gross operating turnover is less than \$2 million)

Nil

Wollongong City Council

Special Purpose Financial Statements 2020

Notes to the Special Purpose Financial Statements

for the year ended 30 June 2020

Note 1. Significant Accounting Policies (continued)

Monetary amounts

Amounts shown in the financial statements are in Australian dollars and rounded to the nearest one thousand dollars.

(i) Taxation equivalent charges

Council is liable to pay various taxes and financial duties. Where this is the case, they are disclosed as a cost of operations just like all other costs.

However, where Council does not pay some taxes which are generally paid by private sector businesses, such as income tax, these equivalent tax payments have been applied to all Council-nominated business activities and are reflected in Special Purpose Financial Statements.

For the purposes of disclosing comparative information relevant to the private sector equivalent, the following taxation equivalents have been applied to all Council-nominated business activities (this does not include Council's non-business activities):

Notional rate applied (%)

Corporate income tax rate – 27.5%

Land tax – the first \$734,000 of combined land values attracts 0%. For the combined land values in excess of \$734,001 up to \$4,488,000 the rate is 1.6% + \$100. For the remaining combined land value that exceeds \$4,488,000 a premium marginal rate of 2.0% applies.

Payroll tax – 5.45% on the value of taxable salaries and wages in excess of \$900,000.

Income tax

An income tax equivalent has been applied on the profits of the business activities.

Whilst income tax is not a specific cost for the purpose of pricing a good or service, it needs to be taken into account in terms of assessing the rate of return required on capital invested.

Accordingly, the return on capital invested is set at a pre-tax level - gain/(loss) from ordinary activities before capital amounts, as would be applied by a private sector competitor. That is, it should include a provision equivalent to the corporate income tax rate, currently 27.5%.

Income tax is only applied where a gain/ (loss) from ordinary activities before capital amounts has been achieved.

Since the taxation equivalent is notional – that is, it is payable to Council as the 'owner' of business operations - it represents an internal payment and has no effect on the operations of the Council. Accordingly, there is no need for disclosure of internal charges in the SPFS.

The rate applied of 27.5% is the equivalent company tax rate prevalent at reporting date. No adjustments have been made for variations that have occurred during the year.

Local government rates and charges

A calculation of the equivalent rates and charges payable on all category 1 businesses has been applied to all land assets owned or exclusively used by the business activity.

Loan and debt guarantee fees

The debt guarantee fee is designed to ensure that council business activities face 'true' commercial borrowing costs in line with private sector competitors.

In order to calculate a debt guarantee fee, Council has determined what the differential borrowing rate would have been between the commercial rate and Council's borrowing rate for its business activities.

Wollongong City Council

Special Purpose Financial Statements 2020

Notes to the Special Purpose Financial Statements

for the year ended 30 June 2020

Note 1. Significant Accounting Policies (continued)

(ii) Subsidies

Government policy requires that subsidies provided to customers, and the funding of those subsidies, must be explicitly disclosed.

Subsidies occur when Council provides services on a less-than-cost-recovery basis. This option is exercised on a range of services in order for Council to meet its community service obligations.

Accordingly, 'subsidies disclosed' (in relation to National Competition Policy) represents the difference between revenue generated from 'rate of return' pricing and revenue generated from prices set by Council in any given financial year.

The overall effect of subsidies is contained within the Income Statement of each reported business activity.

(iii) Return on investments (rate of return)

The NCP policy statement requires that councils with Category 1 businesses 'would be expected to generate a return on capital funds employed that is comparable to rates of return for private businesses operating in a similar field'.

Such funds are subsequently available for meeting commitments or financing future investment strategies.

The actual rate of return achieved by each business activity is disclosed at the foot of each respective Income Statement.

The rate of return is calculated as follows:

Operating result before capital income + interest expense

Written down value of I,PP&E as at 30 June

As a minimum, business activities should generate a return equal to the Commonwealth 10 year bond rate which is 0.88% at 30/6/20.

(iv) Dividends

Council is not required to pay dividends to either itself (as owner of a range of businesses) or to any external entities.

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Wollongong City Council

SPECIAL SCHEDULES
for the year ended 30 June 2020

Wollongong City Council

Special Schedules 2020

Special Schedules

for the year ended 30 June 2020

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Wollongong City Council

Special Schedules 2020

Permissible income for general rates

\$ '000	Notes	Calculation 2020/21	Calculation 2019/20
Notional general income calculation ¹			
Last year notional general income yield	a	170,562	165,477
Plus or minus adjustments ²	b	827	730
Notional general income	c = a + b	171,389	166,207
Permissible income calculation			
Or rate peg percentage	e	2.60%	2.70%
Or plus rate peg amount	i = e x (c + g)	4,456	4,488
Sub-total	k = (c + g + h + i + j)	175,845	170,695
Plus (or minus) last year's carry forward total	l	2	4
Less valuation objections claimed in the previous year	m	(265)	(396)
Sub-total	n = (l + m)	(263)	(392)
Total permissible income	o = k + n	175,582	170,303
Less notional general income yield	p	175,452	170,562
Catch-up or (excess) result	q = o - p	130	(259)
Plus income lost due to valuation objections claimed ⁴	r	37	265
Less unused catch-up ⁵	s	—	(4)
Carry forward to next year ⁶	t = q + r + s	167	2

Notes

- (1) The notional general income will not reconcile with rate income in the financial statements in the corresponding year. The statements are reported on an accrual accounting basis which include amounts that relate to prior years' rates income.
- (2) Adjustments account for changes in the number of assessments and any increase or decrease in land value occurring during the year. The adjustments are called 'supplementary valuations' as defined in the Valuation of Land Act 1916.
- (4) Valuation objections are unexpected changes in land values as a result of land owners successfully objecting to the land value issued by the Valuer-General. Councils can claim the value of the income lost due to valuation objections in any single year.
- (5) Unused catch-up amounts will be deducted if they are not caught up within 2 years. Usually councils will have a nominal carry forward figure. These amounts can be adjusted for in setting the rates in a future year.
- (6) Carry forward amounts which are in excess (an amount that exceeds the permissible income) require ministerial approval by order published in the NSW Government Gazette in accordance with section 512 of the Local Government Act 1993. The OLG will extract these amounts from Council's Permissible income for general rates Statement in the financial data return (FDR) to administer this process.

Wollongong City Council

Special Schedules 2020

Report on Infrastructure Assets

as at 30 June 2020

Asset Class	Asset Category	Estimated cost	Estimated cost	2019/20	2019/20	Net carrying	Gross	Assets in condition as a percentage of							
		to bring assets	to bring to the	Required	Actual			replacement	gross replacement cost	1	2	3	4	5	
		to satisfactory	agreed level of	maintenance ^a	maintenance										cost (GRC)
		standard	service set by												
		\$ '000	Council	\$ '000	\$ '000	\$ '000	\$ '000	\$ '000							
(a) Report on Infrastructure Assets - Values															
Buildings	Buildings	3,091	5,564	25,522	23,938	301,035	543,023	3.2%	16.0%	79.6%	0.5%	0.7%			
	Sub-total	3,091	5,564	25,522	23,938	–	543,023	3.2%	16.0%	79.6%	0.5%	0.7%			
Other structures	Other structures	699	1,324	761	714	9,246	16,204	7.1%	22.7%	61.4%	1.9%	6.9%			
	Sub-total	699	1,324	761	714	9,246	16,204	7.1%	22.7%	61.4%	1.9%	6.9%			
Roads	Sealed roads	46,761	81,509	14,467	15,749	610,258	1,479,575	8.1%	8.3%	74.1%	4.0%	5.5%			
	Bridges	617	1,151	1,704	1,855	118,822	174,322	18.8%	36.5%	44.0%	0.2%	0.5%			
	Footpaths	3,094	5,651	3,387	2,739	158,235	305,485	21.4%	17.9%	57.9%	0.9%	1.9%			
	Sub-total	50,472	88,311	19,558	20,343	–	1,959,382	11.1%	12.3%	68.9%	3.2%	4.5%			
Stormwater drainage	Stormwater drainage	466	912	3,327	3,262	530,148	961,129	5.0%	12.1%	82.9%	0.0%	0.0%			
	Sub-total	466	912	3,327	3,262	–	961,129	5.0%	12.1%	82.9%	0.0%	0.0%			
Open space / recreational assets	Swimming pools & Open Space/Recreation	8,558	15,398	27,685	29,531	123,183	195,875	12.0%	14.0%	58.0%	9.0%	7.0%			
	Sub-total	8,558	15,398	27,685	29,531	–	195,875	12.0%	14.0%	58.0%	9.0%	7.0%			
TOTAL - ALL ASSETS		63,286	111,509	76,853	77,788	9,246	3,675,613	8.4%	12.9%	73.5%	2.3%	2.9%			

^(a) Required maintenance is the amount identified in Council's asset management plans.

Infrastructure asset condition assessment 'key'

(1) Cost to bring up to 'satisfactory standard' refers to the estimated cost to replace the calculated percentage of each asset type that are below satisfactory. Satisfactory standard is generally defined as condition 2 (see Asset Condition Definitions). As identified through Council's Securing our Future community engagement, for footpaths and shared paths, this is condition 3. Cost estimates are based on adopted valuation methodologies and do not generally include the cost of enhancements/upgrades to existing assets. The nominated percentage of each asset type below satisfactory standard is detailed under Condition of Public Works in the Annual Report.

(2) Required Annual Maintenance' is the amount that should be spent to maintain assets in a satisfactory standard and to ensure that the estimated remaining life of the asset is achieved PLUS the new requirement to include the "operating expenses required to keep the asset in a functional state for community use". During the 2016/17 financial year this process was completely reviewed to ensure inclusion of all labour and materials on all 'asset operation' tasks.

continued on next page ...

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Wollongong City Council

Special Schedules 2020

Report on Infrastructure Assets - Values (continued)

as at 30 June 2020

(3) Current Annual Maintenance' is the total expenditure incurred during the year to maintain assets. It also includes the actual expenditure on operational costs to keep the asset in a functional state for community use', which includes costs such as mowing, painting, lighting, cleaning, energy supply etc. It does not include operational overheads (e.g. administration staff) or capital expenditure.

(4) Cost to bring to agreed service level is defined as "The proportion of the GRC that have reached the intervention level based on condition." The outstanding renewal works compared to the total replacement cost of Councils assets. That is calculated as the cost of bringing condition 4 and 5 assets to condition 1.

(5) Below is a summary of asset types included in Asset Class:

Buildings: Community Facilities, Cultural Facilities, Childcare Centres, Public Toilets, Exeloos, Grandstands, Surf Life Saving Clubs, Amenities, Office/Shops, Industrial, Residences, Cabins and Leisure Centres

Roads and Road Related Assets: Road seal, base, sub-base, kerb and gutter, guardrails and traffic facilities (roundabouts, speedhumps, medians etc)

Bridge, Boardwalks and Jetties: Road Bridges, Pedestrian Bridges, Boardwalks and Jetties

Footpaths and Cycleways: Footpaths and cycleways (excluding off road footpaths in parks)

Car parks: Surface car parks (not multi-storey) and Boat ramps

Stormwater: Pits, Pipes, Headwalls, Culverts and Riparian Assets

Recreation: Pools, Playgrounds, Sport Courts, Park Furniture, Picnic Shelters and Landscaping. Does not include non-depreciable land and earthworks

Condition of Public Assets

In assessing the condition of Public Assets, Council has considered the existing condition and function of each asset. The results shown for the condition of assets within each category are an average. Therefore, a proportion of assets in each category are above and below the average result. Council recognise that the ratings scale used (as outlined below) may be different from those used by other Councils but has been based on the rating scale in the International Infrastructure Management Manual.

Asset Condition Definitions

Rating	Status
1	As new
2	Good
3	Fair
4	Poor
5	To be replaced

Wollongong City Council

Special Schedules 2020

Report on Infrastructure Assets (continued)

as at 30 June 2020

\$ '000	Amounts 2020	Indicator 2020	Prior periods 2019 2018		Benchmark
Infrastructure asset performance indicators (consolidated) *					
Buildings and infrastructure renewals ratio ¹					
Asset renewals ²	52,085	102.11%	93.63%	104.26%	>=100.00%
Depreciation, amortisation and impairment	51,011				
Infrastructure backlog ratio ¹					
Estimated cost to bring assets to a satisfactory standard	63,286	3.42%	4.76%	5.50%	<2.00%
Net carrying amount of infrastructure assets	1,850,927				
Asset maintenance ratio					
Actual asset maintenance	77,788	101.22%	100.34%	98.54%	>100.00%
Required asset maintenance	76,853				
Cost to bring assets to agreed service level					
Estimated cost to bring assets to an agreed service level set by Council	111,509	3.03%	3.26%	3.47%	
Gross replacement cost	3,675,613				

(*) All asset performance indicators are calculated using classes identified in the previous table.

(1) Excludes Work In Progress (WIP)

(2) Asset renewals represent the replacement and/or refurbishment of existing assets to an equivalent capacity/performance as opposed to the acquisition of new assets (or the refurbishment of old assets) that increases capacity/performance.

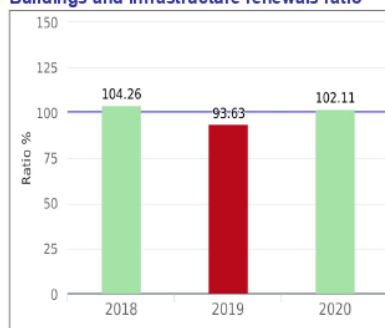
Wollongong City Council

Special Schedules 2020

Report on Infrastructure Assets (continued)

as at 30 June 2020

Buildings and infrastructure renewals ratio



Buildings and infrastructure renewals ratio

To assess the rate at which these assets are being renewed relative to the rate at which they are depreciating.

Commentary on result

19/20 ratio 102.11%

Council expended \$94M on Capital works during 2019/20. A significant proportion of this expenditure went to new infrastructure initiatives supporting major sub-division development works. That is, in 2019/20 \$14M was expended on the completion of the Fowlers Road new bridge project. The impact was not as great as last year, with resulting expenditure on renewal of infrastructure assets at \$52.9M, up from \$49.7M last financial year.

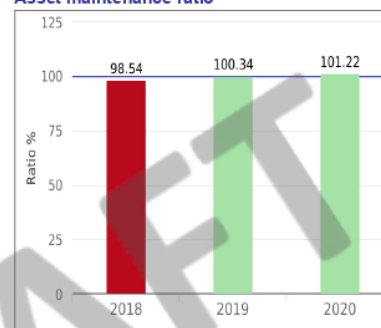
Benchmark: — $\geq 100.00\%$

Ratio achieves benchmark

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Ratio is outside benchmark

Asset maintenance ratio



Asset maintenance ratio

Compares actual vs. required annual asset maintenance. A ratio above 1.0 indicates Council is investing enough funds to stop the infrastructure backlog growing.

Commentary on result

19/20 ratio 101.22%

In recent years Council has reviewed 500 cost centres and all supporting natural accounts to improve the detail of this result. Operational required costs have been set to equal the operational actual costs. The increase to above 100% is primarily due to increased maintenance of Buildings and Facilities.

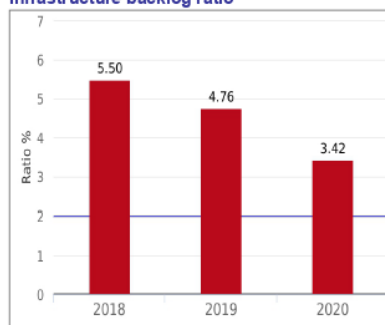
Benchmark: — $> 100.00\%$

Ratio achieves benchmark

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Ratio is outside benchmark

Infrastructure backlog ratio



Infrastructure backlog ratio

This ratio shows what proportion the backlog is against the total value of a Council's infrastructure.

Commentary on result

19/20 ratio 3.42%

The cost to bring to satisfactory is defined as the cost difference of the written down value of the assets in condition 4 and 5, to their written down value at condition 2, with the exception of path assets to condition 3. Council's 'Securing our Future' financial review confirmed condition 3 for paths through public consultation.

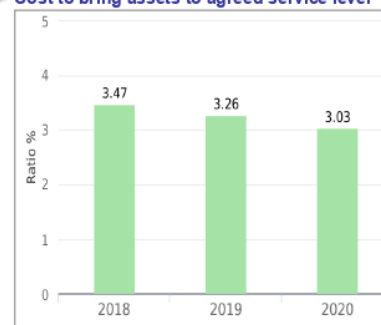
Benchmark: — $< 2.00\%$

Ratio achieves benchmark

Source of benchmark: Code of Accounting Practice and Financial Reporting #28

Ratio is outside benchmark

Cost to bring assets to agreed service level



Cost to bring assets to agreed service level

This ratio provides a snapshot of the proportion of outstanding renewal works compared to the total value of assets under Council's care and stewardship.

Commentary on result

19/20 ratio 3.03%

This ratio is the sum of the outstanding renewal works compared to the total replacement cost of infrastructure assets. That is, the cost to renew assets in condition 4 and 5 as a proportion of the Gross Replacement Cost. This is trending down as Council continues sound programs of replacing failed assets.

Wollongong City Council

Financial Statements 2020

General Purpose Financial Statements

for the year ended 30 June 2020

Statement by Councillors and Management

made pursuant to Section 413(2)(c) of the Local Government Act 1993 (NSW) (as amended)

The attached General Purpose Financial Statements have been prepared in accordance with:

- the *Local Government Act 1993* (NSW) (as amended) and the regulations made thereunder,
- the Australian Accounting Standards and other pronouncements of the Australian Accounting Standards Board,
- the Local Government Code of Accounting Practice and Financial Reporting.

To the best of our knowledge and belief, these statements:

- present fairly the Council's operating result and financial position for the year,
- accord with Council's accounting and other records.

We are not aware of any matter that would render these statements false or misleading in any way.

Signed in accordance with a resolution of Council made on 26 October 2020.

Gordon Bradbery
Lord Mayor
26 October 2020

Tania Brown
Deputy Lord Mayor
26 October 2020

Greg Doyle
General Manager
26 October 2020

Brian Jenkins
Responsible Accounting Officer
26 October 2020

Wollongong City Council

Special Purpose Financial Statements 2020

Special Purpose Financial Statements

for the year ended 30 June 2020

Statement by Councillors and Management made pursuant to the Local Government Code of Accounting Practice and Financial Reporting

The attached Special Purpose Financial Statements have been prepared in accordance with:

- the NSW Government Policy Statement 'Application of National Competition Policy to Local Government',
- the Division of Local Government Guidelines 'Pricing and Costing for Council Businesses – A Guide to Competitive Neutrality',
- the Local Government Code of Accounting Practice and Financial Reporting,

To the best of our knowledge and belief, these statements:

- present fairly the operating result and financial position for each of Council's declared business activities for the year, and
- accord with Council's accounting and other records.

We are not aware of any matter that would render these statements false or misleading in any way.

Signed in accordance with a resolution of Council made on 26 October 2020.

Gordon Bradbery
Lord Mayor
26 October 2020

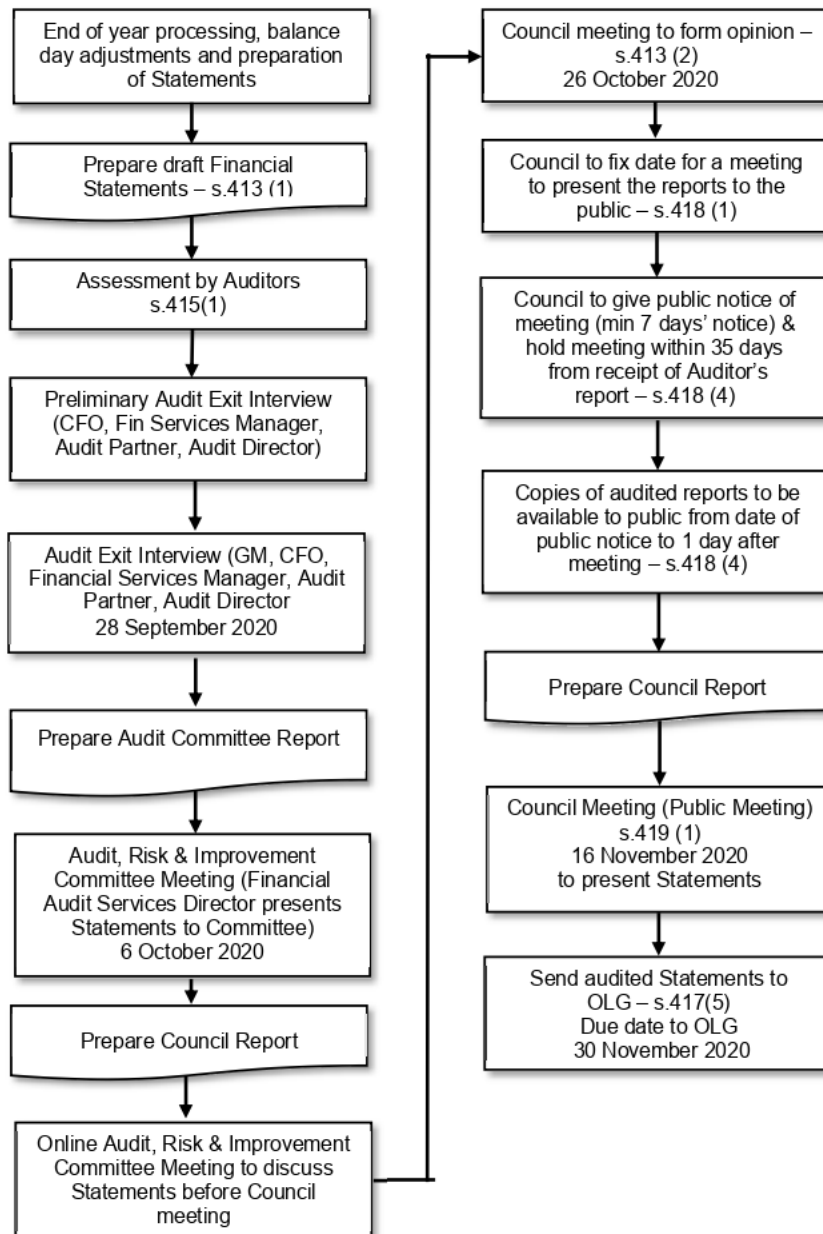
Tania Brown
Deputy Lord Mayor
26 October 2020

Greg Doyle
General Manager
26 October 2020

Brian Jenkins
Responsible Accounting Officer
26 October 2020

Wollongong City Council

Flowchart of the Annual Financial Statements Approval Process



ITEM 2

DRAFT PLANNING PROPOSAL - FORMER CORRIMAL COKE WORKS SITE, 27 RAILWAY STREET CORRIMAL

In October 2017, Council received a draft Planning Proposal request for 27 Railway Street, Corrimal, the former Corrimal Coke Works site. The draft Planning Proposal request sought to rezone the site from IN3 Heavy Industrial and RE2 Private Recreation to R3 Medium Density Residential and RE1 Public Recreation to facilitate medium density residential development.

On 3 April 2018, Council considered a report on the preliminary assessment of the rezoning request and resolved to commence the preparation of a draft Planning Proposal, subject to a number of recommendations. The draft Planning Proposal was subsequently referred to the (then) NSW Department of Planning and Environment and a Gateway Determination was received 20 August 2018 requiring a number of studies to be completed and Ministerial Directions relating to heritage and flooding to be satisfied, prior to the draft Planning Proposal being formally exhibited. The studies have now been completed, and a revised draft Planning Proposal and updated draft Master Plan submitted, supported by draft Development Control Plan provisions and a Letter of Offer to enter into a Planning Agreement.

It is recommended that Council resolve to exhibit a revised draft Planning Proposal, a draft Development Control Plan chapter, a draft Planning Agreement Letter of Offer and associated supporting documentation for community feedback for a minimum period of 60 days.

RECOMMENDATION

- 1 A revised draft Planning Proposal be prepared and exhibited for Lot 1 DP 795791, Lot 5 DP 749492, Lot 11 DP 749492 and Lot 126 DP 598190 known as the former Corrimal Coke Works, to amend Wollongong Local Environmental Plan 2009 as follows:
 - a Amend the Zoning Map (Attachment 7):
 - i Rezone Lot 1 DP 795791 from IN3 Heavy Industrial to R3 Medium Density Residential
 - ii Rezone Lot 5 DP 749492 to part R3 Medium Density Residential and part RE1 Public Recreation
 - iii Rezone Lot 11 DP 749492 from SP2 Infrastructure (Road) to RE1 Public Recreation
 - iv Retain Lot 126 DP 598190 as SP2 Infrastructure (Road).
 - b Amend the Height of Buildings Map (Attachment 7) to introduce heights between nine (9) metres and 22 metres.
 - c Amend the Floor Space Ratio Map (Attachment 7) to introduce FSRs between 0.75:1 and 2:1.
 - d Amend the Minimum Lot Size Map (Attachment 7) to introduce a minimum lot size of 450m² for the area proposed R3 Medium Density Residential zoning.
 - e Amend the Riparian Lands Map (Attachment 7) to identify the realigned riparian corridor.
 - f Amend the Natural Resource Sensitivity – Biodiversity Map (Attachment 7) to identify significant native vegetation communities.
 - g Amend the Key Site Map (Attachment 7) to identify the site as a Key Site under clause 7.18 Design Excellence.
 - h Amend the Urban Release Area Map (Attachment 7) to identify the site as requiring the provisions of Part 6 Urban Release Areas, clauses 6.1 and 6.2.
 - i Include the following new Part 7 Local Provisions:
 - i A “Sun Plane Protection” clause to protect the public open space identified in the Master Plan as “Heritage Plaza” and “Central Park” from excessive overshadowing by restricting the height of buildings.
 - ii A clause to allow increased heights beyond the maximum height control for certain heritage items: C1 North Stack (37m); C1 Brick Chimney Stack (29m) and the C1 Fine Coal Bin (25m), in the event that they need to be rebuilt.
 - iii A clause to grant development consent to development for the purpose of rooftop plant, lift towers, lift motor rooms and or communal open space and access to any structures associated with such space, that would exceed or causes a building to exceed, the height limits set.
 - j Amend Schedule 1 to incorporate the following additional permitted use provision:

Development at ground level for the following purposes within the mapped area:

 - business premises
 - neighbourhood shops with a maximum gross floor area of 150m²
 - food and drink premises with a maximum gross floor area of 250m²
 - neighbourhood supermarket with a maximum gross floor area of 1,000m²

The combined total gross floor area (GFA) for the above uses cannot exceed 2,000m². The objective of the clause is to limit the size of shops to ensure that the Corrimal Town Centre remains the principal retail area.

- 2 The draft site-specific Development Control Plan chapter with the revisions outlined in Section 3.8 of this report and supporting documentation/technical studies be exhibited with the draft Planning Proposal.
- 3 The draft Planning Agreement Letter of Offer (including details of the arrangements for inclusion of 5% affordable rental housing) be exhibited with the draft Planning Proposal.
- 4 The draft Planning Proposal also make a housekeeping amendment to rezone the Cross Street Road reserve and part of the former Lot 12 DP 749492 Cross Street from SP2 Infrastructure (Road) and RE2 Private Recreation to R2 Low density Residential with a Minimum Lot size of 449m, Floor Space Ratio of 0.5:1 and Height of Building limit of 9m, consistent with the adjoining Cross Street residential properties.
- 5 The draft Planning Proposal, draft Development Control Plan chapter, draft Planning Agreement Letter of Offer and supporting documentation be exhibited for a minimum period of 60 days, in accordance with Council's resolution 3 April 2018, noting a community information session may not be possible given COVID-19 restrictions and hence alternative methods of community engagement will be utilised. The exhibition period may be extended due to the Christmas – New Year period.
- 6 During the exhibition period, consultation be undertaken with the following agencies -
 - a NSW Department of Industry – Crown Lands and Water Division
 - b NSW Environment Protection Authority
 - c NSW DPIE - Environment, Energy and Science
 - d Heritage NSW
 - e Endeavour Energy
 - f Transport for NSW (includes former RMS and Sydney Trains)
 - g Sydney Water.
 - h Emergency response organisations (SES, RFS, NSW Fire and Rescue etc)
- 7 A post exhibition report be prepared for Council to consider. The report shall outline any proposed revisions to the draft Planning Proposal, Master Plan, draft DCP chapter and Letter of Offer.
- 8 Council note that the NSW Department of Planning, Industry and Environment will retain the authority and exercise plan making delegations.

REPORT AUTHORISATIONS

Report of: Chris Stewart, Manager City Strategy

Authorised by: Linda Davis, Director Planning + Environment - Future City + Neighbourhoods

ATTACHMENTS

- 1 Location Map
- 2 Current Zoning and Height of Buildings Map
- 3 Council Resolution (8 April 2018)
- 4 Updated Planning Proposal and Masterplan (May 2019)
- 5 2020 Planning Proposal, Masterplan, Site Specific DCP and VPA Letter of Offer
- 6 Housing Trust MOU Letter 7 June 2020
- 7 Proposed WLEP 2009 Maps: Zoning; Height of Buildings; Floor Space Ratio; Minimum Lot Size; Riparian Lands; Natural Resource Sensitivity
- 8 Design Review Panel Report 19 July 2019
- 9 Key View and Shadow Analysis
- 10 Department of Planning, Industry and Environment Letter Ministerial Directions 5 August 2020
- 11 Overview of Challenges in Managing GHFF Camps

BACKGROUND

Subject Site

In October 2017, a draft Planning Proposal request was lodged for 27 Railway Street, Corrimal, formerly known as the Corrimal Coke Works. The site consists of 4 lots and has an area of approximately 18.167 hectares. The site is bounded by the main southern railway line on the east, Memorial Drive and residential buildings on the west, Railway Street to the north and Towradgi Creek and residential buildings to the south (Attachment 1). The site is traversed by Towradgi Creek along the southern extent and North Corrimal Creek through the site.

The site was utilised as the Corrimal Coke Works which operated for over 100 years, ceasing operations in 2014. The site comprises coking ovens, stacks, two constructed dams for coke quenching, associated administration buildings, and coal and coke stockpiling areas in the central part of the site. Since the closure of the coke works in 2014 the site has been vacant with limited maintenance. Established vegetation is present across parts of the site.

The area and zoning of the site is summarised in the following table and is shown on Attachment 2.

Lot	Area	Zoning
Lot 1 DP 795579	7.807 ha	IN3 Heavy Industrial
Lot 5 DP 749492	10.11 ha	RE2 Private Recreation
Lot 11 DP 749492 (located at the end of Cross Street)	30.1m2	SP2 Infrastructure (Road)
Lot 126 DP 598190	0.2467 ha	SP2 Infrastructure (Road)
Total	18.167 ha	

The site is surrounded by low and medium density residential zoning to the north, north-east and east, with some light industrial uses immediately to the north. Open space adjoins the site to the south. The maximum height limit for the site is currently nine (9) metres and the maximum height limit of the adjoining residential areas ranges from nine (9) metres to 13 metres (Attachment 2). The site is located within a Low, Medium and High Flood Risk precinct, and includes a notation that the site may be contaminated due to past industrial land uses.

Regional and Local Context

Corrimal is located 6.5km north of Wollongong City Centre, providing access to a diverse range of services and employment opportunities. The site is adjacent Corrimal Railway Station and is positioned in close proximity to education, recreation and community facilities, including the Corrimal District Community Library, Corrimal Pool and Robert Ziems Park. The site is located within one kilometre of the Corrimal Town Centre (Attachment 1).

Corrimal is identified in the Illawarra Shoalhaven Regional Plan 2036, Community Strategic Plan 2028 and Corrimal Town Centre Plan 2015 as the major urban hub of the northern suburbs, capable of infill development given its location and supporting infrastructure and services.

Council Resolution (3 April 2018) and Gateway Determinations (20 August 2018)

The draft Planning Proposal request submitted in 2017 was accompanied by an indicative design concept report prepared by E8 Urban illustrating a range of lot sizes and the intent to deliver a diverse housing product outcome. It identified the potential provision of 190 lots, achieving 736 dwellings made up of 80 rear loaded terraces, 90 front loaded semi-detached dwellings, three front loaded detached dwellings, six manor homes in two manor house lots, and 556 apartments of between two to six storeys in height. The Concept Plan also proposed the inclusion of limited retail uses within a town square

adjacent the Corrimal Railway Station. The Urban Design Study was indicative only, with further design development required to formalise applicable built form controls across the site.

On 3 April 2018, Council considered a report on the preliminary assessment of the draft Planning Proposal request and resolved to commence the preparation of a draft Planning Proposal, subject to a number of recommendations (Attachment 3).

The draft Planning Proposal was referred to the (then) NSW Department of Planning and Environment and a Gateway Determination was received 20 August 2018, requiring a number of studies to be completed and Ministerial Directions relating to heritage and flooding to be satisfied, prior to the Planning Proposal being formally exhibited for community comment and feedback. An alteration of the Gateway Determination was issued 17 February 2020 to allow more time for the draft Planning Proposal to be completed.

The additional studies required from the Gateway Determination included -

- An Aboriginal cultural heritage assessment.
- A conservation management plan that provides for the long-term conservation of significant coke work heritage components.
- Revised flood study (including flood modelling).
- Geomorphological report.
- A revised ecological assessment.
- A revised traffic impact assessment.
- A revised remediation action plan.

On 3 April 2018, Council also resolved that a site specific Development Control Plan (DCP) be prepared prior to public exhibition, addressing building heights; floor space ratios; lot sizes; building envelopes; road widths; public spaces; streetscapes; housing types; connectivity and access; views and vistas; urban form design; and design excellence. Council also resolved to publicly exhibit the proposal for 60 days and to hold a community information session in Corrimal during the exhibition period.

The Design Review Panel attended a site visit on 16 July 2019 and provided independent feedback on a draft Master Plan and the draft DCP submitted post Gateway Determination in May 2019.

On 29 June 2020, Council resolved to progress a heritage amendment to Wollongong LEP 2009 as a separate process to the rezoning Planning Proposal. The heritage amendment was notified on 11 September 2020, listing part of the site as a Local Heritage Item in Wollongong LEP 2009.

PROPOSAL

Updated Planning Proposal Request

The studies required from the Gateway Determination were completed and submitted to Council on 23 May 2019 with a revised draft Planning Proposal request, incorporating an updated 2019 Master Plan (Attachment 4). The proponent supplied additional information between June 2019 and September 2020 in response to Council Officer, Public Authority and Design Review Panel feedback and information requests. The additional studies, including urban design and view analysis, have resulted in revisions being made to the 2019 Master Plan. A number of technical reports have been prepared for the site in support of the draft Planning Proposal as a requirement of the Gateway Determination and are referenced in this report.

A Final 2020 Masterplan and draft Planning Proposal, accompanying draft Development Control Plan (DCP) and updated draft Planning Agreement (VPA) Letter of Offer were subsequently submitted to Council during August/September 2020 (Attachment 5).

1 Key Aspects of the Final 2020 Master Plan and draft Planning Proposal request (August 2020)

The key aspects of the 2020 Master Plan and draft Planning Proposal request (August 2020) are summarised below, with detailed discussion of each point included in later sections of the report -

- Regional and Council strategies have identified Corrimal as a major urban hub in the northern suburbs, capable of infill development given its location and supporting infrastructure.
- Site specific and strategic merit/well positioned for residential development – proximity to public transport, town centre, jobs, schools, recreational facilities and related infrastructure.
- Economic studies have concluded there is sufficient supply of well-located industrial land in the local government area and new industrial uses are not deemed viable on this site.
- Remediation Action Plan – site can be made suitable for proposed mixed use redevelopment.
- Medium density housing typologies are under-represented in the Illawarra – R3 zoning provides opportunity for a mix of medium density residential products in response to market demand, which may include apartments, terraces/semi-detached dwellings, seniors living/aged care and affordable housing. The proponent's target of 735-760 dwellings for the site remains unchanged.
- Provision for 35 affordable rental dwelling units, provided and managed by a registered Community Housing Provider.
- The site is now heritage listed as an item of local significance, and Heritage NSW is undertaking further assessments to determine if the site satisfies the criteria for a State Heritage listing.
- Heritage Studies have concluded that rezoning the site is the most viable option to ensure the enhancement of heritage values, making the history accessible to the community through adaptive re-use of heritage buildings and interpretation.
- A station heritage plaza adjacent Corrimal Railway Station – commuter and resident convenience focus (small scale retail) and opportunity to celebrate the heritage of the site with the intention to retain some key heritage elements and others repurposed/interpreted.
- Ownership and ongoing responsibility for the maintenance of heritage structures (including funding) to be tied to the retail component.
- Vision for the inclusion of neighbourhood shops, cafes, restaurants, and possibly business start-ups/flexible work spaces in the heritage plaza up to a maximum total GFA of 2,000m², comprising one neighbourhood supermarket of no more than 1000m² in size, and other retail/non retail outlets of no more than 250m² in size, located within 100-150 metres of Corrimal Railway Station.
- Opportunity to improve the streetscape, services and experience of commuters, encouraging active transport and public transit usage, including a street network that facilitates bus, bicycle and pedestrian access to Corrimal Railway Station.
- Traffic modelling identifies the need for intersection improvements/upgrade at Memorial Drive and Railway Street, to be designed to comply with TfNSW specifications. A roundabout at Railway and Harbinger Streets has been proposed as the safest access/egress point for the site
- Vehicular access boulevard entry at Railway Street with views to the retained heritage brick chimney.
- Approximately 52% of the site proposed as open space (originally proposed 43%) to provide a range of recreational settings, including the riparian corridor, the addition of a central neighbourhood park, southern recreation park and a heritage plaza.
- A realigned creek corridor to the western part of the site will be designed to provide a flood free area for development and improve flood, stormwater and drainage on site and at Railway Street/Cross Street. The riparian corridor design demonstrates stability of structure during storm events, will not negatively impact Memorial Drive and will be revegetated to provide an improved environmental outcome to the current highly modified creek on site.
- A bicycle/walking shared path is proposed linking Railway Street through the site to the Council owned open space south of the site.
- A pedestrian and cycle route upgrade along Railway Street and an internal cycling/pedestrian network, to provide enhanced linkage opportunities between Corrimal Railway Station and the town

centre and community facilities, assisting with Council's vision for cycling to be a preferred transport option by 2030 (note the proposed cycleway route connection under Memorial Drive in the original 2017 Master Plan is not featured in the updated 2020 Master Plan).

- Protection of identified threatened species communities in the south of the site.
- An adjustment of proposed R3/RE1 boundary to reflect the 2020 Master Plan to provide a 100-metre buffer between the core mapped habitat of the endangered Grey-headed Flying-fox (GHFF) and proposed residential development.
- Street alignment, urban design and distribution of proposed building heights to ensure key views are created/retained to the heritage chimney stack and escarpment. Central view/boulevard to proposed parks.
- Increased and varying height limits and FSRs requested across the site, with indicative building envelopes supported by a view analysis/modelling.
- Letter of Offer for a Planning Agreement and Schedule of Public Benefits, detailing items to be provided as part of the development, some of which will offset local development contributions.
- Site specific draft Development Control Plan (DCP) Chapter.
- The Department of Planning, Industry and Environment (DPIE) advised in a letter dated 5 August 2020 that, based on a review of the updated flood information provided, the Planning Proposal is now consistent with Ministerial Direction 4.3 – Flood Prone Land.
- DPIE also advised in the letter dated 5 August 2020 that, given a separate Local Heritage Listing amendment to Wollongong LEP 2009 is being finalised and will facilitate heritage protection for the site, any inconsistency with Direction 2.3 Heritage Conservation is therefore considered justified and no further approval is required in relation to this Direction.

Figure 1 below, illustrates the evolution of the master planning for the site.

Initial Master Plan 2017

Proposed Master Plan 2019

Proposed Master Plan 2020

The original 2017 and updated 2019 Master Plans proposed a mix of medium density residential products, including seniors living/aged care and affordable housing, and a small convenience retail space adjacent Corrimal Railway Station (see Table 1 below).

The proposed R3 Medium Density Residential zone permits a variety of housing typologies (including single detached, terraces/semi-detached, apartments, seniors/aged care, shop top housing). The 2020 Master Plan represents the maximum development outcome by way of apartment buildings. The final housing product will be developed in response to future market demand and maintain the proponent's desired yield of 760 dwellings.

The 2016 Census highlighted that the present housing stock across the Wollongong LGA is dominated by detached dwellings (67%), with 21% a medium density product and 10% high density.

The proposed redevelopment of the Coke Works site for housing, adjacent the Corrimal Railway Station, represents an opportunity to supply medium density housing typologies (low scale residential flat buildings, townhouses and semi-detached and seniors housing) and higher density apartments which are under-represented in the Illawarra housing market.

Included in the 2018 Council resolution was a requirement to provide at least 5% Affordable Rental Housing within the development, and information on the proposed management arrangements of the dwellings and housing needs sector to be targeted. The updated draft Planning Proposal includes provision for approximately 35 affordable rental dwelling units, provided and managed by a registered Community Housing Provider.

In a letter dated 7 June 2020 the Illawarra Housing Trust informed Council that they had entered into a Memorandum of Understanding (MOU) with Legacy Property to progress commercial discussions about the delivery of Affordable Rental Housing (Attachment 6). The intent of the MOU is that the Illawarra Housing Trust would acquire a freehold parcel within the development and design, build and manage an Affordable Rental Housing facility. The letter refers to the NSW Ministerial Guidelines for a definition of tenant eligibility for Affordable Rental Housing – an income range of approximately \$52,000-\$72,000 for households without children and \$89,000-\$124,000 for households with one or more children, this cohort is often referred to as “key workers”.

Council's DCP Chapter B1 Residential Development also requires 10% of all dwellings within a residential apartment building, and 10% of all dwellings within a multi dwelling development incorporating more than six dwellings, to be adaptable and universally designed (i.e. capable of adaptation for disabled or elderly residents).

Heritage Civic Plaza

The 2020 Master Plan incorporates a Heritage Civic Plaza designed to -

- Create a safe and accessible public domain providing connectivity to Corrimal Railway Station.
- Provide a community meeting space activated by neighbourhood scale retail uses (shop, café, restaurant, etc).
- Celebrate the history of the site through retention, repurposing and interpretation of heritage features.

The updated draft Planning Proposal includes the original request for neighbourhood scale retail uses to contribute to the activation of the Corrimal Railway Station and new heritage plaza to provide convenience retail for residents. It also raises the possibility of flexible commercial spaces to support tele-working and local business start-ups, a shared community space and child-care centre.

The Heritage Conservation Management Strategy and accompanying Heritage Interpretation Strategy provides for retention of the two chimney elements that “bookend” the C1 coke oven battery (designed to retain key iconic skyline elements of the coke works in the most visible locations) and partial retention of the C1 coke oven battery and the powerhouse building, combined with interpretation of other elements. It is the intention to establish a key view axis to the Brick Chimney from the Railway Street entry.

Riparian Corridor, Central and Southern Park Concepts

The 2020 Master Plan proposes the realignment of North Corrimal Creek to create a new green corridor with improved ecological and flooding outcomes, and the opportunity to provide a shared walking/cycling path through the site from Railway Street to Council's public reserve south of the site.

A small central village park is proposed, incorporating a defined playground area, to encourage community interaction. An informal southern recreation area is also proposed, which will also provide a buffer area between the GHFF core mapped camp and the residential areas in the south.

Figure 2: 2020 Master Plan Public Domain



2 Key Changes to Wollongong LEP 2009 Amendments Sought from Council Resolution and Gateway Determination

A Final updated 2020 Master Plan and draft Planning Proposal was submitted to Council in August 2020 which included a number of proposed Wollongong LEP 2009 amendments being sought that differ to those currently endorsed by Council (3 April 2018) and the Gateway Determinations (20 August 2018 and 17 February 2020) for exhibition. These are discussed below -

2.1 Land Use Zoning

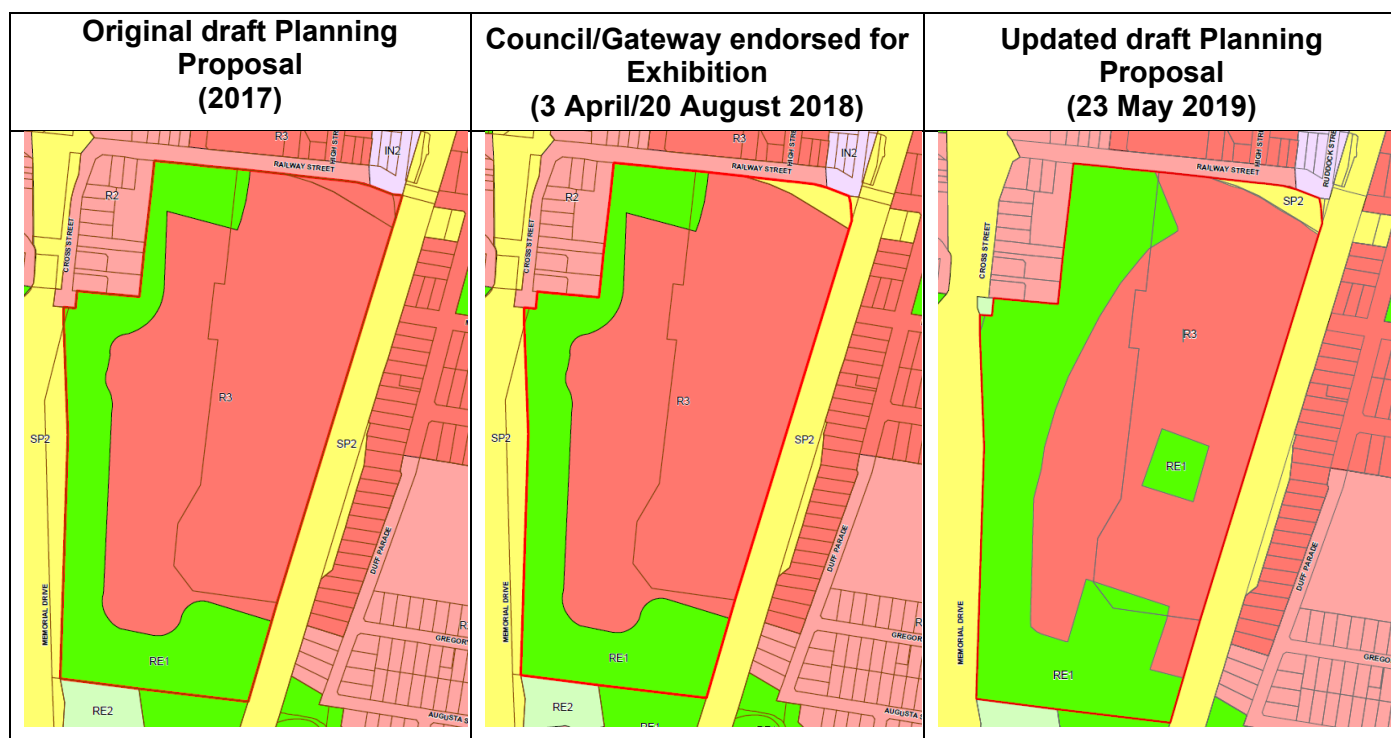
The proposed Zoning amendment endorsed by Council (3 April 2018) and the Gateway Determination (20 August 2018) for exhibition (subject to a number of recommendations) is stated as follows -

- *Amend the Zoning map as follows – rezone from IN3 Heavy Industrial to R3 Medium Density zoning for part of the site to permit a range of medium density housing products (terraces, semi-detached dwellings, apartments) and RE1 Public Recreation zoning for the realigned riparian corridor.*

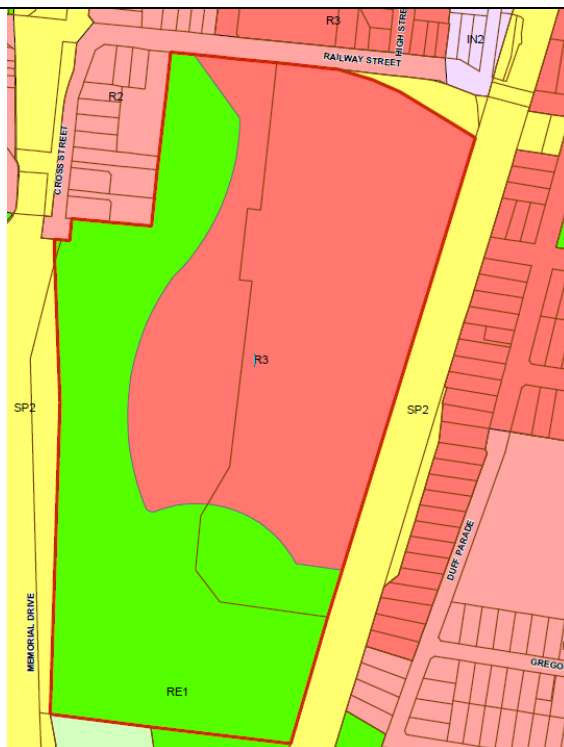
The proposed zoning map has changed a number of times in response to various issues (Attachment 7). The key zoning changes now requested in the 2020 Master Plan include -

- Adjustment of proposed R3/RE1 boundary to reflect the outcomes and recommendations of the updated studies, site constraints (including provision of a 100-metre buffer between the GHFF core camp and proposed development in the south), and resultant revised Master Plan.
- The area of proposed R3 Medium Density Residential zoned land has been reduced in size from 12.9 hectares to 9.8 hectares.
- The Master Plan includes provision for the addition of a central park. It is proposed to zone the location R3 to enable the exact configuration to be resolved at the development application phase. The requirement for the central park to have a minimum area of 3,000m² is included in the site-specific draft DCP Chapter and in the draft Planning Agreement offer.

Figure 3: Proposed Zoning Amendment



**Updated draft
Planning Proposal
(August 2020)**



The proposed zoning of RE1 Public Recreation and transfer of this area into Council ownership will ensure ongoing maintenance and protection from future development following a suitable Vegetation Management Plan (VMP) restoration and management period by the proponent. The proposed RE1 zoning is consistent with the zoning of Towradgi Creek upstream and downstream of the site. The incorporation of a minimum 100 metre buffer between the GHFF core camp and future development will also assist with environmental protection.

2.2 Maximum Height of Buildings

2.2.1 Existing Height Limits and Context

The Coke Works site currently has two height limits aligned with the existing zoning. The RE2 zoned land has a 9 metre height limit, and the IN3 zoned land has an unrestricted height limit. Surrounding the site is a variety of height limits. The R2 Low Density Residential zone has a height limit of 9 metres and the R3 Medium Density Residential zone opposite the site and adjoining the railway station has a 13 metre height limit. The taller height controls in Corrimal are concentrated in the town centre (15 metres). If rezoned for residential uses, the close proximity of the site to the 13 metre and 15 metre permissible areas presents a good contextual argument to support this site having a similar height limit permitted at the boundary areas (Attachment 2).

The existing structures on the IN3 zoned land vary in height. The tallest structure on site is the C1N stack which is approximately 42 metres tall. The items noted as having high historic significance on site have the following heights -

- 1912 Brick Stack~32.6m.
- C1 Coke Oven Battery 2m high and 3m wide.
- Remnant Powerhouse Wall~8m.

The height and shape of the existing structures, including the C1N stack (~42 metres) and grinding plant building, form a well-known part of the area's skyline. The towers particularly provide a significant landmark, marking out the extent of the Coke Works facilities across the site.

Much of the site, the riparian corridor and the adjacent rail and road reserves are covered by tree canopy. This canopy forms a height datum across the site which is visible from surrounding areas,

particularly from the two rail bridges to the north and south. No site survey has been provided showing the heights of the trees within or around the site. It is estimated that the tree canopy generally sits at a height of around 12 metres with several taller trees extending above this in areas to around 25 metres.

Beyond the site to the west, the Illawarra Escarpment State Conservation Area sits as the prominent landscape feature. The escarpment is a unique visual feature of great scenic, cultural and economic importance to the Illawarra, providing a spectacular backdrop that enhances the amenity of the urban areas. It is listed as a 'Scenic Landscape of Statewide Significance' on the Register of the National Trust of Australia (NSW). Broker's Nose, or 'Kurimal', is the name given to the high mountain peak (part of the escarpment ridgeline) in Corrimal. This site has significance for the local community and is a place of deep cultural value for the traditional custodians of Dharawal Country.

Currently, the ridgeline and a significant proportion of the lower areas of the escarpment are visible as a backdrop to the existing built form and tree canopy of the site as viewed from the east. It is important that any rezoning of the site maintains a reasonable degree of visual access to this unique feature.

2.2.2 Requested Height of Buildings Changes

The proposed Height of Building amendment endorsed by Council (3 April 2018) and the Gateway Determination (20 August 2018) for exhibition is stated as follows -

- *Amend the Height of Buildings Map to introduce a varying maximum height limit of 13m and 24m throughout the site (as per map below).*

The original draft Planning Proposal request submitted in 2017 sought a mix of 24 metre and 15 metre height limits across the site. As discussed above, whilst the 15 metre height is reflected in the town centre, there is currently no precedent for height above this in Corrimal. The 24 metre height limit in the northern part of the site was justified by the proponent in terms of the opportunity for public transport use, being adjacent Corrimal Railway Station.

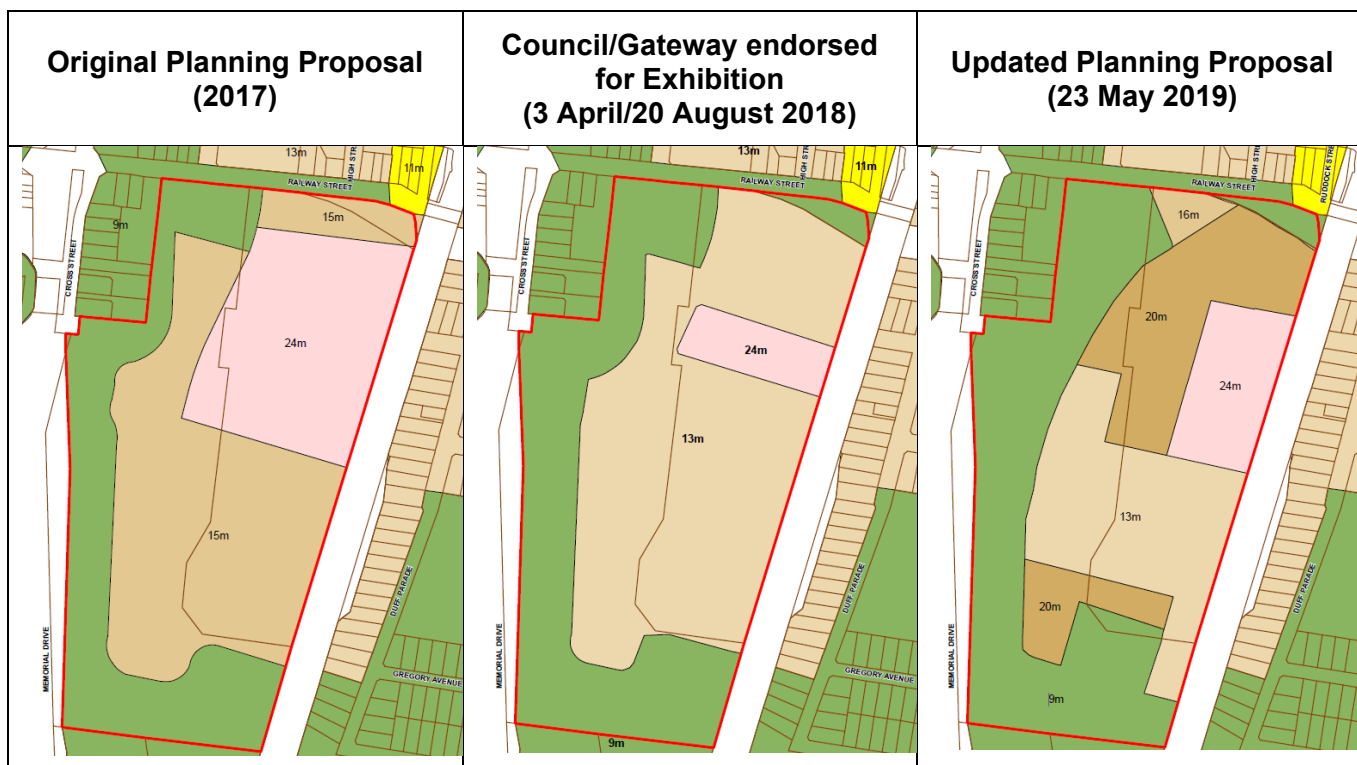
The Council report (3 April 2018) recommended the reduction of the proposed 24 metre built form to a smaller part of the site, reflecting the concentrated area of height of the existing coke works structures. The remainder of the site was limited to a maximum height limit of 13 metres in line with the surrounding development.

The report to Council envisaged a 24-metre height limit or six storeys within the centre of the site to provide some flexibility of offer (e.g. affordable housing) and also encourage Council's future growth for Corrimal as the northern suburbs' housing hub. The proposed surrounding mix of two and three storey diverse housing developments was intended to integrate into the current low density surrounding residential environment. The Council report stated that further design detail was required to ensure appropriate height levels will apply throughout the site and recommended that the detail be incorporated within the design guidelines of a site-specific Development Control Plan (DCP) Chapter. Council specifically asked for a 3D model of the site proposal and associated contextual view analysis to demonstrate its impact.

Updated Planning Proposal (2019)

The proponent submitted a revised draft Planning Proposal and draft DCP chapter to Council on 23 May 2019. The proposal included a new Master Plan with a reduced development area and revised height limits. These limits included 13 metre, 16 metre, 20 metre and 24 metre areas and represented a significant increase to the height of the built form envelope overall from that endorsed for Gateway.

Figure 4: Proposed Height of Buildings Amendment: 2017/2018/2019



Concerns were raised by Council officers as to the appropriateness of these proposed height limits in the context of the surrounding lower built form and as they relate to the heritage elements of the site, and view corridors from the wider area. The proposed development must be compatible with the surrounding built form and scale, social and economic activities, natural and landscape context and heritage significance. The Design Review Panel also expressed this sentiment, providing feedback on the revised Planning Proposal and DCP “with the aim of achieving the expected levels of excellence for a site of this importance and significance”.

It is noted that the Conservation Management Strategy (CMS - Urbis July 2019) submitted in support of the Planning Proposal identified significant views to the site and indicates that these views should be retained. The Biosis Assessment identified the stacks and chimneys as prominent visual landmarks within the surrounding area and recommended the adoption of open spaces and street configurations which mimic the current configuration of the Coke Works. Although there was clear intent in the 2019 Master Plan to retain views to some heritage elements within the site. Additional information was required to demonstrate how views from further afield would be affected under the revised heights.

View Analysis, Modelling & Shadowing Analysis

The proponent was asked to prepare a 3D model to illustrate the proposed built form in context and a view analysis to demonstrate its impact on views to the heritage elements on site and towards the escarpment. The 3D model and view analysis was to respond to a set of Design Principles as outlined below, in order to demonstrate that this unique site could accommodate the height and scale of development being proposed -

Table 2: Design Principles

Views	<ul style="list-style-type: none"> • Create and/or protect key views to retained/interpreted heritage elements both on the site and from surrounding areas. • Protect key views to the escarpment from surrounding areas and from within the site.
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Table 2: Design Principles (continued)

Heritage	<ul style="list-style-type: none"> • Provide an appropriate buffer between proposed development and heritage items including a transition in height. • Organise building massing to assist in way-finding to the heritage plaza and station. • Ensure heritage is protected, including significant buildings, and important heritage fabric is retained/reused. Retention and adaptation of heritage buildings through innovative design and re-use is sought. • Ensure new buildings which adjoin heritage places have regard to their height, scale, character and proportions.
Scale	<ul style="list-style-type: none"> • Ensure heights and bulk of built forms allow important views to be retained (see above). • Provide a sensitive transition down in height to lower scale established neighbouring areas. • Distribute height and FSR to provide for a diverse housing mix. • Ensure that development provides for equitable development outcomes through building separation.
Amenity	<ul style="list-style-type: none"> • Maintain solar access to public open spaces. • Establish mid-block links through larger blocks to achieve permeability and increase accessibility. • Promote and encourage pedestrian activity through street activation and the protection of footpaths and public open spaces from loss of amenity caused by overshadowing and wind effects.

With particular reference to the proposed height amendments, the proponent has submitted the following to guide the location and orientation of streets and buildings, and height distribution across the site -

- Master Plan and Site Visual Analysis.
- 3D modelling of proposed built form in the context of surrounding development and landforms.
- Shadow analysis.
- Draft site specific DCP controls to ensure key views are created/maintained, no overshadowing of public spaces etc (noting that SEPP 65 contains amenity controls for residential flat building developments).

Revised Master Plan (2020)

Subsequent to the 2019 revised Planning Proposal, the draft Master Plan has been through a number of positive iterations in order to respond to key Council and Community concerns and deliver an improved design outcome that responds to the abovementioned Design Principles. A Final 2020 Master Plan and draft Planning Proposal was submitted in August 2020.

The site is a unique location adjacent Corrimal Railway Station, within close proximity to Corrimal Town Centre, employment, recreational, cultural and educational facilities, and hence well positioned for medium density residential development. The location and size of the site, combined with the vision to incorporate green link corridors, open space areas, and walking and cycle linkages, suggest it can accommodate some additional height.

The Design Review Panel (DRP) in their report dated 19 July 2019 (Attachment 8) noted that the site's proximity to the railway station and its significant size creates the potential for some taller building forms on this site, however requested further view analysis be undertaken to determine appropriate height and

the placement/extent of taller buildings on the site. The DRP identified the heritage chimneys as prominent landmarks in the area that should remain as the dominant forms and silhouettes in the skyline in views across the site.

The current 2020 Master Plan has responded as follows -

- Aligns a key east west internal road with Murray Road to the east. This allows a view corridor to be maintained down Murray Road (which extends to the foreshore), through the site and towards the escarpment.
- Aligns the entry road to create views towards the 1912 Brick Stack. As the road does not extend all the way to the stack, views to the stack are proposed to be maintained through two blocks including 22, 20 and 13 metre height controls.
- Reduces the proposed maximum permitted height from 24 metres to 22 metres (six storeys).
- Locates the tallest height zone (22 metres) in a central area of the site and provides a transition in height down (15m) towards the site boundaries and heritage precinct, designed to have the least impact on existing residential areas surrounding the site and respect heritage elements on site.
- Provides a significant green buffer between the proposed tallest height zone and Cross Street residents, through rehabilitation of the riparian corridor.
- Optimises passive surveillance of the riparian corridor by retaining views from adjoining residential dwellings.
- Maintains a height limit to the majority of the north and eastern boundaries of 15 metres, which is 2 metres higher than the adjacent height zones.
- Maintains a height limit of 13 – 15 metres across the reduced heritage curtilage area.
- Provides some variation of building height and design across the site.
- Reduces the overall development area to apply a 100 metre buffer to the core mapped area of the Grey-headed Flying-fox colony.
- Does not reduce the expected dwelling yield in response to the reduction in development area.
- Increases the lots with higher height limits and appears to reduce the likelihood of seeing a range of dwelling type outcomes across the site (in order to maintain yield numbers).

The draft Planning Proposal states that the calculation of building heights is intended to provide some flexibility to accommodate -

- Half in/half out basement car parking (up to 1.2m above ground), and
- New sustainable construction methods that require increased floor to floor heights due to insulation requirements for BCA compliance.

Hence, a 22 metre maximum height limit is being requested to accommodate the above design requirements within a six-storey building. It is proposed that a specific Part 7 Local Provision be included to cater for the site for lift overruns, roof top communal space and parapets. The requested maximum building heights in the 2020 draft Planning Proposal include 13 metre, 15 metre, 18 metre and 22 metre areas (Attachment 7).

Figure 5: Proposed Height of Building Amendment: 2020

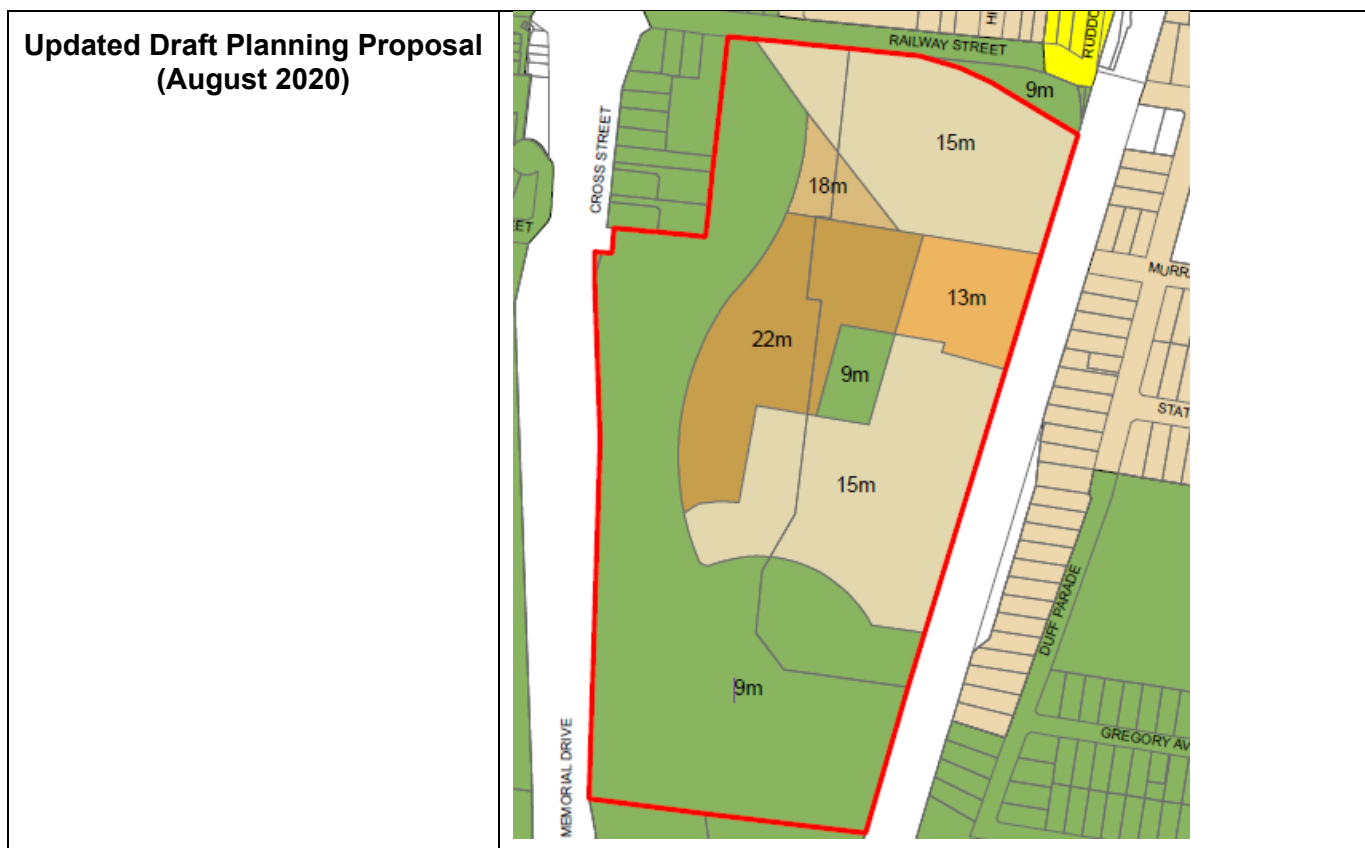


Figure 6 below illustrates the key views the updated 2020 Master Plan will create/protect. A summary of the key view and shadow analysis is included as Attachment 9.

Figure 6: Key View Map



MAINTAINED VIEW CORRIDORS

Legend

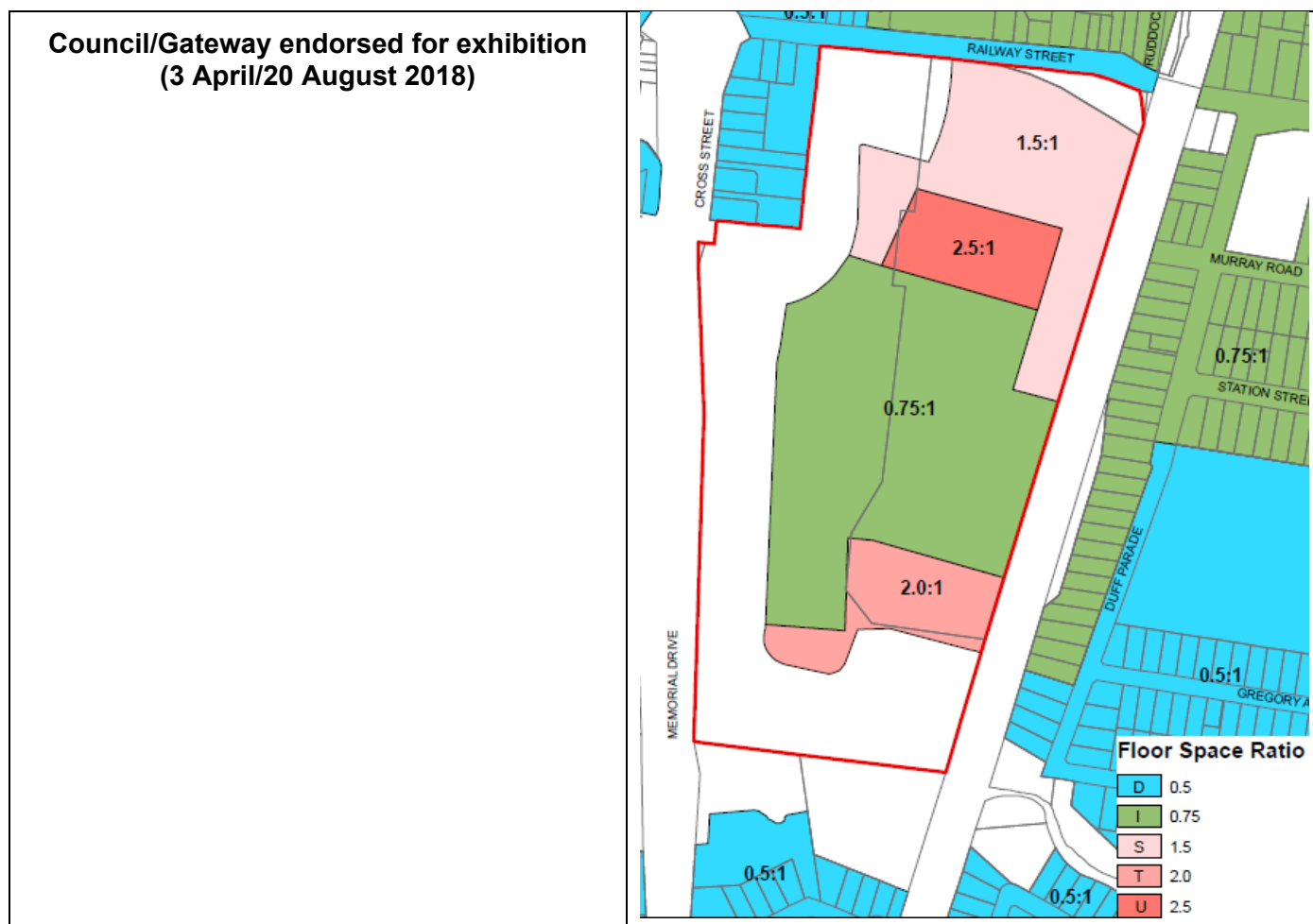
- The Site
- View Corridor to Heritage
- Murry Road New Corridor
- Western View Corridors to Riparian Corridor and Escarpment
- Central View Axis

2.3 Requested Floor Space Ratio Changes:

The proposed Floor Space Ratio amendment endorsed by Council (3 April 2018) and the Gateway Determination (20 August 2018) for exhibition is stated as follows -

- Amend the Floor Space Ratio Map to introduce a varying floor space ratio of between 0.75:1 and 2.5:1 (as per map below).

Figure 7: Proposed Floor Space Ratio Map 2018



Floor space ratio (FSR) is the ratio of the gross floor area (of a building or development) to the site area. FSR helps to guide built form outcomes and regulate the scale and character of development. The desired development outcome is facilitated through the development assessment process.

The applicable FSR is shown on the Floor Space Ratio Map. The FSR can vary across a zone or on a large development site. The FSR for the R3 Medium Density Residential zone on the other side of Railway Street, is 0.75:1. Elsewhere in the LGA the FSR for R3 Medium Density Residential zoned land can be as high as 1.2:1 (i.e. Dapto Town Centre).

The original 2017 draft Planning Proposal requested that no FSR apply to the site. Council Officers consider that it is appropriate for an FSR to guide future development outcomes. The proponent has prepared a draft Floor Space Ratio Map reflecting the 2020 Concept Master Plan, showing FSRs of 0.75:1, 1.2:1, 1.5:1, 2:1 and 2.5:1 for different parts of the site within the development area. The proponent has stated that this equates to an average FSR of 0.92:1 across the development footprint (essentially the R3 zoned area).

The table below shows the heights and corresponding FSRs proposed for the Corrimal Coke Works site and compares this to the FSR controls for relevant areas with the same height controls. The table also

references the FSR controls for the Ashmore precinct in Erskineville, which the proponent sights as an example of the development outcome being sought at the Coke Works site.

Table 3: Height/FSR Comparisons

Corrimal Coke Works Site		Wollongong LGA comparison	Ashmore (Erskineville) comparison
Height limits	FSR	FSR	FSR
9m (open space)	1.5:1	0.5:1 Fairy Meadow Town Centre	1:1
11m	-	2:1 B4 zone - Dapto Town Centre 1.2:1 R3 zone - Adjacent Dapto Town Centre	
13m	0.75:1	0.75:1 B2 zone - Fairy Meadow Town Centre	(12) 1.5:1
15m	1.2:1 – 1.5:1	1.5:1 B2 zone - Fairy Meadow Town Centre 1.5:1 B2 zone - Corrimal Town Centre	1.5:1
18m	2:1		1.5:1
20m	-	2.5:1 B2 zone - Dapto Town Centre 2:1 B2 zone - Huntley Town Centre	
22m	1.5:1 – 2.5:1		1.75:1
24m	-	1.5:1 R1 zone - North Wollongong	1.75:1
30m	-	2.5:1 B3 zone - Dapto Town Centre	
32m	-	1.5:1 R1 zone - North Wollongong	1.75:1

A FSR of 2:1 and above tends to be suited to mixed use development with a large podium base, a continuous street-wall and heights 20m or higher, where the dwelling density is typically 120-160 dwellings per hectare. These developments tend to suit locations along busy streets, where the units

tend to be elevated above a commercial use at ground. They also have larger building footprints and less area for open space and deep soil landscaping.

The proponent provided a 'yield testing examples' document demonstrating the yields possible on two lots under the proposed FSR. These tests demonstrated that in order to achieve the proposed FSR the building envelope would need to maximise the building footprint and height. For Lot 4 particularly, this would result in a number of poor outcomes including -

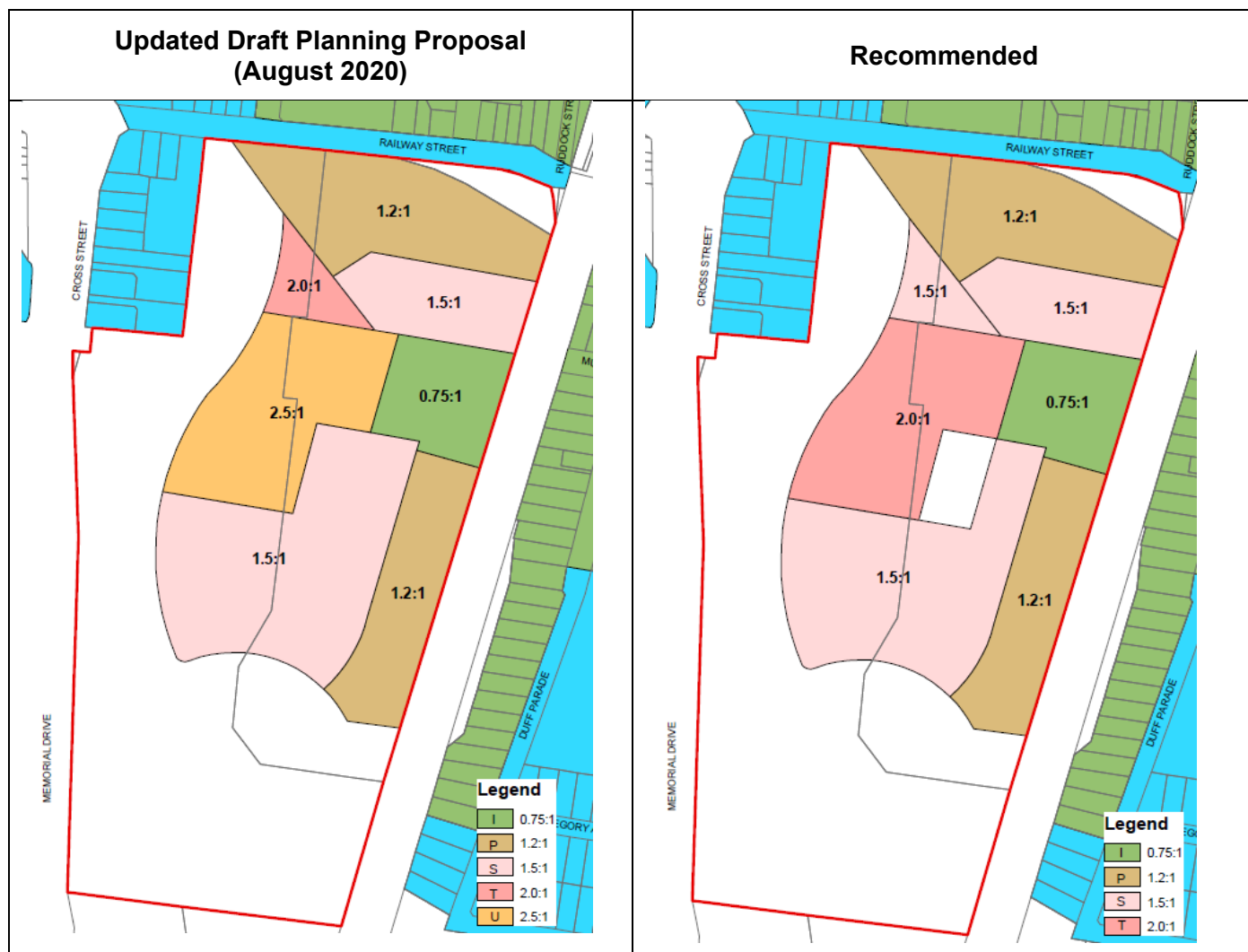
- Problematic overshadowing of the building's own communal space.
- No ability to modulate the height of the building across the expansive footprint.
- An unbroken street wall/podium with no views through the site across its entire area.

These outcomes are common when FSRs are high in relation to the permitted height limit. It is recommended that the FSR be amended to better align with a medium density outcome that anticipates a number of different building typologies in a landscaped setting and doesn't leave the site open to the risk of over-development.

It is recommended that the FSR range not extend beyond 2:1. The area proposed for a height limit of 22 metres is recommended to have a FSR of 2:1 (not 2.5:1 as requested) and the area proposed for a height of 18 metres is recommended to have a FSR of 1.5:1 (not 2:1 as requested). It is also recommended that no FSR be applied to the central park area (Attachment 7).

The proponent's analysis indicates that the proposed FSR reductions will result in an approximately 7% reduction in floor space / yield across the site. This reduction may result in a corresponding reduction in the affordable housing offer, noting the previous Council resolution to provide at least 5% of affordable rental housing as part of the development.

Figure 8: Proposed FSR Amendment 2020 and Recommended FSR Amendment



2.4 Requested Minimum Lot Size Changes:

The proposed Minimum Lot Size amendment endorsed by Council (3 April 2018) and the Gateway Determination (20 August 2018) for exhibition is stated as follows -

- Amend the Minimum Lot Size Map to introduce a minimum lot size of 149m² for the R3 Medium Density zone

The 2018 report to Council outlined potential merit and support for this reduced lot size as an incentive to develop a diverse housing product (terrace/townhouse and attached/detached development), the recommendation to include further design guidelines for the proposed housing typology within the nominated DCP Chapter.

The updated 2020 Master Plan and draft Planning Proposal is seeking a minimum lot size of 449m² for the areas proposed for R3 Medium Density Residential zoning, consistent with the surrounding R2 Low Density Residential and R3 Medium Density Residential zones (Attachment 7). Any compact housing typologies, such as townhouses, can be delivered under strata title or through an integrated housing approval.

2.5 Requested Schedule 1 Changes: Additional Permitted Uses

The proposed Schedule 1 amendment endorsed by Council (3 April 2018) and the Gateway Determination (20 August 2018) for exhibition is stated as follows -

- *Amend Schedule 1 Additional Uses to incorporate additional uses for “food and drink premises” and “shop”, limiting the size of the premises to a maximum of 150m², identifying a site-specific location within a 100m radius of the train station.*

A Retail Market Demand and Economic Impact Assessment prepared by Urbis July 2019 was submitted in support of the integration of neighbourhood scale retail and commuter services within the Heritage Plaza adjacent Corrimal Railway Station.

Under the 2020 draft Planning Proposal, the proponent is now seeking the following additional permitted uses to be included as an amendment to Schedule 1 –

- Development for the purposes of food and drink premises, shops, business premises and neighbourhood supermarket (1,000m²) is permitted with consent, but only if at ground floor level and within 150m of the train station.
- Development for purposes of food and drink premises, shops, business premises and neighbourhood supermarket is permitted up to a maximum of a total GFA of 2,000m², with no one shop (except for the neighbourhood supermarket) to be more than 250m² in size.

The updated draft Planning Proposal includes the original request for neighbourhood scale retail uses to contribute to the activation of the Corrimal Railway Station and new heritage plaza and provide convenience retail for residents. The inclusion of “Business premises” would facilitate services on site such as a hair salon and possibly local business start-ups or tele-working arrangements.

It should be noted that “community facilities”, “childcare facilities” and “neighbourhood shops” are permitted with consent in the R3 Medium Density Residential zone.

“Neighbourhood shop” means premises used for the purposes of selling general merchandise such as foodstuffs, personal care products, newspapers and the like to provide for the day-to-day needs of people who live or work in the local area, and may include ancillary services such as a post office, bank or dry cleaning, but does not include neighbourhood supermarkets or restricted premises.

The additional land uses requested that are not currently permitted in the R3 zone are defined below -

- “Food and Drink Premises”: premises that are used for the preparation and retail sale of food or drink (or both) for immediate consumption on or off the premises, and includes any of the following -
 - A restaurant or café.
 - Take away food and drink premises.
 - A pub.
 - A small bar.
- “Shop”: premises that sell merchandise such as groceries, personal care products, clothing, music, homewares, stationery, electrical goods or the like or that hire any such merchandise, and includes a neighbourhood shop, but does not include food and drink premises or restricted premises.
- “Business premises”: a building or place at or on which –
 - An occupation, profession or trade (other than an industry) is carried on for the provision of services directly to members of the public on a regular basis, or
- A service is provided directly to members of the public on a regular basis, and includes a funeral home and, without limitation, premises such as banks, post offices, hairdressers, dry cleaners, travel agencies, internet access facilities, betting agencies and the like, but does not include an entertainment facility, home business, home occupation, home occupation (sex services), medical centre, restricted premises, sex services premises or veterinary hospital.
- “Neighbourhood supermarket”: premises the principal purpose of which is the sale of groceries and foodstuffs to provide for the needs of people who live or work in the local area.

It should be noted that the request to include “Shop” as an additional permitted use may already be catered for by the land use “neighbourhood shops”, which as noted above are already permitted with consent in the R3 Medium Density Residential zone.

The Retail Market Demand and Economic Impact Assessment (Retail Assessment) provided the following indicative composition of the 2,000m² retail area (noting that the updated 2020 draft Planning Proposal now includes a request for a neighbourhood supermarket to be 1,000m², and development for the purposes of business premises to also be permitted with consent):

Table 4: Indicative Retail Offer

Small scale independent local supermarket	750m ²
Total food specialty retail (2-3 cafes/restaurants)	600m ²
Total non-food specialty retail (e.g. newsagent, pharmacy, hair salon)	450m ²
Total non-retail shopfront (e.g. real estate, dental, physio)	200m ²
TOTAL (GLA)	2,000m²

In Corrimal Town Centre the current estimated supermarket Gross Lettable Area Retail (GLAR) is 7,600m² (spread across three supermarkets). At East Corrimal village is a Food Works supermarket which has a GLAR of approximately 300m².

The Retail Assessment describes the proposed retail for the Coke Works site as modest in scale, positioned to provide dining options and convenience-based shopping for onsite residents, visitors and people in the immediate locality. The Concept Plan envisages this retail element to be developed within a well-designed public realm and streetscape within the heritage civic plaza, serving a convenience retail role for onsite residents and commuters, with onsite amenities creating a focal point for activity.

The Retail Assessment states that the proposed small convenience-based retail offer is supportable from an economic perspective, while not adversely impacting the existing retail hierarchy in the area. The proposed small-scale retail offering is forecast to achieve turnover in 2024 that reflects a minor market share of retail spending (3% of main trade and 11% of primary trade area).

Introduction of additional permitted uses to facilitate a small convenience-based retail offer is supported in principle, with consideration given to location, extent and scale of these uses. It is critical that land uses allowed on the site do not erode the viability of the Corrimal Town Centre. Clause 5.4 of Wollongong LEP 2009 assists in ensuring Corrimal Town Centre remains the principal retail area, by limiting neighbourhood supermarkets to a GFA of 1,000m² (approximately 3 times the size of the East Corrimal Village Food Works) and neighbourhood shops retail floor area to 100m².

The recommendation is to limit the size of individual neighbourhood shops to 150m² in accordance with Council's previous resolution, and also to limit the total commercial offering on site to a maximum of 2,000m². The intention is to repurpose the heritage Powerhouse building on site to accommodate food and drink premises, with the request for outlets to be 250m² to match the building footprint. It should be noted in this regard that the site also potentially benefits from the Heritage Incentives provisions in Wollongong LEP 2009 (Clause 5.10 (10)) which allows flexibility around certain planning controls where a development will allow for the conservation of a heritage site. These provisions align with the development scheme which relies upon the commercial activation to support heritage conservation outcomes.

The 2018, Council resolution proposed to limit retail uses within a 100m radius of the train station. The 2020 draft Planning Proposal is requesting a location within 150m of the station to ensure the heritage Powerhouse building intended for adaptive re-use is included in the designated area. It is recommended that a map of the proposed convenience focused retail area be included in the DCP.

3 Other Key Planning Proposal Considerations

3.1 Loss of industrial lands

The draft Planning Proposal request seeks to transition the heavy industrial site to medium density residential development and open space. Consideration needs to be given to the loss of industrial lands in this location, and the potential impact on the provision of industrial lands in the region.

The proponent submitted an Economic Impact Assessment prepared by Hill PDA September 2017 detailing the viability of the ongoing industrial use of the site. The report concluded -

- There is sufficient supply of well-located industrial land in the Wollongong LGA without the subject site.
- Increased demand in transport, storage and warehousing sectors growing but the subject site is unsuitable.
- Conversion to new industrial uses has been identified to be not viable.

The heavy industrial use ceased in 2014 and the site is not currently providing active employment. A land use conflict exists between the heavy industrial zoning and surrounding residential land uses. The site is not noted to be strategically important industrial land as the majority of industrial land in the Wollongong LGA is found in Unanderra, Kembla Grange and Port Kembla, offering lower cost and better access than the subject site.

There is strategic merit for residential redevelopment of the site given its proximity to the station, retail/commercial hub and open space/community facilities. There is potential for good urban design outcomes and interpretation of identified heritage items, whilst being capable of meeting Council's objectives for the R3 Medium Density Residential zone.

Council resolved (3 April 2018) that the draft Planning Proposal for exhibition include identification of the site as a Key Site under clause 7.18 Wollongong LEP 2009 Design Excellence to deliver the highest standard of architectural and urban design for any redevelopment, and the preparation of a site specific Development Control Plan (DCP) Chapter to guide future development.

In granting the Gateway Determination, the (then) Department of Planning and Environment agreed that the loss of employment land is justified in relation to the relevant section 9.1 Ministerial Direction 1.1 Business and Industrial Zones.

3.2 Heritage

The proposed Heritage amendment endorsed by Council (3 April 2018) and the Gateway Determination (20 August 2018) for exhibition is stated as follows -

- *Amend the Heritage Map and Heritage Schedule to include identified significant fabric (coke ovens, chimney, stacks) as listed items of Local Significance.*
- *The following additional information be submitted before or during the exhibition period to enable Council to consider the information prior to determining whether to finalise the Planning Proposal -*
 - *Advice from the NSW Heritage Council for comment in relation to: the Archaeological significance of the site and the potential requirements and implications of archaeological impacts from future development under Section 140 of the NSW Heritage Act 1977 and the potential for the site to be listed on the State Heritage Register (in light of the findings of the Biosis Report)*
 - *An Aboriginal Cultural Heritage Assessment be prepared*
 - *A Conservation Management Plan for providing for the long-term conservation of significant Coke Works heritage components to be prepared.*

The Gateway Determination (20 August 2018) additionally stipulated that Section 9.1 Ministerial Direction heritage be satisfied prior to exhibition.

A number of Heritage reports have been prepared for the site in support of the draft Planning Proposal request, and as a requirement of the Gateway Determination, as follows -

- Historical Heritage Assessment (Biosis, August 2017).
- Aboriginal Archaeological Survey Report (Biosis, October 2015).
- Heritage Interpretation Strategy (Urbis, May 2019).
- Structural Assessment of Existing Structures (BG&E 2019).
- Conservation Management Strategy (Urbis May 2019).
- Updated Conservation Management Strategy (Urbis July 2019).
- Conservation Management Strategy Peer Review (Weir Phillips July 2019).
- Aboriginal Cultural Heritage Assessment (Kelleher Nightingale Consulting P/L May 2019).
- Historical Archaeological Test Excavation Report (Austral Archaeology March 2020).

Key Heritage Findings and Recommendations

Heritage in NSW is principally protected by the *Heritage Act 1977* and is designed to protect both known heritage items (such as standing structures) and items that may not be immediately obvious (such as potential archaeological remains or “relics”). Heritage items are listed as either local or state significant.

The original Historical Heritage Assessment undertaken by Biosis (2017) indicated that the site met the criteria for consideration of a State Significant listing, while the updated reports prepared by Urbis (2019) and Weir Phillips (2019) concluded that the site does not meet the criteria for State Heritage listing, however provides recommendations in relation to a local listing. Ultimately the decision making in relation to the State Significance is a question for the NSW Heritage Branch/Council.

The Heritage Interpretation Strategy prepared by Urbis (2019) concluded that the former Coke Works site holds heritage significance, demonstrated through its contribution to the growth of the coke and steel manufacturing industry and industrial expansion in the Illawarra area, the connection of the site to local collieries, the site’s electricity generation and the historic connections of a major employer for over a century to the local community. The long continual use of the site (over one hundred years) has created multiple layers of industrial fabric and social history.

The Biosis (2017) assessment concludes that the proposal to rezone the site for residential and commercial use represents the most viable option in terms of enhancing the heritage values, by making the history of the Coke Works accessible to the local community and the Illawarra through adaptive re-use and interpretation.

The elements which comprise the Coke Works contribute to varying degrees to this significance, the most important components being the C1 coke battery and the powerhouse chimney which have been assessed as having a high level of significance.

The Conservation Management Strategy and accompanying Heritage Interpretation Strategy provides for retention of the two chimney elements that “bookend” the C1 coke oven battery, designed to retain key iconic skyline elements of the coke works in the most visible locations, and partial retention of the C1 coke oven battery and the powerhouse building, combined with interpretation of other elements. It is the intention to establish a key view axis to the Brick Chimney. A key heritage strategy is fixing the Brick Chimney as the overall height limit (32.6 metres), while ensuring that significant retained and interpreted elements remain prominent for understanding and celebrating the history of the site.

The retained heritage structures in the civic plaza are proposed to be managed in future private ownership as part of the retail offer, with Council not intending to take on ownership of this asset.

The potential for archaeological deposits associated with the intensive use of the waterway for Aboriginal fishing and food gathering activities in the former filled in creek corridor was identified and the request made for further extensive Aboriginal community consultation in progressing the Planning Proposal.

An Interim Heritage Order (IHO) was issued in May 2019 to provide interim protection against demolition works, following advice that a Complying Development Certificate (CDC) for demolition works had been issued.

Heritage Amendment Planning Proposal progressed separately. Listing the site as a local heritage item in Wollongong LEP 2009 was endorsed by Council 29 June 2020 and the Heritage Amendment was notified on 11 September 2020. The CDC for demolition works has since been surrendered.

Potential for listing on the State Heritage Register is under assessment by Heritage NSW.

The DPIE advised in a letter dated 5 August 2020 that, given a separate Local Heritage Listing amendment to Wollongong LEP 2009 is being finalised and will facilitate heritage protection for the site, any inconsistency with Direction 2.3 Heritage Conservation is therefore considered justified and no further approval is required in relation to this Direction (Attachment 10).

Council's Heritage Officers have identified potential for the site to meet the criteria for State significance in relation to associative significance; aesthetic significance; technical achievement; rarity; and representative significance. On 19 August 2019, in accordance with the Council resolution and the Gateway Determination, the advice of the NSW Heritage Council was sought. Based upon their advice, Council Heritage Officers prepared a nomination accompanied by a Heritage Inventory Sheet and submitted it to Heritage NSW in December 2019.

On 5 February 2020 the nomination was considered by the State Heritage Register (SHR) Committee of the Heritage Council of NSW where it was resolved to progress the State Heritage nomination to the next stage of review and prioritisation. On 5 May 2020 the SHR Committee agreed the item is likely to meet the threshold for State Heritage significance and advised Heritage NSW to proceed with an assessment. The assessment will involve research and further consultation with the owners, Council and other key stakeholders. It is unclear how long this assessment will take.

The Design Review Panel in their report dated 19 July 2019 noted the site has a high proportion of heritage significant buildings and artefacts intrinsic to the character of the site and history of the area, and that the site is currently undergoing assessment as a State Significant item which may potentially have a significant impact on the placement of built form, retention of existing structures and curtilage around areas of significance. The DRP also acknowledged the laudable principles and statements in the Master Plan and associated documents which should be incorporated in the site specific DCP, cataloguing all heritage items on the site and indicating the level of retention/interpretation allowed and expected for each.

3.3 Flood Planning and Proposed Riparian Realignment

The site is located within the lower reaches of the Towradgi Creek catchment and is traversed by both Towradgi Creek along the southern extent and North Corrimal Creek which runs roughly north-west to south-east through the site. An unnamed drainage line also enters the site on the western boundary from under Memorial Drive and discharges into North Corrimal Creek. A significant area of the site and surrounding locality is subject to flooding, with areas of high and medium flood risk mapped on the site.

Council resolved (3 April 2018) that the draft Planning Proposal for exhibition include -

- *An amended Riparian Map identifying the modified riparian corridor reflecting the proposed creek re-alignment integrating with open space resources including pedestrian and cycle paths potentially linking with the broader green link network;*
- *A Geomorphological report be prepared to appraise the proposed watercourse realignment and verify that the proposed design (including alignment and channel dimensions) will enable a sustainable channel pattern and form and long-term channel stability;*
- *An updated flood study to include further pre and post development flood modelling, showing manning's roughness values used, details of the MBNM modelling, Risk Management blockage factors for the 1:100 ARI flood event and mapping of the Flood Planning Area, and a comparison*

between flood levels predicted by Cardno model and those predicted by Council's adopted model demonstrating parity between the models;

- *Review of the local stormwater system ensuring there is adequate capacity to transfer runoff during heavy wet weather events which result in local flooding;*
- *Hydrological review in relation to the rail corridor bridge to the south east of the site.*

The Gateway Determination (20 August 2018) stipulated that Section 9.1 Ministerial Direction flooding be satisfied prior to exhibition. Under the Section 9.1 Direction 4.3 – Flood Prone Land a Planning Proposal may not permit development that will result in significant flood impacts to other properties or is likely to result in increased government spending on flood mitigation.

A number of Flood reports have been prepared for the site in support of the draft Planning Proposal request, and as a requirement of the Gateway Determination, as follows -

- Corrimal Flood Study (Cardno, June 2017).
- Preliminary Development Footprint Review and Advice (Ecological June 2017).
- Geomorphology Assessment (Soil Conservation Service, March 2018).
- Corrimal Coke Works Flood Study (Cardno, May 2019).
- Supplementary Riparian Corridor Information (Clouston Associates, August 2019).
- Supplementary Information Regarding Creek Realignment (BG&E, August 2019).
- Corrimal Coke Works Creek Realignment Stability Assessment (BG&E, March 2020).

Key Flood Planning & Riparian Corridor Findings and Recommendations

The form and alignment of the existing North Corrimal Creek on site has been substantially modified in the past as a result of site filling and construction of online dams associated with previous industrial uses.

A key component of the Planning Proposal request is the proposed realignment of the existing 2nd order stream (North Corrimal Creek) to the western boundary of the site to alleviate local flooding issues and to provide a suitable flood free area to support the intended medium density residential development outcomes.

Significant modelling of a conceptual flood way has been undertaken. The updated Flood and Geomorphological studies incorporate modelling of future flood events and conclude that a realigned riparian corridor can be provided along the western edge of the site such that flood risk levels currently present on site can be significantly mitigated to enable a Probable Maximum Flood (PMF) event free area suitable for residential development, and adjoining lands up and downstream will have an improved or neutral impact.

The current Coke Works proposal is consistent with the Towradgi Creek Flood Study, and as the Towradgi Creek Floodplain Risk Management process continues, the Coke Works site will be included in future Council modelling.

The riparian corridor is proposed for RE1 Public Recreation zoning and future Council ownership. The proponent would be responsible for the preparation of a Vegetation Management Plan (VMP) for environmental restoration works to be undertaken for a period of 5 years prior to hand over of the asset to Council.

On 5 August 2020, DPIE advised that, based on a review of the updated flood information provided, the Planning Proposal is now consistent with Ministerial Direction 4.3 – Flood Prone Land (Attachment 10).

The form and alignment of the existing North Corrimal Creek has been substantially modified in the past as a result of site filling and construction of online dams associated with previous industrial uses. These

past impacts have been more pronounced in the upstream and central sections of the site. Downstream of the dams the creek flows more or less along the original alignment evident in the historic 1948 aerial image (Geomorphology Assessment, Soil Conservation Service 2018). The riparian zone upstream of the dam is dominated by exotic weed species, while downstream a mixture of native and exotic species is evident.

The North Corrimal Creek flowing generally north to south through the middle of the site was dammed to provide make-up water for production. Two lagoons were formed – a Recycle Dam for collection and recycling of process water and a South Dam for provision of make-up water to the Recycle Dam. Stormwater and other surface water accumulation on active areas of the site was directed to the Recycle Dam via sumps and surface drains. The Coke Works was licenced to extract 140ML per year of water from North Corrimal Creek (via South Dam). A dam was present at the current location as early as 1951. Site personnel reported that during periods of heavy rain, North Corrimal Creek over topped the South Dam wall.

The proposed realignment to the western and southern most portions of the site will involve substantial civil works (to accommodate a main channel averaging 11 metres wide, at depths between 5-8 metres) that will include diversion of the sewer trunk main and cut to fill bulk excavation works on the site. As part of any development, riparian corridor buffer zones will be required along the creek lines. Stormwater quality improvement devices and stormwater detention facilities will also be required and may be separate or incorporated within the riparian corridor.

A Corrimal Flood Study prepared by Cardno June 2017 assessed that the flood risk levels currently present on site can be significantly mitigated to enable a Probable Maximum Flood (PMF) event free area of approximately 12.85 hectares. The PMF is the largest flood that could conceivably occur at a particular location. The realignment proposes to divert 550m of channel from the residential neighbourhood alongside the western boundary (Memorial Drive) and return it to the existing creek downstream of the dam.

The Flood Study outlined the potential advantages of adopting the watercourse realignment as follows -

- Significantly improves flooding conditions/drainage within the existing Cross Street and Railway Street residential areas located upstream of the ICC site.
- Improve water quality regimes on site, also benefitting water quality downstream in Towradgi Creek.
- Easy connection of existing drainage on Memorial Drive to the proposed North Corrimal Creek without large culverts or pipe network.
- Opportunity to rehabilitate degraded areas of the ICC site. Realigning the channel could potentially improve environmental conditions by way of rehabilitating the riparian corridor and removing artificial fill introduced on the ICC site and planting native vegetation in the newly proposed Vegetated Riparian Zone.
- Increase developable area in the north-east of the ICC site. The north-eastern site area is generally flood-free and therefore offers an ideal opportunity for development. Realigning the channel to the western ICC site boundary may further increase this flood-free area in the central site area, thereby increasing development opportunities and reducing existing flood hazards.

On 9 December 2019, Council adopted the Towradgi Creek Flood Study to define the existing flood behaviour in the Towradgi Creek study area and consider the influence of potential climate change on future flood behaviour. Using the latest computer flood modelling technology, a wide range of catchment characteristics can be modelled, including calculation of peak flows and critical storm durations via simulation of complex catchment behaviour. The current Coke Works proposal is consistent with the Towradgi Creek Flood Study, and as the Towradgi Creek Floodplain Risk Management process continues, the Coke Works site will be included in future Council modelling.

Any proposed creek realignment will require a controlled activity approval under the *Water Management Act 2000*. EcoLogical Australia (June 2017) prepared a development footprint review identifying that the current creek has been heavily modified and degraded with 490m of waterway historically/previously

realigned within the site. Preliminary consultation with Department of Industry - Crown Lands and Water Division (CL&W), formerly DPI Water, indicated general agreement with the EcoLogical report in terms of water courses within the site and the recommended riparian outcomes. The CL&W will undertake further assessment at development application stage, however recommended further detailed design be undertaken with consideration of the DPI Water 2012 Guidelines for riparian corridors on waterfront land, as well as the following key points -

- Realigned and reconstructed watercourses within the site are to be designed as natural functioning streams including emulation of natural geomorphic units and meander.
- Detention requirements must be designed in accordance with CL&W Guidelines.
- The riparian corridor is to be established using fully structured provenance native vegetation.
- The proposed bike track must be constructed in the outer 50% of the Vegetated Riparian Zone in accordance with the DPI Water Guidelines.
- All non-riparian uses within the required 20m Vegetated Riparian Zone are to be offset in accordance with the Guidelines for riparian corridors on waterfront land.

Preliminary consultation with the (then) Office of Environment and Heritage (OEH) indicated support for the revitalisation of this former industrial site in principle, including a long-term conservation outcome for the riparian corridor and retained areas of remnant native vegetation. The retention of Illawarra Lowlands Grassy Woodland Endangered Ecological Community (EEC), rehabilitation of the existing degraded watercourse and creek realignment works to mitigate flood risk are considered to be environmental priorities.

The updated Flood Study, Geomorphological assessment and Geotechnical assessment incorporate modelling of future flood events. In the post development scenario, one of the key improvements is a decrease in flood levels on Railway Street (see Figures 7 and 8 below).

Figure 9: Probable Maximum Flood (PMF) Extent

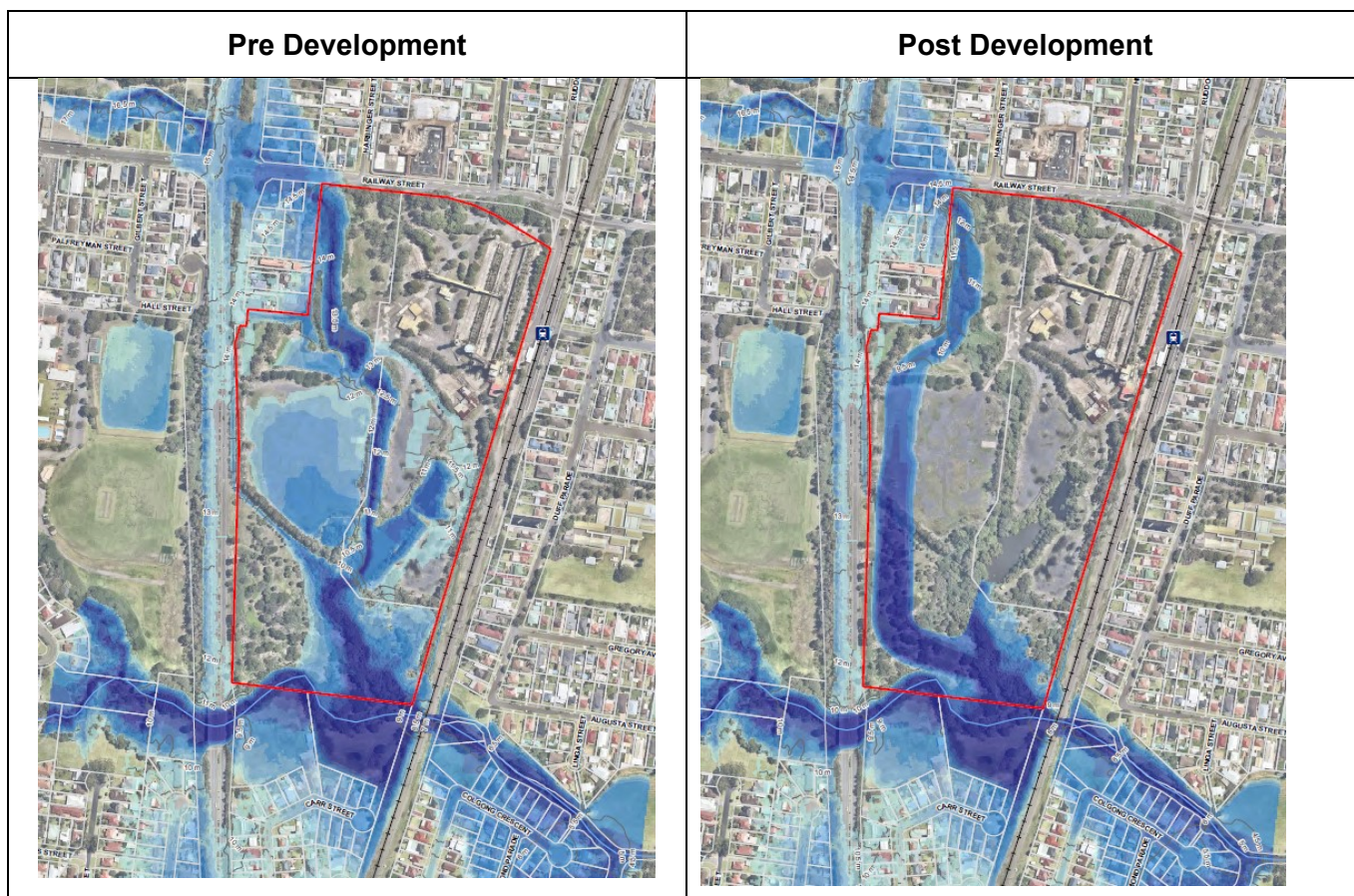
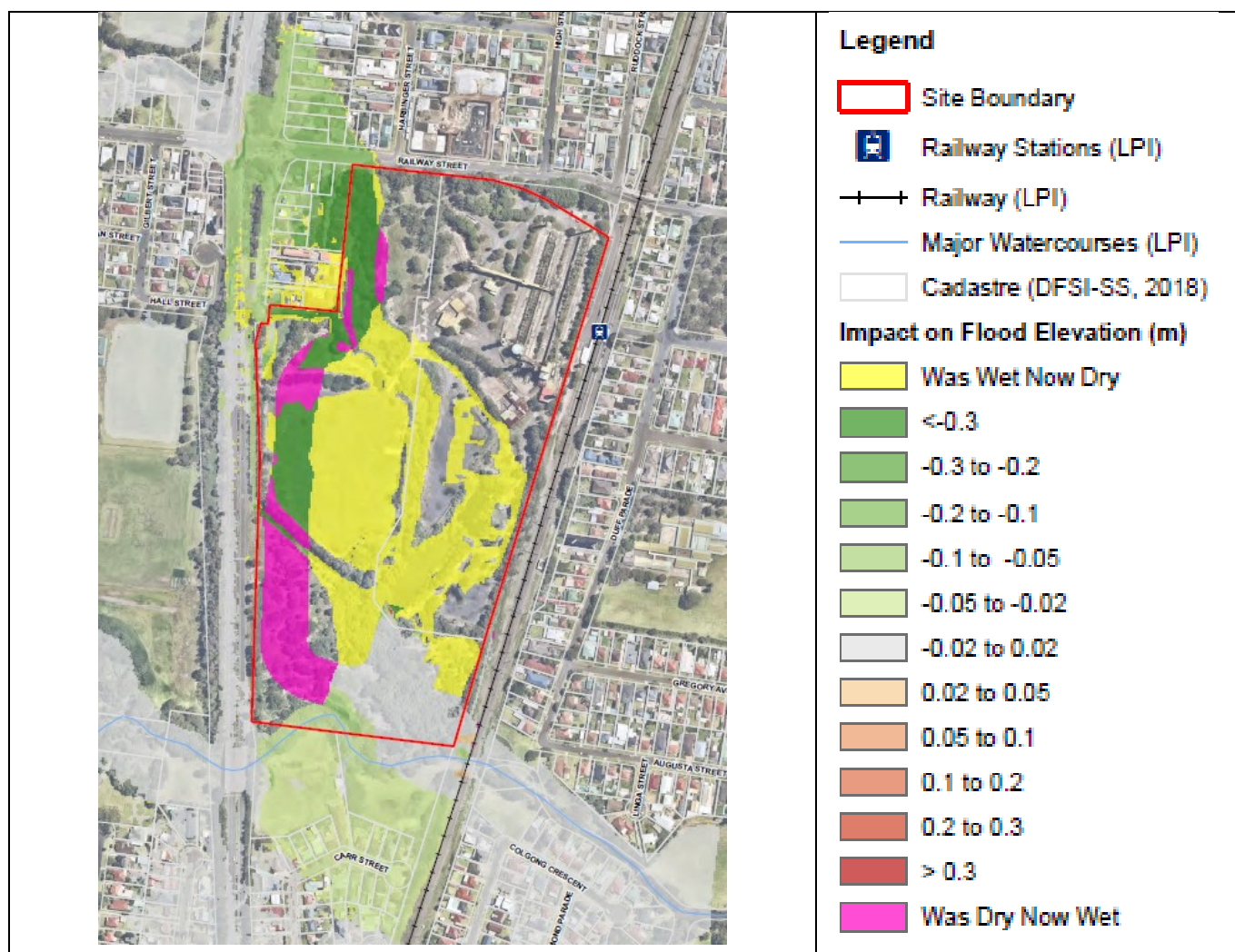


Figure 10: Probable Maximum Flood (PMF) - Post Development Flood Impacts



Preliminary geotechnical assessment and hydraulic modelling indicate the potential to achieve stable bank batters with the proposed realignment, subject to further geotechnical design, and indicates that flow velocities along the bulk of the realignment do not exceed erosion thresholds. The Geomorphological assessment prepared by SCS (2018) indicates the need for stabilisation and energy dissipation measures such as rock protection, pools, rock steps and benches. The assessment concludes that, provided design and construction is undertaken in accordance with geotechnical, civil and structural engineering accepted practice, the realigned channel can withstand flood velocities and perform in events, with both the 1% AEP and PMF events wholly contained.

The Master Plan and updated draft Planning Proposal indicate the intention to zone the realigned riparian corridor RE1 Public Recreation for dedication to Council as a public space. The Wollongong DCP 2009 Chapter 23 Riparian Land Management requires the preparation of a Vegetation Management Plan (VMP) for all riparian lands intended to be transferred to Council ownership via dedication. The VMP will outline the proposed environmental rehabilitation program tasks and detailed costings designed to revegetate and provide long term bank stabilisation and will reference relevant flood modelling studies to ensure that proposed works do not increase the flood affectation upon surrounding properties. All necessary revegetation or other works are to be completed in accordance with the approved VMP to the satisfaction of Council prior to the transfer of land being accepted. The VMP would be subject to a five-year restoration works and maintenance period to ensure the riparian corridor is functioning and in stream works stabilised to the satisfaction of Council prior to hand over of the asset.

To be considered as a future Council asset for the safe recreational use of the wider community there are other key considerations centring on usability and safety of the space created, the desire to create a natural vegetated riparian corridor offering a diversity of habitat opportunities, and ability of Council to maintain that asset. The establishment of viable riparian vegetation is critical to the successful construction of waterways in urban developments, playing an essential role in maintaining a healthy waterway. These considerations have implications for the width of the proposed corridor, for example a natural form waterway will require a larger cross-sectional area (i.e. sufficient width) than a straight-lined channel to convey the same flow.

While the submitted studies conclude that the proposed riparian design concept results in a channel profile capable of containing flood events, it is envisaged that the design will only result in limited passive recreation opportunities for the public, restricted to the eastern embankment. The western embankment is proposed to be heavily vegetated and with no intended public access, designed to minimise maintenance, support long term stability of the steep slope, and provide an improved environmental outcome. Council Officers have expressed concern that the steepness of proposed batters and near vertical sections (up to 1m high) at the creek edge (and hence the need for engineered solutions such as retaining walls and stairs on the eastern embankment) may result in an area not suitable for public recreational use in terms of access problems, CPTED issues due to changing levels making surveillance difficult, and the possibility of a dangerous environment should a flash flood occur. Clarification of the resultant look and feel of the riparian space that will be created through the proposed design, including the precise location of the shared walking/bicycle path proposed to run through the site along the eastern embankment, will require further consultation.

Council Officers carefully considered accepting Council ownership of the realigned riparian corridor as an asset in terms of the large and steep batters and channel depths proposed, proximity to Memorial Drive and the potential risk of impact to stability, function and safety of the freeway, as well as public access and maintenance constraints. Meetings and a design workshop were organised between the proponent and Council engineers to improve the design of the proposed riparian zone through a greater understanding of the following -

- The maintenance requirements and ability for Council to manage the area.
- The rationale on batter slope requirements.
- The critical issues of maintenance and asset burden.
- The risk associated with geomorphological changes during extreme flood events, and reduced setbacks to Memorial Drive and internal roads.

The table below illustrates the key issues raised and the discussion of options.

Table 5: Key Riparian Issues

Concern	Council Officer Comment
<p>The proposed batter with a maximum one in two grade is too steep – a maximum batter slope of one (vertical) in four (horizontal) is considered necessary to enable safe access/maintenance, reduce the risk of scour/erosion and embankment instability, and avoid the need for engineered stability measures (such as terracing, retaining walls and other engineered treatments).</p> <p>Batter slopes at 1:5 or greater provide safe public access to the waterway and allow for mowing. Any batters steeper than 1:3 require bank stabilisation works, particular vegetation designs and consideration of cross-sectional compensation on the opposite bank.</p>	<p>Steeper batters, and engineered structures could be utilised within the design provided the Vegetation Management Plan was one that required little or no maintenance (i.e. to replicate a natural system as close as possible). A design workshop demonstrated sufficient space for Council maintenance vehicles to access the western and lower sections of the creek</p> <p>The result is an engineered drainage channel of 5 to 8 metres in depth, with steep slopes on both the eastern and western sides (1:2 and 1:3), requiring retaining walls for stabilisation. This will serve a flooding and drainage function.</p>

	For recreational uses to be incorporated into the riparian corridor area a wider RE1 zone would be required (in order to achieve less steep slopes of 1:4 or 1:5 for safe public access).
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Concern	Council Officer Comment
<p>Proximity to Memorial Drive: concern not to undermine the road due to scour/slumping in an extreme flood event given Wollongong's flash flood history, and RMS advice that provision for maintenance (or other) access will not be provided from Memorial Drive.</p> <p>Council officers have clarified that any setback is to be measured from the road reserve boundary and not the edge of pavement, and have noted RMS advice that a minimum 10 metre set back is required from the road reserve boundary to the top of the channel plus an additional five metres to accommodate earth berms (where necessary), and would require a maximum batter slope of 1V:4H (which are in line with Council requirements). Council advised design needs to manage worst-case scenario for slumping and failure and what is the best setback to reflect this</p>	<p>TfNSW confirmed need to ensure the top of bank would be setback 10m from Memorial Drive road reserve.</p> <p>The applicant provided an amended design that increased the creek bank setback from 5m to 10m from the road reserve, and also provided an updated study "Corrimal Cokeworks Creek Realignment Stability Assessment 19 March 2020" which analysed the geomorphic and geotechnical constraints of the re-engineered creek. This information was reviewed by TfNSW who agreed that the creek setback was appropriate.</p>
<p>A suitable meander design may not be able to be accommodated within the confines of the proposed channel width. If a channel is to be rehabilitated, it is often desirable to introduce some channel meanders to improve aesthetics, habitat and channel diversity and also increase the effective channel length. Increasing channel length is one of the most effective ways of reducing the bankfull flow velocity. Potential for scouring and blow out at the southern corner/end where a sharp change in direction of flow is proposed.</p>	<p>While the creek itself was defined in its width and geometry, the applicant contended that the detailed engineering design at the DA stage would include a natural meander at the invert of the creek.</p> <p>While the additional geomorphic investigations indicate that due to the natural subsoil conditions for the creek's proposed location will be suitable to manage the velocity thresholds identified in the flood study, this will need to be carefully managed through any future development application</p>
<p>Ecological report requirement noted for a body of water to be included in the design at the southern end as a "dipping pond" for the GHFF.</p>	<p>The current creek work design does not include detail of a proposed open body of water; however, it is considered that there should be sufficient space to provide this in a future detailed design.</p>

A controlled activity approval under the *Water Management Act 2000* is required for any proposed creek realignment, to ensure that proposed modifications are designed and constructed to protect and enhance water flow, water quality, stream ecology and riparian vegetation. Asset protection zones and all ancillary infrastructure such as utility easements, detention basins, water quality control structures, roads and paths/cycle ways should be located outside the riparian corridor. Some works may be allowed to be located within the outer 50 percent of the vegetated riparian zone. Updated studies will be provided to CL&W during the public exhibition to establish they are satisfied with the proposed riparian design. Specifically, we will seek comment on whether the proposed design meets the DPI Water 2012 Guidelines for riparian corridors on waterfront land in terms of the ability to -

- Meet the requirements for riparian corridor width.
- Accommodate fully structured native vegetation.
- Accommodate natural watercourse functions.
- Establish natural bed and bank profiles (e.g. meanders, riffles etc).
- Allow for the movement of sediment and woody debris.
- Prevent scour and erosion of the watercourse bed or banks in storm events.
- Accommodate site hydrological conditions (e.g. maintain low flows).
- Ensure no increase in velocities that result from the constriction of flows.
- Protect against scour by designing and providing scour protection.
- Treat stormwater runoff before discharging it into the riparian corridor.
- Stabilise and rehabilitate disturbed areas to establish the environmental integrity of the realigned corridor.

3.4 Ecological Considerations

Council resolved (3 April 2018) that the draft Planning Proposal for exhibition include -

- *A revised Ecological Impact Assessment to address the potential impacts to micro bats roosting on the site and potential for Green and Golden Bell Frog habitat to be present on the site.*

The Gateway Determination (20 August 2018) additionally required the completion of a revised ecological assessment to consider the potential consequence of rezoning on environmental values prior to exhibition.

A number of Ecological reports have been prepared for the site in support of the draft Planning Proposal request, and as a requirement of the Gateway Determination, as follows -

- Flora and Fauna Assessment (EcoLogical, September 2017).
- Flora and Fauna Assessment (EcoLogical, May 2019).
- Letter 22 July 2019 EcoLogical “*Microchiropteran* bat habitat assessment”.
- Letter 5 August 2019 EcoLogical “Management and monitoring of the *Pteropus poliocephalus* (Grey-headed Flying-fox)”.
- Grey-headed Flying-fox Camp Proposed Management Actions (EcoLogical March 2020).
- Letter 26 March 2020 Ecosure “Corrimal Flying-fox Strategy Peer Review”.

In support of the draft Planning Proposal a Flora and Fauna report was prepared (EcoLogical 2017), with a second report submitted as a Gateway requirement (EcoLogical 2019). The assessment included a review of relevant data and background literature and a site inspection focusing on validating/refining previous vegetation community mapping, compiling a list of flora observed, identifying flora and fauna habitats, and identifying other features of conservation significance.

Key Ecological Findings and Recommendations

There is potential for threatened microbats to utilise the site, and any future demolition will be subject to further assessment.

The site does not contain habitat for the Green and Golden Bell Frog.

One threatened fauna species, *Pteropus poliocephalus* (Grey-headed Flying-fox) occupies the site. A minimum 100 metres is recommended between the mapped core camp area and future residential development. There are financial and staff resourcing liabilities to consider in the decision to accept

dedication of land into future Council ownership and stewardship of a State and Nationally listed threatened fauna species and its habitat. Should Council resolve to accept dedication of the EEC and GHFF camp, a funding mechanism will be required.

Two patches of Illawarra Lowlands Grassy Woodland (listed as EEC under the *NSW Biodiversity Conservation Act 2016*) have been identified on the site – one in the north and one in the south.

The DRP in their report recommended that wherever possible existing remnant native vegetation should be retained and protected. The OEH submission (2017) also stressed the need to demonstrate avoidance of impacts upon threatened species and ecological communities and noted that clearing thresholds under the *NSW Biodiversity Conservation Act 2016* relate to the clearing of all native vegetation, not just EECs.

3.4.1 Fauna

***Microchiropteran* bats (microbats)**

Targeted survey for threatened *microchiropteran* bats (microbats) was conducted throughout the study area, with seven buildings and two rows of coke ovens assessed for potential habitat. The survey stated that no threatened microbats were detected and concluded that they are unlikely to utilise the resources of the subject site.

Following the Council resolution (8 April 2018), a further site inspection of all buildings was undertaken by EcoLogical to determine their use by microbats or suitability as microbat habitat. In a letter dated 22 July 2019, the conclusion drawn was there is a lack of suitable habitat, absence of signs of use or occupation and lack of records from previous ultrasonic detection surveys, indicating “*that microbats rarely if ever use the structures, and if they ever do, it is only very occasionally and in small numbers*”. The letter did acknowledge that it is possible that there may be occasional use of buildings by individual microbats in other seasons of the year, and hence recommended that an ecologist with demonstrated experience in locating microbats inspect structures prior to any demolition and be present during demolition to respond to the presence of any microbats.

Council Officer assessment is that there is potential for threatened microbats to utilise the subject site for roosting and foraging, and hence any demolition applied for will be subject to further assessment in this regard.

Green and Golden Bell Frog

The potential for Green and Golden Bell Frog was investigated in the updated Flora and Fauna Assessment (EcoLogical 2019) in relation to the two dams on site and the riparian corridor, given that there are recorded sightings approximately 1km to the east associated with Bellambi Lagoon. The assessment indicated that the preferred habitat for the Green and Golden Bell Frog are small and shallow ephemeral pools containing sedges or rushes which form fringing vegetation, sunny bank areas, and refuge and basking habitat in the form of rocks and woody debris. The study concluded that the site does not contain potential habitat for the Green and Golden Bell Frog.

***Pteropus poliocephalus* (Grey-headed Flying-fox)**

One threatened fauna species, *Pteropus poliocephalus* (Grey-headed Flying-fox) was found occupying a small patch of Forest Red Gum Thin-leaved Stringybark Grassy Woodland adjacent the dam and existing riparian corridor area at the southern extent of the study area in the EcoLogical 2017 study. The GHFF is listed as vulnerable under both the *Biodiversity Conservation Act 2016* and *Environment Protection and Biodiversity Conservation Act 1999*. It is listed as a threatened species because their numbers have rapidly declined over a relatively short period of time and in recognition of their essential ecological role in pollination and seed dispersal.

The Gateway determination required a revised ecological assessment to consider the potential consequence of rezoning on environmental values. The updated Flora and Fauna report (EcoLogical May 2019) confirmed the presence of the GHFF during the survey at the southern extent of the study area. The Study stated that the GHFF were likely to be using the native vegetation and riparian corridors

on site for roosting and foraging purposes, and that the camp had fluctuated in numbers since May 2017, with the inspection at the time of the report estimating the camp was supporting 150-250 individuals.

The recommendations from the report were as follows -

- Future development of the site will require careful planning and consideration of the interface between the GHFF and humans, and that consideration should be given to the Flying-Fox Camp Management Policy (OEH 2015) including the preparation of a Camp Management Plan consistent with the Camp Management Policy.
- Land currently occupied by the species should be zoned appropriately to ensure the retention of habitat and prevention of conflicting land uses.
- The remainder of the site is considered a low ecological constraint due to the presence of cleared lands, existing derelict infrastructure and Urban Native and Exotic Cover – the areas mapped as low ecological constraint are preferable locations for development.
- Future detailed assessments at the Development Application stage will be required to determine the extent of the area utilised by the GHFF and how the camp is used.

Since May 2017, the camp numbers have fluctuated between no evidence of any individuals in July 2019 to numbers in excess of 6,000 recorded in January 2020, including nursing females. Research has confirmed that fluctuation of numbers is not unusual, with camps emptying over cooler months and then individuals returning in the warmer months.

The Design Review Panel in their report dated 19 July 2019 noted concern that the 2019 Master Plan depicted the ecologically sensitive area to the south of the site flanked by four to five storey building forms on three sides. The DRP concluded that existing wildlife habitats on site should be preserved and incorporated in such a way as to ensure they can co-exist in harmony with the proposed development, noting that the GHFF camp will create a significant level of disturbance to people living in close proximity.

Council Officers have undertaken due diligence extensive research of publications and consultation with State Agencies, Flying-fox specialists including OEH/ESS Conservation Planning and Threatened Species officers, other Councils and the OEH facilitated NSW Flying-fox Land Manager Network in an effort to understand the environmental and management implications of rezoning land for residential use in close proximity to a GHFF colony. This has identified a range of challenges other Councils Australia wide currently face in the management of GHFF camps (Attachment 11). As part of the evidence based assessment, Council officers have attended a number of site visits and used GPS technology to map distances from the edge of camps to monitor noise and smell impacts and have reviewed the DPIE “Working with communities living with Flying-foxes” web page and Flying-fox Management Plans commissioned by a number of Councils.

The Flying-fox Camp Management Policy 2015 (OEH Policy 2015) outlines planning options when considering the location of development near a GHFF camp. The Policy suggests the identification of GHFF camps as a development constraint to prevent future conflicts arising over proposed development, and zone the camps “environmental protection” to ensure the long-term security of GHFF camps, including those used irregularly. The Policy also identifies the need for appropriate zoning of land adjacent GHFF camps, particularly when this land is undeveloped or redevelopment is planned, in the context that camps expand or move locally. The Policy further states that certain land uses are inherently more compatible in the vicinity of a GHFF camp than others, for example light industrial or rural compared with high density urban residential.

The OEH Policy 2015 recommends that landowners consider the location of historically and currently occupied camps or potential GHFF camps early in strategic planning processes, particularly when planning future residential areas, schools or other sensitive infrastructure. This is in recognition that most camps are not occupied continuously. In addition to recognising the presence of existing camps through appropriate land use zoning and development control plans, it is also stated that new development proposals give consideration to appropriate buffers such that any additional hazard reduction activities that become necessary will be able to occur without being unduly impacted on by the existing camp

locations. Sites that have the potential to function as GHFF maternity camps should be a priority for conservation.

The Department of Environment, Energy and Science (EES, formerly OEH) was consulted as to appropriate buffers between the proposed development and the camp. The OEH response recommended the integration of buffers between the camp and residential development *“as wide as reasonably practicable with guidance of up to 300m in width, but determined on a case by case basis having regard to land use conflict and level of impact from Flying-foxes”*. The response acknowledged that a camp of 2,000 individuals may significantly increase impacts where the camp is in such close proximity to houses – *“there exists the potential for significant issues with the co-location of the camp and proposed medium density residential use”*. The letter stated that the dispersal of Flying-foxes is not preferred and should be considered as a last resort only, as dispersal is likely to be costly, ongoing and unlikely to succeed. The submission additionally acknowledged ongoing management issues for Council, given that the draft Planning Proposal includes the proposed dedication to Council of the realigned riparian corridor, including the vegetation patch supporting the GHFF camp.

The original 2017 draft Planning Proposal proposed the removal of 0.06ha of the GHFF habitat, with additional habitat to be provided through the proposed revegetation of the realigned riparian corridor along the western extent of the boundary. The Forest Red Gum – Thin-leaved Stringybark Grassy Woodland which forms part Illawarra Lowlands Grassy Woodland is an Endangered Ecological Community (EEC) and as such Council Officers and the ESS have indicated they would not support any removal of this vegetation community.

The proponent has responded to Council officers’ concerns about the need to maintain a suitable buffer by undertaking fortnightly monitoring of camp use and targeted surveys and commissioning a number of additional reports to provide advice on a suitable buffer. The survey and monitoring report (EcoLogical March 2020) concluded that the camp size fluctuates over time as does the area of occupation and the camp is expected to hold between 100-7,300 bats depending on resource availability, weather and seasonability. The report identifies a core camp estimated to be approximately 100-500 individuals and referenced a camp size fluctuation in January 2020 which may be the result of the unprecedented bushfires over the 2019/2020 summer.

The commissioned reports recommended buffers ranging from 20 metres to 50 metres to minimise the potential conflicts between future residents and the bats. Based on Council due diligence extensive research, the proponent was advised that a minimum 100 metre buffer would be required between the GHFF camp and any future proposed residential development. The 2020 Master Plan now incorporates a 100 metre buffer between the core mapped camp area and planned future development, noting that the total mapped GHFF area is more extensive. The proponent has requested that the Site-specific Development Control Plan include a +/- 10 metre provision to that 100-metre buffer.

The proponent intends to dedicate the land containing the GHFF camp into Council ownership. Implementation of Flying-fox Management Plans require substantial ongoing funds. The draft Planning Proposal, while outlining the need for a Management Plan, remains silent on the issue of how the proposed ongoing management actions would be funded.

Should Council resolve to accept dedication of the EEC and GHFF camp, a funding mechanism will be required for ongoing maintenance. A Biodiversity Stewardship Agreement, which would be established prior to finalising the Planning Proposal, is a possible funding mechanism for further investigation. If the proponent is not agreeable to entering into such a funding agreement, Council may decide not to accept dedication of the land. Possible options include -

- Zone the core habitat an environmental zoning (e.g. E2 Environmental Conservation) rather than RE1 Public Recreation and not accept the transfer of the land; or
- Seek a Planning Agreement contribution for ongoing maintenance of the camp and accept dedication of the land; or
- Seek a Planning Agreement contribution to cover the cost of establishing the camp as a Biodiversity Stewardship site and accept dedication of the land.

This matter will be further explored prior to finalisation of the Planning Proposal.

3.4.2 Vegetation Communities

The Flora and Fauna reports undertaken by EcoLogical in 2017 and 2019 identified that the vast majority of the site supported areas of cleared land (roads, infrastructure, water bodies) or weeds/exotics. The site has a long history of disturbance associated with its use as a Coke Works and it was noted that the vegetation communities across the site have been subjected to moderate to high levels of disturbance including vegetation clearing/thinning of canopy layer, infestation of exotic species and modification of landform and soil profiles.

Three vegetation communities were mapped within the study areas -

- Forest Red Gum Thin-leaved Stringybark Grassy Woodlands.
- Acacia Scrub.
- Urban Native and Exotic.

Forest Red Gum Thin-leaved Stringybark Grassy Woodlands and Acacia Scrub are both native vegetation communities. The Forest Red Gum Thin-leaved Stringybark Grassy Woodlands vegetation community forms part of the Illawarra Lowlands Grassy Woodland in the Sydney Basin Bioregion, an endangered ecological community (EEC) listed under the *NSW Biodiversity Conservation Act 2016* (BC Act). Two patches of Illawarra Lowlands Grassy Woodland have been identified on the site – one in the north and one in the south. Although the patches are identified as EEC and considered a high ecological constraint, the Forest Red Gum Thin-leaved Stringybark Grassy Woodlands was identified as being in a highly modified and disturbed condition with high levels of exotic species, planted non-local native species present and modified landforms within the area of this community. The report concluded that neither of the patches present met the Environment Protection and Biodiversity Conservation Act 1999 definition of the community because the groundcover was <30% native. Although the vegetation in the north of the site has high conservation status (EEC), its conservation significance is low to moderate based on patch size, current and proposed future surrounding land use and the ongoing impacts that are likely to prevail.

It should be noted that the initial ecological report submitted with the draft Planning Proposal request (EcoLogical 2017) included the mapping of Moist Blue Gum Blackbutt Forest in the south of the site. Council received submissions from community members in response to this initial vegetation mapping, speculating about the presence of a patch of 'rare rainforest'. To respond to community concerns Council agreed to follow up and inspect the patch of vegetation in question. Prior to Council's site inspection, EcoLogical undertook more detailed vegetation surveys according to the Biodiversity Assessment Method including plot based floristic, structure and condition assessments and subsequently revised the initial vegetation mapping. Following the joint site inspection with the EcoLogical ecologists, including discussion on the rationale for the revised assessment, it was agreed that the area should be mapped Forest Red Gum Thin-leaved Stringybark Grassy Woodland.

Planted individuals of *Eucalyptus scoparia* which is listed as endangered under the BC Act and vulnerable under the EPBC Act, were identified on site, although these planted individuals were outside their natural range and do not represent constraints to the proposed rezoning. The study area was also considered to represent potential habitat for highly mobile threatened fauna including bats and migratory birds. It is noted that a small area in the south east corner of the site sits within the SEPP mapped coastal environment area layer and thus the aims and objectives of the policy will need to be considered when planning for the site.

Within and surrounding the most disturbed parts of the study area stands of vegetation were comprised of exclusively exotic species or consisted of rows of non-local native planted trees with disturbed predominantly exotic understory. An area of approximately 8.28ha of urban native and exotic was mapped within the study area.

Proposal

The proposed rezoning would result in the realignment and revegetation of the riparian corridor along the western extent of the study area, with the following result for vegetation communities:

Table 6: Ecological Community Outcomes

Ecological Community	Total (ha)	Retained (h)	Impacted/Removed (ha)
Forest Red Gum Thin-leaved Stringybark Grassy Woodland	1.28	0.69	0.59 (in north)
Acacia Scrub	1.29	0.17	1.12
Cleared land	7.29	0	7.29
Urban Native and Exotic	8.28	0	8.28
Total	18.14	0.86	17.28

The updated EcoLogical report (2019) states that any future Development Application stage will require the submission of a Biodiversity Development Assessment Report (BDAR), due to the proposed impacts (clearing) to native vegetation. The BDAR would be prepared consistent with the Biodiversity Assessment Methodology and outline the offset requirements associated with the proposal. The EcoLogical report recommends at DA stage the preparation of a Vegetation Management Plan for the patches of vegetation to be retained and for the riparian corridor to be revegetated. The Vegetation Management Plan would be prepared consistent with DPI Water “Guidelines for vegetation management plans on waterfront land”, including consideration of bushfire risks, Council requirements and proposed landscaping. The proposed rezoning would retain and restore the EEC in the south of the site.

The Design Review Panel in their report recommended that wherever possible existing remnant native vegetation should be retained and protected. The OEH submission (2017) also stressed avoidance of impacts upon threatened species and ecological communities and noted that clearing thresholds under the Biodiversity Conservation Act relate to the clearing of all native vegetation, not just EECs. Where development proposals affect remnant vegetation, the framework identifies a three-step process to ensure an overall increase in the extent and quality of native vegetation -

- Avoid adverse impact resulting in vegetation clearance.
- Where impact cannot be avoided, minimise impact through careful planning, design and management.
- Offset any vegetation loss through appropriate offsets.

Council Officers have reviewed the submitted reports and attended site visits and have concluded that the level of flora and fauna survey effort, vegetation mapping and classification is satisfactory for the purposes of the planning proposal.

The Corrimal Community Action Group (CCAG) has requested consideration of an environmental protection zoning for the EEC to ensure retention of habitat and the conservation outcome for the GHFF camp. Preliminary consultation with the (then) Office of Environment and Heritage also raised the potential for the riparian corridor to be zoned a suitable environmental zoning.

3.5 Contamination & Geotechnical

Council resolved (3 April 2018) that the draft Planning Proposal for exhibition include -

- *A revised Remediation Action Plan addressing further sampling under the structures on the site; for PCBs associated with the powerhouse building and transformers; address exceedances of ecological investigation levels; and address materials containing SMF and Crystalline Silica associated with the coke ovens.*

A number of reports have been prepared for the site in support of the draft Planning Proposal request, and as a requirement of the Gateway Determination, as follows -

- Corrimal Coke Works Asbestos Register (September 2013).
- Preliminary Site Investigation (Environmental Strategies, March 2014).
- Phase 2 Detailed Site Investigation (Environmental Strategies, July 2014).
- Site Audit (C.M. Jewell & Associates P/L, November 2014).
- Corrimal Coke Ovens Assessment (Arcadis 2017).
- Phase 1 Preliminary Site Investigation (PSI) (Environmental Strategies P/L).
- Additional Environmental Assessment (Arcadis April 2017).
- Geotechnical Assessment (Douglas Partners, May 2017).
- Remediation Action Plan (Arcadis, May 2017).
- Environmental Noise and Vibration Assessment (Renzo Tonin & Associates, May 2019).
- Remediation Action Plan (Arcadis, May 2019) and Endorsement of Remediation Action Plan (ZOIC Environmental P/L, May 2019).
- Letter Arcadis 8 May 2019: Remediation of Land.

Key Contamination and Geotechnical Findings and Recommendations

State Environmental Planning Policy 55 – Remediation of Land (SEPP 55), requires Council to consider whether land is contaminated and is required to be remediated before permitting sensitive land uses such as residential and recreation. The proposed remediation works are considered to be classified as Category 1 remediation works, which require development consent.

The Remediation Action Plan (RAP) for the site (Arcadis May 2019) concludes that “although concentrations of contaminants of concern were detected above environmental assessment criteria, they are isolated and minimal compared to the wider site footprint”. The RAP outlines a Remediation Strategy for the site, with the expectation that the site can be made suitable for the proposed mixed-use redevelopment.

The RAP confirms the site is subject to a statutory Site Audit. A Non-Statutory Audit has been completed by Zoic Environmental P/L, a NSW EPA Auditor accredited (No. 0802) under the *Contaminated Land Management Act 1997*. The site audit statement (SAS) and associated site audit report (SAR) will confirm the suitability of the site for the proposed mixed residential land use development and open space associated with a creek alignment. The Audit is being staged, with Stage 1 signed off in an Interim Advice letter endorsing the Remediation Action Plan (RAP).

The Auditor considers the Arcadis RAP (May 2019) is practical and technically robust for the contamination identified on site and that, if implemented, the site is capable of being made suitable for the proposed redevelopment, subject to a number of conditions.

The proponent has been advised that any on site containment would need to be placed under commercial buildings held in private ownership with appropriate section 88B (restriction on the use of land) wording and section 10.7 (Planning Certificate) notation and Environmental Management Plan. It is not proposed to allow emplacement in the riparian or parkland areas, or under roadways due to potential future problems when these areas may need to be disturbed/dug up for maintenance etc.

The Geotechnical Report (Douglas Partners 2017) stated that their findings / analysis must be considered as being preliminary in nature and that additional geotechnical investigations and assessment will be required as the design of the development proceeds – particularly in areas of proposed creek alignments and where excavation into the natural soil profile will be required to construct deep service trenches.

3.5.1 Contamination

Coke manufacturing (the production of metallurgical and foundry coke) has occurred at the site since 1912. ICC made the decision to cease coke manufacturing and close the Corrimal facility in 2014. A Phase 1 Preliminary Site Investigation (PSI) was commissioned as a due diligence exercise to determine the contamination status of the facility in preparation for closure. This PSI included a detailed description of the former site uses and operations:

Table 7: Former Site Uses

Site Area	Description
A – Production Facility	Area comprised coal storage, coke oven batteries and ancillary operations, including car parking, workshops, plant storage sheds, office space, loading areas and weigh bridge
B- Waste Storage Area	These areas were used for the stockpiling of redundant machinery and equipment
C- Coke Stockpile Area	These areas were used for stockpiling of coke. Stockpile areas were fitted with irrigation sprays for dust suppression. The area was also used to store refractory bricks and as a quench ash stockpile for a short time
D- Vacant Land	These were areas of vacant land that had no or minimal history of production activity. The areas generally served as screening and vegetated areas
E- Waterways	The creek line of North Corrimal Creek. This includes the South Dam that flows into Towradgi Creek, immediately beyond the site's southern boundary.

With the intended change of use to residential and open space, a number of studies have been commissioned to identify the areas of the site requiring remedial activities to make the site suitable for the proposed land uses.

The most recent RAP for the site (Arcadis May 2019) included a summary of previous contamination investigations and reports dating back to 1996. The assessment included collection of soil (test pits), sediment samples, sampling of surface water and groundwater and ground gas monitoring from locations across the site. Soil, sediment and water samples were submitted to a National Association of Testing Authorities (NATA) accredited laboratory for the analysis of contaminants of potential concern.

The RAP (2019) concludes that *“although concentrations of contaminants of concern were detected above environmental assessment criteria, they are isolated and minimal compared to the wider site footprint”*. The RAP (2019) outlines a Remediation Strategy for the site, with the expectation that the site can be made suitable for the proposed mixed-use redevelopment.

The RAP (2019) states that the impacted soil must be appropriately re-used, encapsulated or disposed such that it is not readily accessible to proposed site users and any potential ongoing risk to human health and/or ecological receptors is appropriately managed. The RAP notes that in addition to the known site impacts there are data gaps that currently exist within the sample set, primarily relating to areas beneath structures that were not accessible for sampling at the time of previous investigations. The RAP concludes that although additional delineation and potential data gaps exist, there is currently a robust data set on the site with the results indicating the current extent of remediation required is not prohibitive to redevelopment from either a cost or practical perspective.

The RAP (2019) classifies existing coal wash reject as clean fill material, with a proposed DCP control *“the coal washery reject (CWR) can be considered as fill material at the site and addressed as part of the remediation and proposed bulk civil earthworks plan”*. Council Environmental Officers have noted that existing coal wash reject on the site cannot be considered “clean fill” – this was confirmed through consultation with the Environment Protection Authority and further discussions with the Site Auditor. Wollongong DCP Chapter E19 Earthworks states that *“any pre-existing CWR fill is assumed to be uncontrolled fill (as defined in AS3798) and not compliant with this Chapter of the Wollongong DCP”*. As

such, this uncontrolled fill in its current condition constitutes a constraint to development. Further assessment will be required as part of the design process to categorise the fill and to determine its geotechnical suitability for reuse on the site.

Given the proposed remediation will treat and store more than 30,000m³ of contaminated soil or disturb more than three hectares of contaminated soil, Council will treat the remediation as “Designated Development” under Schedule 3 of the *Environmental Planning and Assessment Regulation 2000*, which will require an Environmental Protection Licence (EPL) under the *Protection of the Environment Operations Act 1997*. The Geotechnical Assessment (Douglas Partners 2017) also considers that the reuse of existing filling (particularly that dominated by coalwash and coke products) or new coalwash filling which may be proposed for importation to raise site levels, will be governed by the requirements of the EPA Resource Recovery Order and Wollongong City Council Coal Washery Refuse in Subdivisions Policy. Any proposed remedial works and civil engineering design will need to comply with the Wollongong City Council Combustibility Policy and Wollongong DCP 2009 Chapter E19: Earthworks (Land Reshaping).

The draft DCP provisions propose on-site containment cells to be located within the central park and riparian areas, provided they do not pose an unacceptable risk to human health and ecological receptors. It is noted in the RAP (2019) that a long-term Site Management Plan (SMP) is required to be notified on the Planning Certificate for on site management of encapsulated contamination, promoting awareness of the contamination management and the requirements to avoid disturbance. The proponent has been advised that any on site containment would need to be placed under commercial buildings in private ownership with appropriate section 88B restrictions on the title of the land. Placement in the riparian or parkland areas, or under roadways is not appropriate due to potential future problems when these areas may need to be disturbed/dug up for maintenance etc.

Council’s Officers have raised concerns about any proposal to blend CWR with contaminated soil during civil works for finished levels, in terms of the potential to further impact groundwater and therefore surface waters of Towradgi Creek (noting that local ground water is shallow). It is noted that the nearby lower reach of Towradgi Creek is mapped as Coastal SEPP Coastal Wetland, Biodiversity Value on the OEH Biodiversity Values map and is classed as a Sensitive Estuary in the Illawarra Shoalhaven Regional Plan 2015.

The RAP confirms the site is subject to a Statutory Site Audit. A Non-Statutory Audit has been completed by Zoic Environmental, a NSW EPA Auditor accredited (No. 0802) under the *Contaminated Land Management Act 1997*. The ultimate aim is to enable a site audit statement (SAS) and associated site audit report (SAR) to be prepared that confirms the suitability of the site for the proposed mixed residential land use development and open space associated with a creek alignment. The Audit is being staged, with Stage 1 signed off in an Interim Advice letter endorsing the Remediation Action Plan (RAP). The Auditor considers the RAP (2019) is practical and technically robust for the contamination identified on site and that, if implemented, the site is capable of being made suitable for the proposed redevelopment, subject to conditions.

3.5.2 Acid Sulfate Soils

Wollongong LEP 2009 maps the majority of the site (approximately 97%) as Acid Sulfate Soils Class 5. Approximately 3% is mapped as Class 3 land.

The RAP states that proposed bulk earthworks will disturb soils deeper than 1m below current natural ground surface and has the potential to generate acid sulfate soils (if present). It recommends that prior to commencement of the bulk earthworks a targeted acid sulfate soils investigation should be carried out in general accordance with the NSW ASSMAC (1998) Acid Sulfate Soils Assessment Guidelines – details of this assessment will be included within a Sampling Analysis and Quality Plan (SAQP). The findings of the assessment should be used to determine the requirement for any treatment of soil prior to onsite re-use, in accordance with an acid sulfate soil management plan prepared for the site.

3.5.3 EPA Licence

The Environment Protection Authority (EPA) has advised that they have been regulating the Corrimal Coke Works site under an Environment Protection Licence (EPL) No. 125 (first issued 1 March 2000) for the purpose of coke production. Coke production ceased in April 2014 and since that time the land has been dormant.

With the end of production and the proposed change in land use, ICC applied to surrender this licence. In January 2018 NSW EPA formerly provided notice to Council of the surrender on the license. Wollongong Council is now the environmental regulator of the site. It should be noted that the EPA will continue to assist Council to appropriately manage any environmental matters as the land transitions to a new use.

3.5.4 Acoustic and Vibration

The Environmental Noise and Vibration Assessment (Renzo Tonin & Associates, May 2019) regarding acoustic and vibration impact due to rail and road noise was reviewed by Council Environmental Officers who concluded that noise and vibration issues do not present any constraint to rezoning the site for residential use.

Noise impacts can be suitably mitigated to achieve compliance through standard treatments to future residential developments (e.g. attenuation methods for buildings, mechanical plants and ventilations, or s88 restrictions) which will be stipulated at the development application stage. Acoustic barriers along the railway line that blend with the landscape will be encouraged and Council Officers have noted that the recommended Category 1 and 2 acoustic attenuation measures will most likely have to be upgraded to meet the Infrastructure SEPP criteria for internal living. The proponent has indicated the 2020 Master Plan includes a 25-metre buffer between proposed residential buildings and the rail corridor, consistent with the Infrastructure SEPP.

3.6 Traffic and Access

Council resolved (3 April 2018) that the draft Planning Proposal for exhibition include -

- *An amended Traffic Impact Assessment to address the likely impacts of the development on the surrounding roads, including the rail level crossing and addressing the requirements of Table 2.1 of the RTA Guide to Traffic Generating Development.*

A number of traffic and transport reports have been prepared for the site in support of the draft Planning Proposal request, as a requirement of the Gateway Determination and in response to further information requests from Council staff and State Agencies. The studies are as follows -

- Traffic and Parking Impact Assessment (McLaren Traffic Engineering & Road Safety Consultants, September 2017).
- Traffic and Transport Assessment (Bitzios, April 2019).
- Corrimal Station Residential Development Roundabout Access Intersection Proposal (Bitzios, May 2019).

Key Traffic and Access Findings and Recommendations

Extensive modelling was undertaken to better understand the likely impacts of the proposed development on the surrounding roads, including the rail level crossing. The VISSIM microsimulation traffic model was developed using traffic signal timing and phasing at signalised intersections data provided by the RMS and video footage of peak period operations cross referenced with boom gate opening and closing data provided by Sydney Trains. RMS traffic generation rates were applied to each land use, to calculate the site's expected traffic generation. The impact assessment methodology/modelling was based on a "no worsening approach",

Overall, the modelling revealed that the current intersection and link configurations will accommodate the additional traffic generated between 2018 and 2026 with most of the key intersections expected to retain a similar level of service in 2026 with or without the additional development. However, during the AM peak, the Memorial Drive/Railway Street intersection is the location most affected by the development's traffic, operating with a Level of service (LoS) D and average delay of 55.7 seconds (i.e. "operating at near capacity"). The proposed access to the development site operates with a LoS C and delay of 42 seconds. During the PM peak, the Memorial Drive/Railway Street intersection is expected to operate at a LoS E and an average delay of 62.9 seconds (i.e. "at capacity").

TfNSW has completed an assessment from a State road network perspective, has reviewed the Traffic Impact Assessment (TIA) and notes that the Planning Proposal would generate a significant number of vehicle movements. TfNSW has concluded that major upgrades will be required at a number of intersections on Memorial Drive in the future to accommodate the high traffic demands and has proposed a way forward involving a monetary contribution and other commitments from the proponent.

It should be noted that the original concept included two access points to Railway Street however, with the proposed transfer of Lot 126 DP 598190 to Council for the future rail bridge the number of intersections in the concept plan have been reduced to one.

The recommendation is for a consolidated single-lane roundabout at the intersection of Railway and Harbinger Streets as the preferred approach to access the site. Should a Harbinger Street roundabout be deemed the most suitable access point for the development, then all of the EEC in the north would be unable to be retained due to direct and indirect impacts during construction and then over time, and hence an offset policy/arrangement would be required. It should be noted that although the vegetation in the north of the site in the vicinity of this proposed access point has high conservation status (EEC), its conservation significance is low to moderate based on patch size, current and proposed future surrounding land use and the ongoing impacts that are likely to prevail. There is a mechanism by which Biodiversity loss can be offset (NSW Biodiversity Offset Scheme) whereby the entire 0.59 ha would be considered as lost to development through direct and indirect impacts.

A proposal for an additional left in/left out access off Memorial Drive approximately 480 metres south of its intersection with Railway Street was investigated however not deemed appropriate.

A key vision for the site is the provision of shared paths to increase the attractiveness of walking and cycling to key locations including the railway station, bus stops, the Town Centre, schools and recreation areas. Reduced on-site parking rates apply under Council's DCP in recognition of the proximity to public transport and the Town Centre.

The Traffic and Parking Impact Assessment (McLaren 2017) was reviewed by key stakeholders including Transport for NSW, RMS, Sydney Trains and Wollongong City Council, with additional information requested to address a range of comments regarding traffic, transport and road/rail safety related issues. The supplementary Traffic Impact Assessment (Bitzios 2019) was commissioned to address these concerns, which included -

- Use of a VISSIM microsimulation traffic model to understand the traffic queuing and delay effects of increasing traffic on Railway Street – particularly the queuing and release effects associated with the boom gates at the rail level crossing and the congestion issues at Memorial Drive/Railway Street intersection.
- Analysis of historical crash data.
- Safety risk assessment of the railway crossing area.
- Investigation of pedestrian/cycle links between Short Street and Corrimal Station.

3.6.1 Modelling Traffic Generation

Extensive modelling was undertaken to better understand the likely impacts of the proposed development on the surrounding roads, including the rail level crossing. The VISSIM Model was developed for the 2018 AM and 2018 PM peak traffic conditions, calibrated and validated. The 2018

base model was then used to create the year 2026 base models, which is the estimated year of full development of the site. The 2026 traffic demands base model was created by adding the expected growth in background traffic to the traffic expected to be generated by the development (DA-2010/1562) opposite at 54-62 Railway Street and 10 Harbinger St, comprising 60 residential units and six villa homes. The Coke Works site expected development was then added into the model to arrive at the “2026 Base with Development” traffic demands for the area. Traffic signal timing and phasing at signalised intersections was replicated in the modelling from SCATS data provided by the RMS. The effects of the rail level crossing were also replicated in the model through video footage of peak period operations cross referenced with boom gate opening and closing data provided by Sydney Trains.

In line with the Council resolution, RMS traffic generation rates were applied to each land use, to calculate the site’s expected traffic generation. The impact assessment methodology/modelling was based on a “no worsening approach”, which involved ensuring that the “2026 with development” case impacts were sufficiently mitigated by upgrade works such that the “2026 with development + upgrades” case intersection was no worse than the “2026 Base” case.

Intersection levels of service (LoS) were modelled in line with the following criteria:

Table 9: Intersection Levels of Service

LoS Level	Delay Range (seconds)	Typical Intersection Operations
A	<14	Good operation
B	15 to 28	Good with acceptable delays and spare capacity
C	29 to 42	Satisfactory
D	43 to 56	Operating near capacity
E	57 to 70	At capacity
F	70 and above	Unsatisfactory

The additional traffic introduced into Railway Street by the development was found to equate to one vehicle every 10 seconds (two-way) in the peak hour, which is deemed insignificant on a distributor road such as Railway Street. The study showed an approximate 17% increase in daily traffic on Railway Street. The Traffic Impact Assessment (TIA) predicts 373 vehicle trips in the AM peak time and 456 trips in the PM peak.

Overall, the modelling revealed that the current intersection and link configurations will accommodate the additional traffic generated between 2018 and 2026 with most of the key intersections expected to retain a similar level of service in 2026 with or without the additional development. However, during the AM peak, the Memorial Drive/Railway Street intersection is the location most affected by the development’s traffic, operating with a LoS D and average delay of 55.7 seconds (i.e. “operating at near capacity”). The proposed access to the development site operates with a LoS C and delay of 42 seconds. During the PM peak, the Memorial Drive/Railway Street intersection is expected to operate at a LoS E and an average delay of 62.9 seconds (i.e. “at capacity”). The TIA proposes a traffic capacity upgrade at the Memorial Drive/Railway Street intersection to address the abovementioned congestion/queuing impacts anticipated to be generated by the development.

TfNSW has completed an assessment of the Planning Proposal from a State road network perspective and has indicated they are comfortable with the methodology used in the TIA. TfNSW notes -

- Memorial Drive and the surrounding local roads currently experience high traffic demands in the AM, PM and weekend peaks, particularly around the intersections of Memorial Drive with Towradgi Road, Railway Street and Rothery Street.
- The Planning Proposal would facilitate development which would generate a significant number of vehicle movements (373 vehicle trips in the AM peak and 456 PM peak).

- Traffic modelling has been undertaken to inform the Planning Proposal, with minor upgrades proposed at the Memorial Drive/Railway Street intersection to offset traffic impacts.

TfNSW has concluded, based on the wider review from a State road network perspective, that major upgrades will be required at a number of intersections on Memorial Drive in the future to accommodate existing high traffic demands and continuing growth in the area. TfNSW has proposed the following way forward -

- 1 A monetary contribution, through a Planning Agreement or other mechanism, from the proponent towards major upgrades to Memorial Drive at three intersections (i.e. Towradgi Road, Railway Street and Rothery Street).
- 2 Consideration to local road connections around the broader precinct to reduce the number of movements and/or conflicts at intersections along Memorial Drive and, where appropriate and reasonable, delivery of these connections or planning to preserve these connections as options for the future. For example, consideration to a local road connection between the site and Cross Street. Such a connection could enable the existing junction of Railway Street and Cross Street to be closed, thereby reducing conflicts at the intersection of Memorial Drive and Railway Street and removing a significant constraint to a major upgrade at this location. TfNSW would support the cost of any such connection being offset from the above monetary contribution. TfNSW believes a working group consisting of Council, the proponent and TfNSW be formed to examine local road connections.
- 3 Planning controls (i.e. as part of the DCP) to minimise residential parking on the site, consistent with the provisions of Section 7.4 of the existing Council DCP, recognising the close proximity of the site to public transport.
- 4 The early provision (by the proponent) of shared paths internal and external to the site to increase the attractiveness of walking and cycling for future residents of this site and existing residents in the surrounding area. The design of the network needs to promote walking and cycling to key locations above driving a car. Key locations include the railway station, the bus stops, the town centre, schools, other existing residential catchments, existing shared paths etc. The provision of broader network paths/active transport will reduce the vehicle trip demand on Memorial Drive.
- 5 Appropriate arrangements in and around the site to increase the attractiveness of public transport (both rail and buses).
- 6 Noise mitigation measures (as required) within the site to mitigate traffic noise from Memorial Drive.

TfNSW has no objections to the planning proposal going on public exhibition.

3.6.2 Access

It should be noted that the original 2017 Concept included two access points to Railway Street however, with the proposed transfer of Lot 126 DP 598190 to Council for the future rail bridge the number of intersections in the concept plan have been reduced to one. The proposed second access point would interfere with the location of a future overpass.

The Traffic Impact Assessment (Bitzios 2019) proposed access to the site via a single intersection off Railway Street, approximately 60 metres east of Harbinger Street and approximately 210 metres west of the rail line level crossing. The area has a posted speed limit of 50 km/h. Traffic Modelling confirms that the access intersection can operate efficiently as a channelised “T” intersection with a right turn-in pocket and separate left turn-out and right turn-out lanes, sufficiently catering for entry and exit queues without interrupting through traffic on Railway Street and without queuing back to Harbinger Street intersection. Safe intersection sight distance can be achieved in accordance with Austroads requirements.

Council Officers requested an additional access point be investigated (e.g. Memorial Drive), and the Design Review Panel also raised concern about the single point of vehicle access – *“the single point of vehicle access may create safety issues on a development of this scale. If this entry point is blocked (road works, traffic accident, fire) there should be an alternative point of access and egress”*. The DRP

also noted the importance of ensuring that the vehicular access point connecting to the railway carpark is operational.

Council Officers have recommended one consolidated access point on Railway Street from a traffic safety and capacity perspective – this will result in less vehicle friction and turning movements and hence fewer delays and reduce the number of conflict points where accidents could occur. The fewer access points also assist in providing pedestrian/cyclist priority on the Railway Street shared path. To address the DRP concerns, the Council Traffic Officer recommendation is the consideration of a wide green/pedestrian/cyclist link into the site, which could double as an emergency access (emergency services can mount the kerb if needed to access the development).

Council Officers have raised concerns over the proposed access location on Railway Street, with offset/staggered intersections identified as a sub-optimal outcome. Following further discussion with the TfNSW Roads and Maritime Services, the recommendation was that a consolidated single-lane roundabout at the intersection of Railway and Harbinger Streets would be the preferred approach to access the site, subject to traffic modelling demonstrating no negative traffic impact on the Memorial Drive/Railway Street intersection. A consolidated single lane roundabout at that location would offer a number of safety outcomes as per the Safe System Approach (National Road Safety Strategy) and avoid potential problems inherent in the offset/staggered intersection proposal -

- Calm traffic.
- Provide a more convenient/shorter pedestrian crossing distance.
- The offset/staggered intersection arrangement would require the removal of substantial amounts of residential and visitor parking on Railway Street.
- The offset/staggered intersection arrangement would result in difficulties for drivers attempting to make a right hand turn out of Harbinger Street, as well as the proposed off set development road, resulting in traffic inefficiencies and safety concerns.

Subsequent traffic modelling (Bitzios 2019) has confirmed that a single-lane roundabout at the intersection of Railway and Harbinger Streets performs at a LoS A with an average delay of 7 seconds for both morning and afternoon peak periods and has no negative queuing impact to the Memorial Drive intersection with Railway Street or the railway level crossing. The RMS is supportive of a roundabout at this location.

Should a Harbinger Street roundabout be deemed the most suitable access point for the development, then all of the EEC in the north would be unable to be retained due to direct and indirect impacts during construction and then over time, and hence an offset policy/arrangement would be required. It should be noted that although the vegetation in the north of the site in the vicinity of this proposed access point has high conservation status (EEC), its conservation significance is low to moderate based on patch size, current and proposed surrounding land use and the ongoing impacts that are likely to prevail. There is a mechanism by which Biodiversity loss can be offset (NSW Biodiversity Offset Scheme) whereby the entire 0.59 ha would be considered as lost to development through direct and indirect impacts.

A proposal for an additional left in/left out access off Memorial Drive approximately 480 metres south of its intersection with Railway Street was investigated however not deemed appropriate for the following reasons -

- It would introduce another intersection onto a road which has limited access, impacting its primary through-traffic carrying function and potentially traffic safety.
- It would require long deceleration and acceleration lanes in an 80 km/h speed environment.
- It may introduce weave conflicts associated with movements into turn pockets at the Towradgi Road intersection.
- It was not supported by RMS.

3.6.3 Railway Crossing Risk Assessment

Sydney Trains was consulted over the methodology to assess potential changes in risk at the Railway Street Open Level Crossing (OLC) due to the additional traffic and pedestrians generated by the adjacent development. Sydney Trains ran the development's additional trips through its Australian Level Crossing Assessment Model (ALCAM) to understand potential changes in crash risk associated with the development. The ALCAM is an assessment tool used to identify potential risks at level crossings and to assist in the prioritisation of crossings for upgrades.

Based on the ALCAM model results the development's impacts on the Railway Street level crossing due to additional traffic and pedestrian movement are insignificant. State Rail's ALCAM model calculated a probability of one fatality every 104 years, which is the same risk as without the development. No risk mitigation works at the level crossing are required to facilitate the development.

3.6.4 Land Reservation Acquisition

Lot 126 DP 598190 located on Railway Street adjacent to the level crossing is zoned SP2 Infrastructure (Road) and has been earmarked for Council acquisition since 1974 to enable a bridge to replace the level crossing. The original submitted draft Planning Proposal and concept plan proposed the rezoning of this area and residential development. Following discussions with Council officers, the Planning Proposal was amended to retain the SP2 Infrastructure (Road) zone and the concept plan was amended to remove the residential development. The lot is also identified in the Wollongong LEP 2009 Land Reservation Acquisition Map for Council acquisition. It is envisaged that the acquisition will occur as part of the Development Application process, and the cost will be off set from the development contribution. There is no timeframe or budget for the construction of the bridge and there has been no discussion with State Transport agencies on funding. It is likely that the 1974 design will need to be updated. Securing the corridor is an important step in providing for the future bridge.

3.6.5 Cycle Way/Connectivity and Linkages

The Traffic Impact Assessment (TIA) (Bitzios 2019) concluded that there are few formal bikeways located in close proximity to the development site – the cycling network in and around the site is not very well connected with a number of gaps and missing links. Cycling along Railway Street is not well catered for with narrow kerbside lanes and relatively narrow footpaths.

Several options were considered in the TIA in relation to facilitation of a connection between the existing cycle way link at Short Street west of Memorial Drive through to Corrimal Station. The TIA presents a preferred option being a shared path for pedestrians and cyclists between Short Street (the existing community facilities and pool) and Murray Road via Gilbert Street, Railway Street, and Pioneer Road, thereby providing a missing link between the existing off-road cycleway paths. The preferred option is as follows -

- Widening of the existing path/link between Short Street and Gilbert Street.
- A new shared path on Gilbert Street.
- Widening of the existing footpaths on the southern side of Railway Street and also Pioneer Road to cater for the new proposed shared path.
- Implementation of directional signage along the route.

The preferred route is along the southern side of Railway Street, which is consistent with the Wollongong City Council Bike Plan 2014-2018. The applicant has proposed to construct a shared path along Railway Street from Cross Street to the Railway commuter car parking entrance as part of a Voluntary Planning Agreement (VPA) with Council.

A grade separated crossing (overpass/underpass) of Memorial Drive was deemed not warranted in the TIA – a grade separated crossing would be considered in circumstances such as where there is a history of crashes involving pedestrians or the number of pedestrians using a crossing would significantly reduce the overall performance of the road.

3.7 Development Contributions / Voluntary Planning Agreement

Council resolved (3 April 2018) that the draft Planning Proposal for exhibition include -

- *Advice on the proposed facilities and/or infrastructure, including costings, that are proposed to be incorporated into a draft planning agreement.*

A development contribution levy is a fee to be paid by any person undertaking a new development. The money is used to provide improved or increased public infrastructure and facilities in accordance with the *Environmental Planning & Assessment Act 1979 and Regulation*.

Based on a 1% Section 7.12 (formerly S94A) levy, the proposed \$250 - \$270 million development cost could contribute \$2.5 - \$2.7 million in development contributions. Voluntary Planning Agreements (VPAs) may be accepted as an alternative to development contributions in line with Council's adopted Planning Agreement Policy, involving the provision of a material public benefit such as a monetary contribution, works and/or land dedication. A VPA is an agreement entered into by Council and a developer during Council's consideration of a rezoning application (planning proposal) or a development application and can be either in lieu of or in addition to a development contribution payment.

The proponent has indicated the intention to enter into a Voluntary Planning Agreement (VPA) with Council, submitting a Letter of Offer for a VPA and Schedule of Public Benefits (19 August 2020), detailing a range of public benefits to be provided as part of the development, some of which will offset local contribution payments, wholly or partially (Attachment 5).

The key items identified as a public benefit and therefore consideration for a contribution offset include -

- Provision of a pedestrian/cycling connection along Railway Street from Cross Street to the Railway commuter car parking entrance and from Railway Street to the Council reserve south of Towradgi Creek.
- Road widening land dedication (Lot 126 DP 598190 on Railway Street) to facilitate a future rail overpass bridge.
- Railway Street/Harbinger Street roundabout.
- Station Plaza public access.

The following public benefit items are also proposed with no offset of contributions -

- Provision of two neighbourhood scale parks (3,000m² and 5,000m²).
- Establish riparian corridor with passive recreational, active transport and ecological functions.
- 35 Affordable Rental Housing Dwellings.

3.8 Draft DCP Controls

Council resolved (3 April 2018) that the draft Planning Proposal for exhibition include the following additional information -

- *A site specific DCP Chapter be developed and submitted in conjunction with the amended Urban Design Concept Plan addressing the following -*
 - *Building Heights.*
 - *Floor Space Ratios.*
 - *Lot Size.*
 - *Building envelopes.*
 - *Road widths.*
 - *Public spaces.*
 - *Streetscapes.*

- *Housing types.*
- *Connectivity and Access.*
- *Views and Vistas.*
- *Urban Form Design development.*
- *Clause 7.18 Design Excellence of the Wollongong LEP 2009 (Key Site requirements).*

Independent advice on the draft Master Plan and draft DCP was sought from the Design Review Panel to assist in the evaluation of the updated Planning Proposal request submitted in May 2019. The Design Review Panel attended a site visit on 17 July 2019 and provided independent feedback on the draft Master Plan and draft DCP (Attachment 8). The DRP noted the importance of a site specific DCP to provide the community with a level of certainty that the proposed Master Plan can be delivered and provide guidance and a tool for Council and the Panel in the assessment of future Development Applications for the site.

An updated Master Plan was subsequently submitted (August 2020), with an accompanying site-specific Development Control Plan (DCP) (Attachment 5). The site-specific draft DCP is intended to supplement the land use planning controls contained in Wollongong Development Control Plan (WDGP) 2009, to achieve the vision for this unique site. Where the Master Plan is proposing planning controls that vary significantly to the surrounding area (e.g. proposed heights and FSR), then there is a role for the DCP in providing detailed planning and design guidelines (including diagrams) to ensure the resultant built form can deliver on the vision, respecting heritage elements and key views etc.

It should be noted that SEPP 65 and the NSW Apartment Design Guide (ADG) are the key design controls for residential apartment buildings, and the Low Rise Housing Diversity Code may be applicable for any dual occupancy or multi dwelling houses (terraces, townhouses and villas) proposed for the site. Additionally, Chapter B2 - Residential Subdivision of Councils DCP has recently been updated to include best practice road cross sections that represent new development expectations.

The draft site-specific DCP submitted includes the following provisions -

- Key objectives for the site to achieve the vision.
- Key planning and design principles.
- Elements of the Master Plan.
- Desired future character statements, identifying 5 character precincts.
- Subdivision and built form objectives.
- Views and vistas controls.
- Mapping of key view corridors and key public domain spaces to guide future development - preservation/creation of key view corridors to respect heritage elements/key district views.
- View Map includes -
 - Murray Road corridor.
 - Entry view from Railway St to the Brick chimney.
 - Views to the ridgelines of the escarpment and the brick chimney.
 - Central view corridor.
- Residential building setback controls.
- Architectural diversity and design controls.
- Non-residential development controls, including mapped location of types of non-residential uses proposed and indicative active frontages and loading zones.
- Site specific reduced parking rates are proposed, taking into consideration the site's location adjacent Corrimal Railway Station and in close proximity to the Corrimal Town Centre, including proposed one hour and four hour on street parking limits to discourage all day commuter parking on site.
- Key heritage objectives and controls specific to the site, for example maintaining and establishing views to the Brick Chimney.

- Overall public domain objectives and controls, and specific controls for the 4 public domain areas.
- Flying Fox Management controls.
- Streetscape character, street design and road width controls.
- Flood management and riparian corridor geomorphology / stability objectives and controls to reflect flood studies and engineering design work submitted .
- Contamination and Earthworks controls.

In order to align the draft DCP provisions with the intended vision for the site and recommendations in this report, it is proposed to further develop the draft DCP document prior to public exhibition. The key amendments proposed are tabled below -

Draft DCP inclusion	Officer Recommendation
Table 1: Lists a number of WDCP 2009 Chapters (or part Chapters) that are not applicable to this site.	Delete table – WDCP 2009 applies to the site. The site - specific chapter should only include (and provide justification for) controls to supplement WDCP 2009, where departures to WDCP 2009 are sought.
Draft DCP inclusion	Officer Recommendation
Desired Future Precinct Character expressed as a statement.	Further work required to develop Desired Future Character Objectives, and the development controls intended to deliver the distinct character of each precinct, ensuring the proposed height and FSR combinations deliver the intended character. Precinct controls to consider scale, material, landscaped area, setbacks, articulation zones and streetscape as well as key interfaces, views and connections. Required are images, cross sections, street sections and diagrams of built form typology, setbacks and maximum building length controls etc to support/deliver the diverse precincts envisaged.
5.4 Proposed basement controls contrary to the Apartment Design Guidelines (ADG).	ADG control to be included – “protrusion of car parks should not exceed 1m above ground level. Design solutions may include stepping car park levels or using split levels on sloping sites”.
New controls proposed under the following heading: 7.3.3 Street Design and Road Widths	The B2 Residential Subdivision Chapter has recently been updated to include best practice road cross sections involving extensive consultation across Council divisions – these represent new development expectations and should be relied upon.
New controls proposed under the following heading: 8.2 Stormwater Management	Section 8.2 Stormwater Management be removed from the draft site-specific DCP and Council’s current DCP Chapter (E14 Stormwater Management) be relied on to guide future development.
New controls proposed under the following heading: 8.3 Riparian corridor geomorphology and stability Flood management and riparian corridor geomorphology/stability objectives and controls to reflect flood studies and	The riparian design and geomorphology studies were required to provide confidence that a design solution could be achieved. These are not final designs. Further design work will be required at the DA stage in line with Council’s current policies. Section 8.3 Riparian corridor geomorphology and stability be removed from the draft site-specific DCP and Council’s current DCP Chapters (E13 Floodplain

engineering design work submitted	Management and E23 Riparian Land Management) be relied on to guide future development.
New controls proposed under the following two headings: 8.4 Contamination and remediation 8.5 Earthworks	Sections 8.4 Contamination and Remediation and 8.5 Earthworks be removed from the draft site-specific DCP and Council's current DCP Chapters (E19 Earthworks – Land Reshaping and E20 Contaminated Land Management) be relied on to guide future development.

In addition, Council Officers have recommended that the following should be further explored, while recognising there may be other issues raised through the exhibition -

- Additional site objectives to guide the future development (e.g. protection of the GHFF camp).
- Further development of appropriate active frontages and loading zones in the heritage precinct, and articulation zone and basement car parking/podium controls in relation to residential development.
- Further consideration of the alternative parking rates proposed for specific land uses.
- Consideration of the DRP recommendation to include a Public Artwork section in the DCP, including objectives and controls involving the re-use of suitable heritage elements, salvage of heritage materials for integration into art works on site etc.
- Include residential development controls (as per the DRP recommendation) to reduce the level of disturbance to future residential buildings in the vicinity of the GHFF camp – measures such as orientation of buildings, material selection, noise attenuation etc.
- Clarification that a Vegetation Management Plan (VMP) is required for the ecological restoration of the entire length of the realigned corridor (not just the ecological area in the southern part of the site as referenced in the draft DCP) and that landscaping and vegetation species within the realigned corridor must be designed to prevent the expansion of the GHFF camp further north near future planned residential development.
- Clarification that the solar access controls for the Village Park and Southern Park are to include “at least 4 hours of *continuous* solar access to at least 70% of its area all year round”.

CONSULTATION AND COMMUNICATION

Preliminary agency and internal specialist staff consultation was undertaken as part of the assessment of the original 2017 draft Planning Proposal request in October 2017. Feedback from this consultation was provided to the proponent to inform the preparation of the more detailed studies required as part of the Gateway Determination, further consultation with relevant State agencies, and subsequently the updated draft Planning Proposal and Master Plan.

The Gateway Determination stipulates a number of State agencies to be consulted during public exhibition of the draft Planning Proposal.

State Agency Consultation

Preliminary notification consultation in relation to the original draft Planning Proposal occurred in 2017. The following comments were received -

Department of Primary Industries - Water	Generally in agreement with assessment of water courses and recommended riparian outcomes. Crown Lands and Water Division (CL&W) will undertake further assessment of the proposal at the development application stage. Future detailed design of the development is to be undertaken with consideration of the DPI Water 2012 Guidelines for riparian corridors on waterfront land, as well as the following key points –
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	<ul style="list-style-type: none"> • Realigned and reconstructed watercourses are to be designed as natural functioning streams including emulation of natural geomorphic units and meander. • Detention requirements must be designed in accordance with CL&W Guidelines. • The riparian corridor is to be established using fully structured provenance native vegetation. • The proposed bike track must be constructed in the outer 50% of the Vegetated Riparian Zone in accordance with the DPI Water Guidelines. • All non-riparian uses within the required 20m Vegetated Riparian Zone are to be offset in accordance with the Guidelines for riparian corridors on waterfront land.
Office of Environment and Heritage	<p>Support revitalisation of former industrial site in principle, including a long term conservation outcome for the riparian corridor and retained areas of remnant native vegetation. Support zoning to either RE1 or a suitable environmental zoning.</p> <p>Under the BC Act, biodiversity offsetting is mandated where native vegetation clearing thresholds are exceeded – applies to all native vegetation, not just EECs.</p> <p>Parts of the site are high hazard floodways and flood storage areas. Proposed realignment of riparian corridor should consider the geomorphic stability of the resultant watercourse. It is important that both flooding impacts on the stream and impacts of stream rehabilitation on flood behaviour are considered to ensure a suitable outcome.</p> <p>Support measures to enhance items of heritage and avoid impacts upon heritage. Aboriginal community consultation is required. Aboriginal objects are protected under the NPW Act – an AHIP may be required.</p>
Sydney Water	<p>Trunk water system in the area has adequate capacity to service the proposed redevelopment of the site.</p> <p>Sydney Water cannot provide any wastewater capacity – the existing wastewater system is already experiencing a capacity related issue. If the proponent can provide sufficient on-site storage to ensure that pumped flows would only be discharged to our system under dry weather conditions, and flows limited to 2L/s or less, then may be potential to accommodate the proposed redevelopment servicing. Sydney Water would be willing to work with the developer in developing a wastewater servicing scheme that works within the limitations of the system (i.e. with no wet weather discharge and with limited 2L/s dry weather discharge).</p> <p>The Developer may consider onsite recycling and reuse (including stormwater harvesting) as part of water cycle management/wastewater services in their development site.</p> <p>The proponent should ensure they consider stormwater management as part of their site work/design – the local stormwater system does not appear to have adequate capacity to transfer runoff during heavy wet weather events resulting in local flooding.</p>
National Trust of Australia	<p>The site includes a number of significant industrial heritage items. Note recommendation for need of a Statement of Heritage Impacts – supported.</p> <p>The following aspects should be considered when redeveloping the site -</p>

	<ul style="list-style-type: none"> • Industrial history/archaeology/industrial architecture. • Significant heritage elements to be preserved. In addition to eastern brick chimney, the steel chimney should also be retained. • Commitment to thorough recording of items needed. • Pollutants on site – plan for treatment/disposal. • Fuller impact analysis expected for natural habitats and riparian space. • Notes SEPP Infrastructure applies to site. • Need indication of targeted demographics and affordable housing. • Suggest on site commercial uses (small supermarket, cafes etc). • Prefer maximum height of 5 storeys.
Rural Fire Service	<p>No objections.</p> <p>The bush fire risk that can be posed by the riparian corridor and biodiversity conservation communities proposed to be retained needs to be addressed in subsequent stages. Future DA for subdivision and higher density residential development needs to comply with Planning for Bush Fire guidelines (provision of APZs, access etc).</p> <p>Recommend inclusion of a continuous perimeter road along the bush land interface along the vegetation/open space associated with the riparian corridor.</p>

Post Gateway, and following the completion of additional studies and submission of an updated Master Plan and draft Planning Proposal for the site, the following comments have been received -

TfNSW	<p>Noted that the development proposes to realign North Corrimal Creek. TfNSW concurrence under Section 138 of the Roads Act 1993 is required for structures and disturbances to or under the road reserve. TfNSW will not object to the proposed creek realignment subject to following the DA process and achieving Section 138 approval.</p> <p>Noted that Council is proposing a 15m offset to Memorial Drive – RMS would accept a minimum 10m offset from the Memorial Drive boundary to the top of the channel, provided the batter slope is 1:4.</p> <p>Provision will not be provided for maintenance (or other) access from Memorial Drive – Council needs to be satisfied that an alternative access for maintenance can be provided.</p> <p>If any structures are to be placed between the road boundary and the channel, such as existing earth berms, then sufficient offset (5m) from the base of that structure to the top of the channel must also be provided.</p> <p>Based on the wider review from a State road network perspective, major upgrades will be required at a number of intersections on Memorial Drive in the future to accommodate existing high traffic demands and continuing growth in the area. TfNSW has proposed the following way forward -</p> <ul style="list-style-type: none"> • A monetary contribution, through a Planning Agreement or other mechanism, from the proponent towards major
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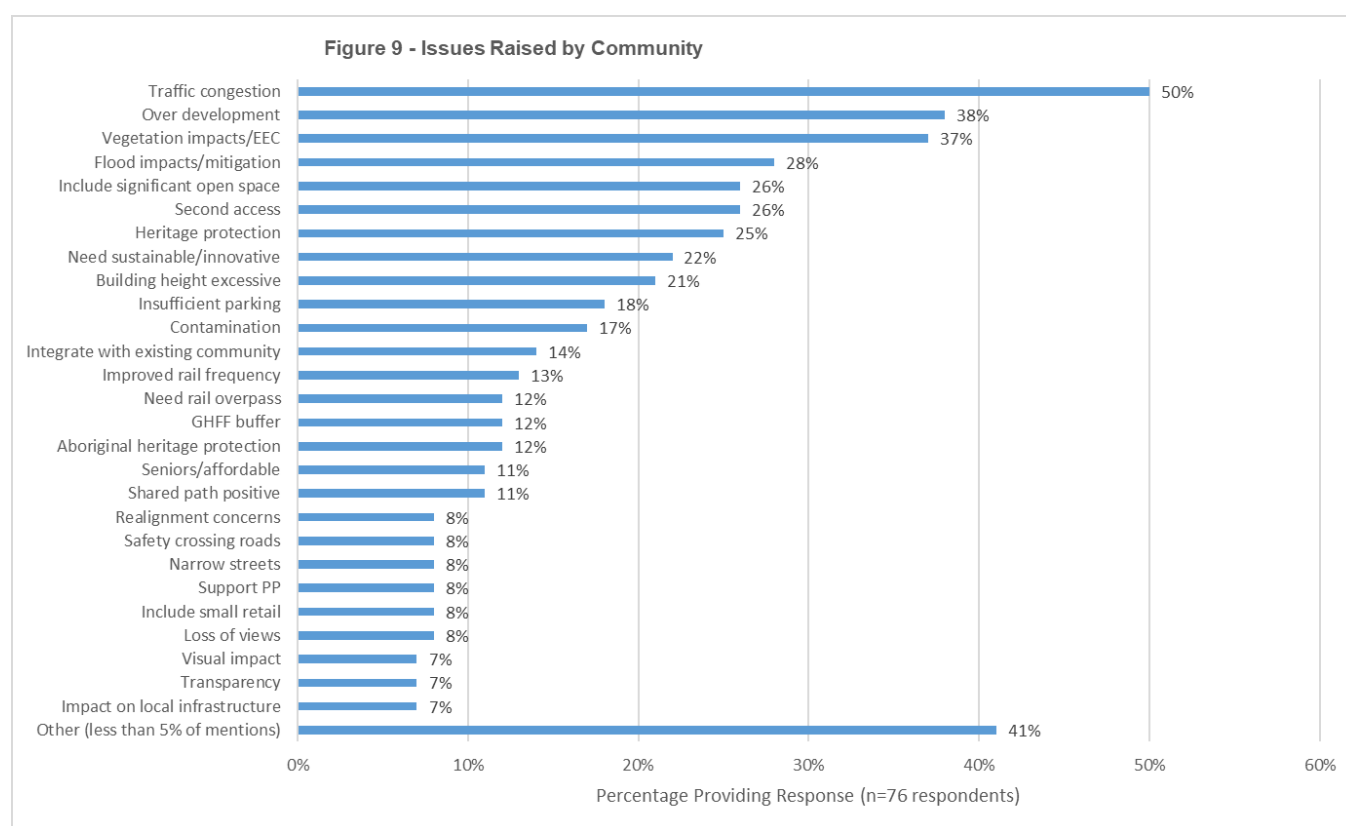
	<p>upgrades to Memorial Drive at three intersections (i.e. Towradgi Road, Railway Street and Rothery Street).</p> <ul style="list-style-type: none"> • Consideration to local road connections around the broader precinct to reduce the number of movements and/or conflicts at intersections along Memorial Drive and, where appropriate and reasonable, delivery of these connections or planning to preserve these connections as options for the future. TfNSW would support the cost of any such connection being offset from the above monetary contribution. • Planning controls (i.e. as part of the DCP) to minimise residential parking on the site, consistent with the provisions of Section 7.4 of the existing Council DCP, recognising the close proximity of the site to public transport.
TfNSW (continued)	<ul style="list-style-type: none"> • The early provision (by the proponent) of shared paths internal and external to the site to increase the attractiveness of walking and cycling for future residents of this site and existing residents in the surrounding area. The provision of broader network paths/active transport will reduce the vehicle trip demand on Memorial Drive. • Appropriate arrangements in and around the site to increase the attractiveness of public transport (both rail and buses). • Noise mitigation measures (as required) within the site to mitigate traffic noise from Memorial Drive. <p>TfNSW has no objections to the planning proposal going on public exhibition.</p>
Department of Environment Energy & Science	<p>Stream realignment: an appropriately designed and constructed natural stream corridor with meander is preferred over a hard engineered and straightened stream which are steeper and more vulnerable to erosion and can result in adverse downstream hydraulic impacts (as identified in the geomorphic assessment). A natural stream profile supported by appropriate riparian vegetation widths would reduce the erosion potential, enable more natural stream treatments and provide ecologically sustainable benefits and provide lower longer-term maintenance costs to future landowners.</p> <p>The final proposed land use zoning should have regard to flood affectation and appropriate vegetated riparian zone requirements as a result of the watercourse realignment.</p> <p>The requirements for emergency services agencies including the SES need to be ascertained, particularly regarding any access and evacuation needs.</p> <p>Recommend Council consider measures to integrate buffers to the flying-fox camp into the Planning Proposal and/or future development applications.</p> <p>Buffers between the camp and residential development should be created as wide as reasonably practicable with guidance of up to</p>

	<p>300m in width, but determined on a case by case basis – opportunity for strategic planning to incorporate suitable buffers.</p> <p>Vegetation modification undertaken to support creation of buffers should avoid areas of Illawarra Lowlands Grassy Woodland EEC and undertaken during the winter window outside of GHFF breeding season.</p> <p>Buffers should be replanted with species not suitable for flying-foxes, comprise trees less than 3m in height and/or managed open space and riparian land.</p> <p>Planning mechanisms including zoning amendments, building setback and building envelope controls should be investigated to support and enable the creation of buffers through vegetation modification.</p> <p>Community awareness of the camp is critical and future nearby residents should be notified about the camp by s. 10.7 planning certificate notifications.</p>
Department of Environment Energy & Science (continued)	<p>Housing residents next a known GHFF camp will necessarily generate conflict, as experienced many times over with the increasing establishment of camps in urban areas. This will lead to increased workload for Council staff in terms of community engagement/education and responding to community requests to remove the flying-foxes.</p> <p>Dispersal of flying-foxes is not preferred – likely to be costly, ongoing and unlikely to succeed. Requires threatened species licence under the Biodiversity Conservation Act 2016.</p>
Heritage NSW	<p>On 5 February 2020 the nomination was considered by the State Heritage Register (SHR) Committee of the Heritage Council of NSW where it was resolved to progress the State Heritage nomination to the next stage of review and prioritisation.</p> <p>On 5 May 2020 the SHR Committee agreed the item is likely to meet the threshold for State Heritage significance and advised Heritage NSW to proceed with an assessment. The assessment will involve research and further consultation with the owners, Council and other key stakeholders.</p>
National Trust of Australia	<p>Opposition to proposed rezoning – support continuing as industrial area (focus on 21st century lean green options).</p> <p>Oppose additional 700 dwellings on site – significant negative impacts surrounding locality.</p> <p>Supports ongoing rehabilitation/maintenance of wetlands/native vegetation communities on site.</p> <p>Site added to National Trust Register in October 2019.</p>
Housing Trust	<p>Entered a Memorandum of Understanding (MOU) with Legacy Property to progress commercial discussions about the delivery of Affordable Rental Housing.</p> <p>The intent of the MOU is that the Housing Trust would acquire a freehold parcel within the development and design, build and manage the Affordable Rental Housing proposed.</p>

NSW Ministerial Guidelines for a definition of tenant eligibility for Affordable Rental Housing – an income range of approximately \$52,000-\$72,000 for households without children and \$89,000-\$124,000 for households with one or more children, this cohort often “key workers”. Housing will be provided at below the market rate

Community Consultation

Preliminary community consultation (non-statutory) was undertaken from 23 October to 24 November 2017 based upon the 2017 Planning Proposal and Masterplan. A total of 18 submissions were received. Council has continued to receive correspondence from the community which at the time of this report included letters/emails from 76 individuals/groups expressing concerns and hopes for the site. The following graph illustrates the key issues raised -



The Heritage Amendment Planning Proposal was placed on public exhibition between 9 March and 8 April 2020. A total of 27 submissions were received and reported to Council on 29 June 2020, where Council resolved to finalise the Planning Proposal (Attachment 10). A Local Heritage Listing Amendment was notified for the site on 11 September 2020.

A public meeting organised by a local community group was held on Sunday 18 February 2018, and reportedly attended by approximately 50 persons. The Illawarra Coke Company (ICC) advised that speakers stated that they are not against the development but want it to be done in such a way that it becomes an asset for Corrimal. Concerns that were raised included traffic and pedestrian road crossing issues, preservation of industrial heritage, damage to local wildlife habitat, flooding issues (including downstream of the site), pressure on local schools and services and a desire that this be a truly sustainable and quality development.

The proponent undertook a number of visioning workshops in December 2018 and March 2019 during the completion of the final technical studies required from the Gateway Determination.

Council at its meeting 3 April 2018 resolved that any public exhibition should be for a minimum 60 days and include a public information session to be held in Corrimal in the first half of the exhibition period. Restrictions due to COVID-19 will mean the resolution to hold a public information session may not be possible, and an Engagement Strategy has been developed in response to these restrictions.

The directive from the State Government in response to COVID-19 is that it is important for all in the planning system to continue to do the work needed to keep the economy moving, including the progression of planning proposals. The Department of Planning has been instigating a number of measures in response to COVID-19, including introducing the *COVID-19 Legislation Amendment (Emergency Measures) Bill 2020* which has removed the requirement for planning decision makers including Councils to display physical copies of documents. These documents will now be available online via the NSW Planning Portal and local Council websites. Until further notice there will be no opportunities for community meetings, face to face consultation or library/customer service centre access to documents.

The focus of the Engagement Strategy is to incorporate some flexibility in the exhibition arrangements to work with community groups and individuals to ensure the community continue to be able to access the documentation and be able to provide their feedback. The Engagement Strategy includes the following options for Council to consider -

- Ensure the exhibition is widely advertised through the media, social media, posters in key locations and in mail outs to surrounding residents and people who have made a submission/sent in correspondence on the Planning Proposal to date
- Include the whole package of information, including technical studies, on Council's website and the Department's planning portal
- Include in the mail outs and on the website, FAQ sheets that provide information on questions that have arisen from feedback to date
- Assistance at libraries for people to make a booking to access the website documentation on exhibition
- Make hard copies of some information available to the community to be managed by a nominated person acting as a librarian
- Development of an interactive engagement hub on Council's website providing the opportunity to -
 - view the site on a map in the context of surrounding development, landforms and infrastructure.
 - use an interactive map to drag a "topic" to a specific location on the map and provide a comment. Visitors to the web page will be able to view these comments and add to the conversation, including the option to upload photos.
 - opportunity to leave comments/requests for further information on the web page that will be monitored and responded to by Council Officers.

The exhibition will commence as soon as possible following finalisation of the package of documents. If the exhibition period coincides with the holiday period, it will be extended as required under Council's adopted Community Participation Plan.

Illawarra Shoalhaven Regional Plan 2036

The Planning Proposal is consistent with the objectives and targets of the Illawarra Shoalhaven Regional Plan 2036, specifically –

Goal 2 – a variety of housing choices with homes that meet the needs and lifestyles.

Direction 2.1: provide sufficient housing to suit the changing demands of the region.

Direction 2.2: support housing opportunities close to existing services, jobs and infrastructure in the region's centres.

The Regional Plan identifies the provision of housing in existing urban areas as a sustainable option, in that it takes advantage of existing job markets, infrastructure, commercial and retail opportunities, public transport and facilities for pedestrians and cyclists. A number of centres have been identified in the Regional Plan for the focus of increased housing activity, including the northern corridor – Thirroul,

Corrimal and Fairy meadow. The subject site has the potential to provide a range of housing choice close to existing services offered by Corrimal Town Centre, providing connectivity and direct linkages to public transport and existing public facilities.

Goal 3 – a region with communities that are strong, healthy and connected.

Direction 3.1: grow the opportunities for investment and activity in the region's network of centres.

Community Strategic Plan – Wollongong 2028

The Planning Proposal is consistent with the delivery of the following Wollongong 2028 Community Strategic Plan objectives:

Objective 1.1 Our natural environment, waterways and terrestrial areas are protected, managed and improved:

1.1.3 The potential impacts of natural disasters, such as those related to...flood...are managed and risks reduced to protect life, property and the environment.

Objective 1.3 The sustainability of our urban environment is improved:

1.3.1 manage land use to strengthen urban areas and improve connectivity close to train stations and key transport routes.

Objective 5.3 Residents have improved access to a range of affordable housing options:

5.3.1 housing choice in the Wollongong Local Government area is improved, taking account population growth, community needs and affordability.

Objective 6.1 Wollongong is supported by an integrated transport system:

6.1.4 integrated communities close to public transport and local services and facilities focused around existing trains stations and town and village centres are planned for and encouraged.

Local Strategic Planning Statement

Council has adopted the Wollongong Local Strategic Planning Statement (LSPS) 2020 to provide a 20 year land use planning vision for the City. It has drawn on the many existing strategies and plans developed, exhibited and adopted by Council.

The LSPS acknowledges that -

- The key employment lands for Wollongong will continue to be at Port Kembla, Unanderra, West Dapto, south of Wollongong, Fairy Meadow, Tallawarra, Helensburgh and Bellambi.
- An additional 23,800 dwellings will be required to house the forecast population growth to 2041.
- An increase in housing stock diversity is required to accommodate changing demographics, including a greater provision of 1-2 bedroom dwellings.
- An increase in the supply of affordable housing stock is required, and as part of residential up-zonings an Affordable Housing Contribution will be expected or a proportion of Affordable Rental dwellings to be provided on-site.
- Corrimal has been identified as the main shopping centre and commercial precinct for the northern suburbs, and an ideal location for increased housing.

Corrimal Town Centre Plan 2015-2025

The Corrimal Town Centre Plan 2015-2025 provides strategic guidance on the future of Corrimal Town Centre, noting that Corrimal is strategically placed to grow. The Plan identifies a number of large sites in proximity to the Town Centre for future development potential, including the former Coke Works site adjacent the Corrimal Railway Station and within walking distance to centre services. The Planning

Proposal is consistent with the “Smart Growth” vision, *“redevelopment in and around the Town Centre provides housing choice and supports the local economy”*.

Wollongong Council Draft Housing and Affordable Housing Options Paper (2020)

An Options Paper was prepared to inform the development of a new Housing Strategy for the Wollongong LGA which has highlighted the need to increase the supply of smaller dwellings (1-2 bedrooms) to meet increasing demand due to an ageing population, increased numbers of lone person households and to better cater for low to moderate income households. The Paper also identifies centres, including Corrimal, as having capacity for additional dwellings through higher densities and appropriate zonings.

Planning Proposal Policy (Wollongong City Council 2018)

In May 2018, Council adopted a Planning Proposal Policy which seeks to guide the preparation and submission of Planning Proposal requests, as well as assist in determining the priority for Planning Proposals and identify the circumstances in which Council would be unlikely to support progression of a proposal. The Policy identifies that planning proposals aiming to implement endorsed Council Strategies or Concepts Plans would be given priority in assessment.

The Planning Proposal is aligned with the Illawarra Shoalhaven Regional Plan which identifies the provision of housing in existing urban areas as a sustainable option and identifies Corrimal for a focus on increased housing activity. The Planning Proposal is also consistent with Council's Community Strategic Plan, Corrimal Town Centre Plan and Wollongong Council Housing Options Paper, as outlined above.

CONCLUSION

The draft Planning Proposal has been evaluated and deemed to have site and strategic merit.

Key planning considerations and outcomes are summarised as follows -

- Corrimal is identified in the Regional Strategy and Council's Strategic Plan as the major hub of the northern suburbs, capable of infill development given its location and supporting infrastructure and services.
- Site and strategic merit in this proposal to rezone for residential use – proximity to public transport, town centre, jobs, schools, recreational facilities and related infrastructure.
- The proposal to rezone the site for residential and commercial use represents the most viable option in terms of enhancing the heritage values, by making the history of the site accessible to the wider community through adaptive reuse and interpretation.
- Sufficient supply of well located industrial land in the LGA and new industrial uses are not deemed viable on this site (land use conflict given surrounding residential development).
- Medium density housing typologies are under-represented in the Illawarra and opportunity to provide Affordable Rental dwellings.
- Areas of increased height are being proposed given the unique position of the site adjacent Corrimal Railway Station, within close proximity to the Corrimal Town Centre and related services and infrastructure, and size of the site.
- The proposed built form is appropriate and has been guided by extensive view analysis to ensure key views are created/preserved in relation to heritage items and the escarpment.
- Opportunity to deliver a number of distinct housing precincts to respond to market demand, supported by a neighbourhood scale retail offer and possible business start-ups, and a central and southern park.
- The modest convenience-based retail offer is supportable from an economic perspective, while not adversely impacting the existing retail hierarchy in the area.

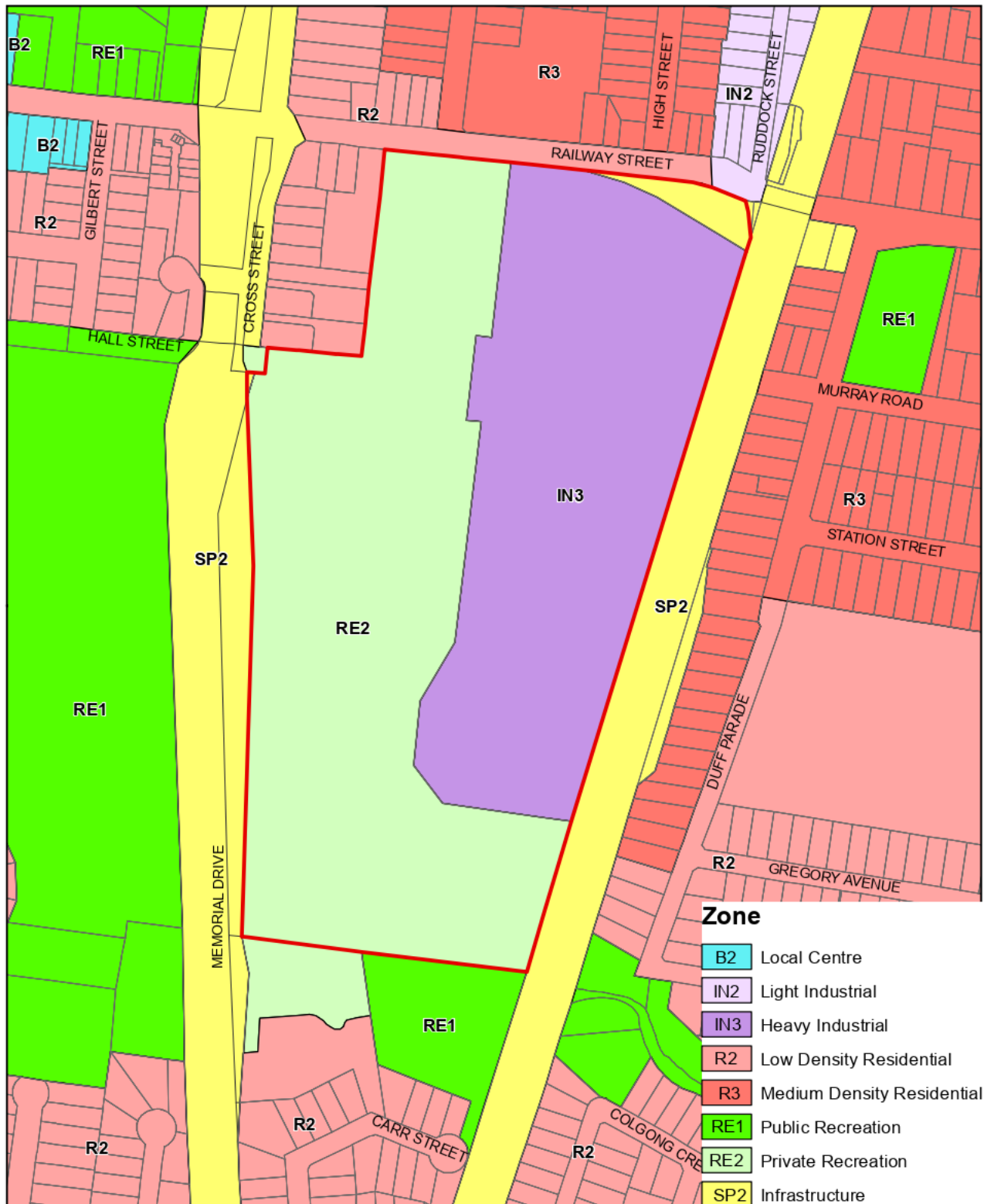
- Traffic impacts have been modelled and can be managed to provide appropriate levels of service.
- Opportunity to make a financial contribution to area wide upgrades that TfNSW has identified are required at a number of intersections along Memorial Drive.
- Flooding outcomes on site and up/downstream are improved.
- The Grey-headed Flying-fox colony is provided with protection and habitat/EEC is enhanced.
- Contamination on site can be remediated to make the land suitable for urban use.

Issues that will continue to be refined and considered in more detail prior to exhibition and/or finalisation of the proposal include –

- The site-specific Development Control Plan.
- The Planning Agreement based on the Letter of Offer.
- The Funding mechanism for maintenance of the Grey-headed Flying-fox habitat.
- The Dual use opportunities of the proposed riparian corridor.

It is recommended that Council resolve to exhibit a revised draft Planning Proposal and associated supporting documentation for community feedback.







Minutes of Ordinary Meeting of Council

3 April 2018

4

DEPARTURE OF COUNCILLORS

Due to a prior disclosure of interest, Councillor Cox departed the Chamber and was not present during debate and voting on Item 2.

During debate and prior to voting on Item 2, Councillor Walters departed and returned to the meeting, the time being from 6.51 pm to 6.53 pm.

ITEM 2 - DRAFT PLANNING PROPOSAL: FORMER CORRIMAL COKE WORKS, 27 RAILWAY STREET, CORRIMAL

A PROCEDURAL MOTION was MOVED by Councillor King seconded Councillor Walters that an additional one minute be granted to Councillor Kershaw to address the meeting in relation to Item 2.

36 COUNCIL'S RESOLUTION – RESOLVED UNANIMOUSLY on the motion of Councillor Kershaw seconded Councillor Colacino that –

- 1 A draft Planning Proposal be prepared for Lot 1 DP 795791, Lot 5 DP 749492, Lot 11 DP 749492 and Lot 126 DP 598190 known as the former Corrimal Coke Works including part of former Lot 12 DP 749492 Cross Street to amend Wollongong Local Environmental Plan 2009 as follows:
 - a Amend the Zoning Map as follows:
 - i Rezone Lot 1 DP 795791 from IN3 Heavy Industrial to R3 Medium Density.
 - ii Rezone Lot 5 DP 749492 to part R3 Medium Density Residential and part RE1 Public Recreation.
 - iii Rezone Lot 11 DP 749492 from SP2 Road Infrastructure to RE1 Public Recreation.
 - iv Rezone part of former Lot 12 DP 749492 from RE2 Private Recreation to R2 Low Density Residential.
 - vi Rezone the Cross Street Road Reserve from SP2 Infrastructure (Road) to R2 Low Density Residential.
 - vii Retain Lot 126 DP 598190 as SP2 Infrastructure (Road) (Attachment 4).
 - b Amend the Floor Space Ratio map to introduce a varying floor space ratio of between 0.75:1 and 2.5:1 (Attachment 4).
 - c Amend the Height of Buildings Map to introduce a varying maximum height of 13m and 24m, throughout the site (Attachment 4).
 - d Amend the Minimum Lot size Map to introduce a minimum lot size of 149m² for the R3 Medium Density zone.
 - e Amend the Heritage Map and Heritage Schedule to include identified significant fabric (coke ovens, chimney, stacks) as listed items of Local significance.
 - f Amend the Natural Resource Sensitivity – Biodiversity Map to identify the two significant native vegetation communities.
 - g Amend the Riparian Map to identifying the modified riparian corridor reflecting the proposed creek re-alignment and its integrating with open space resources including pedestrian and cycle paths potentially linking with the broader green link network.
 - h Amend the Key Site Map to identify the site as a Key Site under clause 7.18 Design Excellence.
 - i Amend Schedule 1 Additional Uses to incorporate additional uses for 'food and

drink premises', and 'shop', limiting the size of the premises to a maximum of 150sqm, identifying a site specific location within a 100m radius of the train station.

j Incorporate a 5% Affordable Rental Housing target.

2 The draft Planning Proposal be forwarded to the NSW Department of Planning and Environment for a conditional Gateway determination, requesting that the following additional studies and information that will inform the proposed planning controls, be submitted prior to public exhibition:

a A Geomorphological report from a suitably qualified geomorphological expert to appraise the proposed watercourse realignment and verify that the proposed design (including alignment and channel dimensions) will enable a sustainable channel pattern and form and long term channel stability, and to provide advice on measures required to be incorporated into the design of the re-alignment in order to ensure these outcomes.

b The following details are required in relation to the flood modelling:

i Plans showing manning's roughness values used in the pre and post development flood modelling;

ii Details of the WBNM modelling including catchment plan, input data, catchment routing, structures, IFD data, and results;

iii Plan showing 2D flood model domain and boundary condition locations, including method, type and location of each model inflow;

iv Details of the hydrograph used at each inflow location; and

v Plan showing comparison between flood levels predicted by Cardno model and those predicted by Council's adopted model, demonstrating parity between the two models as stated in Section 4.2.1 of the report by Cardno.

c The submitted flood study shall be amended to include modelling of 'Risk Management' blockage factors for the 1:100 ARI flood event, and mapping of the Flood Planning Area (FPA), i.e. the area of land below the Flood Planning Level (FPL), being the level of a 1:100 ARI (average recurrent interval) flood event plus 0.5 metres freeboard as defined in the Wollongong LEP2009 and is to include the correct blockage factors for Structures 48 (North Corrimal Creek, D/S Railway Street) and 1 (Carr Creek, Memorial Drive). Structure 48 is a Class 3 structure and requires a design blockage factor of 40%, and Structure 1 is a Class 1 structure and requires a design blockage factor of 70%. The flood modelling and Table 4-2 of the Flood Study report shall be updated accordingly.

3 Should a Gateway determination be issued, consultation be undertaken with the following agencies and stakeholders during public exhibition:

a Crown Lands and Water Division;

b EPA;

c NSW Office of Environment and Heritage;

d NSW Heritage Council;

e NSW Rural Fire Service;

f Department of Education and Communities;

g Endeavour Energy;

h Transport for NSW – Roads and Maritime Services;

i Sydney Water;

j Sydney Trains;

- k RailCorp;
 - l Department of Fisheries;
 - m Illawarra Local Aboriginal Land Council;
 - n SES; and
 - o National Trust of Australia – Illawarra Shoalhaven Regional Branch.
- 4 The draft Planning Proposal be exhibited for a minimum period of 60 days and a community information session be held in Corrimal within the first 30 days of the exhibition period.
- 5 The NSW Department of Planning and Environment be requested to issue authority to the General Manager to exercise plan making delegations, in accordance with Council's resolution of 26 November 2012.
- 6 The following additional information be submitted before or during the exhibition period, to enable Council to consider the information prior to determining whether to finalise the Planning Proposal:
 - a A site specific DCP Chapter be developed and submitted in conjunction with the amended Urban Design Concept Plan addressing the following:
 - i Building Heights;
 - ii Floor Space Ratios;
 - iii Lot Size;
 - iv Building envelopes;
 - v Road widths;
 - vi Public spaces;
 - vii Streetscapes;
 - viii Housing types;
 - ix Connectivity and Access;
 - x Views and Vistas;
 - xi Urban Form Design development
 - x Clause 7.18 Design Excellence of the Wollongong LEP 2009 (key Site requirements).
 - b Advice from the NSW Heritage Council for comment in relation to:
 - i The Archaeological significance of the site and the potential requirements and implications of archaeological impacts from future development under Section 140 of the NSW Heritage Act 1977.
 - ii The potential for the site to be listed on the State Heritage Register (in light of the findings of the Biosis Report).
 - c Further investigation of the relevant wastewater system capacity identifying that the local system is able to accommodate the additional development, including the consideration of onsite recycling and reuse as part of water cycle management/water services in the development site.
 - d Review of the local stormwater system ensuring there is adequate capacity to transfer runoff during heavy wet weather events which result in local flooding.
 - e Hydrological review in relation to the rail corridor bridge to the south east of the site.
 - f An amended Traffic Impact Assessment is to be prepared to better address the likely impacts of the development on the surrounding roads, including the rail level

crossing and addressing the requirements of Table 2.1 of the RTA Guide to Traffic Generating Development.

- g Review of Sydney Trains 33kV Aerial Line asset on the western side of the rail corridor and the proximity/impact of proposed development.
- h A Rail and Road Acoustic and Vibration Assessment be prepared.
- i An Aboriginal Cultural Heritage Assessment be prepared.
- j A Conservation Management Plan for providing for the long term conservation of significant Coke Works heritage components be prepared.
- k Agreement to provide at least 5% Affordable Rental Housing within the development, and advice on the proposed management arrangements of the dwellings and the proposed housing needs sectors to be targeted.
- l A Revised Ecological Impact Assessment be prepared addressing the following:
 - i Potential impacts to micro bats roosting on the site; and
 - ii Potential for Green and Golden Bell Frog habitat on the site.
- m A revised Remediation Action Plan be prepared addressing:
 - i including further sampling under the structures on the site;
 - ii for PCB's associated with the powerhouse building and transformers;
 - iii address exceedance of ecological investigation levels; and
 - iv address materials containing SMF and Crystalline Silica associated with the coke ovens.
- n Advice on the proposed facilities and/or infrastructure, including costings, that are proposed to be incorporated into a draft planning agreement.

DISCLOSURE OF INTEREST

At this stage, Councillor Figliomeni disclosed a conflict of interest in relation to Item 5 as he owns a property in Pine Crescent, Coniston. Item 5 relates to the reclassification, rezoning and sale of four surplus Council properties, one of which is located in Pine Crescent.

ITEM 1 - OUR WOLLONGONG 2028 STRATEGIC PLANNING DOCUMENTS

37 COUNCIL'S RESOLUTION – RESOLVED UNANIMOUSLY on the motion of Councillor D Brown seconded Councillor King that -

- 1 The draft Our Wollongong 2028 Strategic Planning documents suite be placed on public exhibition from 9 April to 7 May 2018.
- 2 Following the public exhibition period, a revised Draft Our Wollongong 2028 Strategic Planning documents suite be presented to Council for adoption.
- 3 The following internal restrictions be consolidated -
 - a The Future Programs restricted asset be absorbed into the Strategic Projects restricted assets.
 - b The Telecommunications Revenue restricted asset be absorbed into Sports Priority Program restricted asset.
 - c The Property and West Dapto Rates restricted assets be combined into a renamed West Dapto restricted asset.



Planning Proposal

Former Corrimal Coke Works

Client: Legacy Property & Illawarra Coke Company

Date: 23 May 2019

A Veris Company

veris

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1 Executive Summary

The Planning Proposal (PP) has been prepared on behalf of Legacy Property (Legacy) and the Illawarra Coke Company (ICC), for the rezoning of the 18.18 hectare (ha) former Corrimal Coke Works site, located at 27 Railway Street, Corrimal in the Wollongong Local Government Area (LGA). The former Coke Works ceased its operations in 2014, and now offers a substantial rezoning and redevelopment opportunity to provide much needed additional housing within Wollongong.

The original draft PP was prepared and submitted to Council in October 2017. The PP was then placed on public exhibition from 23rd October to 24th November 2017. Council prepared a preliminary assessment, which was supported by Council (subject to further technical and design studies) and forwarded to the Department of Planning and Environment (DP&E) for a Gateway determination on 23rd April 2018. The Gateway determination was issued by the DP&E on 20th August 2018, allowing the rezoning proposal for the Corrimal Coke Works site to proceed subject to conditions. This PP addresses and responds to Council's resolution as well as the Gateway determination.

1.1 The Proposal

At present the rezoning area is zoned IN3 Heavy Industrial and RE2 Private Recreation under Wollongong Local Environmental Plan (WLEP) 2009 and has been utilised for industrial works. This application proposes to rezone the site from IN3 and RE2 to R3 Medium Density Residential and RE1 Public Recreation, as well as providing appropriate controls relating to additional permissible uses, minimum lot size, height, Floor Space Ratio (FSR) and heritage. A small portion of the site (0.25ha) to the north east, is currently zoned SP2 Infrastructure - Road, for potential future acquisition. This land has been incorporated into the concept plan and will be retained as SP2.

The rezoning of Corrimal will provide approximately 700 – 750 new residences. It is expected that the site will provide a broad mix of housing types ranging from medium density terraces and townhouses to low and mid rise apartments, as well providing affordable housing and the potential for seniors living accommodation. The proposed central plaza will provide approximately 2,000m² of retail space adjacent to the existing Corrimal Station. The site will be connected to the wider area and Corrimal town centre via a number of cycle and pedestrian links. Approximately 8.6ha will be provided as open space, bushland and riparian corridors.

Legacy Property is also committed to providing a diverse range of housing choices including 5% affordable housing, which is proposed for inclusion within a Voluntary Planning Agreement (VPA). The proposed redevelopment of the site offers an opportunity to deliver a whole-of-life community.

1.2 Strategic Context

Under Wollongong City Council's Corrimal Town Centre Plan 2015-2025 and Implementation Plan, the former Corrimal Coke Works was nominated as a key site to support reshaping the town centre. The Town Centre Plan and Implementation Plan states *"Residential development may be suitable, where it offers housing diversity and improved connectivity"*. R3 Medium Density Residential zoning would be a logical extension of the same zoning to the north, north-east and east of the site, and would enable the delivery of apartments, semi-detached housing and some single dwellings at an affordable price while creating a high-quality urban environment adjacent to recreation space and facilities.

1.3 Site Specific Context

An analysis supported by extensive technical studies has identified the site as being suitable for development due to the following site-specific opportunities:

- » Existing rail connection linking the site to the Wollongong CBD and Sydney
- » 350m walk to Corrimal town centre
- » 1.2km walk to Corrimal beach
- » Encouragement of active transport and public transport usage
- » Integration via pedestrian and cycle links with the surrounding community
- » Retention and enhancement of Towradgi Creek
- » Retention of biodiversity areas
- » Protection of important sightlines to the Illawarra Escarpment
- » Creation of a new neighbourhood retail centre adjacent to the existing Corrimal train station
- » Opportunities for a diverse mix of housing types
- » Resolution of key flooding issues
- » Nearby community and recreational resources including Robert Ziems Park playing fields, Corrimal library, Corrimal swimming pool, East Corrimal Primary School, Corrimal High School
- » Provision of infill development, minimising conversion of rural land for housing

The Concept Master Plan for the rezoning area is shown in **Figure 1**.

1.4 Design Values and Principles

The Master Plan for Corrimal has been based on the following place principles:

- » *Place Principle 1 Made for Friendship: We help people to meet, share and connect, building on and contributing to Corrimal's already genuinely friendly and supportive community. We're creating a safe and supportive place where people grow together, look out for each other and share special moments and events.*
- » *Place Principle 2 Designed for difference: We're building a community of many different ages, shapes and sizes. We champion difference and are designed to attract people at different stages of life. Our place is made up of a wide variety of landscapes, precincts, features, housing types and experiences, creating a vibrant and distinctive destination.*
- » *Place Principle 3 Bringing more to life: We invest in making people and places the best they can be, breathing new life into heritage and green space and helping people reach their potential. We make everyday life easier, so that people have more time and energy to enjoy what's important.*
- » *Place Principle 4 United through stories: We cherish our unique story. From our history, to our unique ecology, we're a place like no other. Together we will celebrate the stories of the past and build new stories, forging a strong sense of character and identity.*

Figure 1 Corrimal Coke Works Master Plan



Source: DKO

The Master Plan and supporting studies have informed the rezoning of the site.

The vision for the site is to integrate diverse housing and deliver a public plaza adjoining Corrimal train station, to promote public-transit usage for residents and the broader community. The key features of the Concept Plan include:

- » Realignment of the riparian corridor to deliver 12.5ha of PMF-free contiguous developable land that maximises avoidance of significant ecological resources where practical.
- » Delivering local open space resources and facilities along the realigned riparian corridor, connected with the coast and community and recreational resources, as well as a new local park for the enjoyment of the local residents.
- » Retention of substantial ecological environmental areas within the northern and southern section of the site and integrated with the riparian corridor
- » Maximising access to Corrimal train station and delivery of a public plaza adjacent to the station with retention of key heritage buildings and also interpretive heritage elements.
- » Enabling neighbourhood and commuter services at the plaza such as child care, neighbourhood shops and services. However, ensuring limitations that prevent any undermining of the Corrimal town centre.
- » Delivering a diversity of housing, particularly affordable housing typologies such as attached and semi-detached dwellings and low-scale residential flat buildings capitalising on connectivity.
- » Providing a suitable buffer distance from the rail and state road corridors adjacent to the site for noise attenuation.
- » Outlining a potential green link to East Corrimal Beach to the east, Robert Ziems Park and community facilities to the west, and through to the north of the subject site.

1.5 Addressing and Achieving Council's requirements

In Council's resolution of 3 April 2018, it was resolved that the below additional information be submitted before or during the exhibition period, to enable Council to consider the information prior to determining the Planning Proposal.

Below is a table outlining a summary of Council's resolution/requirements and a response to the issues raised in the right hand column.

Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
a) A site specific DCP Chapter be developed and submitted in conjunction with the amended Urban Design Concept Plan	A Site Specific DCP has been prepared to support this Planning Proposal.
b) Advice from the NSW Heritage Council for comment in relation to: <ul style="list-style-type: none"> i. The Archaeological significance of the site ii. The potential for the site to be listed on the State Heritage Register 	Three heritage reports have been prepared (refer to Appendices A, I and J) which determine that the site has no archaeological significance, nor does the site have the potential to be listed on the State Heritage Register. However, four items will be retained on site, listed as items of local heritage significance under WLEP 2009.
c) Further investigation of the relevant wastewater system capacity, including water cycle management/water services in the development site.	The Servicing Strategy Report prepared by BG&E (Appendix S) confirms that the wastewater system is capable of accommodating the proposed development.
d) Review of the local stormwater system	The Servicing Strategy Report prepared by BG&E (Appendix S) confirms that the stormwater system is capable of accommodating the proposed development.

Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
e) Hydrological review in relation to the rail corridor bridge to the south east of the site.	A Flood Study has been prepared and is included at Appendix F .
f) An amended Traffic Impact Assessment	An amended Traffic Impact Assessment has been prepared (Appendix T), which assesses the impacts of the development on the surrounding road network. The Railway Street/Memorial Drive intersection is proposed to be upgraded as part of the development.
g) Review of Sydney Trains 33kV Aerial Line asset on the western side of the rail corridor	A review of the Sydney Trains 33kV Aerial Line was undertaken as part of the Servicing Strategy Report Summary by BG&E (Appendix S). Sydney Trains confirmed that feeder 787 does not currently have an easement as the station carpark and access road is owned by Railcorp.
h) A Rail and Road Acoustic and Vibration Assessment be prepared	A noise and vibration assessment was undertaken by Renzo Tonin & Associates (Appendix O). The study found that noise and vibration issues do not present any constraint to rezoning the site for primarily residential uses.
i) An Aboriginal Cultural Heritage Assessment be prepared.	An Aboriginal Cultural Heritage Assessment was undertaken by Kelleher Nightingale Consulting Pty Ltd (Appendix A). The parts of the study area used for the former cokeworks operations exhibited significant levels of disturbance that would have removed/displaced Aboriginal archaeological objects. One Aboriginal archaeological site comprising Aboriginal objects is located within the study area: low density artefact scatter FCCW AFT 1 (AHIMS 52-2-4505).
j) A Conservation Management Plan for providing for the long-term conservation of significant Coke Works heritage components be prepared.	A Conservation Management Strategy has been prepared for the site. Refer to Appendix C .
k) Agreement to provide at least 5% Affordable Rental Housing within the development, and advice on the proposed management arrangements of the dwellings and the proposed housing needs sectors to be targeted.	Legacy is committed to providing 5%/35 Affordable Rental Housing units, as identified in the VPA.
l) A Revised Ecological Impact Assessment be prepared addressing the following: i. Potential impacts to micro bats roosting on the site; and ii. Potential for Green and Golden Bell Frog habitat on the site.	A revised Ecological Assessment has been prepared by EcoLogical (Appendix G). The survey did not identify any signs of occupation or individual microbats, nor did the echolocation survey return any positive or potential microbat calls. The report also found that the study area was not considered to provide habitat for the Green and Golden Bell Frog.
m) A revised Remediation Action Plan	A revised Remediation Action Plan (Appendix P) has been prepared by Arcadis to provide the information required.
n) Advice on the proposed facilities and / or infrastructure, including costings, that are	Proposed facilities and infrastructure are included as part of the VPA.

Required additional information (shortened in this executive summary)**Response (shortened in this executive summary)**

proposed to be incorporated into a draft planning agreement.

1.6 Conclusion

This PP provides the justification for achieving the broader strategic planning framework that will support the delivery of the broader objectives for redevelopment of the rezoning area, focused on providing new housing in close proximity to Corrimal station and Corrimal town centre, approximately 5km from Wollongong city centre.

The PP provides the evidence that the site is developable, responds to the surrounding built and natural environment and seeks to retain elements of the sites past industrial history. The outcome from the review of the sites strategic context as well as the site-specific analysis and merit, provides confidence that the site has the capacity and suitability to be supported for a rezoning.

2 Introduction

2.1 Purpose of the Planning Proposal

This Planning Proposal (PP) has been prepared on behalf of Legacy Property (Legacy) and the Illawarra Coke Company Pty Ltd (ICC). The PP seeks to amend the Wollongong Local Environmental Plan (WLEP) 2009 to rezone the 18.18 hectare (ha) Corrimal Coke Works site (the site) located at 27 Railway Street, Corrimal, within the Wollongong Local Government Area (LGA). The site has frontages to Memorial Drive, Railway Street and Corrimal train station and the railway line, which connects Wollongong to Port Kembla, Waterfall and Bondi Junction.

Corrimal is well located, being adjacent to the railway line and Memorial Drive, which provides a direct road link towards south Wollongong and north to the Princes Highway, linking to Sydney. The site is located approximately 4km north of the University of Wollongong and approximately 5.5km north of the Wollongong CBD and 63.5km south of the Sydney CBD.

Corrimal town centre is a 350m walk from the site, enabling easy access to service stations, major supermarkets, speciality retail, Corrimal Hotel, banks, dentists, pharmacy's, gyms, community centre, restaurants, cafes and a number of other retail uses. Land uses directly surrounding the site consist predominantly of low to medium density residential development.

The site, in its regional context, is shown in **Figure 2** below.

Figure 2 Aerial view of the rezoning area



Source: Sixmaps

An amendment to WLEP 2009 is proposed in order to rezone the existing 18.18ha Corrimal Coke Works site, to accommodate approximately 700 – 750 new dwellings. The site comprises Lot 1 in Deposited Plan (DP) 795791, Lot 5 in DP 749492, Lot 126 DP 598190 and Lot 11 DP 749492.

2.2 Background

The Corrimal site was in operation as a Coke Works from 1912 to 2014. The site was decommissioned in 2014 and since this time the site has been locked up and vacant.

In considering the future of the site, ICC engaged expert consultants to understand the opportunities and constraints of the site. Original concept planning identified the potential for a mix of single and multi-unit dwellings and residential flat buildings under a R3 Medium Density Residential zoning. The remainder of the site was proposed to be rezoned to RE1 Public Recreation.

The original draft PP was prepared and submitted to Council in October 2017. The PP was then placed on public exhibition from 23rd October to 24th November 2017. Council prepared a preliminary assessment, which was supported by Council (subject to further technical and design studies) and forwarded to the Department of Planning and Environment (DP&E) for a Gateway determination on 23rd April 2018. The Gateway determination was issued by the DP&E on 20th August 2018, allowing the rezoning proposal for the Corrimal Coke Works site to proceed subject to conditions. This PP addresses and responds to Council's resolution as well as the Gateway determination.

In December 2018 ICC and Legacy held a series of visioning workshops with key stakeholders from Council, Neighbourhood Forum 4, Corrimal Community Action Group (CCAG), Corrimal Region Action Group (CRAG) and the Corrimal Chamber of Commerce. These workshop outcomes informed the vision and principles included as part of the project development and concept plan for the PP. Once an updated concept plan and further detailed technical studies were undertaken further consultation was held in late March 2019.

The PP has since been amended and updated in accordance with the findings from the community consultation, subconsultant studies and advice provided by Council and the DP&E. This PP is now final and is submitted to Council for approval and subsequent amendments to the WLEP 2009 by the DP&E.

2.3 Summary of the Planning Proposal

The site is well placed to provide 'in-fill' land redevelopment, given its adjacency to existing residential development in Corrimal, the location next to Corrimal train station and its proximity to the Corrimal town centre.

As part of the PP, consultation has been undertaken with the DP&E, other state agencies and Wollongong Council in order to inform the proposed planning controls on the site. A summary table identifying the proposed outcomes of this PP are identified in **Table 1** below.

Table 1 Summary of the Proposal

Property Details	<p>The total site comprises 4 existing lots:</p> <ul style="list-style-type: none"> » Lot 1 in DP 795791 » Lot 5 in DP 749492 » Lot 126 DP 598190 » Lot 11 DP 749492
Area	18.18ha (181,754m ²)
Proposal	<p>Amend WLEP 2009 as follows:</p> <p>Land Zoning Map 024</p> <p>Remove the RE2 Private Recreation and IN3 Heavy Industrial zoning from the site and replace with:</p> <ul style="list-style-type: none"> » R3 Medium Density Residential » RE1 Public Recreation

Minimum Lot Size (MLS) Map 024

Remove the 1.99ha MLS from the part of the site zoned IN3 and introduce a new MLS of part 135m² and part 149m² for land zoned R3 Medium Density Residential.

Height of Building (HOB) Map 024

Remove the 9m height control from part of the site zoned RE2.

Insert a height control of 9m over all RE1 zoned land.

Insert the following height controls over R3 zoned land; 9m, 11m, 13m, 16m, 20m and 24m

Floor Space Ratio (FSR) Map 024

No FSR control currently exists for the site.

Insert the following FSRs over all R3 zoned land; 0.5:1, 0.75:1, 1.2:1, 1.5:1, 2:1 and 2.5:1

No FSR control is proposed for RE1 land

Heritage Map 024

The heritage map will be amended to include 'the Corrimal Coke Works Site' with a curtilage, which consists of four new local heritage items and associated curtilage. These items will also be included in Schedule 5 of the WLEP 2009, as follows:

- » The Corrimal Coke Works Site:
 - > C1 North Stack
 - > C1 coke oven battery
 - > Old Power House
 - > C1 Brick Chimney stack

Riparian Land Map 024

Amend the location of the riparian corridor to reflect the creek re-alignment

Remove the foreshore building line from the map

Remove any land located below the foreshore building line from the map

Natural Resource Sensitivity Map 024

Amend the Natural Resource Sensitivity map to include 2 new areas of biodiversity sensitivity to the north and south of the site.

Part 7 Local Provisions – General

The following clause will be included in Part 7 to allow increased heights, beyond the maximum height control of 24m, for C1 North Stack and C1 Brick Chimney Stack, which currently sit at heights of approximately 36.8m and 29m respectively:

7.20 Former Corrimal Coke Works**Height of Development**

- (1) *The height of any development on the former Corrimal Coke Works site is not to exceed the height limit shown on the Height of Buildings Map, with the exception of the following heritage items:*
 - a) C1 North Stack
 - b) C1 Brick Chimney Stack
- (2) *The following height limits are permissible for the heritage items:*
 - a) C1 North Stack: 37m

	<p>b) C1 Brick Chimney Stack: 29m</p> <p>Affordable Housing</p> <p>(1) <i>The consent authority may permit a variation to the Maximum Height of Buildings Control of up to 3.5m and the associated increase in FSR for any Residential Flat Buildings, where the additional Gross Floor Area is utilised for the provision of Affordable Housing</i></p>
	<p>Schedule 1 Additional permitted uses</p> <p>The following clause will be included in Schedule 1 Additional Permitted Uses to ensure that a full complement of neighbourhood and commuter services near Corrimal train station can be provided:</p> <p><i>Use of certain land at the former Corrimal coke works site:</i></p> <p>(3) <i>This applies to land at the former Corrimal coke works site</i></p> <p>(4) <i>Development for the purposes of Food and Drink Premises, and neighbourhood supermarket is permitted with consent, but only if at ground floor and within 200m of the train station</i></p> <p>(5) <i>Development for neighbourhood shops and neighbourhood supermarket is permitted up to a maximum of a total GFA of 2,000m².</i></p>

These outcomes are explained in further detail in **Part 2** of this report. A Site Specific Development Control Plan (DCP) has also been prepared to support the proposed rezoning.

In order to support the proposed rezoning, a Concept Master Plan has been developed to identify future potential land uses. The design principles of the Concept Master Plan for the rezoning area include:

Principle 1 – Community

- » Deliver a public open space network that forms the focus and identity for the precinct, providing a range of recreational opportunities. Access to this public open space is to be maximised to enable the community to gather together

Principle 2 – Innovation

- » Provide innovation in the design of built form and public realm to ensure the character of the site reflects different character precincts.

Principle 3 – Celebrating History

- » Integrate the brick chimney, coke ovens and the powerhouse into the overall development of the public plaza, in order to celebrate the history of the site

Principle 4 – Great streets

- » Design a safe and functional road network with increased walkability to Corrimal station, as this will be a direct link to the heart of the community
- » Traffic control and circulation within the community is a primary consideration to provide connectivity with the surrounding Corrimal community

Principle 5 – Improved access for all

- » Deliver a site that is highly permeable to pedestrians and cyclists, linking the precinct to surrounding amenities, services and facilities, public transport, recreational opportunities and beach and broader regional pedestrian/cycle path network

Principle 6 – A diverse community

- » Create a residential community with housing diversity and neighbourhood and commuter services creating some employment opportunities, capitalising on the excellent public-transit advantages of the site. Housing

types to include detached dwellings, semi-detached dwellings, attached dwellings, apartments and seniors living.

Principle 7 – Compliment the town centre

- » Improve linkages between the Corrimal town centre and the train station by offering attractive streetscapes
- » Offer neighbourhood and commuter services near the train station for added convenience but at a scale that does not detract from the efficacy of the Corrimal town centre

Principle 8 – Ecological Enhancement

- » Conserve, manage and improve native vegetation and biodiversity located to the south and north-west of the site, as well as the new riparian corridors
- » Allow for earthworks to improve and manage the drainage across the site in order to reform and rehabilitate a riparian corridor to improve water management and ultimately enhance the biodiversity values on the site

Principle 9 – Design Excellence

- » deliver serviced development with quality infrastructure services and diverse architectural elements

Principle 10 – Unique Character

- » Create view lines and linkages to the heritage significant core

Figure 3 Concept Master Plan



Source: DKO

2.4 Objective of this report

The PP has been prepared on behalf of Legacy Property and ICC to support the amendment to WLEP 2009 for the site at Corrimal. The proposed amendment to the WLEP 2009 for the rezoning of the site will facilitate redevelopment to accommodate residential land uses, intended to assist in meeting housing targets for the Wollongong LGA and the suburb of Corrimal.

The PP is consistent with the requirements of Section 55 of the Environmental Planning and Assessment Act 1979 (EP&A Act), in particular the guidelines issued under Section 55(3) of the EP&A Act and has been prepared having regard to the DP&E's 'A guide to preparing planning proposals' (2016) and 'A guide to preparing local environmental plans'.

The PP provides:

- » comprehensive details on the subject site and its surrounds;
- » responses to the Council resolution and the Gateway requirements (refer to **Section 2.5** below).
- » identification of the Environmental Planning Instruments (EPIs), non-statutory planning documents (such as Development Control Plan (DCPs)) and other strategies (planning or otherwise) applying to the subject site and/or the PP, as well as identification of the relevant controls and requirements contained within those EPIs, non-statutory planning documents and strategies;
- » a statement of the objectives and intended outcomes of the PP;
- » explanation of the provisions that are to be included in the proposed instrument;
- » thorough justification of the PP against the questions set out in the above-mentioned guidelines, demonstrating that the proposal is in the public interest, and is worthy of approval;
- » identification of the WLEP 2009 maps which would be amended under the PP;
- » details of the community consultation that has been/will be undertaken on the PP;
- » a project timeline detailing anticipated timeframe for the plan-making process.

The preparation of this report and supporting technical studies has involved the collaboration of a multi-disciplinary team to ensure all relevant issues have been addressed. The documentation submitted in support of the PP is identified in **Table 2** below. This PP should be read in conjunction with these technical reports.

Table 2 Supporting Technical Reports

Report	Prepared by/author	Date	Appendices
Aboriginal Cultural Heritage Assessment	Kelleher Nightingale Consulting Pty Ltd	May 2019	Appendix A
Additional Environmental Assessment	Arcadis	12 April 2017	Appendix B
Conservation Management Strategy	Urbis	2019	Appendix C
Consultation Outcomes Report	Elton Consulting	1 May 2019	Appendix D
Economic Impact Assessment	Hill PDA	September 2017	Appendix E
Flood Study	Cardno	2 May 2019	Appendix F
Flora and Fauna Assessment	EcoLogical	May 2019	Appendix G

Report	Prepared by/author	Date	Appendices
Geomorphology Assessment	Soil Conservation Service	March 2018	Appendix H
Geotechnical Assessment	Douglas Partners	May 2017	Appendix I
Heritage Interpretation Strategy	Urbis	7 May 2019	Appendix J
Historical Heritage Assessment	Biosis	1 August 2017	Appendix K
Illawarra – Shoalhaven: Housing Market Report for Corrimal Coke Works	Macro Plan Dimasi	February 2019	Appendix L
Landscape Master Plan Report	Clouston Associates	15 May 2019	Appendix M
Master Plan Report	DKO	17 May 2019	Appendix N
Noise and Vibration Assessment	Renzo Tonin & Associates	13 May 2019	Appendix O
Remediation Action Plan	Arcadis	13 May 2019	Appendix P
Remediation Action Plan Endorsement	Zoic Environmental Pty Ltd	17 May 2019	Appendix Q
Retail Market Demand and Economic Impact Assessment	Urbis	February 2019	Appendix R
Servicing Strategy Report Summary	BG&E	15 April 2019	Appendix S
Traffic Impact Assessment	Bitzios Consulting	29 April 2019	Appendix T
VPA Schedule	Legacy Property	May 2019	Appendix U

2.5 Wollongong Council resolution

In Council's resolution of 3 April 2018, it was resolved that the below additional information be submitted before or during the exhibition period, to enable Council to consider the information prior to determining the Planning Proposal.

Below is a table outlining Council's resolution/requirements and a response to the issues raised in the right-hand column.

Table 3 Response to Council's Resolution

Required additional information	Response
o) A site specific DCP Chapter be developed and submitted in conjunction with the amended Urban Design Concept Plan addressing the following: <ul style="list-style-type: none"> i. Building Heights; 	A Site Specific DCP has been prepared to support this Planning Proposal.

Required additional information	Response
<ul style="list-style-type: none"> ii. Floor Space Ratios; iii. Lot Size; iv. Building envelopes; v. Road widths; vi. Public spaces; vii. Streetscapes; viii. Housing types; ix. Connectivity and Access; x. Views and Vistas; xi. Urban Form Design development xii. clause 7.18 Design Excellence of the Wollongong LEP 2009 (key Site requirements). 	
<p>p) Advice from the NSW Heritage Council for comment in relation to:</p> <ul style="list-style-type: none"> iii. The Archaeological significance of the site and the potential requirements and implications of archaeological impacts from future development under Section 140 of the NSW Heritage Act 1977. iv. The potential for the site to be listed on the State Heritage Register (in light of the findings of the Biosis Report). 	<p>Three heritage reports have been prepared (refer to Appendices A, J and K) which determine that the site has no archaeological significance, nor does the site have the potential to be listed on the State Heritage Register. However, four items will be retained on site, listed as items of local heritage significance under WLEP 2009. These items will collectively be known as 'former Corrimal Cokeworks'.</p>
<p>q) Further investigation of the relevant wastewater system capacity identifying that the local system is able to accommodate the additional development, including the consideration of onsite recycling and reuse as part of water cycle management/water services in the development site.</p>	<p>The Servicing Strategy Report prepared by BG&E (Appendix S) confirms that the wastewater system is capable of accommodating the proposed development.</p>
<p>r) Review of the local stormwater system ensuring there is adequate capacity to transfer runoff during heavy wet weather events which result in local flooding.</p>	<p>The Servicing Strategy Report prepared by BG&E (Appendix S) confirms that the stormwater system is capable of accommodating the proposed development.</p>
<p>s) Hydrological review in relation to the rail corridor bridge to the south east of the site.</p>	<p>A Flood Study has been prepared and is included at Appendix F.</p>
<p>t) An amended Traffic Impact Assessment is to be prepared to better address the likely impacts of the development on the surrounding roads, including the rail level crossing and addressing the requirements of Table 2.1 of the RTA Guide to Traffic Generating Development.</p>	<p>An amended Traffic Impact Assessment has been prepared (Appendix T), which assesses the impacts of the development on the surrounding road network. The report concludes that the only impacts would be to the Memorial Drive/Railway Street intersection. This intersection is proposed to be upgraded as part of the development.</p>
<p>u) Review of Sydney Trains 33kV Aerial Line asset on the western side of the rail corridor and the proximity/impact of proposed development.</p>	<p>A review of the Sydney Trains 33kV Aerial Line was undertaken as part of the Servicing Strategy Report Summary by BG&E (Appendix S). Sydney Trains confirmed that feeder 787 does not currently have an easement as the station carpark and access road is owned</p>

Required additional information	Response
	by Railcorp. It is not envisaged that this electrical feeder will pose any significant constraint to rezoning and development of the site.
v) A Rail and Road Acoustic and Vibration Assessment be prepared	<p>A noise and vibration assessment was undertaken by Renzo Tonin & Associates (Appendix O). The study found that noise and vibration issues do not present any constraint to rezoning the site for primarily residential uses.</p> <p>Any noise impacts can be suitably mitigated to achieve compliance through standard treatments to future residential development that can be detailed at the development application stage.</p>
w) An Aboriginal Cultural Heritage Assessment be prepared.	<p>An Aboriginal Cultural Heritage Assessment was undertaken by Kelleher Nightingale Consulting Pty Ltd (Appendix A). The report confirmed that extensive previous modifications and disturbance associated with former industrial land use have diminished or negated the archaeological potential of the majority of the study area. The parts of the study area used for the former cokeworks operations exhibited significant levels of disturbance that would have removed/displaced Aboriginal archaeological objects.</p> <p>One Aboriginal archaeological site comprising Aboriginal objects is located within the study area: low density artefact scatter FCCW AFT 1 (AHIMS 52-2-4505). FCCW AFT 1 is located on an elevated floodplain landform in association with Towradgi Creek, in the southern portion of the study area. The entirety of the site area would be impacted by the proposed redevelopment works. An AHIP issued by the Office of Environment and Heritage under section 90(1) of the National Parks and Wildlife Act 1974 is required prior to any activity which may harm an Aboriginal object.</p>
x) A Conservation Management Plan for providing for the long-term conservation of significant Coke Works heritage components be prepared.	A Conservation Management Strategy has been prepared for the site. Refer to Appendix C .
y) Agreement to provide at least 5% Affordable Rental Housing within the development, and advice on the proposed management arrangements of the dwellings and the proposed housing needs sectors to be targeted.	Legacy and ICC are committed to providing 5%/35 Affordable Rental Housing units, as identified in the VPA.
<p>z) A Revised Ecological Impact Assessment be prepared addressing the following:</p> <ul style="list-style-type: none"> iii. Potential impacts to micro bats roosting on the site; and iv. Potential for Green and Golden Bell Frog habitat on the site. 	<p>A revised Flora and Fauna Assessment has been prepared by EcoLogical (Appendix G). The survey undertaken as part of the assessment did not identify any signs of occupation or individual microbats, nor did the echolocation survey return any positive or potential microbat calls. No microbats were witnessed during the survey and it is considered that the built structures were unlikely to provide habitat for the species.</p> <p>The report also found that the study area was not considered to provide habitat for the Green and Golden</p>

Required additional information	Response
	Bell Frog. In addition, the presence of the Plague Minnow in both the water bodies on site would have inhibited the reproductive capacity of any Green and Golden Bell Frogs as the minnow excludes native species through aggressive behaviour towards other species and amphibians.
aa) A revised Remediation Action Plan be prepared addressing: <ul style="list-style-type: none"> i. including further sampling under the structures on the site; ii. for PCB's associated with the powerhouse building and transformers; iii. address exceedance of ecological investigation levels; and iv. address materials containing SMF and Crystalline Silica associated with the coke ovens. 	<p>A revised Remediation Action Plan (Appendix P) has been prepared by Arcadis to provide the information required.</p> <p>The revised Remediation Action Plan has been endorsed by a NSW accredited, independent site auditor (Appendix Q)</p>
bb) Advice on the proposed facilities and / or infrastructure, including costings, that are proposed to be incorporated into a draft planning agreement.	Proposed facilities and infrastructure are included as part of the VPA.

2.6 Gateway Determination

Gateway approval was received on 20th August 2018 by the DP&E. The approval was granted subject to conditions. These conditions, along with our responses, are identified in the table below.

Table 4 Response to the DP&Es conditions

Condition	Response
1. To ensure consistency with 9.1 Directions 2.3 Heritage Conservation and 4.3 Flood Prone Land, the following studies are to be completed prior to public exhibition: <ul style="list-style-type: none"> • An Aboriginal Cultural Heritage Assessment • A Conservation Management Plan that provides for the long-term conservation of significant coke work heritage components • Revised flood study (including flood modelling); and • Geomorphological report 	These required reports have all been provided. Refer to Tables 2 and 3 above.
2. The following studies are also to be completed prior to public exhibition: <ul style="list-style-type: none"> • A revised ecological assessment • A revised traffic impact assessment; and • A revised remediation plan 	These required reports have all been provided. Refer to Tables 2 and 3 above.
3. Public Exhibition is required under section 3.34(2)(c) and schedule 1 clause 4 of the Act as follows: <ul style="list-style-type: none"> a) The planning proposal must be made publicly available for a minimum of 28 days; and 	The Planning Proposal will be exhibited in accordance with these requirements.

Condition	Response
<p>b) The planning proposal authority must comply with the notice requirement for public exhibition of planning proposals and the specifications for material that must be made publicly available along with planning proposals as identified in section 5.5.2 of A guide to preparing local environmental plans (Department of Planning and Environment 2016).</p>	
<p>4. Consultation is required with the following public authorities/organisations under section 3.34(2)(d) of the Act and/or comply with the requirements of relevant section 9.1 Directions:</p> <ul style="list-style-type: none"> • Roads and Maritime Services; • Department of Primary Industries – Water; • Environment Protection Authority; • Office of Environment and Heritage; • Sydney Water; • RailCorp; • Department of Education; • Heritage Council; • National Trust of Australia (Illawarra Shoalhaven Regional Branch); • Endeavor Energy; • Transport for NSW; • Department of Primary Industries – Fisheries NSW; • Catchment Management Authority; and • State Emergency Service <p>Each public authority is to be provided with a copy of the planning proposal and any relevant supporting material, and given at least 21 days to comment on the proposal.</p>	<p>Consultation with the specified authorities/organisations will be carried out in accordance with the DP&E's requirements.</p>
<p>5. A public hearing is not required to be held into this matter by any person or body under section 3.34(2)(e) of the Act. This does not discharge Council from any obligation it may otherwise have to conduct a public hearing (for example, in response to a submission or if reclassifying land).</p>	<p>Noted.</p>
<p>6. The timeframe for completing the LEP is to be 18 months following the date of the Gateway determination.</p>	<p>Noted.</p>

A review of the table above demonstrates that this PP has addressed the Gateway study requirements.

3 Site Analysis

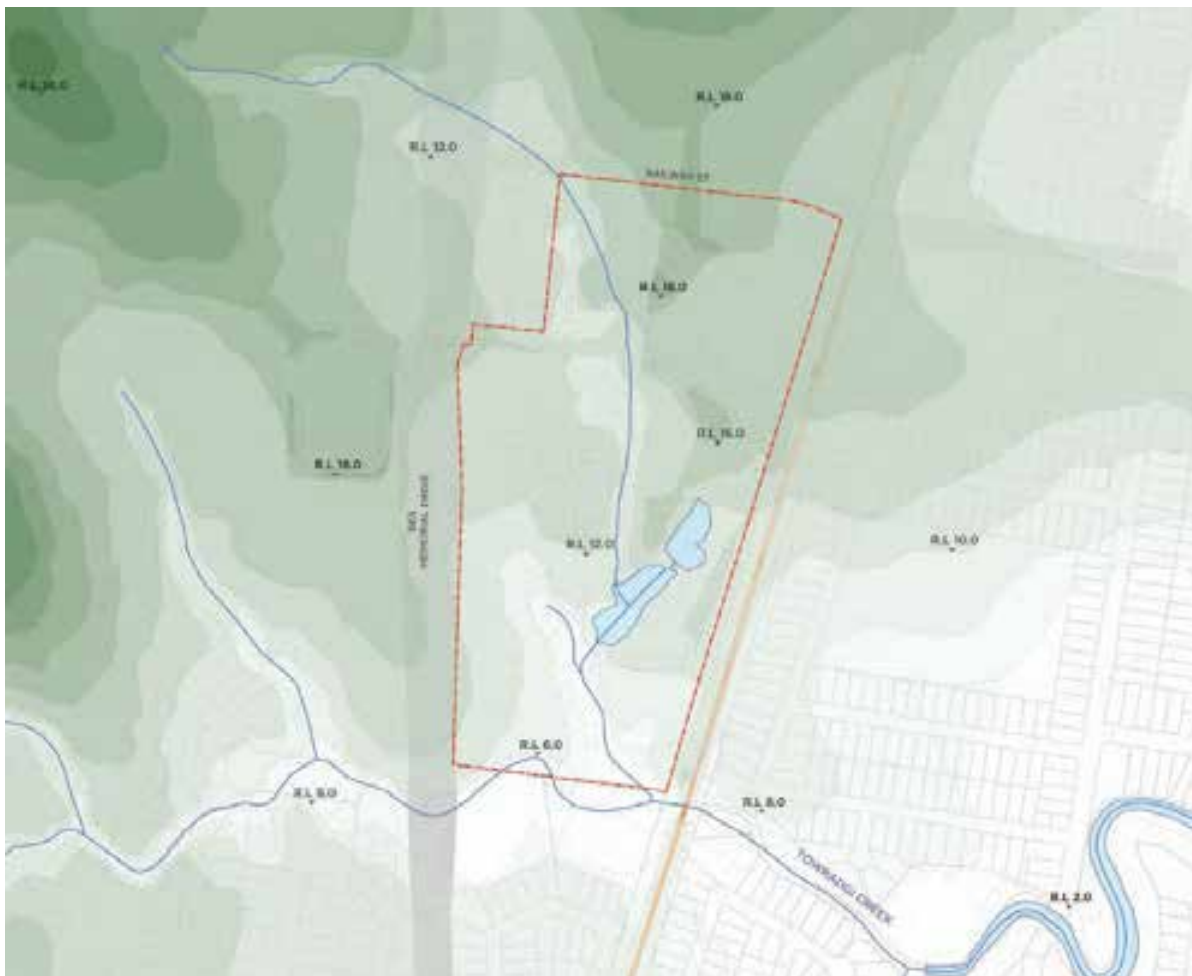
Legacy and ICC have undertaken extensive research and assessment to form the suitability and capacity of the site for redevelopment. This section contains a summary analysis of the rezoning area, including ecology, flooding and hydrology, land contamination, traffic and transport, infrastructure utilities and services and heritage.

This section of the report also provides an overview of the social infrastructure in the area, an analysis of the demographics as well as housing need. Further details of the analysis can be found in the supporting documentation accompanying this rezoning, outlined in **Table 2**.

3.1 Topography

The topography of the site is relatively flat. There is a minor fall in the landscape from north to south, with the low point of the site being associated with Towradgi Creek at the southern extent. **Figure 4** shows the existing topography of the site based on 2m GIS contours. Topography does not present a significant constraint to development.

Figure 4 Site Topography



Source: E8urban

3.2 Soil

In accordance with the Acid Sulfate Soils (ASS) Assessment Guidelines, there is typically a requirement for assessment of the potential for estuarine ASS where the ground surface levels across the site are below RL 5.

Most of the site is classified as no known occurrence of Acid Sulphate Soils – Class 5 with a small portion classified as Low Probability Class 3. As part of future investigation, it is recommended that additional testing of soils be carried out in areas proposed for creek realignment or deep trench excavation for services.

Douglas Partners indicate that even if acid sulfate soils or acid soil conditions are encountered, they can be mitigated through engineering solutions.

3.3 Geology

The Wollongong – Port Hacking 1:100 000 Geological Series Sheet indicates that the site is underlain by the Erin Vale Formation (comprising fine to medium grained lithic sandstone) and Pheasants Nest Formation (comprising interbedded lithic sandstone, coal, carbonaceous claystone, siltstone and claystone). Almost all of the site is mantled by alluvium (comprising quartz and lithic “fluvial” sand, silt and clay) associated with North Corrimal Creek and Towradgi Creek.

Geological investigations undertaken by Douglas Partners inferred the following geological model for the site:

- » bedrock comprising weathering-prone shales, siltstone and sandstone with minor included coalbands, possibly representing the Unanderra Coal Member
- » a highly variable thickness of clay-rich residual and alluvial soils underlying filling within areas previously affected by coke production and storage and directly underlying the surface within the mostly undisturbed, south-western section of the site. These natural soils range from very soft to hard condition. The natural soils have been locally removed to provide foundation surfaces for sections of the coke works. Soils and weathered rock underlying or adjacent to the coke ovens have significantly reduced moisture contents and may have been “baked”.
- » a capping layer ranging from 0.2 m to in excess of 3.5 m of mostly uncontrolled filling within areas previously affected by coke production and storage. The predominant filling materials are coke (from fines to cobble size), coal, coalwash and clay
- » a probably modified course of North Corrimal Creek underlying sections of former coke stockpile areas.

3.4 Geotechnical

Douglas Partners undertook a geotechnical assessment of the site, which indicates that the site can be made suitable for residential development from a geotechnical perspective. While no geotechnical conditions present a constraint to rezoning and development of the site, the following considerations will need to be incorporated into the design of earthworks to facilitate urban development:

- » existing uncontrolled filling (particularly that comprising coal and coke waste and associated waste fines). The degree of modification (if any) that will be required to provide a suitable fill material will be determined by future investigation;
- » minor constraints relate to erosion potential of disturbed materials, localised waterlogging, localised abnormal moisture content within the coke production area, sodicity and soil aggressively, which can be effectively handled during the earthworks design; and
- » as part of future investigations, an earthworks methodology will need to be prepared to ensure the reuse of existing site soils and optimised procedures for the site.

Regarding specific issues, Douglas Partners outlined the key considerations to take into account, as shown in **Table 5**. These factors can be addressed through earthworks design.

Table 5 Geotechnical conditions and mitigations

Geotechnical condition	Mitigation
Erosion Potential - The geological mapping identified the development of minor erosion rills within existing filling of the previous stockpile areas. No bank erosion has been observed along the creek alignments nor is there any WCC record of major scour events within the subject sections of North Corrimal Creek or Towradgi Creek.	<p>The potential for erosion of exposed fill materials (particularly silt and sand size fractions) by concentrated flow is assessed as a minor constraint to development which can be addressed with good engineering practice. To minimise the constraint imposed by erosion potential, earthworks within the site should be undertaken in stages, with adequate erosion and sedimentation controls in place.</p> <p>Treatment of batters constructed as part of the future earthworks should include:</p> <ul style="list-style-type: none"> » filling using select materials (i.e. non-dispersive or erodible) placed under controlled conditions; » provision of temporary surface cover (e.g. pegged matting) during the period of stream floor or batter revegetation. » channel lining or piping of drainage paths where appropriate.
Waterlogging - waterlogging and softening of alluvial soils and underlying residual soils are expected within creek floors, areas of new batters, areas adjacent to current water ponds and areas requiring removal of deeper sections of existing filling prior to placement of engineered filling.	<p>Precautionary and remedial works for site preparation for residential development should include:</p> <ul style="list-style-type: none"> » improvement of surface drainage including the interception of overland flow. » installation of subsurface drainage to protect road subgrades. » allowance for placement of a granular bridging layer over stripped alluvial or residual soils where trafficability or placement of engineered filling is affected by soft surface conditions.
Uncontrolled Filling and Reuse particularly that dominated by coalwash and coke products, or new coalwash filling which may be proposed for importation to raise site levels, will be governed by the requirements of the EPA Resource Recovery Order and WCC Coal Washery Refuse in Subdivisions Policy. It should be noted that coke products are not specifically included in the EPA Resource Recovery Order. However, as a combustible material with associated waste derived from coal, it is considered appropriate to include this material for assessment similar to coal washery reuse, in accordance with Council guidelines.	<p>Further assessment will be required as part of the design process to categorise the fill and to determine its geotechnical suitability for reuse on the site. Earthworks design will need to consider WCC Coal Washery Refuse in Subdivisions Policy whereby:</p> <ul style="list-style-type: none"> » very coarse materials (>150 mm) or fine slurry materials (tailings) are to be rejected. » structures are to be slab on ground design. Other footing designs by a Structural/Geotechnical Engineer may be considered. » combustible contents to be determined from site sampling at specified regular frequency. » compaction to be in layers under full engineering control to at least 100% standard density. » combustible contents to be at a mean value not greater than 30% with the upper value not exceeding 40%. » inert fill should be used to backfill services trenches. » coalwash is to be covered by at least 300 mm of inert cover. » proper site control to prevent run-off or dust nuisance. <p>This is achievable to ensure that the site can be developed.</p>
Abnormal Soil Moisture Content - Below the kiln flue, and likely the coke kilns, as well as the extensive concrete paving	<p>Following removal of the existing structures, it is anticipated that exposure to cyclic wetting and drying will result in greater soil swelling than nearby natural soils. Subject to inspection and testing following removal of structures, over-excavation and moisture re-</p>

Geotechnical condition	Mitigation
about the production area is likely to preserve low moisture contents.	conditioning of materials may be required. This is not expected to be a constraint to development.
Aggressivity - The pH, chloride, Sulfate and resistivity of soil and groundwater samples were compared to the requirements of the AS 2159 – 2009 for exposure classification of concrete and steel piles in soil. Most of the results indicated non-aggressive exposure classification, with only two of seven pH results for soils indicating a “Mild” exposure classification for concrete piles.	Aggressivity of soils is not considered to be a constraint to development.

3.5 Geomorphology

The site is traversed by Towradgi Creek, which runs along the southern extent and North Corrimal Creek, which runs roughly North-west to South-east through the site. In addition, an unnamed drainage line enters the site on the western boundary from under Memorial Drive and discharges into North Corrimal Creek.

Based on topographic datasets, North Corrimal Creek is considered a 2nd order stream (Strahler) with an approximate catchment area of 1.6km² upstream of the site. The steep headwaters drain the escarpment through well vegetated bushland before joining at the boundary of developed urban areas near Cox Avenue.

The form and alignment of the existing North Corrimal Creek has been substantially modified in the past through site filling and construction of online dams. These past impacts have been more pronounced in the upstream and central sections of the site. Downstream of the dams, the creek flows more or less along the original alignment.

The existing channel form through the site largely consists of a continuous low flow channel typically 3 – 5m wide inset within a broader macro channel, approximately 30m wide and 4 - 6m deep. Bounding sediments of the low flow channel consists of mud and silt and there are no significant accumulations of mobile bed load deposits.

The riparian zone upstream of the dam is dominated by exotic weed species, while downstream a mixture of native and exotic species is evident. The riparian corridor is proposed to be realigned as part of the development. The current design of the realignment consists of a concept level design of the macro-channel and involves:

- » Channel reshaping and enlargement along the initial approximate 170m of the existing creek line at the upstream extent of the site.
- » Realigning the channel to the western boundary of the site over a distance of approximately 560m.
- » Maintaining the downstream 70m of the existing creek up to the confluence with Towradgi Creek.

The limited evidence of bedload transport through the existing North Corrimal Creek system means that the realignment design does not need to account for the transport of bedload sediment in any significant quantity. As a result, further progression of the realignment design will need to focus on the identified areas of potential channel instability while also providing for a complex assemblage of potential habitats and community amenity.

3.6 Hydrology

The site is located within the lower reaches of the Towradgi Creek catchment, which is characterised by an extensively developed floodplain, with relatively underdeveloped steep upper slopes. Located approximately 5km north of the Wollongong CBD, the Towradgi Creek catchment has a total area of approximately 7.3km². The drainage network of Towradgi Creek catchment is comprised of the following tributaries:

- » Towradgi Creek
- » South Angels Creek
- » North Angels Creek
- » South Corrimal Creek
- » North Corrimal Creek
- » Carr Creek
- » Parker Creek

Towradgi Creek and North Corrimal Creek traverse the site. The majority of the site drains directly into the North Corrimal Creek, with the main production area generally draining south into a small on-site dam.

Being relatively low lying and located within the Towradgi Creek floodplain and its tributaries, there is potential for flooding of the site to occur. A significant area of the site surrounding the creeks is considered flood prone land.

3.6.1 Hydrogeology

Groundwater is likely to be present within the deeper bedrock that consists of the Illawarra Coal Measures. The depth to the bedrock has been mapped at greater than 10m. Groundwater storage within the bedrock sequence would be dominated by fractures. The intensity, connectivity and orientation of the fracture network would determine the groundwater flow direction and velocity. The anticipated regional groundwater flow direction would be east towards the coast. The hydrogeology is summarised below (ES PSI (2013) and ES DSI (2014):

"The standing water level within the existing wells onsite ranged between 3.03m to 4.69m below top of casing. The inferred local groundwater flow direction on the site is to the south to south east. This opinion is based on a review of the predominant slope of the natural topography to the south, the southerly surface water flow direction of North Corrimal tributary across the Site and the easterly flow direction of Towradgi Creek towards the coastline."

3.6.2 Flooding

The Corrimal Flood Study by Cardno found that significant flooding currently occurs on the low point of Railway Street, located to the north-west of the site. In the 100-year Average Recurrence Interval (ARI) event, flooding extends to the south, inundating the existing residential development at 29, 31 and 33 Railway Street. In the Probable Maximum Flood (PMF) event, areas of the existing Cross Street residential development are also inundated.

The proposed creek realignment has been designed to convey major flows up to Probable Maximum Flood (PMF) event within North Corrimal Creek through the site. This results in a total flood free developable area of 12.5ha. The proposed creek works would significantly improve existing flood conditions in the vicinity of Railway Street and within existing residential development on Cross Street and would provide an opportunity to improve drainage of the existing Cross Street residential area.

Maintaining flood plain storage in the 100-year ARI event is the main floodplain management constraint for the site. To compensate for the loss of existing floodplain storage, the realigned creek channel requires widening below the 100-year ARI flood level, with new areas of compensatory earthworks proposed in the southern site extent.

3.7 Contamination

The land is not listed on the Environmental Protection Agency's (EPA) Contaminated Land Register. However, considering the former uses of land, comprehensive contamination assessments have been undertaken in order to

understand the potential environmental liabilities associated with the historical uses of the site. These previous assessments include the following:

- » ADI Limited Stage 1 Site Investigation Report, Station Street, Corrimal (1996) Phase 1 and Phase 2 Contamination Reports
- » EnviroRisk Phase 1 Environmental Assessment, (2005)
- » EnviroRisk, Phase 2 Environmental Assessment, (2006)
- » ES Preliminary Site Investigation, 27 Station Street, Corrimal, March 2014
- » ES Detailed Site Investigation, 27 Station Street, Corrimal, July 2014
- » Arcadis Additional Environmental Assessment Works, 27 Station Street, Corrimal, April 2017
- » Arcadis Additional Environmental Assessment – Production Area, 27 Station Street, January 2018
- » Arcadis Flue, Stack and Powerhouse Assessment, 27 Station Street, March 2018
- » Arcadis, Remediation Action Plan (RAP) 13 May 2019

Previous environmental assessments have identified isolated areas of hydrocarbon impacted soil that have the potential to pose a risk to human health under the proposed land use. Hydrocarbon, copper and zinc are also present in soil and have the potential to pose a risk to ecological receptors. Asbestos fibres and fragments were identified in soil at isolated portions of the site.

There were minor exceedances of dissolved heavy metals and ammonia detected in shallow groundwater. These exceedances are considered typical of regional shallow groundwater conditions and are unlikely to pose a risk to human or local ecological receptors.

Although concentrations of contaminants of concern were detected above environmental assessment criteria, they were isolated and minimal compared to the wider site footprint. It is anticipated that through integration of the remedial strategies outlined in the RAP by Arcadis (refer to **Appendix P**) into the bulk earthworks civil design and construction stages of the project, the site can be suitable for the proposed use.

It is considered that the objectives of the onsite remediation will be achieved subject to the successful implementation of the actions contained in the RAP, which will enable the site to be made suitable for the proposed residential, commercial and open space uses.

3.8 Industrial suitability

An assessment of the viability of on-going industrial use of the site was undertaken by Hill PDA as part of the original PP. The report has concluded:

- » There is sufficient supply of well-located industrial land in the Wollongong LGA without the subject site
 - > The Industrial Lands Audit in 2014 found that the Illawarra Region contained 3,110ha of industrial land. Of the total area of industrial land, 603ha was vacant (19%). The Wollongong LGA contains the highest proportion of vacant industrial land supply in the region, totalling 321 hectares (53% of the region). An examination of the take up of industrial land since the 2014 revealed that 45ha of land has been developed, however, 42ha of this was for the Prixcar processing, storage and transport facility.
 - > The majority of industrial land in the Wollongong LGA is in Unanderra, Kembla Grange and Port Kembla and offers lower cost and better access than Corrimal.
- » Increased demand in transport, storage and warehousing sectors, but the subject site is unsuitable
 - > Large-scale transport, storage and warehousing sector (or logistics) have locational needs that are determined by efficient supply chains, access to customers and suppliers, land availability and main road access. This describes firms that have specific land and infrastructure needs and potentially buffer distance requirements from residential or other sensitive land uses. Consequently, the site is not as

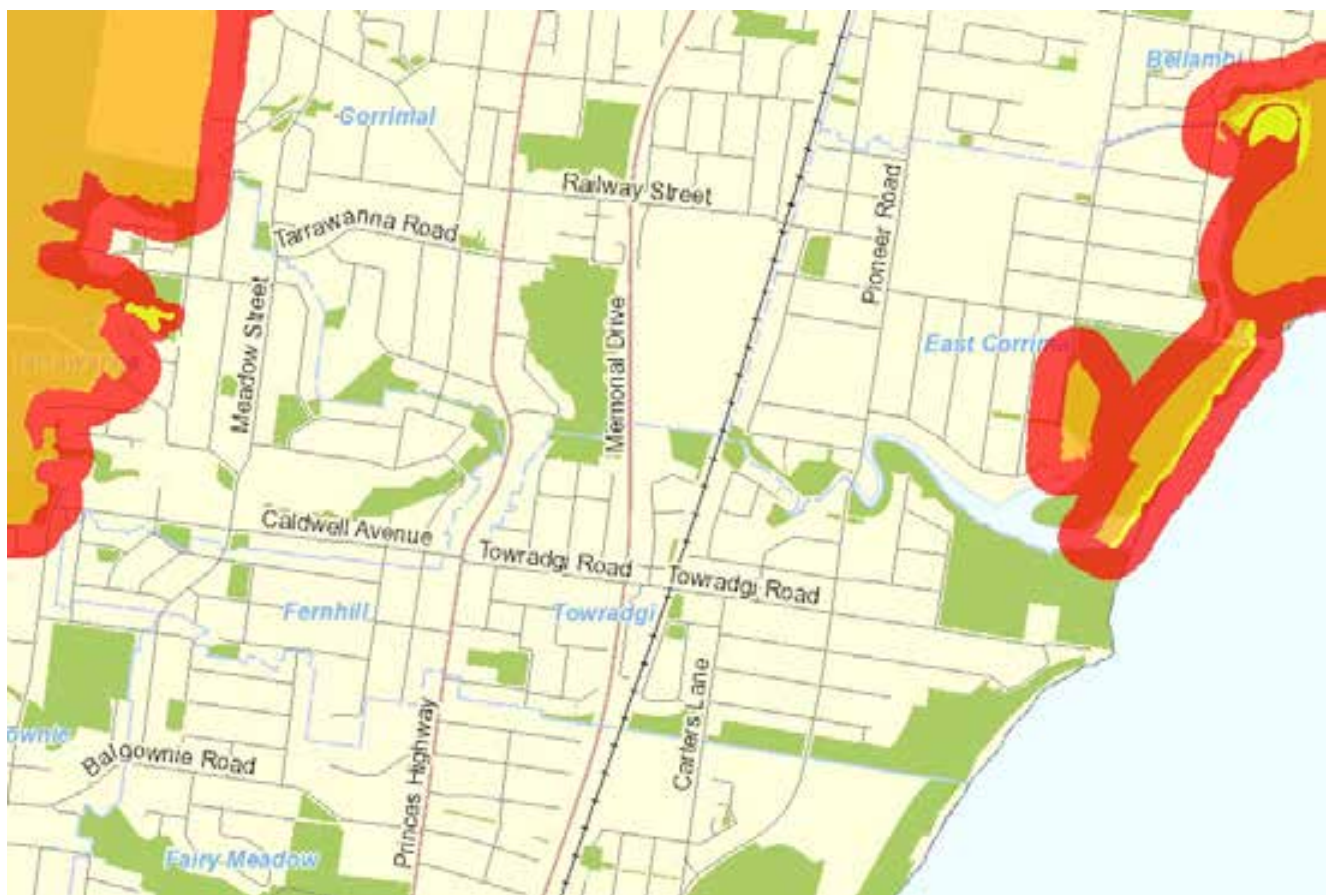
attractive as locations such as Unanderra, in the Illawarra, for these industrial uses due to land use conflict issues due to the proximity of residential dwellings.

- » Conversion to new industrial uses is not viable
 - > The costs in demolition, remediation, land development, subdivision, external works and open space embellishments (around \$25m) would exceed the end value of the industrial parcels (around \$20m to \$22m based on an end sale value of \$200 to \$250/m²). Therefore, this option is not financially viable and would not be realised in the current climate or in the foreseeable future.

3.9 Bushfire

The site is not identified as being Bushfire prone on the bushfire prone land map.

Figure 5 Bushfire Prone Land



Source: Wollongong Council

3.10 Ecological

An assessment of the environmental impacts of the proposal was undertaken by EcoLogical (refer to **Appendix G**). The assessment determined that the proposed rezoning would result in the following:

- » realignment and revegetation of the riparian corridor along the western extent of the study area
- » removal of 8.28 ha of Urban Native and Exotic vegetation
- » removal of 0.18 ha of Forest Red Gum – Thin-leaved Stringybark Grassy Woodland
- » removal of 1.12 ha of Acacia Scrub

» removal of 0.06 ha of *Pteropus poliocephalus* (Grey-headed Flying-fox) habitat

The proposed rezoning would retain 1.10 ha of Forest Red Gum – Thin-leaved Stringybark Grassy Woodland.

Figure 6 Rezoning area



Source: Ecological

3.10.1 Flora

Vegetation Communities

The vast majority of the study area accommodates areas of cleared land (including roads, infrastructure, water bodies) or weeds/exotics. Three native vegetation communities were identified within the study area:

- » Forest Red Gum Thin-leaved Stringybark Grassy Woodlands - occurred as two patches, comprising approximately 1.28ha.

The Forest Red Gum Thin-leaved Stringybark Grassy Woodland was identified as being in a highly modified and disturbed condition with high levels of exotic species, with planted non-local native species and modified landforms present. This vegetation community forms part of the 'Illawarra Lowlands Grassy Woodland in the Sydney Basin Bioregion' Endangered Ecological Community (EEC) listed under the NSW Biodiversity Conservation Act 2016 (BC Act). Neither of the patches present met the Commonwealth Environment Protection and Biodiversity Conservation Act (EPBC) requirements. The proposed rezoning would retain and restore the majority of this endangered ecological community.

- » Acacia Scrub – A total of 1.30ha occurred within the south of the study area in association with the tributary of Towradgi Creek which runs through the southern portion of the site.

This vegetation community occurred as a tall dense shrubland to low closed forest with the height and density of the canopy increasing in proximity to the tributary of Towradgi Creek. It is unclear whether these species were part of the original vegetation type within this area, or if they have colonised this area following the formation of a dense canopy of Acacias and exotic species.

- » Urban Native and Exotic - an area of approximately 8.28 ha was mapped within the study area

Within and surrounding the most disturbed parts of the study area, stands of vegetation were comprised of exclusively exotic species or consisted of rows of non-local native planted trees with a disturbed predominately exotic understorey.

All vegetation communities across the site were identified as having been subjected to moderate to high levels of disturbance including vegetation clearing/thinning of the canopy layer, infestation of exotic species and modification to the landform and soil profiles.

The only areas of vegetation, beyond the areas which have previously been disturbed, which will be impacted as a result of the proposal is 0.18ha of Forest Red Gum Thin-leaved Stringybark Grassy Woodland and 1.12ha of Acacia Scrub. All efforts have been made during the Master Planning process to ensure the majority of the native vegetation communities are not impacted. The removal of these small areas of vegetation would not impact on the health of the overall community.

Figure 7 Threatened Ecological Communities



Source: Ecological

Flora

One species listed under the EPBC Act and BC Act, *Eucalyptus scoparia* (Wallangarra White Gum), was identified within the study area. Within the study area, approximately eight individuals of this species were observed in a row adjacent to an internal road.

No other threatened flora species were recorded during the site inspections. Given the limited habitat available and its highly degraded condition, no other threatened flora species are considered likely to occur. A total of 108 species were identified during the field survey, including 53 exotic species, 48 endemic (locally native) species and five non-local planted natives.

Seven exotic flora species recorded within the study area are listed as requiring management consistent with the South East Regional Strategic Weed Management Plan (SERWMP) 2017.

3.10.2 Fauna

One threatened fauna species, *Pteropus poliocephalus* (Grey-headed Flying-fox) was identified in the study area during surveys. The Grey-headed Flying-fox is listed as vulnerable under the BC Act and EPBC Act and was identified as occupying the patch of Forest Red-gum Thin-leaved Stringy Bark Grassy Woodlands at the southern extent of the study area, adjacent to the dam and existing riparian corridor. The camp was estimated to support 150-250 individuals.

The proposed rezoning will retain the majority of the Grey-headed Flying Fox habitat (refer to **Figure 7**). However, the Ecological Assessment recommends that future assessments for development consider Grey-headed Flying-fox habitat. The proposed revegetation of the riparian corridor along the western extent of the study area would provide additional habitat for this species.

The remainder of the site is considered a low ecological constraint due to the presence of cleared lands, existing derelict infrastructure and Urban Native and Exotic Cover. The study area was not found to provide potential habitat for the Green and Golden Bell Frog (*Litoria aurea*) or Threatened microchiropteran bats.

3.11 Noise and vibration

Renzo Tonin & Associates conducted an environmental noise assessment of rail noise and vibration on the Corrimal site (refer to **Appendix O**) in order to assist in understanding any potential constraints to rezoning. The assessment was undertaken in accordance with NSW State Environment Planning Policy (Infrastructure) 2007 (ISEPP), the associated Development in Rail Corridors and Busy Roads – Interim Guideline, and other relevant vibration standards.

The findings of this study are:

- » Some facades of proposed residential buildings with exposure to road and rail noise will require acoustic facade treatments to meet the ISEPP criteria. The potentially affected building locations have been identified and indicative facade treatment recommendations provided.
- » Vibration impacts from the rail line have been found to be compliant with human comfort vibration criteria.
- » Ground borne rail noise during train pass-bys is marginally compliant, but should be confirmed during the design development of the residential buildings closest to the rail line.

In summary, noise and vibration issues do not present any constraint to rezoning the site for primarily residential uses. Any noise impacts can be suitably mitigated to achieve compliance through standard treatments to future residential development. These can be detailed at the development application stage.

3.12 Heritage

The Corrimal Coke Works site is not listed on any statutory or non-statutory heritage lists. However, the heritage significance of the Corrimal Coke Works site was assessed by Urbis (refer to **Appendix J**). Overall, the site holds heritage significance to the local area, through its contribution to the growth of the steel industry in the Illawarra Region, the connection of the site to local collieries, the sites previous electricity generation and the historic connections of a major employer for over a century to the local community.

The former coke works operated from 1911 to 2014 and is closely associated with the nearby Corrimal Colliery, which fed the coke ovens coal from 1912 until 1985. Over its operational life, the site underwent substantial changes and upgrades, creating a layered industrial history.

The former Corrimal Coke Works provide an industrial landscape within the context of a residential suburb. The site as a whole, demonstrates an industrial aesthetic, a number of elements within the site portray the industrial heritage in an aesthetic sense. These elements include:

- » C.1912 Brick Chimney – a handsome and historic landmark within the area.
- » Cokeoven batteries, in particular C1 Coke Oven Battery. It is noted that the aesthetic of these ovens has been reduced due to decay.

Later added industrial elements including steel stacks, coke oven hoods, uptakes and ductwork do have aesthetic qualities, however, do not reach the threshold required to attain Aesthetic Heritage Significance.

The assessment by Urbis graded four elements of the site as having high significance in relation to the overall heritage significance of the site. These elements are:

- » Brick Chimney Stack
- » C1 Coke Oven Battery (including load tracks)
- » C1 Coke Oven Ram Tracks
- » Remnant Power House – west elevation

These four items are proposed to be retained within a curtilage and classified as local heritage items under WLEP 2009.

3.12.1 Curtilage

The subject site is proposed to have a reduced heritage curtilage as outlined below. A reduced heritage curtilage is reserved for places where the heritage curtilage is defined as larger than the allotment. This classification is particularly relevant where views to and/or from a site are significant.

The proposed reduced curtilage takes into account the history of the site, the key industrial heritage and machinery involved in the processing of coke at Corrimal, the scale of the plant and balances this with the proposed adaptive re-use of the site. Historic elements within the recommended reduced heritage curtilage which will provide an understanding and interpretation of the above process include:

- » Remnant railway tracks (from earlier coal transportation to the site).
- » Retention, reconstruction of a portion of the c.1912 C1 Coke Oven Battery and C1 Coke Over Battery Ram Car tracks. The C1 Coke Oven Battery was constructed and operational in c.1912. Due to this early construction, it has a higher grade of heritage significance than the later added C2 Coke Oven Battery.
- » The c1912 brick chimney stack and section of the underground flue connecting to the brick chimney stack.
- » The post-1960 C1N steel stack.
- » An interpretation of the quenching towers.
- » The c1912 remnant wall of the former powerhouse.

The reduced heritage curtilage allows structures associated with the industrial heritage of the site to be retained and interpreted through a combination of key in-situ elements. The proposed reduced heritage curtilage provides for the industrial centre of the former coke works to be retained and interpreted. The proposed reduced heritage curtilage will retain, conserve and interpret the brick chimney stack, underground flue and alignment, the remnant powerhouse, associated rail tracks and alignments, quenching towers and coke ovens and incorporates the spatial length and width of the C1 Coke Battery, associated ram car tracks and C1N steel stack.

3.12.2 Heritage retention

While new built forms may be included in any development of the site, the retention of tall, landmark elements will ensure the former industrial site is conserved and interpreted as an industrial element. Any internal access routes, road layouts, built envelopes and landscape elements should provide views and vistas to the reduced heritage curtilage. This could be achieved through view lines and interpretation of the existing railway/roadway and transportation in coke production. The elements to be retained and/or interpreted are detailed below.

Remnant Railway Tracks

The site contains a number of remnant railway tracks, these include to the south of the Quench Towers and near the Coke Screen House. These tracks are remnants only and originally provided access to the South Coast mainline, prior to the use of trucks for transportation. The remnant tracks are in fair condition.

C1 Coke Oven Battery

The C1 Coke Oven Battery consists of two 'banks' of Coke Ovens. The original southern 'bank' was constructed in c.1912 and contains 40 ovens. An additional 10 ovens were built in 1930. These were sited in line with, but separate, from the original ovens, creating a northern 'bank'.

The rectangular coke oven battery is constructed of brick with the ovens themselves being arched. The ovens have flues running under them, which connect to stacks. Steel doors are located on either side of each of the ovens. The east door being used by the ram car (on rails) to push the coke out of the oven through the west doors and onto cars for quenching. Each oven has three holes above, for charging. Charging was done by electric charge cannisters, running on rails along the length of the coke oven battery. Each oven is identified by metal numbers located above the oven.

The C1 Coke Oven Battery has been assessed by structural engineers and requires substantial intervention. The visible oven structures have significant cracking, flagging and structural deformities. Established wild vegetation on the battery has grown into the masonry hastening the structural decay. Some elements of the structures are in fair to good condition, including metal identifier numbers and metal doors.

Brick Chimney Stack

The Brick Stack was constructed in c.1912. The stack is visible from outside the site due to its height and distinctive form. The stack lies on the eastern boundary and adjacent to the Corrimal Railway Station. The stack is constructed of brick and is fed by an underground flue which connects it to the C1 Coke Ovens. The brick stack is the remaining one of two, which were originally on the site (the other was located to the north of the C1 Coke Battery and was replaced with a steel stack in 1985).

The Brick Stack has been assessed by structural engineers who have identified that it requires remediation. Steel bracing bands are currently supporting the masonry structure.

Steel Stacks

The site contains three steel stacks. C1N replaced the former brick stack at the north end of the C1 Coke Battery in 1985. C2N and C2S were constructed in the 1960's, with C2S completed in 1962 along with the C2 Coke Battery.

All steel stacks show surface rusting and have graffiti. C2N stack has had additional bracing applied to its base. The stacks have been assessed by structural engineers and additional assessment is currently being undertaken.

C1N has been identified for retention, subject to further structural assessment, or alternatively interpretation as it provides a visual 'bookend' to C1 Coke Oven Battery and counterpoint to the brick chimney.

Remnant wall of former Power House

The remnant wall of the former Power House was constructed in c.1912. The remnant wall includes the original west elevation of the power house and an adjoining section of the north elevation. The remnant wall features three windows. A portion of the lower section of the wall has been reconstructed with modern fabric in the latter 20th century.

3.12.3 Aboriginal Cultural Heritage

Archaeological assessment by Kelleher Nightingale determined that one Aboriginal archaeological site comprising Aboriginal objects is located within the study area: low density artefact scatter FCCW AFT 1 (AHIMS 52-2-4505). FCCW AFT 1 is located on an elevated floodplain landform in association with Towradgi Creek, in the southern portion of the study area. The entirety of the site area would be impacted by the proposed redevelopment works. An Aboriginal Heritage Impact Permit (AHIP) issued by the Office of Environment and Heritage (OEH) under section 90(1) of the National Parks and Wildlife Act 1974 is therefore required prior to any activity.

Extensive previous modifications and disturbance associated with former industrial land use have diminished or negated the archaeological potential of the majority of the study area. The parts of the study area used for the former cokeworks operations exhibited significant levels of disturbance that would have removed/displaced Aboriginal archaeological objects.

Detailed Aboriginal community consultation did not identify specific Aboriginal cultural value at the one Aboriginal archaeological site identified within the study area, however, consultation has identified that the creeklines and adjacent areas have intangible Aboriginal cultural value associated with the use of these areas by past Aboriginal people for acquiring aquatic and terrestrial resources. The intangible Aboriginal cultural value of these areas can be conserved and enhanced through the remediation of disturbance from past land use and the establishment of a green-space corridor with native vegetation which would be more representative of the past resource gathering environment.

3.13 Services infrastructure

BG&E were engaged to update the previous Services Summary Report by ADW Johnson (**Appendix S**). It has been confirmed that all utility services are available to the site, noting the following:

- » Potable Water, Gas and Telecommunication networks require no augmentation to service the overall development;
- » No funding or construction of extensive feeder infrastructure will be required for electrical servicing, with only minor augmentations to the existing Endeavour Energy network envisaged; and
- » After further consultation with Sydney Water and a comprehensive MOUSE Modelling process, it has been confirmed that the wastewater network will be able to accommodate the full development yield.
- » The wastewater servicing strategy involves the provision of additional storage capacity by upsizing the existing trunk main through the site, which will be undertaken in conjunction with a realignment of that trunk main during the bulk earthworks phase.

3.13.1 Potable Water Servicing

Sydney Water Corporation (SWC) was consulted regarding servicing the proposed development with potable water and the associated impacts on existing SWC potable water infrastructure as a result of the increased potable water network demand imposed by the proposed development. SWC confirmed that the existing DN200

CICL potable water main fronting the development along Railway Street has sufficient capacity to accommodate the proposed development.

3.13.2 Wastewater servicing

Results and findings from modelling and associated consultation with SWC has determined the existing Sydney Water wastewater network will be able to accommodate the proposed Former Corrimal Cokeworks development upon implementation of planned network upgrades and adjustment and upsize of the on-site wastewater trunk main.

3.13.3 Electrical servicing

Review of the existing electrical network shows that there are two 11kV feeders (CR1228 and CR1280) located opposite the development site along Railway Street, however both of these feeders have been estimated to have a total available capacity of approximately only 0.5MVA, which is not sufficient to accommodate the development site. However, Endeavor Energy (EE) noted that the overall development can potentially be serviced by Feeder RV1206 from Russell Vale Zone Substation (ZS) which is deemed to have the required available capacity to accommodate the mature load of the development.

EE confirmed that the development would not expect to fund or construct extensive feeder works in order to utilize the available capacity from Feeder V1206 and required augmentations would be limited to network switching and minor augmentation of the existing HV distribution network by linking existing HV networks in various locations. It is envisaged that HV linkage points will be Underground to Overhead (UGOH) terminations on feeder CR1280, with one in both Railway Street and High Street. Details surrounding HV linkages and network configuration will be confirmed when a firm application for load is submitted to EE.

3.13.4 Telecommunications servicing

Design It Telco Pty Ltd was consulted about the capacity of existing NBN and Telstra networks. It was confirmed that:

- » Current NBN and Telstra infrastructure could handle the overall development
- » As the development is over 100 lots it will automatically qualify for fibre to the premises (FTTP); and
- » NBN Co will supply a backhaul cable to cater for future lots.

It has been confirmed that the future development does not require any upgrades to telecommunications networks.

3.13.5 Gas servicing

A Technical Review Request was submitted to Jemena Limited, to assess the impacts of the proposed development and to confirm the gas network has adequate capacity to service the overall development. It was confirmed that Jemena has suitable gas mains located on Railway Street within the vicinity of this proposal which currently have adequate capacity to service the overall development at this time.

3.14 Traffic and transport

Existing transport infrastructure in the study area includes:

- » Memorial Drive (state road) which is a key strategic route through Corrimal carrying high volumes of north-south through traffic. Memorial Drive connects Corrimal to the Princes Highway at Bulli and the M1 Princes Motorway at Gwynneville via North Wollongong;

- » Railway Street between Memorial Drive and Pioneer Road, with intersections at Cross Street, Harbinger Street, High Street, Ruddock Street, Duff Parade and Park Road (all local roads);
- » signalised intersections at Railway Street/Memorial Drive and Railway Street/Pioneer Road;
- » a level crossing on Railway Street with boom gates and flashing lights across the Illawarra Railway Line, which carries both passenger trains (all stop and express services) and freight trains;
- » Corrimal Railway Station (located adjacent to the site), serviced by the South Coast Line between Kiama, Port Kembla and Sydney via Wollongong. Passenger trains generally run every 15-30 minutes during peak hour directly servicing Wollongong with 'all stop' services towards Sydney CBD; and
- » Bus stops along Railway Street in both directions, serviced by:
 - > Route 4: Bulli to Wollongong
 - > Route 92: Bulli to Wollongong
 - > Route 93: Bulli to Wollongong University
- » The site provides direct access to the existing pathway network on Railway Street. There are very few formal bikeways located in close proximity to the development site.

Bitzios Consulting were engaged to provide a supplementary Traffic Impact Assessment (refer to **Appendix T**). Key conclusions drawn from the assessment of the traffic and transport impacts and needs associated with the redevelopment of the site into a mixed-use development includes:

- » The most appropriate location for development access is via Railway Street, approximately 60m east of Harbinger Street. A channelised T intersection with a right turn-in pocket and separate left turn-out and right turn-out lanes is sufficient to provide a safe and efficient access although the access could be signalised to cater for controlled pedestrian crossing movements as well.
- » A left in/out access off Memorial Drive is inconsistent with the function of Memorial Drive and may introduce a safety risk in the high speed, high volume environment in closed proximity to the Towradgi Road intersection
- » The development will introduce 373 vph into the network in the morning peak and 456 vph into the network in the evening peak with almost three-quarters of this traffic orientated towards the Memorial Drive intersection and the remainder orientated towards the Pioneer Road intersection with Railway Street. This increase in vehicles per hour can be catered for through the introduction of a channelised 'T' intersection at the sole site access intersection on Railway Street, approximately 60m east of Harbinger Street.
- » The development's traffic impacts of significance are isolated to the intersection of Memorial Drive / Railway Street
- » The additional traffic introduced into Railway Street by the development equates to one vehicle every 10 seconds (two-way) in the peak hour, which is insignificant on a distributor road such as Railway Street.
- » Railway Street will experience an approximate 17% increase in daily traffic. Its forecast volume of approximately 8,700 vehicles per day at the level crossing is well within the capacity of a single lane each way distributor road.
- » The development's impacts on the Railway Street level crossing due to additional traffic and pedestrian movement are insignificant. State Rail's ALCAM model calculated a probability of one fatality every 104 years, which is the same risk as without the development. No works are required at the level crossing by the development on this basis.
- » The preferred route is along the southern side of Railway Street, which is consistent with the Wollongong City Council Bike Plan 2014-2018. The applicant has proposed to construct a shared path from Cross Street to the commuter car parking entrance as part of a Voluntary Planning Agreement (VPA) with Council.
- » Site specific parking rates are proposed taking into consideration the site is located adjacent to the Corrimal railway station and the localised catchment for retail, food and drink and restaurant uses.

- » The proposed site configuration provides a direct line of site from the primary access street to Corrimal Station for traffic, bus (potentially), Kiss and Ride (KnR), pedestrians and cyclists. It also allows for KnR traffic circulation and the potential for direct connection to the roadway within State Rail land immediately west of the rail line should State Rail see the benefits in such a connection. No Park and Ride (PnR) facilities are being allowed for and no on street parking management measures would be necessary unless Corrimal Station was serviced by express trains to Sydney in the future.

3.15 Economic

3.15.1 Existing centres and supermarkets

Within the main trade area, in the Corrimal Town Centre (Secondary West), there is a Woolworths and Coles supermarket. A 1,500m² ALDI supermarket is also currently under construction. There is also a small shopping village in East Corrimal with a Food Works (approximately 300m²)

Fairy Meadow represents the key competition from south of the trade area. From the north, retail competition comes from Woonona, Bulli and Thirroul town centres. The current number of supermarkets and their floor space in surrounding town centres, is shown in **Table 6** below.

Table 6 Supermarket provision

Town Centre	Supermarkets	Estimated supermarket Gross Lettable Area Retail (GLAR)
Corrimal	3	7,600m ²
Fairy Meadow	3	7,800m ²
Woonona	1	1,000m ²
Bulli	1	3,200m ²
Thirroul	2	2,200m ²

The Corrimal town centre is the largest and closest retail offer to the subject site, located around 350m to the west. Key retail includes:

- » Lederer Shopping Centre Corrimal is a single-level centre with undercroft and at-grade car parking. The centre has multiples pedestrian access points from Railway Street and Princes Highway. The centre comprises:
 - > Woolworths (~3,500m²)
 - > Specialty retail (~3,100m²) comprising a mix of convenience retail, retail services, and fast casual dining and cafes
 - > Dan Murphy's mini major
- » Corrimal Park Mall is a dated single-level centre at the northern end of the Corrimal town centre, comprising:
 - > Coles supermarket (~2,600m²)
 - > Nine specialty retail tenants (~1,200m²), orientated towards convenience retail
- » Strip retail along Princes Hwy and Railway St:
 - > Around 60 retail specialty tenants (estimated 5,800m²), including strong provision of hair and beauty tenants (14), takeaway food (6), cafes (6), and massage (5)
 - > Some 36 non-retail tenants, including banks (5), employment and financial services (10), and real estate agents (4)

- > An ALDI supermarket (~1,500m²) is currently under construction in a mixed-use development on Russel St, expected to be complete in late 2019.

3.15.2 Retail demand

Urbis has undertaken an analysis (refer to **Appendix R**) based on an assessment of key opportunities relating to the subject site and its competitive context, which demonstrate:

- » Retail at the subject site should target on-site residents first and foremost. Residents need on-site amenities to create a focal point for activity.
- » A small-scale independent local supermarket could serve a convenience retail role for on-site residents.
- » The critical mass of new residents on site could support a small convenience-based retail offer, potentially including the likes of a pharmacy, newsagent, and beautician, despite good provision of these in Corrimal town centre.
- » There is a noticeable gap in the local market for higher-quality sit-down dining venues and contemporary cafes. Such an offer at the subject site could draw weekend visitors from beyond the subject site.
- » Retail should be integrated with a strong public realm to establish the precinct as a small neighbourhood centre; a vital selling point for future residents of the development.
- » The new retail can leverage its location next to the train station and the historical reputation of the Corrimal Coke Works, well known throughout the region, to serve commuters and visitors to the Wollongong area.

3.15.3 Conclusion

- » A small supermarket on site could be supported by some cafes and restaurants and a limited convenience offer.
- » The indicative composition that could support a small format supermarket on site, based on Urbis' understanding of key retail and design principles, includes:
 - > 2-3 cafes/restaurants, that also serve a takeaway role, and a specialty food store
 - > A newsagency or small pharmacy
 - > Personal services such as a hair salon, beautician, massage etc.
- » The above mix could occupy around 800m², taking total floorspace to around 1,550m². To provide some flexibility, Legacy and ICC are considering up to 2,000m².
- » Total retail sales are estimated at \$10.1 million in 2024, growing to \$12.1 million in 2026. These sales reflect an overall retail sales performance of \$6,500 per m² in 2024 and \$7,800 per m² in 2026.
- » Retail specialties are estimated to trade at around \$7,344 per m² by 2026, assuming an optimised tenancy mix

This aggregate sales potential could support a store is in the range of 500 –1,000m² (assumed 750m²), this translates to an average trading level of around \$6,900 per m² initially, stabilising at around \$8,300 per m² in 2026.

The proposed retail on site would generate an estimated 79 ongoing jobs during the operational period. Up to an additional 16 indirect jobs in supporting and supplying industries could also be provided, indicating that the development could support up to 95 total jobs in the region.

3.16 Population

In 2016, the Illawarra-Shoalhaven region had a population of 404,650. Wollongong LGA had the largest population, accounting for 52% of the region's total, followed by Shoalhaven LGA (25%), Shellharbour LGA

(18%) and Kiama LGA (5%). Between 2001 and 2011 the Illawarra-Shoalhaven region grew at compound annual growth rate of 0.8%, 0.4% slower than Greater Sydney's growth of 1.2%, but faster than growth in regional NSW of 0.7%.

Growth in the Illawarra-Shoalhaven region between 2016 and 2036 will lift its population by 67,000, to 471,700 people in 2036. Within the Illawarra-Shoalhaven region, Shellharbour has experienced the fastest growth, in part reflecting supply with the presence of more greenfield options (vs Wollongong). The projections have this trend continuing, with Shellharbour growing at 1.3%, compared with 0.8% for Wollongong.

3.17 Housing

The projected growth in population will translate to demand for a net addition to the dwelling stock of 45,000 dwellings or 2,250p.a in the period 2016 – 2036. Historically, detached housing has been the dominant form of housing in the Illawarra-Shoalhaven region, accounting for 76.4% of the housing stock in 2016. More recently, particularly in Wollongong but also in Shellharbour, there has been a higher share of growth in dwellings accommodated by in-fill developments, and by medium- and high-density housing types. This in part reflects a long-term trend towards higher density.

The D&PE's projections for 2015/16-2019/20 and 2020/21-2024/25 assume that detached housing will account for 50% of growth in the housing stock, with medium and high-density housing accounting for the other 50%. The DP&E notes that 60-70% of demand might be met from greenfield developments and 30-40% from in-fill. In the past ten years to 2015, there has been 7,634 dwelling completions from in-fill (in the Illawarra-Shoalhaven region).

In 2016, the most common households within the Illawarra-Shoalhaven region were couples with children which accounted for 28.7% of all households in 2016. This was followed by couple only households (26.7%) and lone person households (24.1%). Heading into the future the fastest growth is expected to be in couple only households (1.4%) and lone person households (1.4%) which include older households.

In order to cater to the region's changing household structures, diversification of dwelling structures is required. The downsizer market and single young professionals are likely to want smaller and more compact housing. While there has been growth in medium and higher density development, particularly in Wollongong CBD, provision of these housing types is still relatively under-serviced in relation to potential demand. Housing diversity will also be important in maintaining housing affordability through the provision of a range of more compact dwelling types.

3.18 Social Infrastructure

3.18.1 Recreational/Community Infrastructure

Recreational/Community Infrastructure in the vicinity of the site includes:

- » Corrimal Memorial Swimming Pool (250m west from the western boundary)
- » Corrimal District Library & Community Centre (250m from the western boundary of the site)
- » Robert Ziems Park featuring multiple playing fields (66m west of the site)
- » Towradgi sporting fields, tennis courts and bowling greens at Moray Road, Towradgi (850m south-east of the site)

There is an opportunity to link all of these community recreational resources, the coast and the site via a green link aligning with the Towradgi Creek corridor. This would also effectively link with the Grand Pacific Walk currently being developed in stages by the Wollongong City Council, that will ultimately provide a continuous shared pathway from the Royal National Park to Lake Illawarra.

In addition, significant walking and cycling facilities are proposed as part of the concept plan for the site to promote an active transport and public-transit lifestyle.

3.18.2 Educational Infrastructure

Educational institutions surrounding the sites include:

- » Corrimal High School (375m to the east)
- » Corrimal East Public School (350m to the east)
- » Corrimal Public School (750m to the north-west)
- » Bellambi Public School (1,000m to the north-east)
- » Wollongong High School of the Performing Arts & Keira High School (3.5km to the south-west)
- » TAFE NSW Wollongong (3.5km to the south-west)
- » University of Wollongong (4km to the south-west)
- » Saint Columbkille's School (822m to the north-west)
- » Towradgi Public School (1.2km to the south)
- » Specialist schools such as the Autism Association of NSW - South Coast School (850m to the north-west).

An assessment of likely capacity for schools has been performed by reviewing information on the My Schools website. This is provided in **Table 7** below.

Childcare facilities are available surrounding the site, but there is also an excellent opportunity to provide childcare facilities on site, servicing both residents and commuters in particular.

Table 7 Maximum enrolment and current enrolments for relevant schools near the subject site

School	Enrolments		Estimated spare capacity
	Maximum	2016	
Corrimal High School (375m to the east)	496 (2008)	345	151+
Corrimal East Public School (350m to the east)	295 (2008)	202	93+
Corrimal Public School (750m to the north-west)	184 (2016)	184	Additional capacity unknown
Bellambi Public School (1,000m to the north-east)	256 (2008)	164	92+
Wollongong High School of the Performing Arts (3.5km to the south-west)	Additional capacity unknown		
Keira High School (3.5km to the south-west)	949 (2015)	938	9
Saint Columbkille's School (822m to the north-west)	422 (2016)	422	Additional capacity unknown
Towradgi Public School (1.2km to the south)	164 (2014)	160	4+
Fairy Meadow Public School (3.5km south)	421 (2008)	374	47+

School	Enrolments		Estimated spare capacity
Woonona High School (3.5km north)	787 (2008)	599	188+
Woonona Public School (3.5km north)	342	512	Additional capacity unknown
Woonona East Public School (3.5km north)	310 (2008)	222	88+

Source: www.myschool.edu.au

3.18.3 Health Infrastructure

A brief assessment of health infrastructure would indicate that the subject site is well serviced. In Wollongong, (approximately 5.5km from the subject site) this includes:

- » Wollongong Hospital which is currently undergoing \$100 million worth of capital works enhancements which include the construction of the Illawarra Elective Surgical Services Centre
- » South Coast Private Mental Health Hospital
- » Wollongong Day Surgery
- » Wollongong Private Hospital
- » Numerous specialist centres.

In the local area surrounding the subject site, numerous primary care facilities are available with at least four medical centres and a community health centre.

Significant community, health, educational and recreational resources are available surrounding the site.

3.19 Suitability of the site

It is estimated that development of the site for residential land uses has the opportunity to provide for a diversity of housing in order to cater for the changing demographics of the Wollongong LGA. Importantly, the key factors that make the site suitable for residential development include:

- » Not viable for industrial use
 - > The coke works ceased operation in 2014
 - > A financial feasibility assessment has found that it would not be viable to redevelop the site for industrial purposes (considering the cost of redevelopment versus return)
 - > Significant industrial land resources are available in the Wollongong LGA that provide better access and less land use conflict than the subject site.
- » Planning
 - > The 18.18ha site is able to offer a mix of residential and recreational resources where site constraints have been investigated and effective mitigation measures or other strategies have been developed
 - > 12.5 hectares of flat, readily developable land adjacent to Corrimal train station offering the opportunity for transit-oriented development
 - > Logical extension of the R3 Medium Density Residential zoning to the north, north-east and east of the site

- > The capacity to deliver a diverse range of housing within an R3 Medium Density Residential zone, from single dwellings through to residential flat buildings, delivering housing typologies that are in short supply in the Illawarra region.
- > An R3 Medium Density Residential rezoning could also support small-scale neighbourhood and commuter services to deliver convenience for residents and the broader community
- » Connectivity
 - > Rail and major arterial road access connect the site to the Wollongong CBD (5.5kms) and Sydney CBD (63.5km)
 - > Ideally located within 350m of the Corrimal town centre
 - > 8-minute bus ride to Wollongong University and the broader innovation employment zone, with a stop on Railway Street adjacent to the site
 - > 500m or less from primary and secondary schools with over 10 schools within an 8km radius of the site.
- » Amenity
 - > 500m to recreational facilities including sporting fields, Corrimal pool, library and community centre
 - > 1.2 km to patrolled beaches
 - > Proposed green link, cycle way/pathway to Corrimal Beach and adjacent sporting fields
 - > Superb views to the Illawarra Escarpment
 - > Potential views to the Wollongong coastline.

4 Statutory Planning Context

The rezoning has been prepared having regard to the existing planning framework in context of the WLEP 2009 and other applicable Environmental Planning Instruments.

4.1 Wollongong Local Environmental Plan 2009

The aims of WLEP 2009, which guide the preparation of the PP for the site at Corrimal are as follows:

- a) to provide a framework for land use management,*
- b) to encourage economic and business development to increase employment opportunities,*
- c) to encourage a range of housing choices consistent with the capacity of the land,*
- d) to improve the quality of life and the social well-being and amenity of residents, business operators, workers and visitors,*
- e) to conserve and enhance remnant terrestrial, aquatic and riparian habitats, native vegetation and fauna species,*
- f) to conserve and enhance heritage,*
- g) to ensure that development is consistent with the constraints of the land and can be appropriately serviced by infrastructure,*
- h) to ensure that significant landscapes are conserved, including the Illawarra Escarpment, Lake Illawarra, the drinking water catchment and the coastline.*

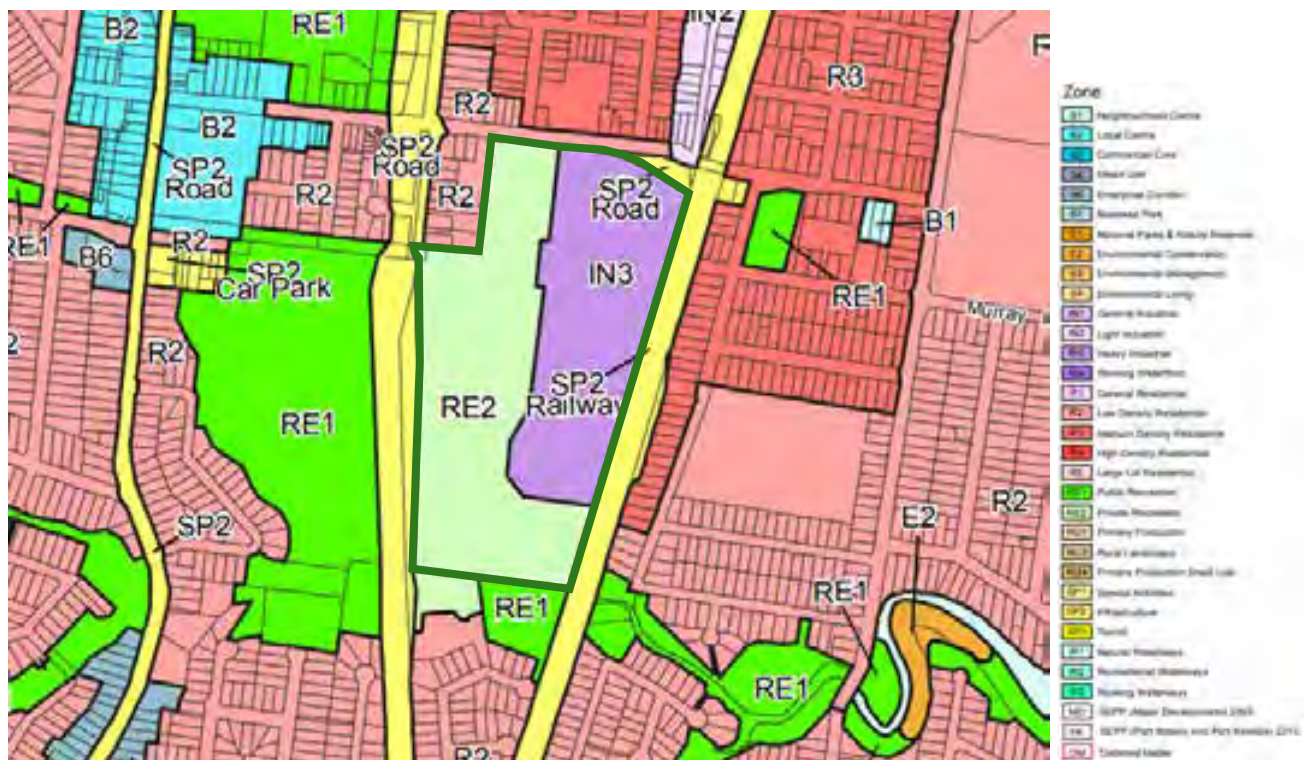
The PP gives effect to these objectives, as outlined in **Part 1**.

4.1.1 Zoning

Under the WLEP 2009 the subject site is zoned IN3 Heavily Industrial and RE2 Private Recreation. The objectives of these zones are as follows:

- » **IN3:**
 - > *To provide suitable areas for those industries that need to be separated from other land uses.*
 - > *To encourage employment opportunities.*
 - > *To minimise any adverse effect of heavy industry on other land uses.*
 - > *To support and protect industrial land for industrial uses.*
 - > *To facilitate the ongoing sustainability of steel making and steel product manufacturing that will contribute to the economic and employment growth of Wollongong.*
- » **RE2:**
 - > *To enable land to be used for private open space or recreational purposes.*
 - > *To provide a range of recreational settings and activities and compatible land uses.*
 - > *To protect and enhance the natural environment for recreational purposes.*

The zoning of the site is identified in **Figure 8** below.

Figure 8 Current zoning map – WLEP 2009

Source: WLEP 2009 Land Zoning Map LZN_024

4.1.2 Minimum lot size

Clause 4.1 of WLEP 2009 sets a Minimum Subdivision Lot Size, as indicated in the associated MLS map. Half of the site has an MLS of 1.99ha, while the other half of the site is not subject to a MLS, as shown in **Figure 9** below.

Figure 9 Current MLS map – WLEP 2009

Source: WLEP 2009 Minimum Lot Size Map LSZ_024

4.1.3 Height of Buildings

Clause 4.3 of the WLEP 2009 sets a maximum height limit for buildings, with heights shown on an associated Height of Buildings Map. One half of the site is not subject to a height control, while the other half has a 9m height control, as shown in **Figure 10** below.

Figure 10 Current height of buildings map – WLEP 2009

Source: WLEP 2009 Height of Buildings Map HOB_024

4.1.4 Floor Space Ratio

Clause 4.4 of the WLEP 2009 sets a maximum Floor Space Ratio (FSR), with FSRs shown on an associated FSR map. The subject site has no FSR control.

4.1.5 Land Reservation Acquisition

Clause 5.1A of WLEP 2009 aims to limit development on certain land intended to be acquired for a public purpose. The site contains Lot 126 DP 598190, which is intended to be utilised for the expansion of Railway Street, as shown in **Figure 11** below.

As per Clause 5.1 if there is an owner-initiated acquisition process the following applies:

5.1 Relevant acquisition authority

1. *The objective of this clause is to identify, for the purposes of section 27 of the Act, the authority of the State that will be the relevant authority to acquire land reserved for certain public purposes if the land is required to be acquired under Division 3 of Part 2 of the Land Acquisition (Just Terms Compensation) Act 1991 (the owner-initiated acquisition provisions).*

Note.

If the landholder will suffer hardship if there is any delay in the land being acquired by the relevant authority, section 23 of the Land Acquisition (Just Terms Compensation) Act 1991 requires the authority to acquire the land.

2. *The authority of the State that will be the relevant authority to acquire land, if the land is required to be acquired under the owner-initiated acquisition provisions, is the authority of the State specified below in relation to the land shown on the Land Reservation Acquisition Map (or, if an authority of the State is not specified in relation to land required to be so acquired, the authority designated or determined under those provisions).*

Type of land shown on Map

Authority of the State

Zone SP2 Infrastructure and marked "Local road"

Council

3. *Development on land acquired by an authority of the State under the owner-initiated acquisition provisions may, before it is used for the purpose for which it is reserved, be carried out, with development consent, for any purpose.*

Figure 11 Land Reservation Acquisition



Source: WLEP 2009 Land Reservation Acquisition Map LRA_024B

4.1.6 Heritage

Clause 5.10 of the WLEP 2009 sets controls for heritage items, heritage conservation areas, archaeological sites and Aboriginal objects or places. There are no heritage listed items of Commonwealth, State or local heritage significance within the site nor is the site located within a heritage conservation area.

4.1.7 Bushfire Hazard Reduction

Clause 5.11 of the WLEP 2009 relates to bushfire hazard reduction. The site is not identified as being bushfire prone.

4.1.8 Natural Resource Sensitivity

Clause 7.2 of the WLEP 2009 identifies controls for areas of natural resources sensitivity – biodiversity. As shown in **Figure 12** below, the site contains areas of natural resource sensitivity.

Extensive investigation into the biodiversity values of the site have been undertaken to ascertain the biodiversity values of the site. The areas of greatest significance are contrary to those provided on the LEP mapping.

The concept planning process has ensured the development is designed, sited and managed to avoid potential adverse environmental impacts and incorporates measures to minimise the environmental impact.

Figure 12 Natural Resource Sensitivity – Biodiversity Map – WLEP 2009



Source: WLEP 2009 Natural Resource sensitivity – biodiversity Map NRB_010

4.1.9 Flood Planning

Clause 7.3 of WLEP 2009 is designed to ensure that:

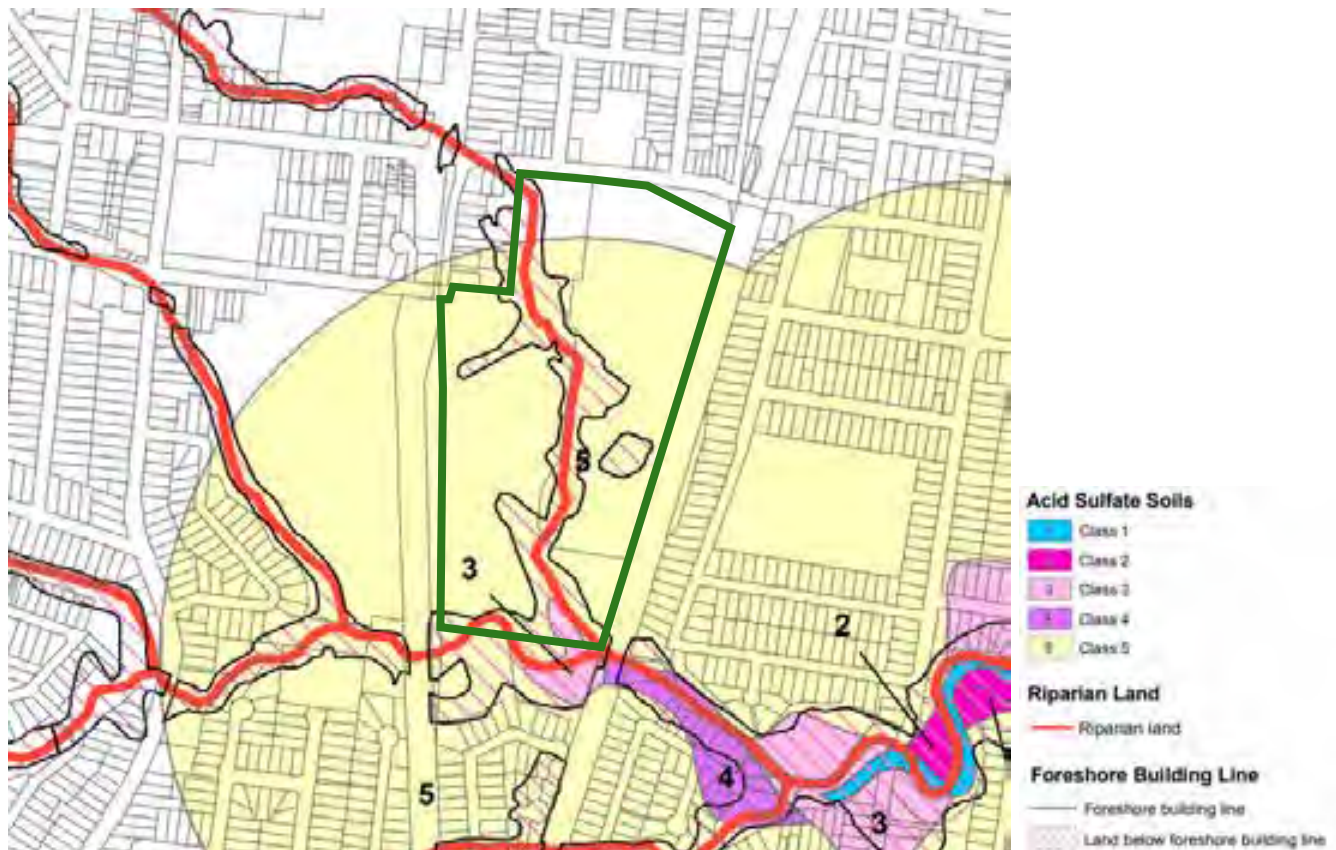
- a. to maintain the existing flood regime and flow conveyance capacity,
- b. to enable evacuation from land to which this clause applies,
- c. to avoid significant adverse impacts on flood behaviour,
- d. to avoid significant effects on the environment that would cause avoidable erosion, siltation, destruction of riparian vegetation or a reduction in the stability of river banks or watercourses,
- e. to limit uses to those compatible with flow conveyance function and flood hazard.

The subject site is identified on the accompanying Flood Planning Mapping as part of the Wollongong Development Control Plan 2009 Part E13 Towradgi Creek Flood Plain as shown opposite.

4.1.10 Acid Sulfate Soils

Clause 7.3 sets out controls for flood planning, Clause 7.4 identifies controls for Riparian Lands, while clauses 7.5 and 7.7 identify controls for Acid Sulfate Soils (ASS) and the Foreshore Building Line. The site contains Class 5 ASS and areas of riparian land and land below the foreshore building line, as shown in **Figure 13** below.

Figure 13 Acid Sulfate Soils, Riparian Land, Foreshore Building Line and Flood Planning Map – WLEP 2009



Source: WLEP 2009 Acid Sulfate Soils, Riparian Land and Foreshore Building line Map CL1_024

Clause 7.3 states that development consent must not be granted for land to which this clause applies unless the consent authority is satisfied in relation to all the following matters:

- all habitable floor levels of the development will be above the flood planning level,*
- the development will not adversely affect flood behaviour resulting in detrimental increases in the potential flood affectation of other development or properties,*
- the development will not significantly alter flow distributions and velocities to the detriment of other properties or the environment of the floodplain,*
- the development will not affect evacuation from the land,*
- the development will not significantly detrimentally affect the floodplain environment or cause avoidable erosion, siltation, destruction of riparian vegetation or a reduction in the stability of river banks or watercourses,*
- the development will not result in unsustainable social and economic costs to the community as a consequence of flooding,*
- if located in a floodway area—the development will not be incompatible with the flow conveyance function of, or increase a flood hazard in, the floodway area.*

Clause 7.4 aims to ensure that development does not adversely impact upon riparian lands. However, it should be noted that the existing riparian corridor is significantly degraded as a result of the previous industrial use of the land.

Clause 7.5 aims to ensure that development does not disturb, expose or drain ASS and cause environmental damage. The majority of the site is class 5 ASS. Under Clause 7.5 Class 5 ASS permits "*works within 500 metres of adjacent Class 1, 2, 3 or 4 land that is below 5 metres Australian Height Datum and by which the watertable is likely to be lowered below 1 metre Australian Height Datum on adjacent Class 1, 2, 3 or 4 land.*" The ASS map also indicates a small section of the south-eastern corner of the site which is Class 3 ASS, however, this area of the site is not proposed to be developed.

Clause 7.7. provides foreshore building line controls. The clause aims to ensure that development in the foreshore area will not impact on natural foreshore processes or affect the significance and amenity of the area. However, as previously stated the riparian land and adjacent foreshore area is extremely degraded.

4.2 Wollongong Development Control Plan

The site is also subject to the Wollongong Development Control Plan (WDCP) 2009. The WDCP 2009 applies to the entire Wollongong LGA and includes general development guidelines. The main objective of the WDCP 2009 is to assist in the realisation of the aims of the WLEP 2009.

Details of the relevant provisions of the WDCP 2009 have not been undertaken as part of this PP due to the fact a site specific DCP has been prepared for the Corrimal site and is submitted with this PP (refer to **Appendix A**).

5 Proposed rezoning concept

5.1 Planning Proposal Description

The aim of the PP is to rezone the previous Corrimal Coke Works site to integrate a diversity of housing with a focus on delivering a public plaza adjoining Corrimal train station whilst retaining the sites industrial heritage.

5.2 Urban Design Principles

In order to support the proposed rezoning, a Concept Master Plan has been developed to identify future potential land uses. The key design principles of the Concept Master Plan for the rezoning area include:

- » Deliver a public open space network that forms the focus and identity for the precinct, providing a range of recreational opportunities.
- » Provide innovation in the type of built form and public realms to ensure the character of the site reflects a cohesive yet diverse group of design elements and different character precincts.
- » Integrate the brick chimney, coke ovens and the powerhouse into the overall development of the public plaza, in order to celebrate the history of the site
- » Design a safe and functional road network with increased walkability to Corrimal station, as this will be a direct link to the heart of the community
- » Ensure traffic control and circulation within the site to Railway Street to connect with the surrounding Corrimal community and provide adequate street parking.
- » Deliver a site that is highly permeable to pedestrians and cyclists, linking the precinct to surrounding amenities, services and facilities, public transport, recreational opportunities and beach and broader regional pedestrian/cycle path network
- » Create a residential community with housing diversity, neighbourhood hub and commuter services near the train station for added convenience but at a scale that does not detract from the efficacy of the Corrimal town centre
- » Conserve, manage and improve native vegetation and biodiversity located to the south and north-west of the site, as well as the new riparian corridors
- » Allow for earthworks to improve and manage the drainage across the site in order to reform and rehabilitate a riparian corridor to improve water management and ultimately enhance the biodiversity values on the site
- » Deliver serviced development with quality infrastructure services and diverse architectural elements
- » Create view lines to the creek and linkages to the heritage significant core

5.3 Place principles and values

The Master Plan for Corrimal has also been based on the following place principles:

- » Place Principle 1 Made for Friendship: We help people to meet, share and connect, building on and contributing to Corrimal's already genuinely friendly and supportive community. We're creating a safe and supportive place where people grow together, look out for each other and share special moments and events.
- » Place Principle 2 Designed for difference: We're building a community of many different ages, shapes and sizes. We champion difference and are designed to attract people at different stages of life. Our place is

made up of a wide variety of landscapes, precincts, features, housing types and experiences, creating a vibrant and distinctive destination.

- » Place Principle 3 Bringing more to life: We invest in making people and places the best they can be, breathing new life into heritage and green space and helping people reach their potential. We make everyday life easier, so that people have more time and energy to enjoy what's important.
- » Place Principle 4 United through stories: We cherish our unique story. From our history to our unique ecology, we're a place like no other. Together we will celebrate the stories of the past and build new stories, forging a strong sense of character and identity.

5.4 Master Plan and rezoning area

The Master Plan is a progression of the 2017 Concept Plan which established the potential for the site to be rezoned from predominantly industrial uses to residential zoning. The 2017 Concept Plan established key design drivers including:

- » A Strategy to retain some former industrial elements of the Site including the 1912 Brick Chimney Stack.
- » The identification of ecological communities in the north and south of the Site.
- » The opportunity to position local services around Corrimal Station.
- » The opportunity to access the Site from Railway Street in the north.
- » The realignment of Corrimal Creek to the west of the Site to establish an enhanced green corridor, managing flooding and consolidate the developable area

Taking into account the above factors, it is proposed to rezone the site from IN3 Heavy Industrial and RE2 Private Recreation, under WLEP 2009, to R3 Medium Density Residential and RE1 Public Recreation. A small portion (0.25ha) of the subject site, currently zoned SP2 Infrastructure - Road, is also proposed to be retained. Appropriate controls relating to Floor Space Ratio (FSR), minimum lot size, height, biodiversity and Acid Sulfate Soils are also proposed.

The rezoning of the site would accommodate approximately 700 – 750 dwellings, providing for more diversity of housing through the provision of a mix of residential flat building development, attached and semi-detached housing. The proposed civic place will provide for small scale retail space in close proximity to Corrimal Station. The site will be connected to the wider Corrimal area through cycling and pedestrian links.

The riparian corridor, which is currently severely degraded, is proposed to be realigned to reduce flood risks, offer a mix of passive and active recreational uses and handle water quality management. The riparian corridor will link existing public open space at the south of the site. Two ecological communities have been identified on the site, which are proposed to be preserved.

The indicative concept Master Plan for the suite is shown in **Figure 14**.

Figure 14 Proposed Master Plan



Source: DKO

Key features of the Master Plan include:

- » An expanded plaza adjacent to Corrimal Station.

- » Increased retention of former Coke Works Structures.
- » Refinement of the realigned North Corrimal Creek corridor to provide opportunities for active and passive recreation.
- » Measures to enhance stormwater quality.
- » A refined street network that facilitates bus access to Corrimal Station.
- » An expanded area for local services around Corrimal station.
- » A new local 'park in the heart' of the Site.
- » The identification of a number of character precincts within the Site that respond to local conditions and the project vision.
- » Further development of the built form strategy to allow for a diverse mix of innovative housing.
- » An identification of locations for seniors housing and affordable housing.
- » An extensive network of active transport and pedestrian links to key destinations within and around the Site.
- » Updated staging and implementation planning.

5.5 Proposed Land Use Strategy

5.5.1 Housing Delivery

The new community will support approximately 700 – 750 new dwellings. The site offers ideal development potential for infill housing, adjacent to Corrimal train station and within walking distance of Corrimal town centre. The proposed R3 Medium Density Residential zoning permits the following housing typologies under WLEP 2009:

Attached dwellings; Dual occupancies; Dwelling houses; Multi dwelling housing; Residential flat buildings; Shop top housing; Semi-detached dwellings; Seniors housing; Serviced apartments

The proposed R3 zoning is a natural extension of the existing R3 zoned areas located directly to the north and east of the site. The intended built form is a mix of housing products; attached and detached housing and residential flat buildings, increasing the housing choice and affordability in the area. It is intended to locate the residential flat buildings closest to the train station and the entrance to the site, to ensure maximum accessibility to public transit and reduce any amenity impacts on surrounding lower scale residences.

These housing typologies are significantly under-represented in the Illawarra housing market, and respond to the changing demographics of the area. The Illawarra-Shoalhaven area has an increasing number of 1-2 person households due to an aging population. A more diverse housing mix is also required to respond to an increasing need for more affordable housing. The exact mix of the built form will be guided by market demand.

5.5.2 Affordable Housing

Legacy and ICC are committed to ensuring that the site is rezoned and developed with the provision of 5% affordable housing, which is around 35 dwelling units. The affordable rental housing will be provided and managed by a registered Community Housing Provider. This is to ensure that a wide range of housing choice, based on the belief that housing affordability is underpinned by providing a high level of housing diversity, including appropriate compact housing types and noting that the present housing stock of Corrimal is dominated by detached dwellings.

Further, although Legacy Property and ICC are committing to 35 dwellings on a parcel of land, there could be opportunities for affordable housing “salt and peppered” within other parts of the site. For this reason it is proposed to permit a variation to the Maximum Height of Buildings Control of up to 3.5m and the associated increase in FSR for any Residential Flat Buildings, where the additional Gross Floor Area is utilised for the provision of Affordable Housing over and above the 35 units of affordable housing.

5.5.3 Open Space

Landscape design has been integrated into the built form and introduced landscape objectives derived from the following design principles:

- » Create a new public parkland along the realigned North Corrimal Creek
- » Incorporate landscape edge vegetation to preserve key interfaces for surrounding residents
- » Retain and enhance structures associated with the former uses on the Site
- » Enhance proposed Civic Hub adjacent to Corrimal station
- » Define a fine grain network of streets, lanes and pedestrian links
- » Frame internal views and vistas
- » Create a system of cycle paths that link to key destinations and connect into wider network.

5.5.4 Riparian Corridor

Surrounding the proposed re-alignment of the creek, an RE1 Public Recreation zone will provide excellent accessibility to open space resources for residents and improve green links for the entire Corrimal community.

5.5.5 Proposed non- residential uses

It is proposed to accommodate a small supermarket on site, supported by some cafes and restaurants and a limited convenience offer (such as newsagency or small pharmacy, personal services such as a hair salon, beautician etc.) with a gross floor area of around 2,000m², all within 200m of the railway station.

In the future, some community uses can also be accommodated on the site.

As the site is close to the railway station and in proximity to Corrimal town centre, it is considered that the small commercial/retail neighbourhood uses should not be rezoned to a B1 zoning, but rather to retain propose the whole site as a R3 zoning and allow additional permissible uses to specifically apply to that part of the site which is within 200m of the railway station.

5.5.6 Land Use Character

The Master Plan for the rezoning area would result in a diverse range of building forms which respond to their individual location and context. The Master Plan encompasses five character areas; Civic Hub, Treetop Escarpment, Creek Edge, Village Park and Riparian Corridor.

Civic Hub

The new Civic Hub forms a central element in the larger scheme and is positioned close to the existing Corrimal train station. A plaza where the community can experience the old heritage items either retained, reinterpreted or re-purposed as outlined below:

- » The Brick Chimney will be retained and made safe
- » The site of the C1 Coke Ovens will be re-interpreted with a steel frame
- » Quench Tower will also have a re-interpretational element
- » Power House will be re-purposed
- » A section of the existing paving will be retained and re-interpreted
- » C2 Crusher House interpreted in the paving to help inform and educate

A bus stop will be introduced to stop at the plaza to make access and travel time to the train station more efficient. New shops and cafes will line the plaza activating the space with multiple functions. Surrounding the plaza will also be residential housing of a higher density and built form due to its proximity to the railway station and the non-residential uses adjacent to the plaza. Importantly the heritage items of the Brick Chimney and C1 Chimney will retain their heights to provide for a reflection of the site's history and built form.

A place where people can relax and enjoy the different spaces provided throughout the plaza landscape. This will also be a gathering area where the local community can enjoy and experience the sites history through unique, vibrant and exceptional landscape design with heritage interpretation.

Treetop Escarpment

This sub-precinct forms an important interface with Railway Street and the wider urban area. It references the mature trees on the sites northern boundary and the dynamic form of the escarpment to the west. This precinct will be predominantly residential uses, providing a general housing diversity of apartments and retirement living.

Creek Edge

Along the North Corrimal Creek public open space, development will have high quality-built form fronting open spaces, with a clear delineation of the public and private domain. The residential development will provide for multiple dwellings, townhouses and single dwellings in the western and central area. In the southern area, adjoining the creek and biodiversity area, higher apartment building are proposed to ensure surveillance of the creek and open spaces, and for people to enjoy the parkland surrounding the apartment buildings.

Landscape setbacks from the built form would reflect and contribute to the green landscape character and the built form would be designed to address the green outlook.

Village Park

A local park will be sited in the heart of the new development. This location ensures that all residences will have access to the park within 400 meters (10 min walk). The park will provide for day-to-day passive recreational needs of the community.

The local park will have a road edge on three sides of the park maximising passive surveillance. The residential development will predominantly be multiple dwellings, townhouses and single dwellings. Entry courtyards of residences will front the park on the fourth side. Landscape is used to create a visual buffer between these units and the open space for privacy.

An open lawn area will be incorporated inside this local park with shade trees for the residents to enjoy a picnic setting or children kicking a ball around.

Clear access pathways will cut through the local park and connect the community with the open space. A cyclist stopping and resting zone will be introduced inside the local park for local or regional cyclists passing through the site. This will also help with passive surveillance and increase the number of users.

This sub-precinct is focused around the central local public open space. Development will address the local park and provide a clear urban edge.

Riparian Corridor

The riparian corridor brings people closer to nature with clear accessible walkways linking to the creek and down to the water edge. The significant biodiversity areas in the north and southern areas will be retained within the corridor precinct.

Cycle ways will also be introduced connecting into the local network. The varied path alignment, seating and rest places, varied planting palette and the opportunity to introduce artwork from salvaged material will all add ample variety within the Riparian Corridor.

There will also be a strong emphasis and focus on views towards the escarpment by positioning of new trees and designing around existing tree locations to open up viewpoints. This in turn will allow connection to the open space and to enjoy picnic areas under the existing mature tree canopies.

Provision has also been made for a small amphitheatre space positioned in the natural slope of the creek embankment to be used for outdoor events, concerts and markets.

5.5.7 Lot mix

The majority of the site is proposed to be subject to a 135m² minimum lot size, with some areas being subject to a 149m² MLS. There is no proposed MLS for open space or riparian areas.

The minimum lot size allows for:

- » Smaller housing typologies, such as townhouses, to be developed on Torrens titled lots to support the housing need.
- » Dwellings on varying lot sizes, including on one side a zero-lot boundary, to provide for housing for families
- » Providing for a minimum lot size, however allowing larger lot sizes to be developed where necessary with apartments, affordable housing or retirement living.

5.6 Proposed road and public realm strategy

5.6.1 Road networks

A new hierarchy of streets is proposed within the site to ensure easy and adequate access for residents and visitors. The key access point to the site will be from Railway Street.

The traffic assessment has indicated that the development's traffic impacts of significance are isolated to the intersection of Memorial Drive/Railway Street, therefore an upgrade to this intersection is required to mitigate the development's impacts. The upgrade will involve:

- » one of the two departure lanes on the eastern and western legs of Railway Street reassigned a third approach lane (i.e. three approach lanes in each direction, one right turn pocket, one through lane and one left turn lane);
- » extending the right turn pocket length in the southern approach from 95m to 150m; and
- » changing the signal phase times of the intersection to better flows and align with the geometrical changes.
- » The additional traffic introduced into Railway Street by the development equates to one vehicle every 10 seconds (two-way) in the peak hour, which is insignificant on a distributor road such as Railway Street.

5.6.2 Pedestrian cycle network

Pedestrian and cycling connections are proposed throughout the site, linking Corrimal town centre to Corrimal train station, and further south along a shareway across the Towradgi Creek to the Council's sport fields. It is also proposed to enhance the shareway along the section of railway Street on the northern side of the site for cyclists and pedestrians, so to link to the broader cycle network.

5.6.3 Streetscapes

The concept masterplan integrates a street hierarchy that meets vehicle, pedestrian and cycle circulation needs and enhances intuitive way-finding. To develop a clear identity for all streets and to maximise environmental and amenity benefits of tree canopy, a palette of tree species has been selected.

To best integrate the urban fabric with its surrounding landscape the selected species draw on native species, many being locally endemic. The species selection includes those recommended by the WDCP 2009.

Planning Proposal

This section of the report provides justification and clarity in terms of the intended outcomes and the strategic merit of the proposal to enable the determining authority to issue a Gateway determination consistent with s3.33 of the EP&A Act.

In accordance with s3.33 of the EP&A Act and 'A Guide to Preparing Planning Proposal' (the guidelines), this section contains the basis of a PP for the WLEP 2009 amendment. It is comprised of:

- » Part 1 – objectives and intended outcomes
- » Part 2 – explanation of provisions
- » Part 3 – justification
- » Part 4 – mapping
- » Part 5 – community consultation
- » Part 6 – project timeline

Part 1 – Objectives or intended outcomes

The primary objective of this PP is to amend WLEP 2009 to facilitate the development of the site to deliver a residential development at the former Corrimal Coke Works site. The intended outcomes of the PP are:

- » create a residential site for approximately 700 - 750 dwellings
- » provide opportunities for a mix of housing types
- » create a new civic hub adjacent to Corrimal train station
- » retain key heritage elements of the site
- » realign and regenerate the riparian corridor
- » deliver an open space network, providing a range of recreational opportunities
- » reform and rehabilitate the riparian corridor to improve water management and ultimately enhance the biodiversity values on the site
- » provide quality traffic circulation within the residential community and improve the performance of the surrounding traffic network
- » deliver a serviceable road network with improved walkability to the station.

Part 2 – Explanation of Provisions

Part 2 of the PP is an explicit statement of how the objectives outlined in Part 1 are to be achieved through an LEP amendment.

The current RE2 Private Recreation and IN3 Heavy Industrial zoning on the site inhibits redevelopment. A rezoning of the site is necessary in order to create a high-quality residential development.

Viable development of the site can only be realised by amending WLEP 2009 to enable the type of development envisaged by the Master Plan.

5.7 Amendments to WLEP 2009 Mapping

The proposed outcomes will be achieved by an amendment to WLEP 2009 as follows:

- » **Amend the Land Zoning Map**
 - > Rezone the site from **RE2** Private Recreation and **IN3** Heavy Industrial to **R3** Medium Density Residential and **RE1** Public Recreation.
- » **Amend the Minimum Lot Size Map**
 - > On land zoned **R3** specify a minimum lot size of 135m² and 149m²
 - > On all other land there will be no minimum lot size control
- » **Amend the Height of Buildings Map**
 - > On land zoned **R3** specify a height of 9m, 11m, 13m, 15m, 16m, 20m and 24m
 - > On land zoned **RE1** specify a height of 9m
- » **Amend the Floor Space Ratio Map**
 - > On land zoned R3 specify FSRs of 0.5:1, 0.75:1, 1.2:1, 1.5:1, 2:1 and 2.5:1
 - > On all other land there will be no FSR control
- » **Amend the Heritage map**
 - > Include the following 4 heritage items and associated heritage curtilage:
 - C1 North Stack
 - C1 Coke Oven Battery (including load tracks)
 - Old Power House
 - C1 Brick Chimney Stack
- » **Amend the Acid Sulfate Soils, Riparian Land, Foreshore Building Line Map, Flood Planning Map**
 - > Amend the location of riparian land to reflect the corridor realignment.
 - > Delete the foreshore building line.
 - > Delete land below foreshore building line.
- » **Amend the Natural Resource Sensitivity – Biodiversity Map**
 - > Include two additional areas of natural resource sensitivity in the north and south of the site

5.7.1 Land use zones

The proposed **R3** Medium Density Residential zoning facilitates a mix of higher density residential development on the site. This is consistent with the requirement for increased density housing within Wollongong LGA and the vision for increased density housing surrounding train stations within the northern corridor of Wollongong (from Thirroul to Fairy Meadow).

It is envisaged that the development will include semi-detached and terraced housing, as well as residential flat buildings, located closer to Corrimal train station and the entrance of the site. Surrounding the civic hub (within 200m of Corrimal train station) there is the potential to provide shop-top housing, integrated with small scale retail development at the ground floor level.

The **RE1** Public Recreation zoning is proposed for the passive and active open spaces provided within the development in order to ensure that there are designated areas for public recreation. The **RE1** land can be identified on a land acquisition map if necessary, to enable the land to be transferred to public ownership in the future.

5.7.2 Minimum lot size

The principle development standards for the Minimum Lot Size (MLS) is proposed to be amended. At present half of the site has a 1.99ha MLS. As part of the proposed amendments, the part of the site zoned **R3** will have a 135m² and 149m² MLS. The part of the site zoned **RE1** will have no MLS.

The designated MLSs have been established to ensure that there is sufficient area to accommodate the proposed typologies of built development and to establish a character suitable to offer a diversity of housing choice.

5.7.3 Height of Buildings

The proposed instrument will amend the principle development standards for the Height of Buildings (HOB) with an amended HOB map. At present the site has a HOB control of 9m over the part of the site zoned RE2 Private Recreation.

Building heights will range from 9m, over the part of the site zoned **RE1** to heights of 11m- 24m over the part of the site zoned **R3**. The 24m height control will be reserved for the area surrounding Corrimal station. The 13m HOB control is intended to be a continuation of the HOB control for surrounding R3 areas.

5.7.4 FSR

No Floor Space Ratio (FSR) control currently exists for the site. The proposed instrument will amend the principle development standards for the FSR with an amended FSR map. As part of the proposed amendments, the area to be zoned R3 will accommodate a variation of FSRs from 0.5:1 – 2.5:1 in order to permit the diversity of development typologies and built form character envisaged for the site.

5.7.5 Heritage

The site currently does not include any items of state or local heritage significance. The PP proposes to classify the following four items, and their associated curtilage, which will be grouped as 'former Corrimal Coke Works', as heritage items:

- » C1 North Stack
- » C1 Coke Oven Battery (including load tracks)
- » Old Power House
- » C1 Brick Chimney Stack

5.7.6 Acid Sulfate Soils, Riparian Land, Foreshore Building Line Map, Flood Planning

The proposed realignment of the riparian corridor will be reflected in the amended Acid Sulfate Soils, Riparian Land, Foreshore Building Line, Flood Planning map. The Foreshore Building Line and Land below the foreshore building land will also be deleted to reflect the fact that realignment will remove the probability of flooding on the site.

5.7.7 Natural Resource Sensitivity – Biodiversity Map

The Natural Resources Sensitivity Map will be amended to include two new areas of natural resource sensitivity, in response to the biodiversity assessment undertaken by EcoLogical.

5.8 Amendments to Part 7 Local Provisions – General

5.8.1 Former Corrimal Coke Works

The following clause will be included in Part 7 to allow increased heights, beyond the maximum height control of 24m, for C1 North Stack and C1 Brick Chimney Stack, which currently sit at heights of approximately 36.8m and 29m respectively:

7.20 Former Corrimal Coke Works

Height of Development

- 1) *The height of any development on the former Corrimal Coke Works site is not to exceed the height limit shown on the Height of Buildings Map, with the exception of the following heritage items:*
 - a. *C1 North Stack*
 - b. *C1 Brick Chimney Stack*
- 2) *The following height limits are permissible for the heritage items:*
 - a. *C1 North Stack: 37m*
 - b. *C1 Brick Chimney Stack: 29m*

5.8.2 Affordable Housing

The following clause will be included in Part 7 to permit increased heights for Affordable Housing Developments.

Affordable Housing

- 1) *The consent authority may permit a variation to the Maximum Height of Buildings Control of up to 3.5m and the associated increase in FSR for any Residential Flat Buildings, where the additional Gross Floor Area is utilised for the provision of Affordable Housing*

5.9 Amendments to Schedule 1 Additional permitted uses

The following clause will be included in Schedule 1 Additional Permitted Uses to ensure that a full complement of neighbourhood and commuter services near Corrimal train station can be provided:

Use of certain land at the former Corrimal coke works site:

- 1) *This applies to land at the former Corrimal coke works site*
- 2) *Development for the purposes of Food and Drink Premises, and neighbourhood supermarket is permitted with consent, but only if at ground floor and within 200m of the train station*
- 3) *Development for neighbourhood shops and neighbourhood supermarket is permitted up to a maximum of a total GFA of 2,000m².*

5.10 Corrimal Development Control Plan

A Site-Specific Development Control Plan (DCP) has been created for the Corrimal site.

5.11 Voluntary Planning Agreement/ Section 7.11 Contributions

Discussions regarding a VPA are currently ongoing. A preliminary schedule of public benefits is provided at **Appendix U**. The proposed VPA will provide for offsets to local contributions due to the extent of work and public benefit being proposed.

Part 3 - Justification

Part 3 sets out the justification for the PP, against its strategic planning context, considering the environmental, social and economic impacts of the proposal and the interests of the State and Commonwealth Governments.

In accordance with the guidelines, the level of justification is to be commensurate to the stage in the LEP making process. The potential impacts of the proposal have been identified in sufficient detail to confirm the suitability of the rezoning.

Section A – Need for the Planning Proposal

Q1 Is the Planning Proposal a result of any strategic study or report?

The PP was not prepared as a result of any specific strategic study for utilisation for an urban development purpose.

Q2 Is the Planning Proposal the best means of achieving the objectives or intended outcomes, or is there a better way?

In order to achieve the intended development outcomes and consistency with State Government strategic directions, the site requires rezoning. This is the only alternative to achieve the objectives of the strategic directions. The current controls allow for industrial development only.

Section B – Relationship to Strategic Planning Framework

Q3 Is the Planning Proposal consistent with the objectives and actions of the applicable regional, sub-regional or district plan or strategy (including any exhibited draft plans or strategies)?

This section of the report supplements the PP by addressing the provisions of the relevant regional and subregional plans and strategies.

Building Momentum: State Infrastructure Strategy 2018-2038 (2018)

Economic activity is growing around Wollongong, where the focus is on providing jobs and housing, growing the capacity of Port Kembla and driving greater economic diversity in priority sectors. Wollongong is recognised as a major city, key international gateway and significant economic activity centre.

By 2036 the Wollongong and Shellharbour area is likely to be home to more than half a million people. The area will drive the economic growth, employment and diversification of the broader Illawarra-Shoalhaven region, while also contributing to Greater Sydney's economy and labour force.

Wollongong will become increasingly connected to Greater Sydney by 2056, enabled by its proximity to Greater Sydney's jobs and services and improved road and rail connections. Improved connectivity between Wollongong and Port Kembla, the National Land Transport Network, the Western Sydney Airport and intermodal terminals in the Western Parkland City will also be important for the city's ongoing economic growth.

The NSW Government has committed to upgrading the Princes Highway at Albion Park Rail and the section between Berry to Bombaderry. This investment will improve access between the Shoalhaven and Wollongong and Sydney. Various other NSW Government and private investments will enhance Wollongong over the coming years, including an expansion of the Wollongong Hospital. Future investments should be focused on:

- » growing the amenity of Wollongong by providing good transport connections and local services
- » growing the capacity of the port at Port Kembla as an international trade gateway, enabled by dedicated rail connections

- » strengthening links between Wollongong, Port Kembla and Greater Sydney, with extra capacity for rail services and improved road connections across the Illawarra Escarpment and to the Western Parkland City
- » building on existing strengths and supporting economic diversity through growth in priority sectors including tourism, health, disability and aged care, ICT/knowledge services, education and training, and freight and logistics.

Future Transport Strategy 2056 (2018)

By 2056, economic and housing growth around Greater Sydney will drive integration across the city's hinterland, establishing areas such as Gosford and Wollongong as 'satellite cities'. As part of the strategy, the following infrastructure upgrades are intended:

- » Regional NSW Initiatives for investigation (0-10 years):
 - > Sydney-Wollongong faster rail improvement
 - > Wollongong Rapid Bus Package
 - > Wollongong Place Plans
 - > Bus headstart for Wollongong

Illawarra – Shoalhaven Regional Plan (2015)

The Illawarra-Shoalhaven Regional Plan, applies to the LGAs of Kiama, Shellharbour, Shoalhaven and Wollongong. By 2036, the population of the Illawarra-Shoalhaven is forecast to grow to 463,150, an increase of 60,400 from 2016. Population growth will result from natural increases as well as the sustained migration of young families and retirees.

The make-up of the population will change over the next 20 years. Growth will be moderate in most age groups, except in the 65-and-over group, particularly in Kiama and Shoalhaven. There will also be more one- and two-person households. Growth will necessitate at least 35,400 new homes.

This Regional Plan for the Illawarra-Shoalhaven provides the strategic policy, planning and decision-making framework to guide the region to sustainable growth over the next 20 years.

Within the Plan, Corrimal is marked as an urban centre, as shown in **Figure 15** below.

Figure 15 Corrimal Urban Centre



Source: Illawarra-Shoalhaven Regional Plan

The following key principles underline the planning framework for the plan:

- » identify and protect land with high environmental value and recognise cultural heritage values
- » support the sustainable use of land and water resources and build resilience to natural hazards and climate
- » support a strong, resilient and diversified economy that will enable the community to respond to environmental, economic and social challenges change
- » integrate transport and land use planning, and support improvements in active transport (walking and cycling), public transport and transport infrastructure (including freight)
- » take a balanced approach to housing that provides choice, affordability, and supports the orderly supply of land for development
- » increase housing density around centres that have access to jobs and transport and are already appealing to residents
- » encourage urban design that reduces car dependency, improves the public domain, promotes energy efficiency and supports healthier environments
- » improve infrastructure coordination

To achieve the vision for the Illawarra-Shoalhaven, a number of goals have been identified, as shown in **Table 8** below.

Table 8 Key Goals, Directions and Actions in the Illawarra-Shoalhaven Regional Plan 2015

Goals and Directions	Justification and response
Goal 1 – A prosperous Illawarra Shoalhaven	
DIRECTION 1.3 Grow regional strategic assets to support economic growth across the region	The subject site is not listed as being of strategic economic importance for continued industrial development in the plan. The Corrimal Centre is identified as an important Urban Centre servicing the needs of residents in the northern portion of the Wollongong LGA.
DIRECTION 1.4 Support new and expanded industrial activity by providing well-located and serviced supplies of industrial land	<p>There is a ready availability of better located heavy industrial land in the Wollongong LGA. In addition, the conversion of the land to light industrial is likely unfeasible due to remediation and other costs likely to exceed return on investment.</p> <p>The location of the site within a residential area also marks it as a less than ideal location to continue industrial activities due to land use conflict issues.</p>
Goal 2 – A variety of housing choices, with homes that meet needs and lifestyles	
DIRECTION 2.1 Provide sufficient housing supply to suit the changing demands of the region.	Within the Wollongong LGA, the projected housing need from 2016-2036 is 14,600 dwellings. The combined demands from tourism and the housing market, particularly in coastal towns, will require new housing developments. The changing demographics and market demand of the LGA will require a mix of housing.
DIRECTION 2.2 Support housing opportunities close to existing services, jobs and infrastructure in the region's centres.	<p>Ensuring additional housing development in existing urban areas is a sustainable option in terms of housing supply as it takes advantage of existing job markets infrastructure, commercial and retail opportunities, public transport and facilities for pedestrians and cyclists.</p> <p>Corrimal has been identified as a focus for increased housing activity within the Northern Corridor. An analysis of current planning controls in the region shows capacity for 24,100 new homes in existing urban areas, including townhouses and multi-unit dwellings.</p>

Goals and Directions	Justification and response
	<p>The plan notes that additional residential development in locations such as Corrimal could also act as a catalyst to enhance their existing recreational and environmental features.</p> <p>The subject site presents an opportunity to provide a diversity of housing types adjacent to public transit, in close proximity to an existing urban centre. It offers strong recreational opportunities nearby and, in the Wollongong LGA, Northern Corridor generally.</p>
GOAL 3 – A region with communities that are strong, healthy and well-connected	
DIRECTION 3.2 Enhance community access to jobs, goods and services by improving connections between centres and growth areas.	<p>The proximity of the northern corridor (which encompasses Corrimal) to Sydney means that there is considerable scope to support commuters and also attract business to the region relocating from Sydney. Over 21% of the workforce in the northern corridor commutes to Sydney for work.</p>
ACTION 3.2.2 Improve access to centres, particularly in the northern corridor, to encourage development	<p>In order to capitalise on improved public transport, new housing should be focused in and around centres in the rail corridor. The location of the site adjoining the Corrimal train station, linked with the Sydney and Wollongong CBDs, provides an excellent commuter location.</p> <p>The site also offers an opportunity to create local jobs in servicing the population such as childcare, seniors living and small-scale neighbourhood shops servicing local residents and commuters (all permissible under the proposed R3 zoning).</p> <p>A key objective in the concept plan is to make public transit more convenient and attractive for both residents of the development and the wider Corrimal community.</p>
DIRECTION 3.2 Build socially inclusive, safe and healthy communities	<p>New neighbourhoods and centres should be designed to offer a high-quality lifestyle and to be environmentally sustainable, socially inclusive, easy to get to, healthy and safe and opportunities should be taken to connect neighbourhood communities with the surrounding landscape.</p>
ACTION 3.3.2 Support Council-led revitalisation of centres	<p>A key aim of the Corrimal redevelopment is to connect the site with the wider community through the establishment of a number of green links providing pedestrian and cycling tracks.</p> <p>The Corrimal urban centre has been subject to investigation by Wollongong City Council through the Corrimal Town Centre Revitalisation Strategy, which has been exhibited. The proposed development is in line with the vision for the revitalisation of Corrimal.</p>
ACTION 3.4.1 Conserve heritage sites when preparing local planning controls	<p>Although the site contains no heritage items at present, it is proposed to retain some of the features of the Corrimal Coke Works as heritage items, in order to ensure the history of the site is incorporated into future development.</p>
GOAL 5 – A region that protects and enhances the natural environment	
DIRECTION 5.1 Protect the region's environmental values by focusing development in locations with the capacity to absorb development	<p>The regions 'high environmental value' land have been mapped. These include the Illawarra Escarpment. Due to the high value of this landscape, a key element of the proposed redevelopment of Corrimal has been to maintain viewlines west towards the escarpment.</p>
ACTION 5.1.1 Avoid, minimise and mitigate the impact of development on significant environmental assets	<p>Through the remediation and re-use of an existing industrial site, with high levels of accessibility to public transit and Corrimal town centre, will result in a positive impact on Corrimal as a whole.</p> <p>Portions of the subject site have been identified as being of high biodiversity value. Redevelopment of the site will ensure the</p>

Goals and Directions	Justification and response
ACTION 5.1.4 Create a consistent approach to protect important riparian areas in planning and development controls	management and maintenance of these assets, adding value to residents and the broader community alike. The majority of the biodiversity areas have been avoided in the development footprint. The proposal also includes re-alignment of the riparian corridor due to the fact it is currently significantly degraded and in light of the potential benefits realignment will have in terms of alleviating flooding on the site.
DIRECTION 5.3 Improve the environmental outcomes for waste management and air quality	The site presents an opportunity to support sustainable development given its public-transit access and proximity to the Corrimal town centre and recreational resources. The street network has been designed to deliver efficient waste management to work with the current regime in place in Wollongong. Investigations have been made as to the impact of the creek re-alignment on Towradgi Creek. These considerations and controls are expected to guide conditions to ensure the on-going health of the estuary.
ACTION 5.4.1 Protect sensitive estuaries and coastal lakes	

Illawarra Regional Transport Plan (2014)

Population and employment in the Illawarra region are focused on the Wollongong metropolitan area and identified key urban centres, one of which is Corrimal.

Wollongong metropolitan area has a population of around 250,000 and is the focus for employment and residence in the Illawarra region, as well as for education and health care within the region. The following actions for Wollongong are identified within the Plan:

- » Action: Improve public and active transport access to Wollongong
 - > Deliver actions to increase the public transport share of commuter trips to and from Wollongong in peak hours to and from the CBD to 15% by 2016.
 - > Strengthen the role of rail in connecting local communities to Wollongong city centre.
 - > A network of bus services will provide local access with peak period frequencies supporting convenient access to work and education
 - > Local rail services integrated with high quality bus services, and supported by pedestrian and cycle networks
- » Action: Improve opportunities for walking and cycling
 - > Support the implementation of better facilities for walking and cycling, including the provision of cycle parking facilities at transport interchanges, centres, schools and hospitals.
- » Action: Invest in public transport infrastructure
 - > Identify opportunities to improve the infrastructure that supports public transport services, such as bus stops and shelters, terminal facilities and customer information.
- » Action: Deliver road upgrades
 - > Continue to improve the Princes Motorway (M1), Princes Highway (A1) and Mount Ousley Road to boost capacity, improve travel time, support public transport operations and provide efficient freight connections to Port Kembla

Draft Regional Growth and Infrastructure Plan (2014)

The draft Plan states that to meet the demands of this growing population, the Region will need 45,000 new homes and 32,150 new jobs. It will need upgrades to existing infrastructure and investment in new infrastructure

to deliver the services for a community that is not only growing but also changing, by 2031 there will be an increasing ageing population, and there will be more one and two-person households.

One of the aims of the plan is to provide well located, more diverse and more affordable housing in order to provide for people at all stages of life, and is close to jobs and services, affordable and well designed. The urban corridor between Fairy Meadow and Thirroul has been identified as being able to provide housing opportunities. Corrimal is included within this area. This corridor, can play a significant role in transforming the Illawarra by capitalising on its access to job opportunities in Sydney.

The plan identifies six transformative places that:

- » Play a role beyond their local area and are important contributors to the regional, and in some cases, State economy
- » Have good prospects for additional investment
- » Are in a strategic location and have a key economic function
- » Have the potential to drive additional economic growth through collaboration across all levels of government and industry

One of these places is the Northern Growth Corridor. The northern growth corridor runs from Fairy Meadow to Thirroul and has two key transport corridors which act as a 'spine' for the area: the South Coast Railway Line and the Princes Highway – both provide relatively high volume transport access to the Wollongong Centre or northwards to Sydney.

The northern growth corridor can play a significant role in transforming the Illawarra by capitalising on its access to global jobs in Sydney and the opportunities for new housing in key centres. The opportunities for growth in the corridor will come from the development of multi-dwelling housing (townhouses and villas) close to town centres. The plan states that in Corrimal there are also opportunities for higher density apartments.

The proximity of the corridor to Sydney means that there is considerable scope to support commuters and also attract business to the Region relocating from Sydney. Over 21% of the workforce in the northern growth corridor commutes to Sydney for work. Therefore, the rail network and the M1 motorway are a particular focus for the corridor as they play an important role in connecting residents to employment, education, recreation and social opportunities.

Multi-dwelling housing is already occurring in the corridor, with 40% of new multi-dwelling housing and apartment completions over the last five years in the Wollongong LGA, being within the northern growth corridor.

Housing

At least 45,000 new homes will be required between now and 2031 to cater for an additional 65,000 residents. With one in four residents aged 65 years and older (115,000 people) and more one and two person households the location of new housing and the mix of available housing will be an important consideration. Housing targets for the Wollongong LGA are included below:

Table 9 Housing Targets to 2031

LGA	Single dwelling housing	Multi dwelling housing	Apartments	Total
Wollongong	6,850	8,650	2,950	18,450

The northern growth corridor is considered an appropriate area to accommodate the majority of new multi dwelling housing and apartments in the region and could feasibly contribute 5,000 dwellings. The redevelopment of Corrimal would contribute up to 750 of these dwellings.

NSW Long Term Transport Master Plan (2012)

Specific actions for the Illawarra (Wollongong), as outlined in the NSW Long Term Transport Master Plan are as follows:

» Short term:

- > The Growth Centres Roads Program will continue to deliver a road network that supports strong growth. We will continue to improve the Princes Motorway (M1), Princes Highway (A1) and Mount Ousley Road to boost capacity, improve travel time, support public transport operations and provide efficient freight connections to Port Kembla.
- > Strengthen bus operations in major centres, as well as connections between these centres. This will occur through the development of the Regional Transport Plan and the Outer Metropolitan Bus Service Planning Guidelines. This will reflect emerging customer needs in all our bus contracts.
- > The timetable changes being investigated for Sydney's Rail Future, the creation of NSW TrainLink and modern signalling technology will deliver faster travel times and greater travel reliability between Wollongong and Sydney. Our goal is to reduce public transport travel time between Wollongong and Sydney. We will support these rail services with an integrated bus service.
- > Deliver the Port Kembla Growth Plan to plan for Port Kembla's future trade requirements and ensure its long-term access (see Chapter Seven).
- > Work with Wollongong City Council on a plan for achieving the NSW 2021 public transport mode share target by 15 percent (by 2016) for Wollongong CBD and on a transport strategy for key precincts such as West Dapto, as part of the Illawarra Regional Transport Plan.

» Medium to longer term:

- > Continue to enhance and expand bus services and strategic bus corridors to reflect developing demand and land use changes.
- > Future intercity train services will focus on the needs of longer distance customers rather than modifying short distance trains.

Q4 Is the Planning Proposal consistent with a Council's local strategy or other local strategic plan?

The PP is considered to be consistent with the aims and objectives of Council's local planning strategies, as discussed below.

Wollongong 2028 Community Strategic Plan (2018)

The Wollongong 2028 Community Strategic Plan outlines the Wollongong community's priorities and aspirations, providing directions for the provision of key projects and services. Relevant directions are included in **Table 10** with an outline of how the PP for the site responds to these directions.

Table 10 Relevant Strategic Plan directions and justifications and response

Strategic Plan Directions	Justifications and response
Goal 1: We value and protect our environment	
Objective 1.1 Our natural environment, waterways and terrestrial areas are protected, managed and improved	
1.1.1 The community is actively involved in the expansion and improvement of our green corridors and other natural areas connecting the escarpment to the sea.	The proposed redevelopment of the site will include green pedestrian and cycling connections linking Corrimal town centre to Corrimal train station. In the future this could form part of a wider green link corridor connecting the site to the coast.
1.1.2 Manage and effectively improve the cleanliness, health and biodiversity of creeks, lakes, waterways and oceans.	The riparian corridor on the site is proposed to be realigned. At present the corridor is severely degraded. It is considered that realignment of the corridor would improve it's health while also reducing the potential of flooding on the site.

Strategic Plan Directions	Justifications and response
1.1.3 The potential impacts of natural disasters, such as those related to bushfires, flood and landslips are managed and risks reduced to protect life, property and the environment.	Realignment of the riparian corridor would significantly reduce the potential of flooding on the site and improve drainage outcomes for adjacent neighbours. The site is not bushfire prone land.
Objective 1.2 We practice sustainable living and reduce our ecological footprint	
1.2.1 Reduce our ecological footprint, working together to mitigate the impacts of climate change and reduce waste going to landfill.	The development footprint of the PP has been designed so as to avoid all areas of site considered to have high biodiversity values.
Objective 1.3 The sustainability of our urban environment is improved	
1.3.1 Manage land uses to strengthen urban areas and improve connectivity close to train stations and key transport routes.	The proposed development has incorporated a number of pedestrian and cycling green links into the overall concept in order to allow connectivity between Corrimal train station and Corrimal town centre. The aim is to incorporate the development into the wider residential area.
Objective 1.4 We recognise and celebrate our heritage	
1.4.1 Programs and projects that achieve proactive heritage management, education and promotion are developed and implemented.	Celebration of the heritage of the site is included in the proposed Civic hub, where four elements of the sites previous industrial history are proposed to be retained and included within the WLEP 2009 as local heritage items.
Goal 5 We have a healthy community in a liveable city	
Objective 5.1 There is an increase in the health and well-being of our community	
5.1.4 Urban areas are created to provide a healthy and safe living environment for our community.	The urban area, public open spaces and green link connections have been designed so as to promote active transport and increase public transport usage.
Objective 5.2 Participation in recreational and lifestyle activities is increased	
5.2.1 Provide a variety of quality public spaces and opportunities for sport, leisure, recreation, learning and cultural activities in the community.	The proposed development incorporates a large riparian corridor and several public recreation spaces including an amphitheatre as well as green links through the site, enabling active and passive recreation. Interpretation/learning installations will be integrated referencing the sites history and ecology. The site is not of the size to accommodate sporting facilities, however, Robert Ziems Oval is located immediately to the west of the site.
Objective 5.3 Residents have improved access to a range of affordable housing options	
5.3.1 Housing choice in the Wollongong Local Government Area is improved, taking into account population growth, community needs and affordability.	Development of the site would provide an opportunity to increase the amount and variety of housing in the LGA from townhouses, to residential flat buildings. The LGA is currently in need of higher density housing to accommodate for the increasing number of 1 -2 person households. The diversity of housing proposed will also increase affordability options.
Goal 6 We have sustainable, accessible and affordable transport	
Objective 6.1 Wollongong is supported by an integrated transport system	

Strategic Plan Directions	Justifications and response
6.1.2 Work with partners to decrease car dependency and facilitate sustainable transport to provide convenient movement throughout the city, with sustainable transport modes such as walking and cycling.	The proposed development has been centred around decreasing car usage of future residents and increasing the patronage of train services from Corrimal station, as well as promoting active transport through pedestrian and cycling connections from the station to Corrimal town centre.
6.1.4 Integrated communities close to public transport and local services and facilities focused around existing train stations and town and village centres are planned for and encouraged.	The site is located adjacent to the Corrimal train station and 350m from the Corrimal Town Centre representing an opportunity to develop a diverse integrated community close to a major transport link and significant urban centre.

Places for People: Wollongong Social Infrastructure Planning Framework 2018 – 2028 (2018)

Places for People, Wollongong's Social Infrastructure Planning Framework is a blueprint for long term strategic planning and management of Council's social infrastructure. It is the vision of Wollongong Council that all residents, workers and visitors will have access to quality, sustainable social infrastructure that meets their needs and reflects Wollongong's role as a leading regional city.

Wollongong is divided into ten planning areas, based on population size, common topography, historical and affiliative perceptions of 'place' and patterns of people movement along road and rail systems. Corrimal is located within Area 2/3 of the Northern Catchment. It is expected that the Northern Catchment will continue to grow, but the overall share of the population will drop from 37% to 33% by 2036. However, the population of the Northern Corridor is still expected to expand from 78,425 people in 2016 to 81,811 people in 2036.

Within the planning framework Corrimal is identified as a key urban centre within the Northern catchment. The current social infrastructure provision by catchment area indicates that the northern catchment has a total foundation GFA of 9,183m², a total of 15 foundation facilities, 14,482m² of total supporting GFA and a total of 40 supporting facilities.

Key outcomes for the Northern catchment are:

- » Consider the role of Social Infrastructure in Council's town centre planning for Corrimal – possible impact of proposed higher density residential developments for social infrastructure.
- » Recognise availability of two, existing 'District level multi-purpose community centre and library facilities at Thirroul and Corrimal
- » Recognise limited availability of dedicated, creative, cultural social infrastructure within the Northern catchment, while noting that Thirroul and Corrimal community centres and libraries offer spaces that can be used for exhibition and performance.

Discussion

No council social infrastructure is proposed to be developed onsite. However, provision for social infrastructure within the region will be provided generally through Section 7.11 contributions.

Corrimal Town Centre Plan (including Memorial Park) 2015 – 2025 (2015)

The plan provides a strategic direction for the Corrimal town centre. The plan identifies the former Corrimal Coke Works site as being a 'key site', adjacent to the railway station and in walking distance to the Corrimal Town Centre services. However, the following issues were identified:

1. Investigations into flooding and contamination constraints would be required in order to inform the development capacity of the site

2. A quality east-west linkage from the town centre to the site and the railway station and the beach would need to be established and any development would need to have an active street frontage with Railway Street
3. Any future development would need to be complementary to the town centre
4. Investigations would be required into the archaeological and cultural significance of the site
5. Future works would need to be cognisant of flood impacts and in accordance with the existing Vegetation Management Plan.

A PP is necessary to address the above considerations.

Discussion

In response to the above:

1. Investigations into the contamination constraints of the site have revealed that there are a limited number of locations that require remediation on the site and these can be made suitable for residential and recreational uses.
2. The concept plan will deliver a significant improvement to the streetscape connecting the Corrimal town centre and Corrimal train station. Both road edge and through site routes will be created, particularly concentrating on pedestrian and cycling links in order to increase active transport. Pedestrian and cycling links will connect with a potential green link along the Towradgi Creek corridor to Corrimal Beach. This would also link with the Grand Pacific Walk and the community and recreational resources west of Memorial Drive.
3. The proposed development would be complementary to the town centre in that it would result in an increased population, subsequently increasing economic activity in Corrimal. A R3 Medium Density zoning is proposed for the developable area of the site. This is consistent with surrounding zoning to the north, north-east and east of the site and will result in the delivery of housing diversity.

Some retail and commercial development is intended to be located adjacent to Corrimal station. However, the low-scale nature of these will not undermine the efficacy or further fragment to the core retail area of the Corrimal town centre.

4. Archaeological investigations have been undertaken on the site. No Aboriginal Archaeological values have been identified. However, four significant items associated with the former Coke Works have been identified as having local heritage significance and will be retained on the site and incorporated into the overall development.
5. Extensive investigations have revealed that creek re-alignment to the Western boundary of the site delivers the best option for the following reasons:
 - Improves the overall flood risk of the site, neighbouring sites and Corrimal generally
 - The current riparian corridor is significantly degraded
 - The re-aligned corridor would provide a good noise buffer between Memorial Drive and residential development
 - Assists in rationalising traffic movement in and out of the site
 - Avoids as much as possible of the high biodiversity on the site.

Q5 Is the Planning Proposal consistent with the applicable State Environmental Planning Policies?

The PP considers the State Environmental Planning Policies (SEPPs) which are applicable for the proposed urban development of the site, as identified in **Table 11** below.

Table 11 Applicable SEPPs

SEPP	Applicable	Consistency
SEPP No 1 – Development Standards	No	N/A
SEPP No 14 – Coastal Wetlands	No	N/A
SEPP No 19 – Bushland in Urban Areas	No	N/A
SEPP No 21 – Caravan Park	No	N/A
SEPP No 26 – Littoral Rainforests	No	N/A
SEPP No 30 – Intensive Agriculture	No	N/A
SEPP No 33 – Hazardous and Offensive Development	The site has previously been utilised for industrial uses, however operations ceased 5 years ago.	N/A
SEPP No 36 – Manufactured Home Estates	No	N/A
SEPP No 44 – Koala Habitat Protection	The site does not represent ‘potential koala habitat’ or ‘core koala habitat’ as defined in SEPP 44. Accordingly, there is no requirement to prepare a Koala Plan of Management.	Consistent
SEPP No 50 – Canal Estate Development	No	N/A
SEPP No 55 – Remediation of Land	A Contamination Assessment has been prepared for the site in accordance with the SEPP requirement. A Remediation Action Plan has been prepared for the subject site.	Consistent
SEPP No 62 – Sustainable Aquaculture	No	N/A
SEPP No 64 – Advertising and Signage	This proposal allows future development to meet the requirements of the SEPP	Consistent
SEPP No 65 – Design Quality of Residential Flat Development	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP No 70 – Affordable Housing (Revised Schemes)	No	N/A
SEPP (Building Sustainability Index: BASIX) 2004	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Housing for Seniors or People with a Disability) 2004	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Infrastructure) 2007	The proposal is supportive of planned and required upgrades to services, facilities and infrastructure which will support the development of the site.	Consistent

SEPP	Applicable	Consistency
SEPP (Mining, Petroleum and Extractive Industries) 2007	No	N/A
SEPP (Miscellaneous Consent Provisions) 2007	No	N/A
SEPP (Exempt and Complying Development Codes) 2008	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Affordable Rental Housing) 2009	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Coastal Management) 2018	<p>All the subject site fits within the Coastal Area Use Area Mapping associated with the Coastal Management SEPP.</p> <p>Division 4 applies to this area and the most relevant provision is that development <i>has taken into account the type and location of the proposed development, and the bulk, scale and size of the proposed development.</i></p> <p>The proposal can comply with the requirements of the SEPP.</p>	Consistent

Q6 Is the Planning Proposal consistent with applicable Ministerial Directions?

The PP gives consideration to the relevant Ministerial Directions issued under Section 117 (now 9.1) of the EP&A Act. The Minister for Planning and Environment issues Local Planning Directions that councils must follow when preparing a PP. The directions cover the following broad categories:

- » employment and resources,
- » environment and heritage,
- » housing, infrastructure, and urban development,
- » hazard and risk.

Table 12 provides information for consideration with regard to the consistency of the PP with regard to the relevant ministerial directions.

Table 12 Section 9.1 Directions

Section 9.1 Direction	Comment
Employment and Resources	
1.1 Business and Industrial Zones This direction applies when a relevant planning authority prepares a planning proposal that will affect land within an existing or proposed business or industrial zone (including the alteration of any existing business or industrial zone boundary).	A key objective of this ministerial direction is to protect business and industrial zones. The site is not considered suitable for the use of any further heavy or light industrial land uses.
1.2 Rural Zones This direction applies when a relevant planning authority prepares a planning	Does not apply as the land is not zoned rural and does not apply in the Wollongong LGA.

Section 9.1 Direction	Comment
proposal that will affect land within an existing or proposed rural zone (including the alteration of any existing rural zone boundary).	
<p>1.3 Mining, Petroleum Production and Extractive Industries</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that would have the effect of:</p> <p>(a) prohibiting the mining of coal or other minerals, production of petroleum, or winning or obtaining of extractive materials, or</p> <p>(b) restricting the potential development of resources of coal, other minerals, petroleum or extractive materials which are of State or regional significance by permitting a land use that is likely to be incompatible with such development.</p>	<p>To date no mining activity has been approved or undertaken below the site.</p>
<p>1.4 Rural Lands</p> <p>This direction applies when:</p> <p>(a) a relevant planning authority prepares a planning proposal that will affect land within an existing or proposed rural or environment protection zone (including the alteration of any existing rural or environment protection zone boundary) or</p> <p>(b) a relevant planning authority prepares a planning proposal that changes the existing minimum lot size on land within a rural or environment protection zone.</p>	<p>Not applicable as the land has not been used for rural purposes and does not apply in Wollongong City Council LGA.</p>
Environment and Heritage	
<p>2.1 Environmental Protection Zones</p> <p>This direction applies when a relevant planning authority prepares a planning proposal.</p>	<p>It is noted that this Ministerial Direction applies when a relevant planning authority prepares a PP.</p> <p>The PP does not seek to reduce the environmental protection standards affecting the site.</p>
<p>2.3 Heritage Conservation</p> <p>This direction applies when a relevant planning authority prepares a planning proposal.</p> <p>The objective of this direction is to conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.</p>	<p>There are no items of Commonwealth or State heritage listing within the site. However, the following four items are proposed to be retained as local heritage items as part of the PP</p> <ul style="list-style-type: none"> » Brick Chimney Stack » C1 Coke Oven Battery (including load tracks) » C1 Coke Oven Ram Tracks » Remnant Power House – west elevation
Housing, Infrastructure and Urban Development	
<p>3.1 Residential Zones</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will affect land within:</p>	<p>The proposal will provide diverse housing choices with a mix of medium density housing typologies in close proximity to public transit infrastructure. The proposed provision of approximately 700 - 750 new dwellings will help ensure that Wollongong City</p>

Section 9.1 Direction	Comment
<p>(a) an existing or proposed residential zone (including the alteration of any existing residential zone boundary),</p> <p>(b) any other zone in which significant residential development is permitted or proposed to be permitted.</p>	<p>Council deliver a diverse range of housing options as an infill project.</p> <p>The proposed development will make efficient use of existing and proposed infrastructure, promote active living and seek to minimise the potential impacts of housing on the environment.</p>
<p>3.3 Home Occupations</p> <p>This direction applies when a relevant planning authority prepares a planning proposal.</p>	<p>This Ministerial Direction requires a PP to contain 'home occupations' as a use that is permissible without consent in dwelling houses.</p> <p>However, 'Home Business' and 'Home occupation' as additional permitted uses is permitted through SEPP Exempt and Complying Development Code.</p>
<p>3.4 Integrating Land Use and Transport</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will create, alter or remove a zone or a provision relating to urban land, including land zoned for residential, business, industrial, village or tourist purposes.</p>	<p>The PP integrates with the adjacent Corrimal train station to offer an opportunity for transit-oriented development.</p> <p>Improved pedestrian and cycling infrastructure will also reduce car dependence for the broader Corrimal community. Commuter-related services, without undermining the efficacy of the Corrimal town centre, can be offered under the PP such as neighbourhood shops, food and beverage premises and childcare to offer convenience for both residents and commuters.</p>
<p>4.1 Acid Sulphate Soils</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will apply to land having a probability of containing acid sulfate soils as shown on the Acid Sulfate Soils Planning Maps.</p>	<p>The site is identified as having a probability of acid sulphate soils. The majority of the site is Class 5 with a small portion Class 3 (however, this is not proposed to be developed).</p> <p>Assessment has shown it is not a constraint to development but further investigation and management of acid Sulfate soils will need to be undertaken.</p>
<p>4.2 Mine Subsidence and Unstable Land</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that permits development on land that:</p> <p>(a) is within a mine subsidence district, or</p> <p>(b) has been identified as unstable in a study, strategy or other assessment undertaken:</p> <p>(i) by or on behalf of the relevant planning authority, or</p> <p>(ii) by or on behalf of a public authority and provided to the relevant planning authority.</p>	<p>It is understood that the site is not located within a mine subsidence district.</p>
<p>4.3 Flood Prone Land</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that creates, removes or alters a zone or a provision that affects flood prone land.</p>	<p>The site is flood affected. Mitigations are proposed as part of this PP to ensure that all land proposed to be zoned R3 will be PMF-free and assist in alleviating flooding issues for surrounding properties. All other land that will remain flood affected will be zoned RE1.</p>
<p>4.4 Planning for Bushfire Protection</p> <p>This direction applies when a relevant planning authority prepares a planning</p>	<p>This direction applies when a relevant planning authority prepares a PP that will affect, or is in proximity to land mapped as bushfire prone land. The subject site is not classed as bushfire prone by Wollongong City Council. The proposal will</p>

Section 9.1 Direction	Comment
proposal that will affect or is in proximity to land mapped as bushfire prone land.	be delivered in accordance with this direction and Planning for Bushfire Protection 2006.
Regional Planning	
5.1 Implementation of Regional Strategies This direction applies when a relevant planning authority prepares a planning proposal.	The site is not within any areas included in the Regional Strategies listed in this Ministerial Direction.
5.2 Sydney Drinking Water Catchments This Direction applies when a relevant planning authority prepares a planning proposal that applies to land within the Sydney drinking water catchment.	The site is not within a Sydney drinking water catchment area as provided in mapping associated with State Environmental Planning Policy (Sydney Drinking Water Catchment) 2011.
5.8 Second Sydney Airport: Badgerys Creek Planning proposals must not contain provisions that enable the carrying out of development, either with or without development consent, which at the date of this direction, could hinder the potential for development of a Second Sydney Airport.	The development of the site will not hinder the development of a second Sydney Airport.
5.10 Implementation of Regional Plans	The PP is consistent with the Illawarra-Shoalhaven Regional Plan 2015 as above.
6.1 Approval and Referral Requirements	The proposal does include provisions that will require concurrence regarding development adjacent to a state road and rail line. The creek re-alignment and development within areas designated under the draft SEPP (Coastal Management) 2016. These concurrences are likely to be required.
6.2 Reserving Land for Public Purposes	The proposal includes provisions and dedication of land for public services and facilities.
6.3 Site Specific Provisions	No site-specific provisions are proposed.

Section C – Environmental, Social and Economic Impacts

Q7 Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal.

A Flora and Fauna Assessment was undertaken by Eco Logical Australia (refer to **Appendix F**) in order to provide a description of the biodiversity values of the site and the impact and proposed outcomes associated with the PP.

Forest Red Gum Thin-leaved Stringybark Grassy Woodlands was found to occur as two patches in the study area, comprising approximately 1.28ha. However, these woodlands were identified as being in a highly modified and disturbed with high levels of exotic species, planted non-local native species and modified landforms present. This vegetation community forms part of the 'Illawarra Lowlands Grassy Woodland in the Sydney Basin Bioregion' which is an EEC listed under the BC Act. The proposed rezoning would retain and restore a majority of this EEC.

One flora species listed under the EPBC Act and BC Act, *Eucalyptus scoparia* (Wallangarra White Gum), was identified within the study area, where approximately eight individuals of this species were observed in a row

adjacent to an internal road. No other threatened flora species were recorded during either of the site inspections and given the limited habitat available and its highly degraded condition, no other threatened flora these species are considered likely to occur.

All vegetation communities across the site were identified as having been subjected to moderate to high levels of disturbance including vegetation clearing/thinning of the canopy layer, infestation of exotic species and modification to the landform and soil profiles.

In regards to fauna, one threatened fauna species, *Pteropus poliocephalus* (Grey-headed Flying-fox) was identified in the study area during surveys. The Grey-headed Flying-fox is listed as vulnerable under the BC Act and EPBC Act and was identified as occupying the patch of Forest Red-gum Thin-leaved Stringy Bark Grassy Woodlands at the southern extent of the study area, adjacent to the dam and existing riparian corridor. The camp was estimated to support 150-250 individuals. The proposed rezoning will retain the majority of the Grey-headed Flying Fox habitat. The proposed revegetation of the riparian corridor along the western extent of the study area would provide additional habitat for this species.

The proposed rezoning will result in the re-alignment of the riparian corridor to provide an improved environmental planning outcome. The corridor would be realigned along the western extent of the site and would be revegetated and restored as part of the works, providing improved riparian zones and additional habitat on site. The realignment is unlikely to significantly impact any ecological values given the following:

- » the creekline has a history of disturbance relating to the previous use of the land as a cokeworks
- » the corridor is dominated by exotic flora species
- » the corridor bed and banks are degraded

Conclusion

The assessment concludes that the PP would not impact any EEC, flora or fauna species. All EECs will be retained and enhanced. All areas of significant biodiversity value are proposed to be retained.

Q8 Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed?

Additional environmental investigations were undertaken to inform the PP. The following table provides a summary of the key findings of these investigations.

Table 13 Environmental investigation area and outcome of findings

Environmental Factor	Site Implication
Contamination and Remediation	Site investigations have revealed minimal contamination considering the former heavy industrial uses. A Remediation Action Plan by Arcadis (May 2017) was completed for the site pre-Gateway. The investigations summarised in the report revealed that only six soil samples that exceeded acceptable levels of human health assessment criteria considering future residential and open space use. Three locations are contaminated with asbestos, two with lead and one with TEQ BaP. The locations are isolated in nature and will require delineation sampling to determine the lateral extent prior to remediation.
Geotechnical	Douglas Partners concluded that all geotechnical conditions can be managed to facilitate development. As part of future investigations, an earthworks methodology will need to be prepared to ensure the reuse of existing site soils and optimised earthworks and procedures. Considerations in redevelopment of the site will include: <ul style="list-style-type: none"> » uncontrolled fill assessed and processed for suitability for redevelopment » further test and management of any Acid Sulphate Soils in earthworks, design and construction

Environmental Factor	Site Implication
	» minor constraints relate to erosion potential of disturbed materials, localised waterlogging, localised abnormal moisture content within the coke production area, sodicity and soil aggressiveness, which can be effectively handled during the earthworks design.
Bushfire	The site is not identified as bushfire prone.
Overland flow and riparian corridors	A flood study undertaken by Cardno concludes that flooding issues can be effectively managed and that development of the site will not increase flood impacts upstream of downstream of the site. Proposed creek realignment has been designed to convey major flows up to PMF event within North Corrimal Creek through the site. This results in total flood free developable areas of 12.5 ha.
Heritage	The Heritage Report undertaken by Urbis confirms that there are no existing statutory heritage listings on the site, nor in the vicinity of the site. The report recommends a reduced heritage curtilage which enables significant heritage elements of the site to be retained in situ and/or interpreted. It is recommended that these individual elements and or curtilage be incorporated as items of local significance within Schedule 5 of the WLEP 2009
Visual	A visual impact analysis was performed by e8urban pre-Gateway to inform typology and building heights in the concept plan. The view to the Illawarra Escarpment from development to the east of the subject site was considered of primary importance. As a result, low building heights have been suggested for the southern extent of the R3 zone, while a more substantial building height is provided for closer to the rail station.
Traffic serviceability	The Traffic and Transport Assessment revealed that the development's traffic impacts of significance are isolated to the intersection of Memorial Drive/Railway Street and that an upgrade to this intersection is required to mitigate the development's impacts. These upgrade works are proposed to be undertaken as part of the development.
Servicing	» ADW Johnson have assessed that the site is capable of being serviced with gas, electricity, sewer and NBN telecommunications » Re-alignment of the sewer line on the site is likely to be required to facilitate the creek re-alignment » Consultation with various providers is underway to optimise servicing arrangements
Mining and mining subsidence	Review of available mine subsidence maps indicates that the site is not located in a mine subsidence. To date there has not been any underground mining in the area.

Q9 Has the planning proposal adequately addressed any social and economic effects?

Economic Effects

The urban development of Corrimal is forecast to generate \$761M in additional economic activity (Hill PDA). Construction and indirect jobs will be supported during the delivery of the project. The relevant social and economic effects of the proposed development include:

- » The provision of contributions towards social and community infrastructure within the Wollongong LGA
- » Location in proximity to current transport services

- » Building capacity within the local community to increase housing and lifestyle diversity, employment, economic viability, social activity and opportunity.

A detailed assessment of the economic impacts of the PP was undertaken by Hill PDA pre-Gateway. Hill PDA assessed the land as not being viable for on-going industrial purposes due to the high cost of remediating the land, compared to the return of investment for industrial purposes. Hill PDA has also outlined the large supply of better located land for industrial purposes readily available in the Illawarra.

Redevelopment of the site will provide economic benefits in terms of jobs and value added. While the R3 Medium Density zoning does not specify employment-generation as a specific intended use, and the development will be primarily residential, there are a number of permitted uses and proposed additional permitted uses that are expected to generate employment within a 200m location of Corrimal train station.

Key permitted uses that are likely to generate jobs are:

- » Neighbourhood shops (but not large-scale retail which would undermine the Corrimal town centre)
- » Food and Beverage Premises
- » Child Care Centres
- » Seniors Housing

Urbis has undertaken an Economic assessment post-Gateway which concludes the following:

- » The proposed 1,550m² of retail, comprising a small supermarket and supporting dining and convenience-based retail, will be well supported by market demand, driven by strong on-site population growth
- » The modest retail offer is forecast to achieve turnover in 2024 that reflects:
 - > A minor market share of retail spending (2% of main trade area and 9% of primary trade area)
 - > Only a portion of the expected growth between 2018 and 2024 (25% of primary trade area growth and 14% of main trade area growth)
- » Residual spending and spending growth will continue to support existing and proposed retail centres.
- » The proposed retail at the subject site will also have significant benefits relating to providing valuable amenity for current and future residents of the immediate area, and employment, consumer and economic value benefits for the local region and state economy.

Social Effects

The PP has the following intended social effects:

- » Delivery of approximately 700 - 750 dwellings with excellent access to public transit, providing quality options for people to commute either to Sydney or the Wollongong and knowledge employment areas surrounding Wollongong University
- » Provide a diversity of housing, particularly under-represented housing typologies such as semi-detached and townhouse products, to meet affordable price points for residents in the Illawarra
- » Provide neighbourhood and commuter services to deliver convenience for residents and commuters (e.g. neighbourhood shops, childcare facilities, food and beverage premises) promoting the use of public transit
- » Deliver improved connections through the site to encourage active transport to and from Corrimal train station
- » Provide a public plaza at Corrimal station to provide a forum for social activity and celebrate the heritage of the site
- » Capitalise on the proximity to health, education, recreational and community resources near the site, including Wollongong Hospital that is undergoing an upgrade, schools within walking distance that are believed to have additional capacity, and the significant community resources such as Corrimal pool and library nearby.

Section D - State and Commonwealth Interests

Q10 Is there adequate public infrastructure for the Planning Proposal?

A Utility Services Report was prepared by BG&E (refer to **Appendix S**). The report confirms that all utility services are available on the site, noting the following:

- » Potable Water, Gas and Telecommunication networks require no augmentation to service the overall development;
- » No funding or construction of extensive feeder infrastructure will be required for electrical servicing, with only minor augmentations to the existing Endeavour Energy network envisaged; and
- » After further consultation with Sydney Water and a comprehensive MOUSE Modelling process, it has been confirmed that the wastewater network will be able to accommodate the full development yield. The wastewater servicing strategy involves the provision of additional storage capacity by upsizing the existing trunk main through the site, which will be undertaken in conjunction with a realignment of that trunk main during the bulk earthworks phase.

The likely infrastructure upgrade requirements are summarised in **Table 14** below.

Table 14 Infrastructure requirements

Infrastructure requirements	
Electricity	<p>Endeavor Energy (EE) estimated that the load of the overall development is approximately 2.6MVA (Mega Volt Amp) based on 3.5kVA (Kilo-Volt-Amp)/unit Average Daily Maximum Demand (ADMD) for apartments.</p> <p>Review of the existing electrical network shows that there are two 11kV feeders (CR1228 and CR1280) located opposite the development site along Railway Street, however both of these feeders have been estimated to have a total available capacity of approximately only 0.5MVA – not sufficient to accommodate the development site. However, EE noted that the overall development can potentially be serviced by Feeder RV1206 from Russell Vale Zone Substation (ZS) which is deemed to have the required available capacity to accommodate the mature load of the development.</p> <p>It is envisaged that HV linkage points will be Underground to Overhead (UGOH) terminations on feeder CR1280, with one in both Railway Street and High Street. Details surrounding HV linkages and network configuration will be confirmed when a firm application for load is submitted to EE.</p>
Telecommunications	<p>Design It Telco Pty Ltd was consulted about the capacity of existing NBN and Telstra networks on 9 November 2018, which confirmed:</p> <ul style="list-style-type: none"> » Current NBN and Telstra infrastructure could handle the overall development (750 dwellings); » As the development is over 100 lots it will automatically qualify for fibre to the premises (FTTP); and » NBN Co will supply a backhaul cable to cater for future lots. <p>It has been confirmed that the future development does not require any upgrades to telecommunications networks.</p>
Gas	<p>Jemena Ltd confirmed that there are suitable gas mains located on Railway Street within the vicinity of this proposal which currently have adequate capacity to service the overall development at this time.</p>
Water	<p>Sydney Water was consulted at a meeting dated 20 December 2017, regarding servicing the proposed development with potable water and the associated impacts</p>

Infrastructure requirements

	<p>on existing SWC potable water infrastructure as a result of the increased potable water network demand imposed by the proposed development.</p> <p>A letter from SWC dated 8 March 2018 confirmed that the existing DN200 CICL potable water main fronting the development along Railway Street has sufficient capacity to accommodate the proposed development.</p>
Sewer	<p>Results and findings from modelling and associated consultation with SWC has determined the existing Sydney Water wastewater network will be able to accommodate the proposed development upon implementation of planned network upgrades and adjustment and upsize of the on-site wastewater trunk main.</p>
Roads	<p>Modelling and impact assessment by Bitzios has revealed that the development's traffic impacts of significance are isolated to the intersection of Memorial Drive/Railway Street and that an upgrade to this intersection is required to mitigate the development's impacts. The upgrade involves:</p> <ul style="list-style-type: none"> » one of the two departure lanes on the eastern and western legs of Railway Street reassigned as third approach lane (i.e. three approach lanes in each direction, one right turn pocket, one through lane and one left turn lane); » extending the right turn pocket length in the southern approach from 95m to 150m; and » changing the signal phase times of the intersection to better flows and align with the geometrical changes.
Rail	<p>Sydney Trains – Transport NSW was consulted regarding the existing high voltage 33kV overhead electrical network (Feeder 787) located outside of the eastern boundary of the proposed development, traversing Corrimal Station carpark.</p> <p>It was confirmed that Feeder 787 does not currently have an easement, as the station carpark and access road is owned by Railcorp. However, it was noted that the electrical poles were previously located against the development boundary but were relocated to their current alignment many years ago, with the old easement (Y254942) being rescinded on gazette notification.</p> <p>It is not envisaged that this electrical feeder will pose any significant constraint to rezoning and development of the site.</p>

Q11 What are the views of state and Commonwealth public authorities consulted in accordance with the Gateway determination

Consultation with DP&E

A meeting was held with Council and DPE to discuss and obtain comment on the planning provisions and the achievement of the studies outlined in the Gateway.

Consultation with RMS, Railcorp and TfNSW

Consultation with RMS and TfNSW was undertaken on 27th November 2018 and 6th February 2019 in order for input to be provided on the indicative concept plan, with a particular focus on vehicle access, level crossings and pedestrian/cycle connections.

Consultation with Council

Consultation with Council has occurred on a regular basis, with meetings held every 2 – 4 weeks at Council's offices. The aim of these meetings has been to progress the development, obtain comments and input, and discuss and draft the new Corrimal Site Specific DCP.

Part 4 – Mapping

Part 4 presents clear and accurate mapping depicting relevant aspects of the PP. The subject PP seeks amendments that relate to land use zoning, height of buildings, minimum lot size, natural resource sensitivity and acid sulfate soils map, riparian land map & foreshore building line mapping.

Thus, the mapping amendments of the WLEP 2009 required are listed below:

- » Land zoning
 - > Sheet LZN_024
- » Minimum lot size
 - > Sheet LSZ_024
- » Height of Buildings
 - > Sheet HOB_024
- » Floor Space Ratio
 - > Sheet FSR_024
- » Heritage
 - > Sheet HER_024
- » Natural Resource Sensitivity - Biodiversity
 - > Sheet NRB_010
- » Acid Sulfate Soils Map Riparian Land Map Foreshore Building Line Map
 - > Sheet CL1_024

The proposed WLEP map amendments are identified in **Figures 16 – 22** below. The maps will be provided in the format prescribed by the technical guidelines for LEP maps published by the DP&E for exhibition purposes as required.

Figure 16 Proposed Land Zoning Map

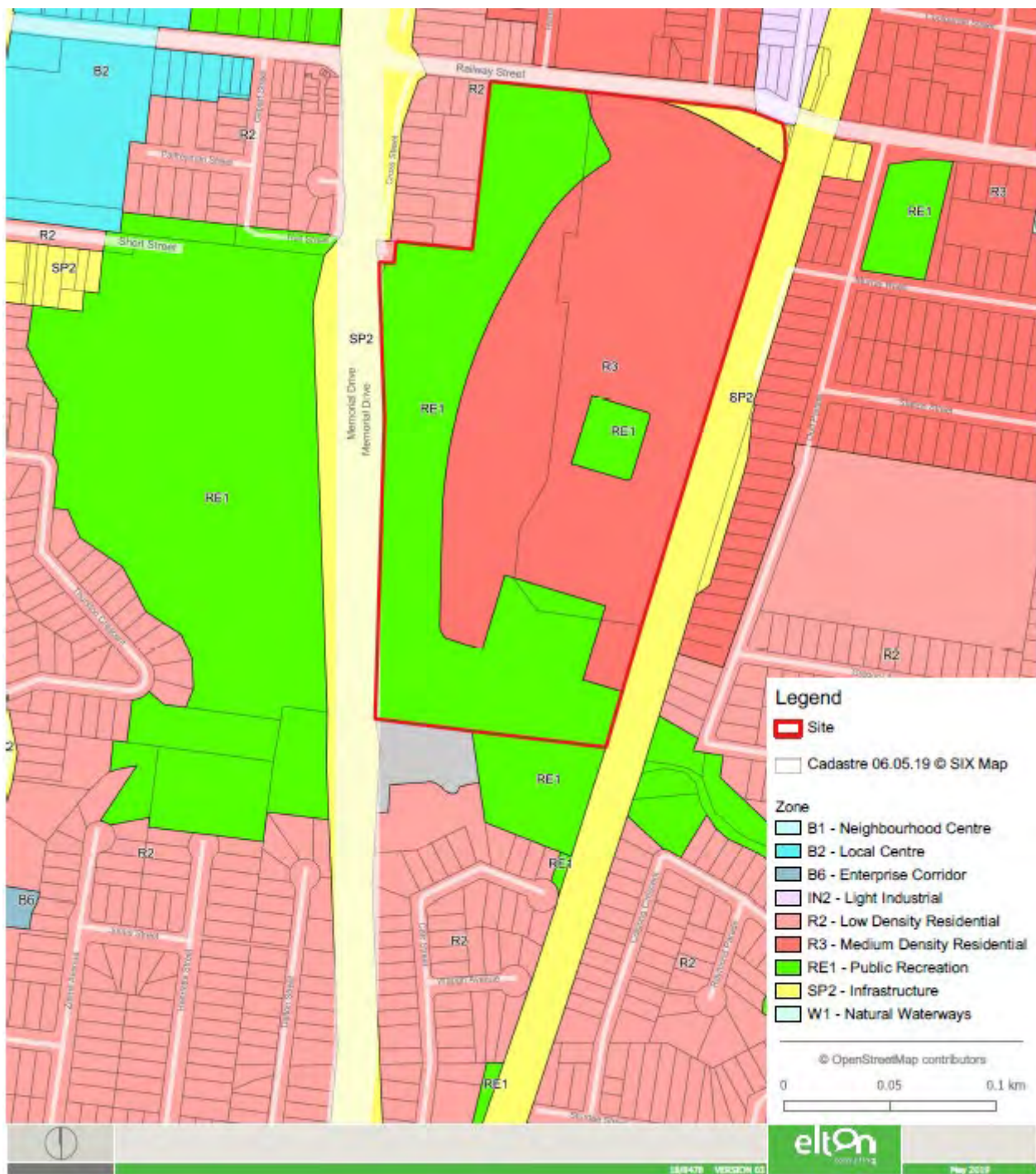


Figure 17 Proposed Minimum Lot Size Map

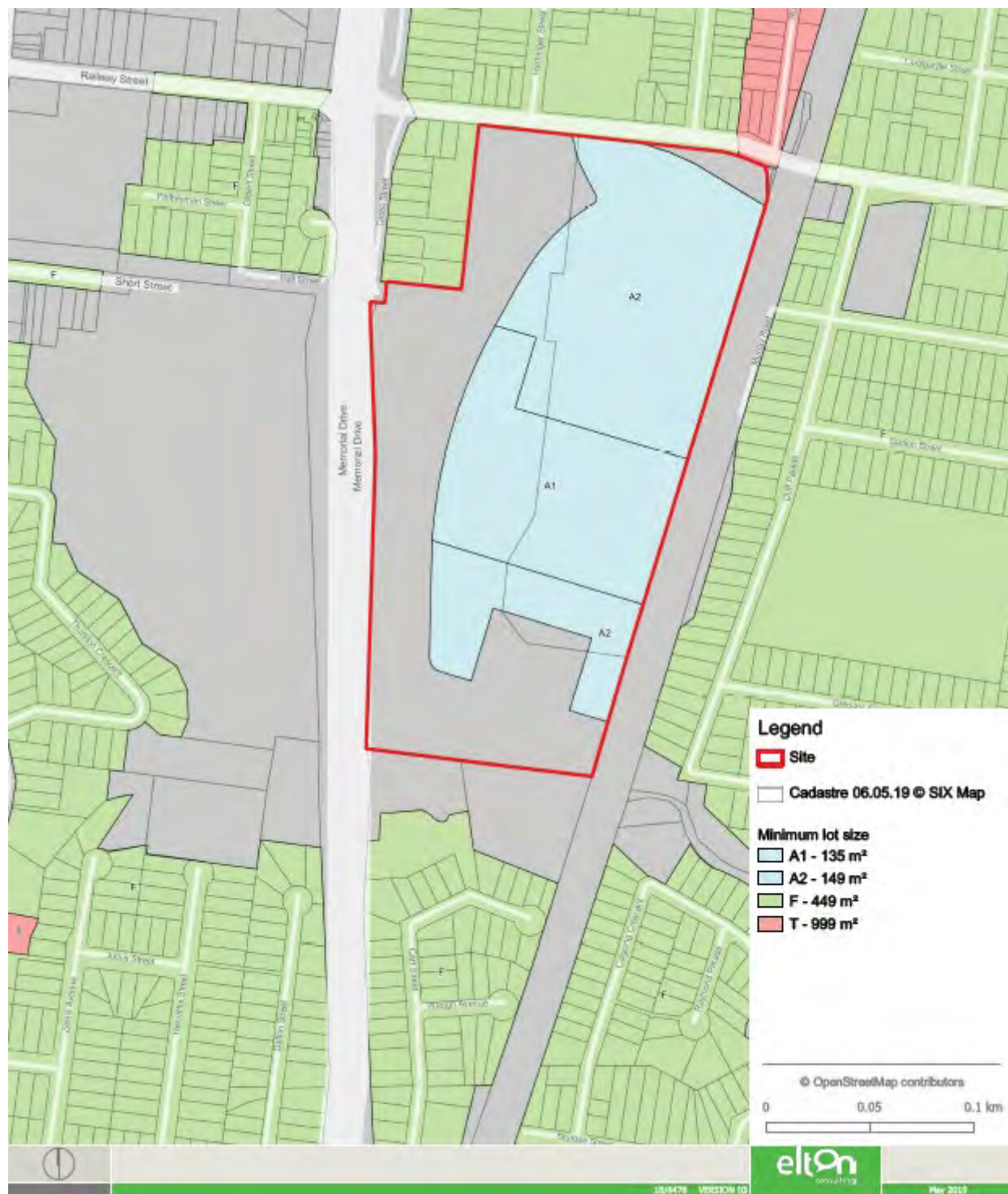


Figure 18 Proposed Height of Buildings Map

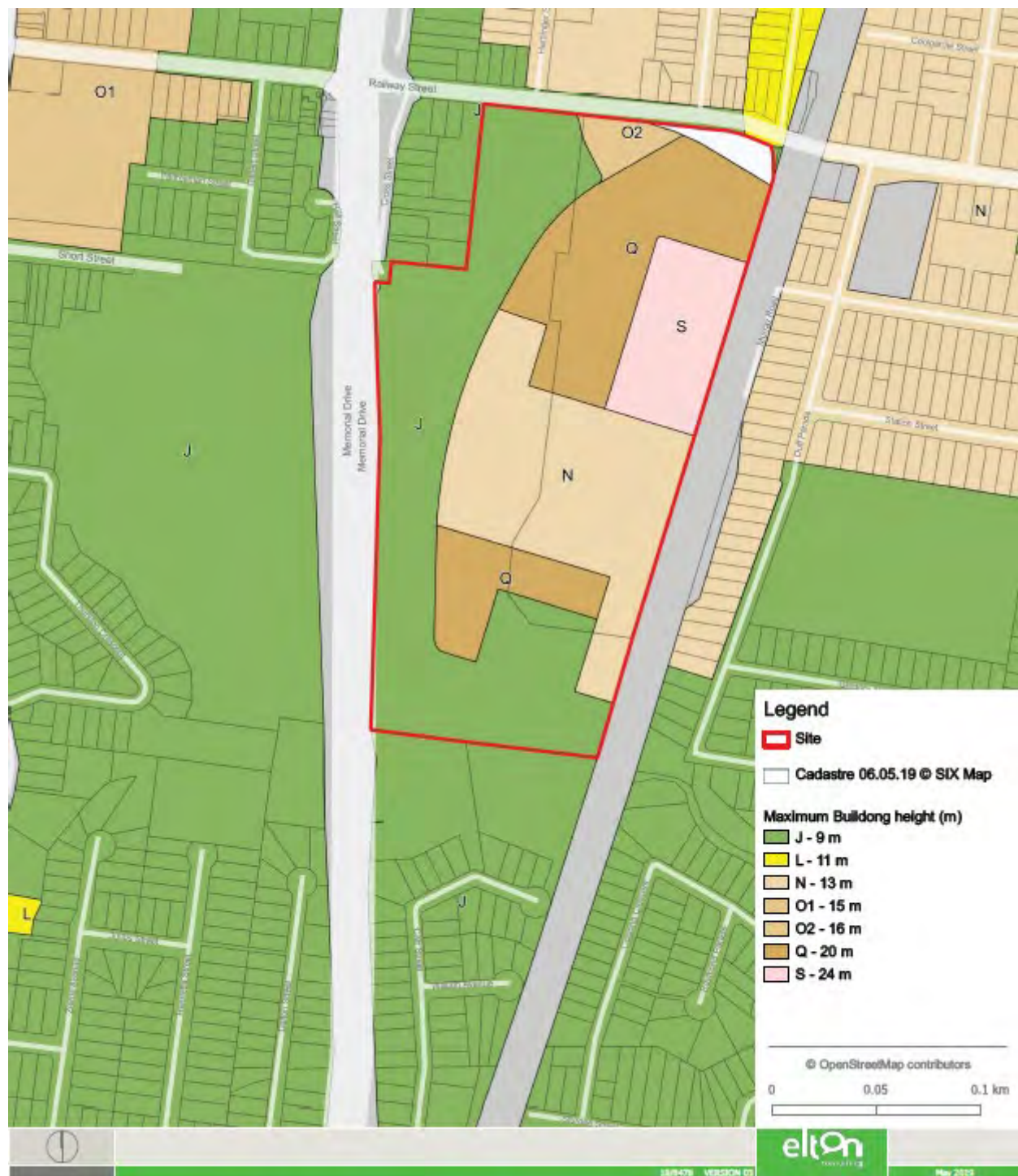


Figure 19 Proposed Floor Space Ratio Map



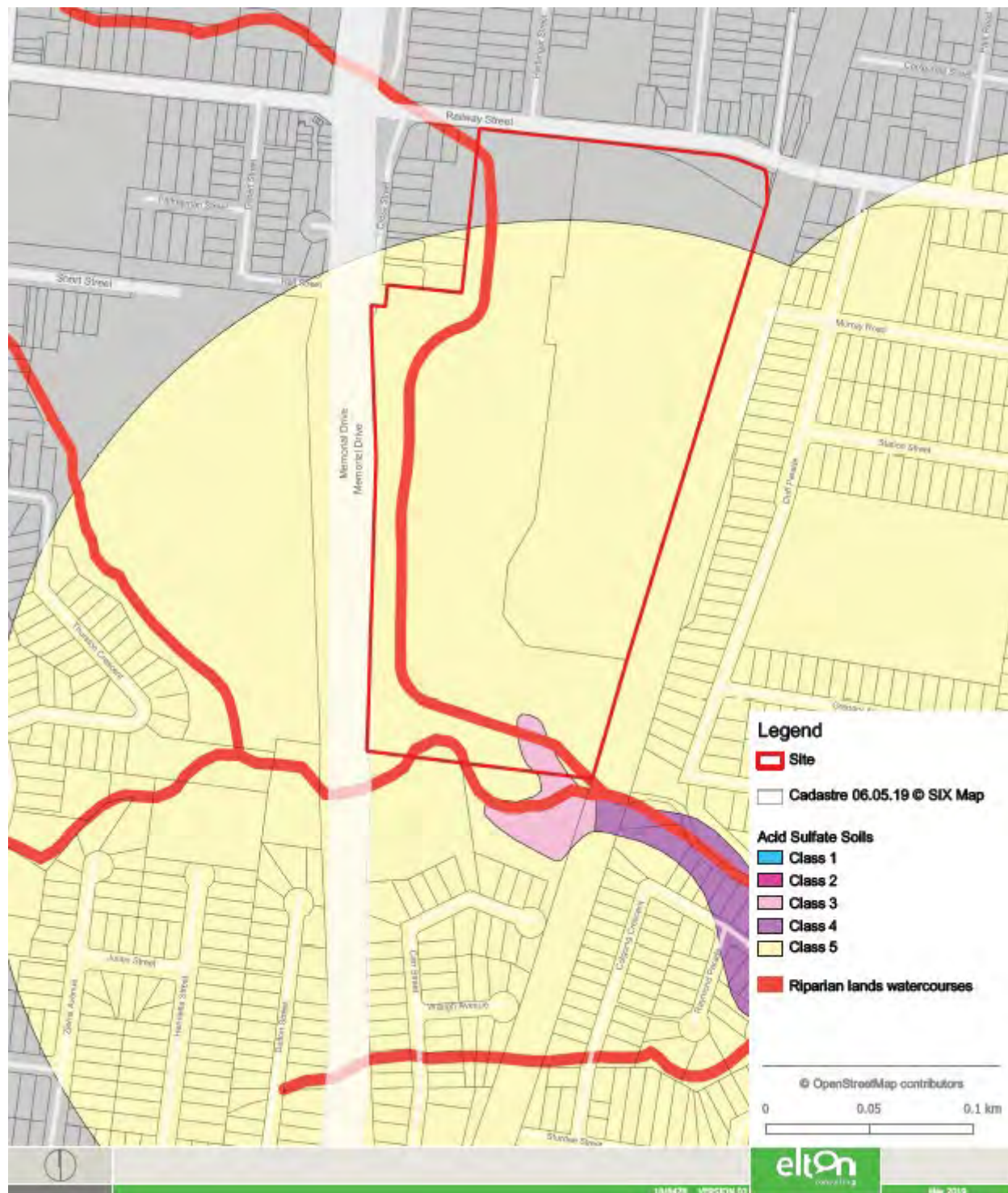
Figure 20 Proposed Heritage Map



Figure 21 Proposed Natural Resource Sensitivity – Biodiversity Map



Figure 22 Proposed Acid Sulfate Soils Map, Riparian Land Map, Foreshore Building Line Map



Part 5 – Community Consultation

Legacy Property and ICC are committed to transparent and proactive communication with the communities in and around this project to ensure all parties are informed about the plans and have the opportunity to have their say.

At Corrimal, this has meant:

- » Being open, clear and sensitive in all dealings with directly affected landowners, stakeholders and the community
- » Making the greatest possible effort to ensure community members are aware of the proposal
- » Providing opportunities for participation and collaboration

Initial community consultation commenced 5 years ago with ICC holding workshops with key community groups recommended by WCC including Neighbourhood Forum 4, the Corrimal Chamber of Commerce and the Corrimal Revitalisation Action Group. A booth was also set up at the Spring into Corrimal 2015 event to answer questions raised by the public. In addition a dedicated website was launched in 2017 (www.corrimalcokeworks.com.au) to provide information on the project and to answer specific questions from the public. Community newsletters were also produced and distributed to adjacent neighbours approx. 1000.

Since obtaining Gateway approval, ICC and Legacy Property have conducted a range of more formal consultation activities as outlined below with a specific focus on residents immediately adjoining the rezoning area and in surrounding areas.

Consultation has also occurred with the Registered Aboriginal Parties.

5.12 Statutory consultation requirements

Sections 56 and 57 of the EP&A Act sets out statutory community consultation requirements for PPs. It is envisaged that, at a minimum, this will involve the public exhibition of the PP and supporting information for a period of 28 days. Council has previously resolved to exhibit the PP for 60 days. Notification of the PP will likely occur:

- » On Wollongong City Council's website;
- » In the applicable local newspaper(s);
- » In writing to the owners of adjacent and nearby land parcels, relevant stakeholders and neighbourhood/community groups and the surrounding community in the immediate vicinity of the site; and
- » In writing to relevant Government agencies.

5.13 Consultation workshops

In December 2018, ICC and Legacy Property held a series of visioning workshops with key stakeholders from Council, Neighbourhood Forum 4, Corrimal Community Action Group (CCAG), Corrimal Region Action Group (CRAG) and Corrimal Chamber of Commerce. This followed the issuing of a Gateway Determination in August 2018.

ICC and Legacy Property advised participants in December 2018 that the project team would consider the feedback received and other outputs of the sessions and come back to each stakeholder group by the end of March 2019 to detail how their feedback had been incorporated into detailed development of the proposal.

These follow-up sessions, as well as information sessions for the broader community, were held in late-March 2019. The information session invitation was letterbox dropped to 1000 residences in the surrounding area of the site and was shared with key stakeholders who in turn shared with their networks. Digitally, the invitation was

emailed to all those who had registered interest in the project online and was further posted on the official website as a pop-up box as well as a static notice on the homepage.

As well as the information sessions, pop ups were held on Friday and Saturday 30/31 March 2019 in order to ensure a broad cross section of the community had the opportunity to provide their views on the proposed development.

The scale of that consultation has significantly exceeded what is mandated for a rezoning process, demonstrating a genuine commitment to an inclusive and transparent process that meaningfully engages the community and key stakeholders.

A consultation outcomes report can be found at **Appendix D**.

5.14 Additional consultation methods

Other methods of consultation ICC and Legacy Property have undertaken include:

- » Regular distribution of newsletters to 1,000 residences in the area. Eight newsletters were distributed between October 2017 and December 2018.
- » Developing a website for the project, which publishes key information about the project including:
 - > A basic outline of the rezoning proposal
 - > An overview of the suitability of the site for rezoning
 - > Public benefits of the rezoning including
 - Assisting to resolve flooding in the local area
 - New and improved connectivity
 - Better neighbourhood and commuter services
 - Diversity of housing
 - > A frequently asked questions page including short technical issues summary e.g. remediation etc
 - > A 'provide feedback' page.

5.15 Response to Submissions

A number of submissions have been received by Council (and provided to the Applicant) since the Gateway determination was made, in response to the proposed development. These submissions are summarised and responses provided in **Table 15** below.

Table 15 Residents/stakeholder Concerns and Responses

Concerns	Response
Road Network	
Traffic	
Increased traffic	<p>The proposed development would result in an increase in traffic, however the traffic assessment undertaken by Bitzios indicates that any significant traffic impacts are isolated to the intersection of Memorial Drive/Railway Street and that an upgrade to this intersection is required to mitigate the development's impacts.</p> <p>ICC and Legacy propose to upgrade this intersection in accordance with this recommendation.</p>

Concerns	Response
A Master Traffic Plan is required	A Traffic Impact Assessment has been prepared for the proposed development by Bitzios Consulting.
Engagement with RMS should occur to examine mitigation strategies for the Memorial Drive/Railway Street intersection	Consultation with RMS and TfNSW has occurred on an ongoing basis. The Memorial Drive/Railway Street intersection will be upgraded in accordance with the recommendations made by the Traffic Impact Assessment.
RMS need to undertake a fully independent assessment that's available to the public	RMS has been consulted and will continue to be consulted regarding this project. Any recommendations which are made by RMS or TfNSW will be incorporated into the proposed development scheme.
Traffic generation should be calculated on dwellings types with height traffic generation	Traffic generation has been based on dwelling types, including terraced/semi-detached, 1, 2 and 3 bedroom apartments, Seniors Living apartments, affordable housing dwellings and Aged Care beds.
The intersection of Memorial Drive and Station Street data for SIDRA is from 2014 and an inaccurate baseline	The data utilised for traffic modelling is considered to be the most accurate and up to date data available.
Distribution at the junction of Harbinger Street and Railway Street should be included in traffic distribution	Traffic distributions at the intersection of Railway Street/Harbinger Street are assessed as part of the Traffic Impact Assessment
Parking	
Not enough parking	Adequate parking will be provided on site in accordance with the requirements of the Site Specific DCP.
Undercroft parking is unsuitable, will act as a heat sink, reduce panting area, attract vandalism, prefers basement parking.	The parking typologies included onsite will form part of the later DA stage. However, any proposed development will be supported by a Crime Prevention Through Environmental Design (CPTED) report in order to determine the crime prevention features to include in the proposed development.
Access	
Direct access to memorial drive should be provided	Consultation has been undertaken with RMS and TfNSW to discuss the possibility of access from Memorial Drive, however this was not supported. It was also tested in modelling and shown to have very limited benefit.
The development proposes limited road access	Road access from Memorial Drive was discussed with RMS and TfNSW but was not supported. Therefore, one access from Railway Street is proposed.
There needs to be a turning lane into the site	Traffic assessment undertaken has demonstrated the need for a proposed right-hand turn from the west to turn into the site. All proposed road upgrades are outlined in the traffic assessment report by Bitzios.
More points of ingress and egress are required	See above.
Access for emergency services will be difficult due to increase traffic	An adequate access route will be provided for emergency services.
Community	
Community Facilities	

Concerns	Response
Increased pressure on public services such as hospitals and schools	An assessment has been undertaken and the conclusion made, in consultation with Council, that there is no need to provide for any social infrastructure on the site. The Department of Education was contacted, with the outcome that there is likely to be adequate classrooms for the future volume of children resulting from the proposed development.
Should contain more facilities for the broader community	As stated above, Council has determined that there is no need to provide for any social infrastructure on the site. However small scale retail land uses are proposed surrounding Corrimal train station, which will be accessible to the future population of the site as well as the broader community.
Demographics	
The development will attract a younger demographic and this is going to have a negative effect on the current elderly demographic	The proposed development is to provide for a diversity of housing and a diversity of demographics in order to create an inclusive community linking with the existing surrounding Corrimal community.
Environment	
Flooding	
Making sure the development does not increase the risk of flooding downstream	The Flood Study demonstrates that flooding issues can be effectively managed and that development of the site will not increase flood impacts upstream or downstream of the site.
How will waterways be measured and monitored for water levels and contamination ongoing	The waterways will be monitored during creek alignment works. Refer to below regarding contamination.
Too many hard surfaces reducing rainfall absorption	See above.
Creek should remain following natural course.	<p>The existing creekline and riparian zone has been subject to high levels of modification and disturbance associated with the cokeworks operations.</p> <p>The proposed creek realignment has been designed to convey major flows up to PMF event within North Corrimal Creek through the site. This results in total flood free developable areas of 12.5 ha. The proposed creek works also provide an opportunity to improve drainage of the existing Cross Street residential area.</p>
Ecology	
Coke works one of the last natural areas in the suburb and should not be taken away	The site has previously been used for industrial purposes and therefore is not considered to be a natural area. All areas of the site which demonstrate biodiversity significance are proposed to be retained.
Tree canopy should remain and mature trees should be kept	As stated above, all areas which demonstrate biodiversity significance are proposed to be retained.
Lack of green space	Approximately 8.6ha of the site are proposed to be retained as public open space.
Protection of bat habitat	The proposed rezoning will retain the majority of the flying fox habitat present on the site. The proposed revegetation of the

Concerns	Response
	riparian corridor along the western extent of the site would also provide additional habitat for this species.
Riparian zone along the creek should be retained	The riparian zone to the east of the site, adjoining the realigned creek, will be retained.
Retain ponds for the ecology	The onsite dams were not found to contain any threatened species or threatened species habitat.
Contamination	
Concerned about contamination being properly assessed and dealt with	<p>Only 5 incidences of concentrations of contaminants of concern were detected above environmental assessment criteria, however these were isolated and minimal compared to the wider site footprint.</p> <p>Through integration of the remedial strategies outlined in the RAP by Arcadis into the bulk earthworks, civil design and construction stages of the project, the site can be suitable for the proposed use.</p> <p>Any site contamination will be dealt with through implementation of the RAP.</p>
Independent contamination report must be done	A Preliminary Site Investigation, Additional Environmental Assessment and a Remediation Action Plan have all been prepared to support this PP.
Water testing needs to be conducted	Water quality treatment will be required on site. The requirement will be fulfilled using either catchment wide 'treatment drains' or end-of-line approaches. Stormwater quality treatment measures will be incorporated into the site design, for example, on public open areas or on road reserves
Heritage	
There should be a museum on site for both aboriginal and industrial heritage of the site	Elements relating to the industrial heritage of the site will be retained and incorporated into the overall development, with an opportunity to have a small part of the building or place to capture the history/heritage of the site.
Need to respect aboriginal heritage	Extensive consultation with the Registered Aboriginal Parties has been undertaken, and site visits held. Any works will be undertaken in consultation with the Groups. No significant items of Aboriginal heritage have been identified on site.
Built form	
Density	
Building height and density too high compared to surrounds. Should be low density village.	<p>The proposed density has been part of ongoing discussions with Council and is considered to be acceptable. The development is not proposed to extend beyond the height of the towers already present on site.</p> <p>The development, as proposed would result in revitalisation of the site and up to 750 additional residences, contributing to the housing needs of Corrimal and the wider Wollongong area. The development of a low-density village would be a substantial underutilisation of the 18.18ha site.</p>
Zoning	

Concerns	Response
The zoning should remain the same and the land should be employment land	Wollongong Council supported the PP and strategic merit for rezoning in 2018. A Gateway determination by DP&E also supported the strategic merit for rezoning the site. Technical studies have been undertaken to demonstrate the suitability of the site for residential land uses. Thus, the land will not be retained for employment. Rather the key heritage items and interpretation of the employment site will be undertaken.
Need retail included	Small scale retail land uses have been included within the proposed development, surrounding Corrimal train station.
It should be a mixed used zone	See above. Although the zoning is for R3 Medium Density Residential, the site is proposed to accommodate some small-scale residential land uses surrounding Corrimal train station.
Detailed Design	
Development should only take place on the dumpsites and cleared areas	The proposed development will be located on areas which have been previously cleared and were previously utilised for industrial activities. Minimal clearing of areas of ecological value is proposed.
Apartment size is too small	All future apartments will be designed to be compliant with SEPP 65 and the ADG.
The artistic impressions of the housing type is not detailed enough	The development is currently only at the rezoning stage. Additional detail regarding housing typologies will be provided at the DA stage.
Should be world class design	The proposed development is intended to be of a high-quality design.
Needs to be age friendly	Seniors housing and assisted living are elements which will be incorporated into the proposed development.
Green development	
Needs to include bike paths and improve pedestrian pathways	The proposed development will include a wide network of pedestrian and cycling connections, linking the site with the wider Corrimal community.
Building should be a green building, solar panel and water catching	The proposed development is proposed to include sustainable elements, however these will be decided at the future DA stage.
Views	
Not impact on residences view of escarpment	The proposed development intends to retain views to the escarpment by proposing heights which are lower than the heights of the existing brick and steel towers.
Planning process	
Planning process must be transparent	The PP application has been undertaken in accordance with all legislative requirements. The proponent has conducted consultation that exceeds legislative requirements.
All documents should be made publicly available	All documents will be made publicly available during the exhibition period.
An independent assessment of all reports should be undertaken	Council and the DP&E will undertake an assessment of the PP as well as all associated documentation.

Part 6 – Project Timeline

Milestone	Timeframe
Gateway Determination issued	20 th August 2018
Planning Proposal lodged with Council	May 2019
Public exhibition and public authority consultation	August - September 2019
Consideration of submissions	October 2019
Reporting the Planning Proposal to Council	December 2019
Submission to NSW DP&E	December 2019
Publication of WLEP 2009 amendment	March 2020

Appendices

- A Aboriginal Cultural Heritage Assessment
- B Additional Environmental Assessment
- C Consultation Outcomes Report
- D Economic Impact Assessment
- E Flood Study
- F Flora and Fauna Assessment
- G Geomorphology Assessment
- H Geotechnical Assessment
- I Heritage Conservation Management Strategy
- J Heritage Interpretation Strategy
- K Historical Heritage Assessment
- L Illawarra – Shoalhaven: Housing Market Report for Corrimal Coke Works
- M Landscape Master Plan Report
- N Master Plan Report
- O Noise and Vibration Assessment
- P Remediation Action Plan
- Q Remediation Action Plan Endorsement
- R Retail Market Demand and Economic Impact Assessment
- S Servicing Strategy Report Summary
- T Traffic Impact Assessment
- U VPA Schedule

A **Aboriginal Cultural Heritage Assessment**

B Additional Environmental Assessment

C Consultation Outcomes Report

D **Economic Impact Assessment**

E Flood Study

F Flora and Fauna Assessment

G **Geomorphology Assessment**

H Geotechnical Assessment

I Heritage Conservation Management Strategy

J Heritage Interpretation Strategy

K **Historical Heritage Assessment**

L Illawarra – Shoalhaven: Housing Market Report for Corrimal Coke Works

M Landscape Master Plan Report

N Master Plan Report

O Noise and Vibration Assessment

P Remediation Action Plan

Q Remediation Action Plan Endorsement

R **Retail Market Demand and Economic Impact Assessment**

S Servicing Strategy Report Summary

T Traffic Impact Assessment

U VPA Schedule



Corrimal Coke Works Corrimal

POST-GATEWAY MASTER PLAN REPORT
MAY 2019

REVISION B

LEGACY PROPERTY

10.05.2019

REPORT PREPARED BY
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ISSUE	DATE	PREPARED BY	REVIEWED BY	COMMENT
P1	21.03.19	NT	DR	DRAFT
P2	13.05.19	NT	DR	DRAFT
P3	15.05.19	NT	DR	DRAFT
A	17.05.19	NT	DR	FINAL FOR COUNCIL SUBMISSION
B	23.05.19	NT	DR	REVISED FINAL FOR SUBMISSION

05	Executive Summary
08	Introduction
18	Key Principles and Drivers
22	Master Plan Development
28	Proposed Master Plan
32	Structure Plan Design Components
38	Movement and Access
50	Built Form and Architecture



PROJECT TEAM

<p>OWNER & DEVELOPER</p> <div><p>LEGACYPROPERTY</p></div>	<p>URBAN DESIGN & ARCHITECTURE</p> <div><p>e8urban</p></div>	<p>LANDSCAPE & PUBLIC DOMAIN</p> <div><p>CLOUSTON associates</p></div>	<p>TOWN PLANNING & CONSULTATION</p> <div><p>elton consulting</p></div>	<p>HERITAGE & RETAIL</p> <div><p>URBIS</p></div>	<p>CIVIL & SERVICES</p> <div><p>BG & E</p></div>	<p>ECOLOGY</p> <div><p>eco logical AUSTRALIA A TETRA TECH COMPANY</p></div>	<p>TRAFFIC & TRANSPORT</p> <div><p>BITZIOS consulting traffic engineering • transport planning</p></div>	<p>PLACE VISIONING</p> <div><p>Frost*</p></div>
	<p>GEOMORPHOLOGY</p> <div><p>SOIL CONSERVATION SERVICE NEW SOUTH WALES</p></div>	<p>CONTAMINATION</p> <div><p>ARCADIS</p></div>	<p>ABORIGINAL HERITAGE</p> <div><p>Kelleher Nightingale</p></div>	<p>FLOODING</p> <div><p>Cardno</p></div>	<p>NOISE & VIBRATION</p> <div><p>RENZO TONIN & ASSOCIATES acoustic consultants</p></div>	<p>GEOTECHNICAL</p> <div><p>Douglas Partners Geotechnics • Environment • Groundwater</p></div>		

INTRODUCTION

Lead master planners DKO Architects have been commissioned by Legacy Property to prepare a precinct master plan (the Master Plan) for the former Corrimal Coke Works Site (the Site) in Corrimal NSW.

The 2019 Master Plan provides a vision for an exceptional new residential community with a unique public domain, great open spaces, new local retail services and an innovative mix of housing on this former industrial site.

The Site is situated in the Wollongong City Council (WCC) Local Government Area 5.5 kms north of the Wollongong CBD and 63.5 kms south of the Sydney CBD.

The Site is approximately 18 hectares in area, located between the Corrimal Railway Station on the Sydney Wollongong line to the East and the Major Arterial Road, Memorial Drive, to the West.

The Site is also within walking distance to Corrimal town centre (350 m), local primary and secondary schools (500 m) and community recreational facilities including sporting fields, Corrimal Pool, Corrimal District library and community centre (500 m) and Corrimal Beach (1.2 km).

PLACE AND MASTER PLAN VISION

The vision for Corrimal Coke Works is to create a strong community with diverse people and places that is built on shared stories and experience. This will be achieved by basing the development around the values of friendship, diversity, revitalising and heritage.

METHODOLOGY

The Master Plan is a progression of the 2017 Concept Plan that was developed to support a planning proposal for the rezoning of the Site.

The 2017 Concept Plan established the potential for the site to be rezoned from predominantly industrial uses to residential zoning. The 2017 Concept Plan was supported by a number of key technical studies that established design drivers including:

- The realignment of Corrimal Creek to the west of the Site to allow for a consolidation of developable land close to Corrimal Station.
- A strategy to retain some former industrial elements of the Site including the 1912 Brick Chimney Stack.
- The identification of ecological communities in the north and south of the Site.
- The opportunity to position local services around Corrimal Station.
- The opportunity to access the Site from Railway Street in the north.

The Master Plan has built on these drivers and has further developed the spatial plan for the Site with inputs from these specialist consultants and Gateway Determination requirements.

The project methodology followed a series of steps including stakeholder and Council workshops and an intensive period of design development and testing.

The Master Plan is heavily informed by the requirements of Wollongong City Council. Further studies will be required at every subsequent stage of development.

THE 2019 MASTER PLAN

Key features of the 2019 Master Plan include:

- An expanded plaza adjacent to Corrimal Station.
- Increased retention of former Coke Works Structures if high significance
- Refinement of the realigned North Corrimal Creek corridor to provide opportunities for active and passive recreation.
- Measures to enhance stormwater quality.
- A refined street network that facilitates bus access to Corrimal Station.
- An expanded area for local services around Corrimal station.
- A new local ‘park in the heart’ of the Site.
- The identification of a number of character precincts within the Site that respond to local conditions and the project vision.
- Further development of the built form strategy to allow for a diverse mix of innovative housing.
- An identification of the potential inclusion of seniors housing and affordable housing.
- A extensive network of active transport and pedestrian links to key destinations within and around the Site.
- Updated staging and implementation planning.

THE NEW PLANNING FRAMEWORK

The 2019 Master Plan is supported by a Site Specific Development Control Plan (DCP) and new Local Environmental Planning (LEP) controls for the Site to ensure that the vision for the Site is delivered in a coordinated manner with a high quality public domain and a diverse built form.

LANDSCAPE AND PUBLIC DOMAIN OBJECTIVES

Objective 1:

- Create a new public parkland along the realigned North Corrimal Creek.

Objective 2:

- Incorporate landscape edge vegetation to preserve key interfaces for surrounding residents

Objective 3:

- Retain and enhance key heritage structures associated with the former uses on the Site.

Objective 4:

- Enhance proposed Civic Plaza adjacent to Corrimal station.

Objective 5:

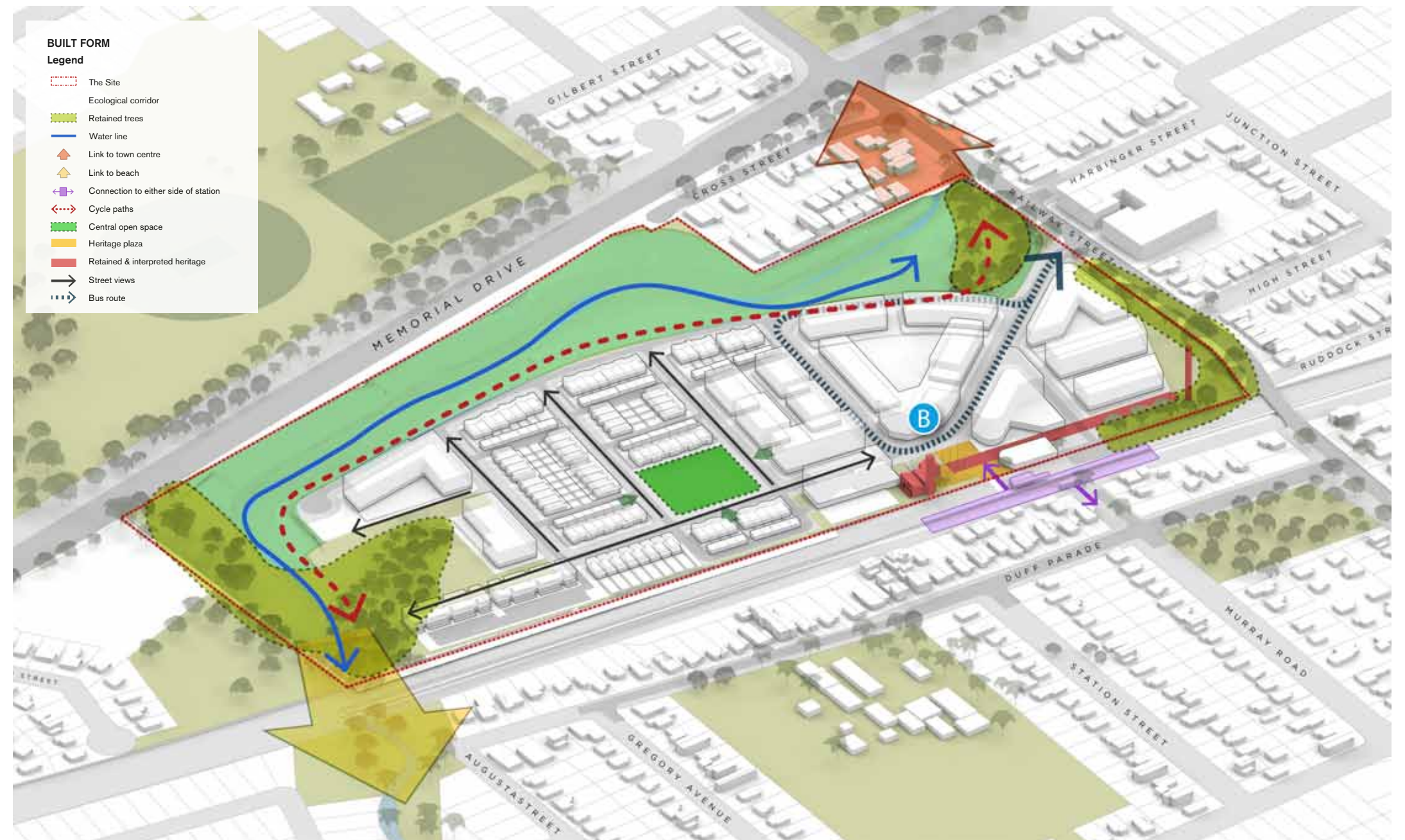
- Define a fine grain network of streets, lanes and pedestrian links

Objective 6:

- Frame internal views and vistas along new streets that connect residents to the extensive public open space network.

Objective 7:

- Create a system of cycle paths that link key destinations and connect to the wider network.



LAND USE AND BUILT FORM OBJECTIVES

Objective 1:

—Provide for a diverse range of building forms that respond to the particular location context and conditions.

Objective 2:

—Define the main access street with clear and consistent urban edges.

Objective 4:

—Focus the tallest buildings close to Corrimal station and plaza.

Objective 5:

—Integrate and re-use of retained structures with new development.

Objective 6:

—Locate new retail and services close to Corrimal Station.



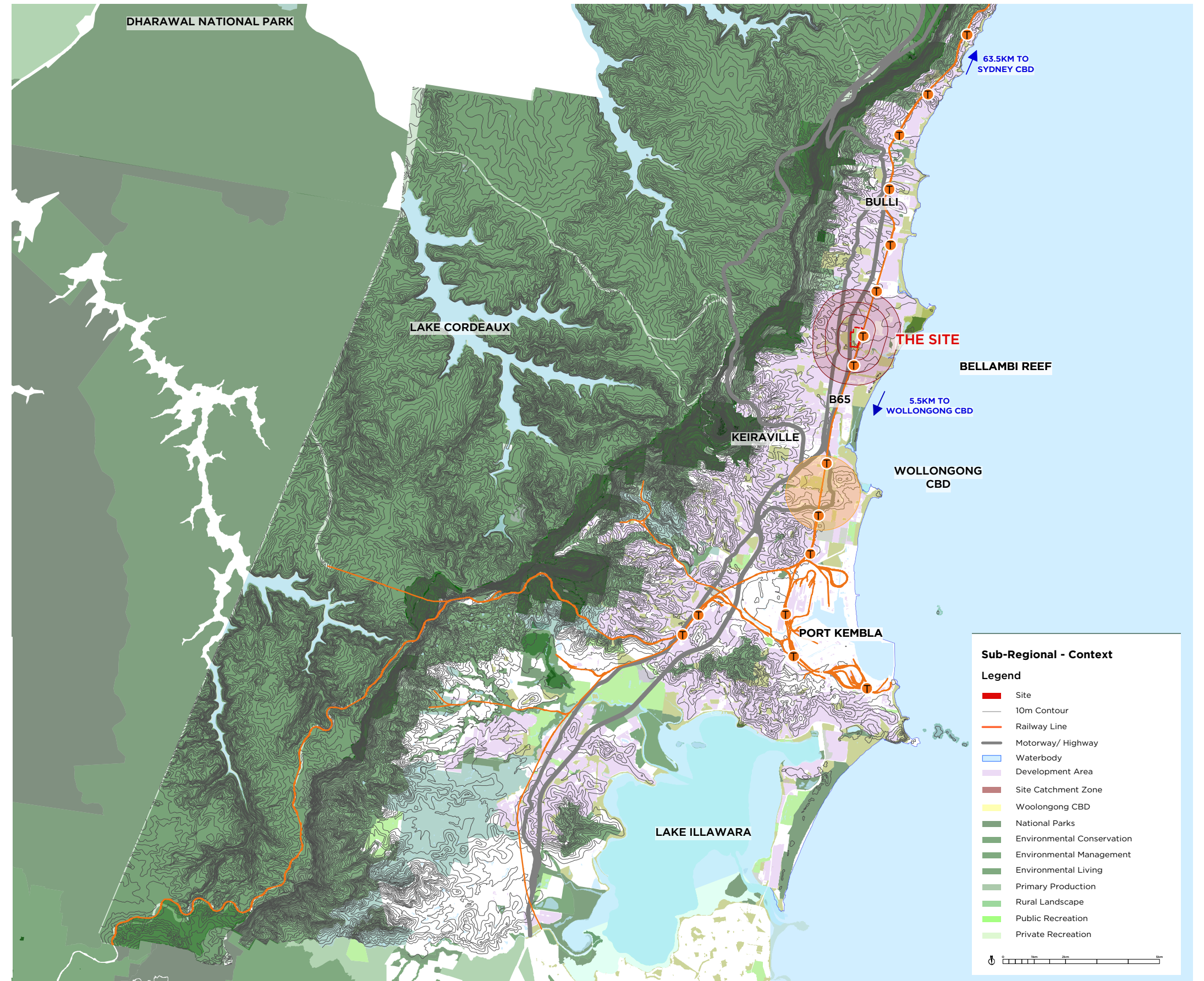


Introduction

CORRIMAL COKE WORKS WOLLONGONG

THE SITE AND RELATIONSHIP TO CORRIMAL AND WOLLONGONG

- The Site sits in the northern portion of the Wollongong City Council Local Government Area.
- Located 5.5 kms north of the Wollongong CBD and 63.5 km south of the Sydney CBD.
- Situated between Corrimal Railway Station on the Sydney Wollongong line to the East and the Major Arterial Road, Memorial Drive, to the West.
- Within walking distance to the Corrimal town centre (350 m), local primary and secondary schools (500 m) and community recreational facilities including sporting fields, Corrimal Pool, Corrimal District Library and Community centre (500 m) and Corrimal Beach 1.2 km).



The history of the Site and surrounding area is summarised below.

INDIGENOUS

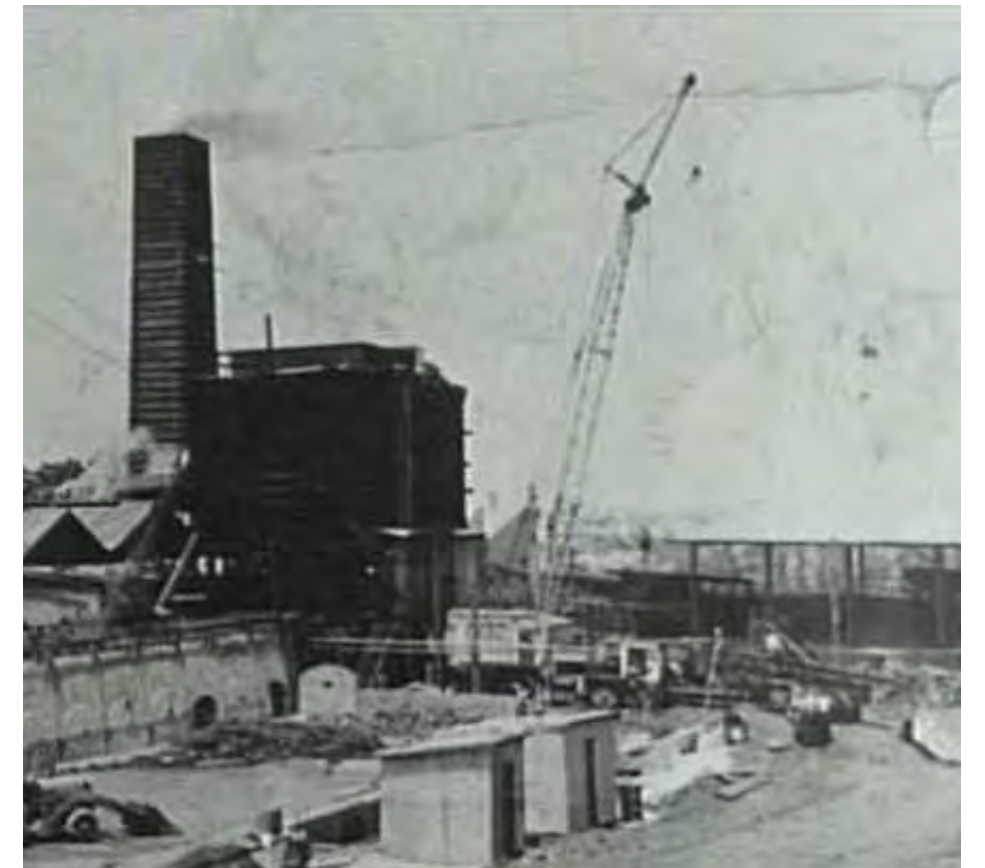
- The Wodi Wodi and the traditional people of the Illawarra area.
- The word ‘Illawarra’ is derived from the Aboriginal Tharawal word ‘allowrie’, which translates as ‘pleasant place near the sea’ or ‘high place near the sea.’
- The Wodi Wodi made particular use of the region’s rich natural resources for the creation of stone tools and other utilitarian objects.

EUROPEAN SETTLEMENT

- The Illawarra region was settled by Europeans in the late 1700’s.
- Earliest settlements in the colony were generally located in areas nearby rivers and coastlines and most early settlers focused on pastoral grazing.
- By 1830, large parcels of land were taken up as a result of the potential for coal.
- The thriving coal mining industry formed the backbone of the area’s economy for over 100 years.

TODAY

- The Illawarra region is diversifying from its industrial past as Wollongong grows, becoming a dynamic and vibrant regional city with an expanding CBD, regional services, a university and a rich natural environment.



The Site consists of two principal lots:

- Lot 5// DP749492.
- Lot 1// DP795791.

As well as two other smaller lots:

- Lot 126// DP598190 (zoned SP2)
- Lot 11// DP749492

INTERFACES

The Site has a range of boundary conditions that will require different urban design responses.

North

- Railway Street, along the northern boundary of the Site, serves as the main connection route between the Corrimal Town Centre and the train station. The route is in a degraded state at present however redevelopment of the Site will significantly improve the serviceability of the streetscape.

East

- The Eastern boundary is defined by the train line. On the adjacent side of the train line is a mixture of R3 Medium Density and low density residential development, Corrimal High School and Corrimal East Public School and the Corrimal Beach Tourist Park which extends approximately 1.1km to Corrimal Beach. The most direct route from the Site to the coastline is via Railway Street and Murray Road.

West

- West of the Site is Memorial Drive, which is a major arterial road that consists of two lanes in each direction. Opposite the Site on Memorial Drive are the major recreational resources of

Corrimal including:

- » Robert Ziems Park featuring playing fields.
- » Corrimal Pool.
- » Corrimal Library and Community Centre.

South

- South of the Site is the Towradgi Creek riparian corridor and low density residential. The riparian corridor forms a green link between the coast and the Illawarra Escarpment and could be developed as a cycling/green link from the coast, through the Site to the Corrimal town centre.

CHARACTER

The land around the Site is generally flat, with R3 Medium Density zoning to the north, north east and east of the Site. The Escarpment to the west is a dominant visual feature. The original chimney on the Site is also a unique local landmark.

The adjacent plan shows the extent of the Site and immediate context.





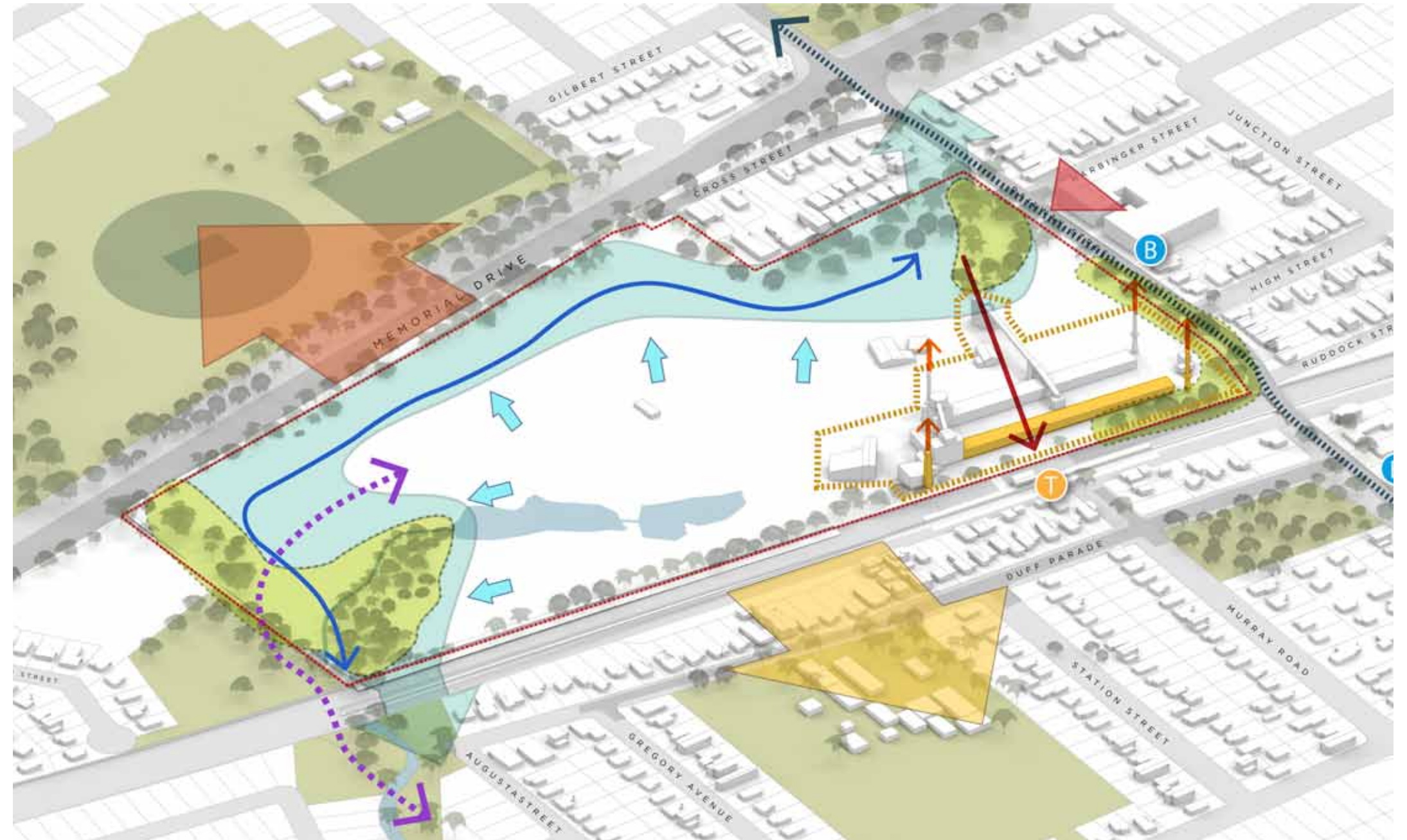


OPPORTUNITIES

- Realignment of North Corrimal Creek to create a consolidated development area and extensive new open space.
- Retention and interpretation of on-site structures that tell the story of the Sites' history.
- Preserving and enhancing ecological communities.
- Creating a new connection to Corrimal Station.
- Green Grid connections to the local public open space network.
- Locating new homes and services close to public transport.
- Linking and connecting to the local community and Corrimal Town Centre.

Legend














- The Site
- Open space
- Bus routes and stops
- Site access points
- Escarpment views
- Possible ocean views
- Ecological corridor
- Water ecology
- Creek views
- Ecological communities
- Key heritage elements
- Heritage interpretation
- Vertical elements
- Visual link to train station

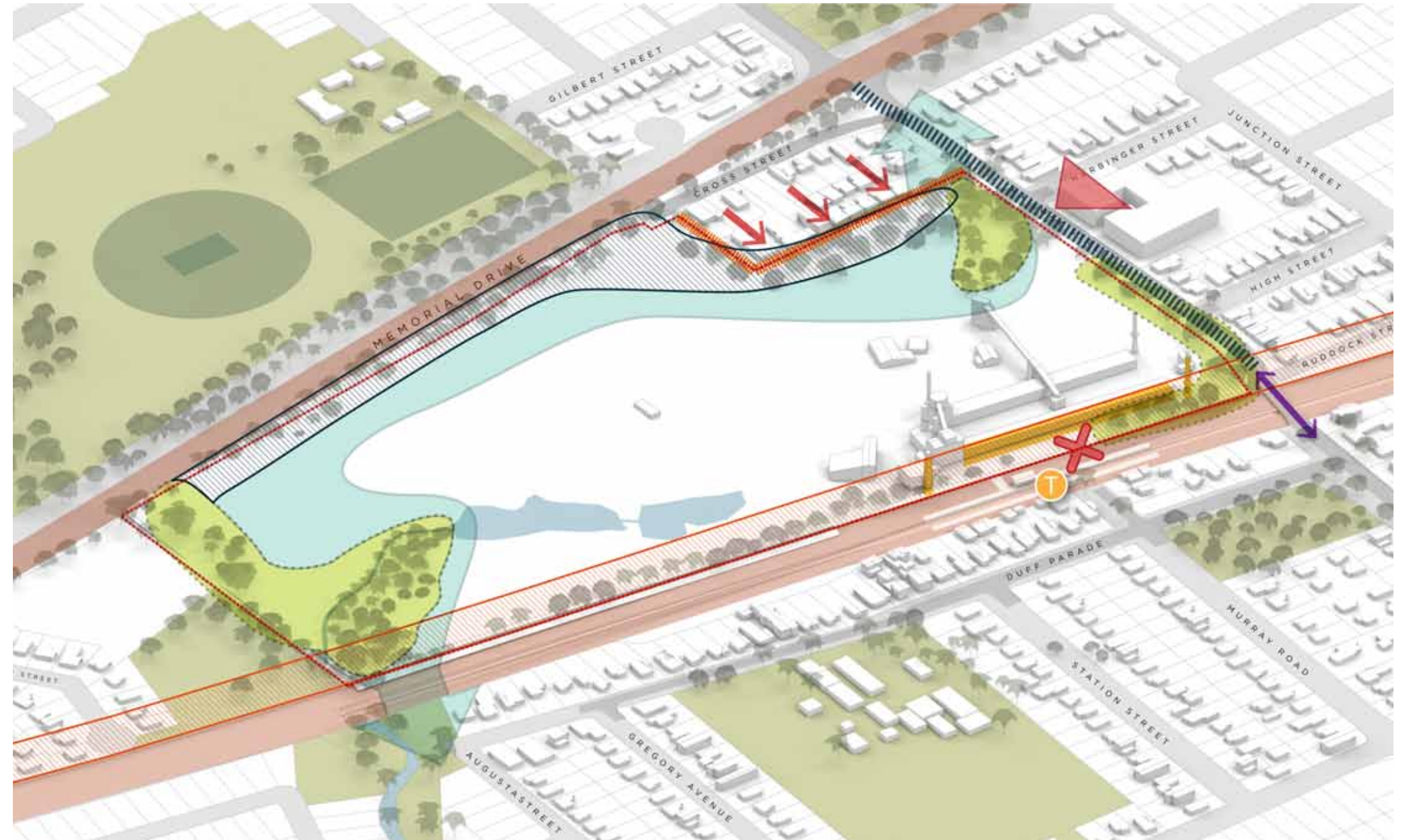


CONSTRAINTS

- Development in Rail Corridors - 20 metre interface zone with possible restrictions on built form.
- Restrictions on access to Corrimal Station.
- Future plans for rail overpass bridge to replace the existing level crossing on Railway Street
- Restriction of access to ecological zones.
- Interfaces with neighbours - overlooking, views, privacy.
- Limited vehicular access points
- Noise from Memorial Drive and the rail corridor

Legend

-  The Site
-  Noise source
-  20m rail corridor offset
-  Railway street bridge
-  Site access
-  Limited access
-  Cross street proximity
-  Rear fences
-  Ecological corridor
-  Existing Edge Vegetation
-  Ecological communities
-  Key heritage elements
-  Restricted access to train station



2017 INDICATIVE CONCEPT PLAN

The adjacent plan shows the 2017 Concept Plan for the Site. The key features of the design include:

- A new Station Plaza with local small scale retail opportunities and interpretative heritage elements.
- A naturalistic creek corridor with walking trails and play areas based on a creek re-alignment strategy developed by flood consultants.
- A pedestrian and cycle route providing linkage opportunities to the town centre to the west and beaches to the east.
- Medium density terraces and town houses designed to meet Illawarra market price-points.
- Retained vegetation on the site boundary to maintain privacy for neighbouring residents.
- Open space areas developed around pockets of

remnant vegetation.

- Apartment development of between three and six storeys close to the station and in the south of the Site utilising design that minimises construction costs to meet price-points in the market (new-style undercroft car parking arrangements).
- A cycle path to connect Railway Street to Corrimal station.
- A single vehicular access points off Railway Street

The 2017 Concept Plan and associated urban design report was developed to support the initial rezoning, submitted to WCC.

The sketches at the foot of this page were developed to highlight the possible form of development envisaged in the 2017 Concept Plan.



KEY CHANGES

The Master Plan provides the following enhancements to the 2017 Concept Plan:



Greater Housing Diversity

- Greater housing diversity on the Site to meet community needs, with affordable housing, retirement/seniors living, medium density housing, townhouses and residential apartments.



Increased Open Space and Ecological R

- Open spaces has increased from 39% to 43%, including the inclusion of an additional local park and community garden.
- Revised development footprint to reduce impacts on ecological areas



Greater Heritage Retention & Interpretation

- Significantly increased retention and interpretation of heritage elements to showcase the Site's history.



Increased and Clear Access to Station and Plaza

- Additional cycle and pedestrian connections through the Site.
- Opportunities for more integration with and connection to Corrimal train station.



Diversified Retail Area

- Provision of a small neighbourhood retail precinct to provide activity at Corrimal train station.





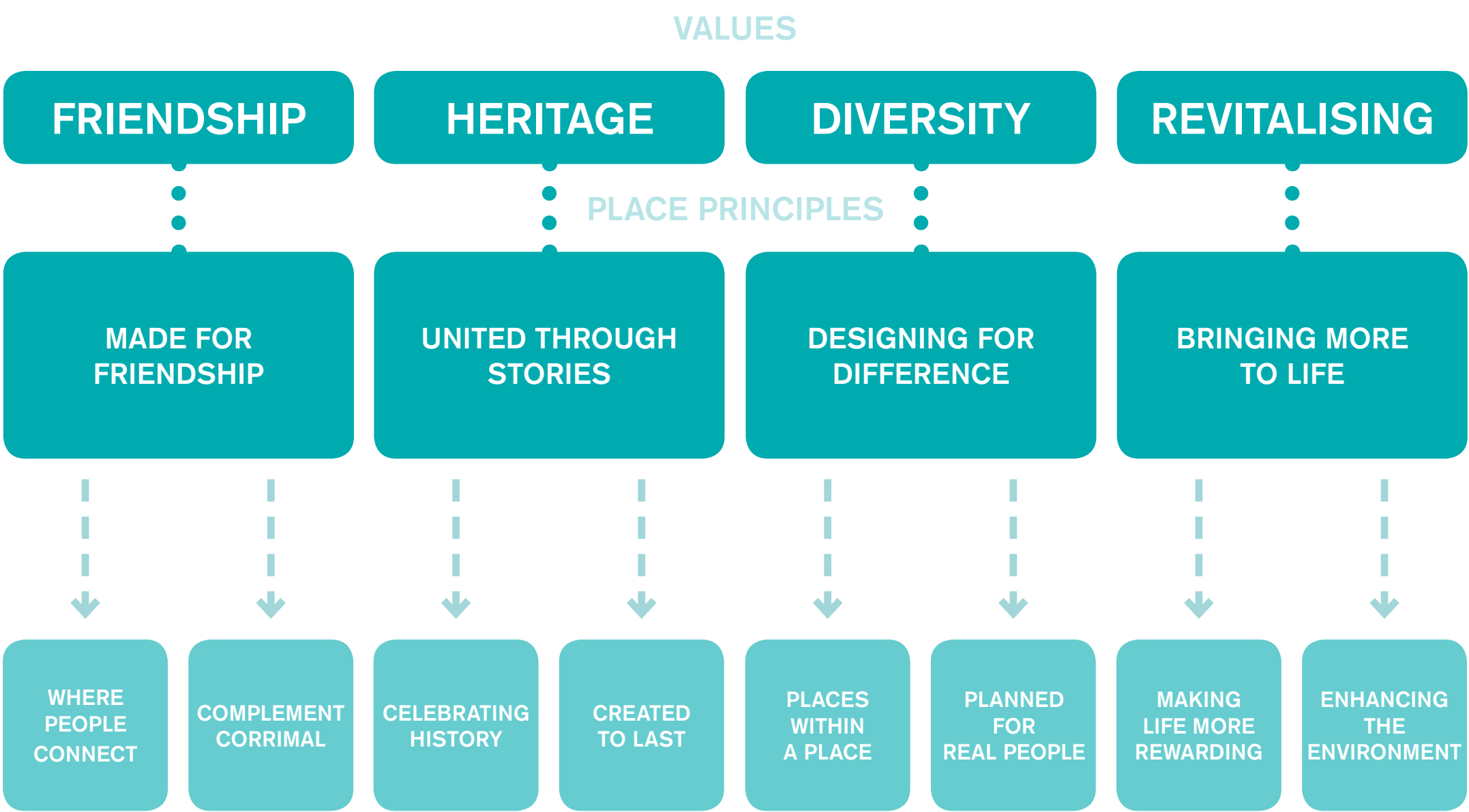
Key Principles and Drivers

CORRIMAL COKE WORKS WOLLONGONG

PLACE PRINCIPLES

The Values and Place Principles have been developed following a number of vision workshops and consultation sessions with the client, design team, key stakeholders and the community, as well as in-depth market research conducted by Ruby Cha Cha to understand the needs of potential future residents.

This work has been delivered by Frost, who facilitated the workshops, commission the research and synthesised the feedback, insights and ideas into the set of over-arching Values and Place Principles. These values and principles will guide all aspects of decision making about the development, and to create a unified approach for the design and client team.



KEY DRIVERS

From the contextual analysis of the wider locality and site analysis including the opportunities and constraints of the Site, a number of Key Drivers were established.

—Heritage Fabric Management

Retention, interpretation and adaptive reuse of a significant number of original elements on the Site, and incorporate the Site's historic, social and representative significance into the desired character of the Master Plan.

—Riparian Corridor Use

Use realignment of corridor to reduce flood risks, offer a mix of active and passive recreational uses and handle water quality management. Utilise the corridor to link to existing adjacent public open space and facilitate a possible town centre to beach link.

—Valuing Ecological Communities

Enhance and maximise preservation of identified ecological communities at the North and South of the Site

—Open Space and Public Domain Diversity

Open space will be high quality and provide for the needs of all ages. A distribution of a diversity of spaces will ensure equitable access for residents throughout the Site and flexible use.

—Connecting Into the Local Network

Creation of vehicular and active movement links to provide access to Corrimal Station, Corrimal town centre and local parks and open spaces. Incorporate a street network that is legible, safe and designed to add to amenity and a cool micro climate within the Site.



DESIGN PRINCIPLES

The Key Drivers and the Place Values and Principles meet together and inform the Master Plan through a series of Design Principles. These Design Principles have guided the specific aspects of the proposed urban design and built form approach. The Design Principles will also continue to influence the further refinement and detailing of the Master Plan, as it progresses through to various design development stages and eventually to construction.

GUIDING VALUES AND PLACE PRINCIPLES

FRIENDSHIP

**MADE FOR
FRIENDSHIP**

HERITAGE

**UNITED THROUGH
STORIES**

DIVERSITY

**DESIGNING FOR
DIFFERENCE**

REVITALISING

**BRINGING MORE
TO LIFE**

DESIGN PRINCIPLES

**KEY
DRIVERS**

**PLACES THAT
FOSTER
COMMUNITY**

**COMPLEMENT
AND
BE A
CONNECTOR
TO
CORRIMAL**

**DWELLING
DIVERSITY AND
INNOVATION**

**QUALITY
STREETS AND
HIGHLY
ACCESSIBLE**

**ENHANCE
ECOLOGY AND
EXISTING
LANDSCAPE**

**DESIGN
EXCELLENCE
AND LINKED TO
PLACE**

**SHOWCASE
AND
INTEGRATE
HERITAGE**



Master Plan Development

CORRIMAL COKE WORKS WOLLONGONG

CIRCULATION AND OPEN SPACE
NETWORK OPTIONS

During the refinement of the 2017 Concept Plan the design team tested a number of high level concepts that explored alternative approaches to circulation and open space location.

Four of these options are presented on the following pages with a brief summary of the advantages and disadvantage of each option. The options developed were:

- Option 1: Loop road with linear park
- Option 2: Station Hub road with central park
- Option 3: Central road with central park
- Option 4: Creek Edge road with south park

The final Master Plan combines elements from each of the options and has been developed to incorporate the findings of the additional technical studies on the Site.

Legend

The Precinct

Main road

Access point

Creek parklands

Central local park

Development area

Corrimal train station

Plaza



OPTION 01

Advantages

- Direct access to Corrimal Station
- All streets accessed by main road
- Positive arrival interface with creek
- Green north south link

Disadvantages

- Poor interface with rail corridor
- Low flexibility of linear park



OPTION 02

Advantages

- Direct access to Corrimal Station
- Central park is highly accessible

Disadvantages

- Qualities of the creek are not maximised



OPTION 03

Advantages

- All streets accessed by main road

Disadvantages

- No direct access to Corrimal Station
- Poor interface with rail corridor



OPTION 04

Advantages

- All streets accessed by main road

Disadvantages

- No direct access to Corrimal Station
- Low accessibility of south park



STAKEHOLDER AND COMMUNITY ENGAGEMENT

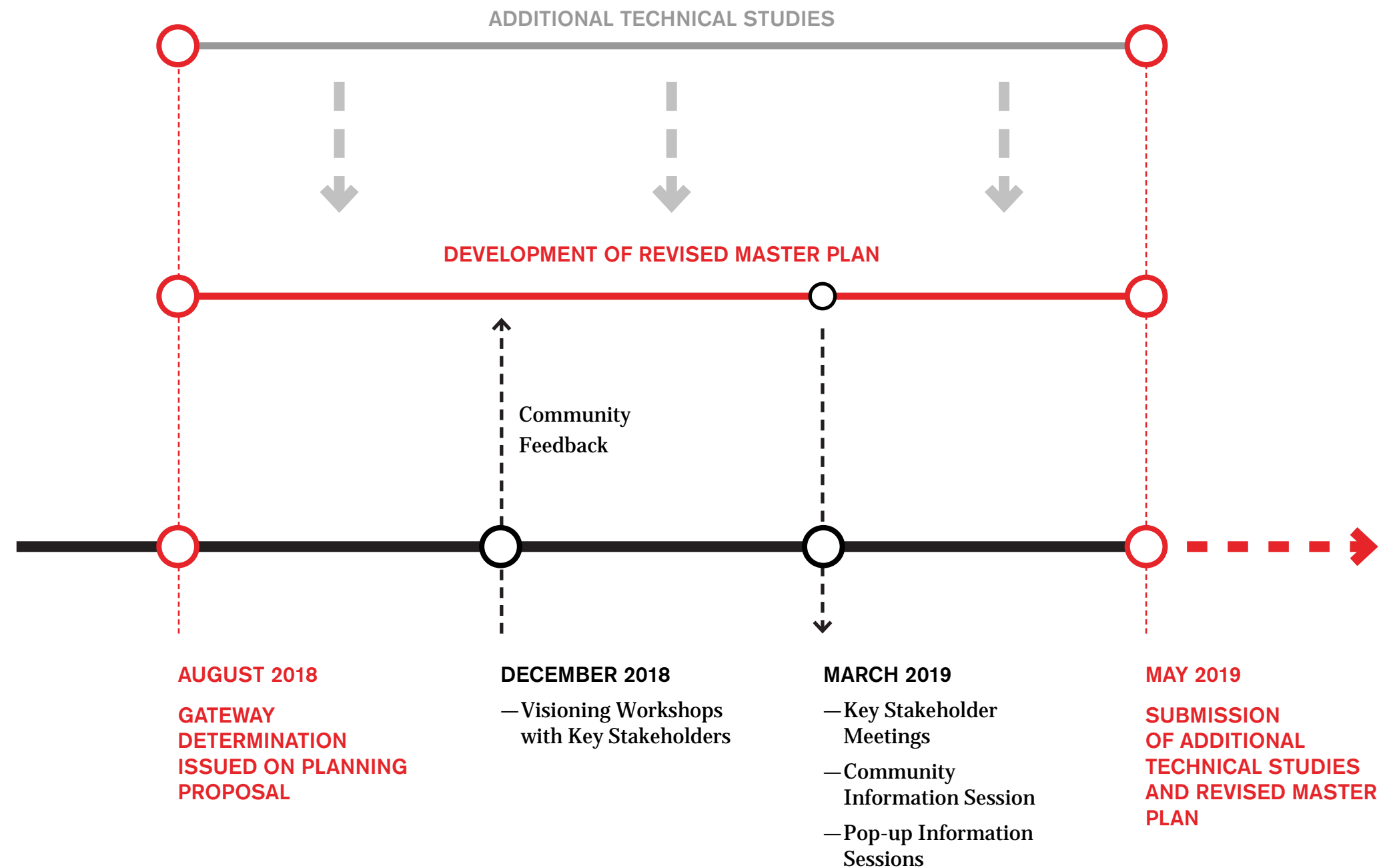
Since the Gateway approval of the 2017 Planning Proposal submission, the Illawarra Coke Company and Legacy Property have undertaken a number of stakeholder and community consultation events.

These events included Visioning Workshops, Key Stakeholder meetings, Community Information Sessions and Pop-up Information sessions. These events were facilitated by Elton Consulting, with the Vision Workshops conducted by Frost. The key stakeholder groups included members from:

- Wollongong City Council
- Corrimal Community Action Group (CCAG)
- Corrimal Region Action Group (CRAG)
- Corrimal Chamber of Commerce.

The following pages outline the time line of these events, and some of the feedback that was received.

A full summary of the outcomes from these events are detailed in the Community Consultation Outcomes Report prepared by Elton Consulting.



DECEMBER 2018 - VISIONING WORKSHOPS

Three visioning workshops were held in December, with one being a focus group held with Wollongong City Council Staff, and two community workshops with representatives from key stakeholder groups. Out of each workshop was a primary 'vision' of the group, and a series of 'must haves' and 'must nots' relating to the public domain, built form and experience of the Master Plan.

Each workshop had slightly differing visions and concerns including:

- Connecting the Site to the wider Corrimal Community
- Preservation of and connection to the Site's industrial history and ecology
- The development should include functional private and communal spaces in addition to the public spaces
- Integrated, inclusive and vibrant spaces which in still a sense of community and provide a place to interact and gather
- Creative, high quality built form design
- Mix of active uses



MARCH 2019 - STAKEHOLDER MEETINGS AND COMMUNITY INFORMATION SESSIONS

The number of sessions were conducted in March 2019 to present the project's progress, recent amendments to the Master Plan, project time frames and key issues on the Site.

The Key Stakeholder meetings provided feedback on how the contributions to the Visioning Workshops had been incorporated into the revised Master Plan and how identified issues and concerns had been explored by detailed technical studies.

A Community Information Session was an invited meeting for residents in the surrounding area of the site, the networks of key stakeholders and those who had registered interest online.

Pop-up informations consulted with a broad cross-section of the community and provided the opportunity to voice views on the proposed redevelopment. They were located in public spaces (Corrimal's main street, the local shopping centre, Bunnings, Robert Ziems Oval and Towradgi Beach) at busy times.

Across all the sessions a survey was undertaken. The results of the survey showed:

- 68% of respondents were generally supportive of the Corrimal Coke Works redevelopment into a residential community
- 59% of respondents believed the proposed redevelopment will have a positive impact on Corrimal
- Key elements which respondents wanted to see incorporated were walking and cycle paths and retention of existing ecological areas and heritage items



CONSERVATION MANAGEMENT STRATEGY

Showcasing, better integrating and responding to the Site's history and former use as a coke works has been a key driver in the development of the revised Master Plan.

The proposed extent of heritage and interpretation has significantly increased from the original proposal and now provides a robust outcome for understanding and celebrating the history of the Site. This includes an expansion of the Civic Plaza adjacent to Corrimal Station (and will encompass many heritage items) and reinforcing the Brick Stack as a landmark element within the Master Plan with defined view corridors to it from within and around the Site.

In line with this and as one of the additional studies requested by council, a Conservation Management Strategy (CMS) has been prepared by Urbis to detail the future management of heritage on the Site. The CMS contains a comprehensive history for the site, an assessment of the heritage significance including a significance grading for the various elements on site, as summarised by the adjacent diagram. The CMS also provides a description of the condition of these elements, many of which are poor condition, and some are an immediate safety concern.

The CMS for the site has recommended a (approximate) reduced heritage curtilage for the site, as shown in the drawing on the right hand side of this page. This encompasses what is considered as the key elements which provide the industrial heart of the Corrimal Coke Works



HERITAGE INTERPRETATION STRATEGY

A Heritage Interpretation Strategy (HIS) has been produced by Urbis in parallel to the Conservation Management Strategy for the Site.

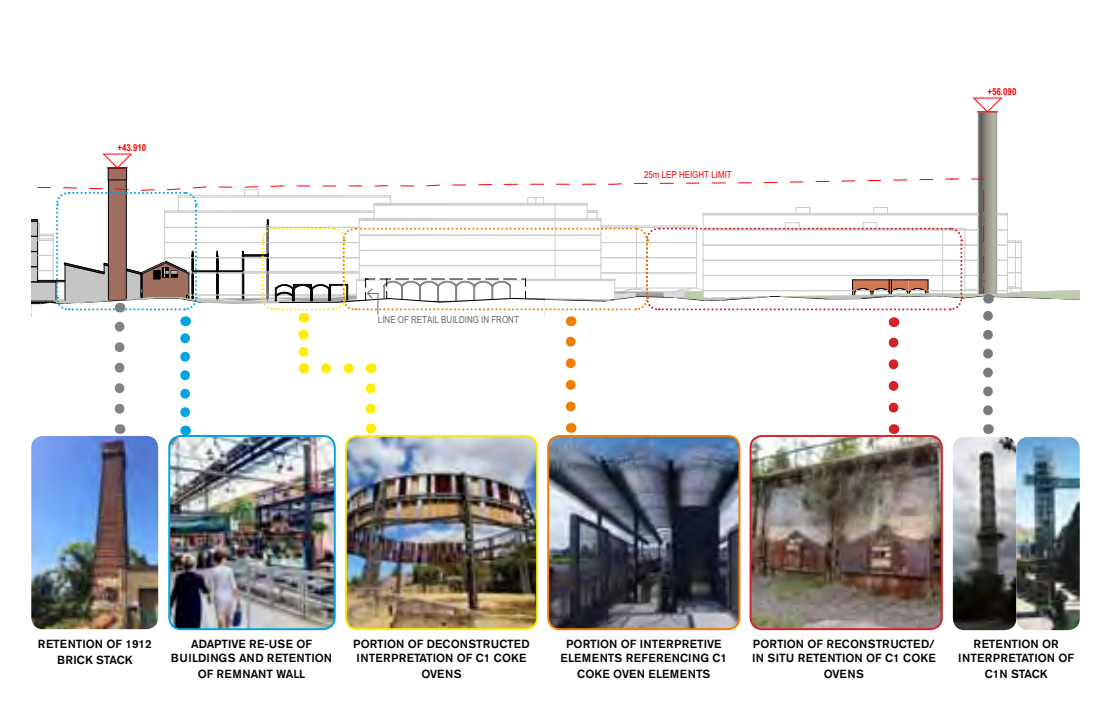
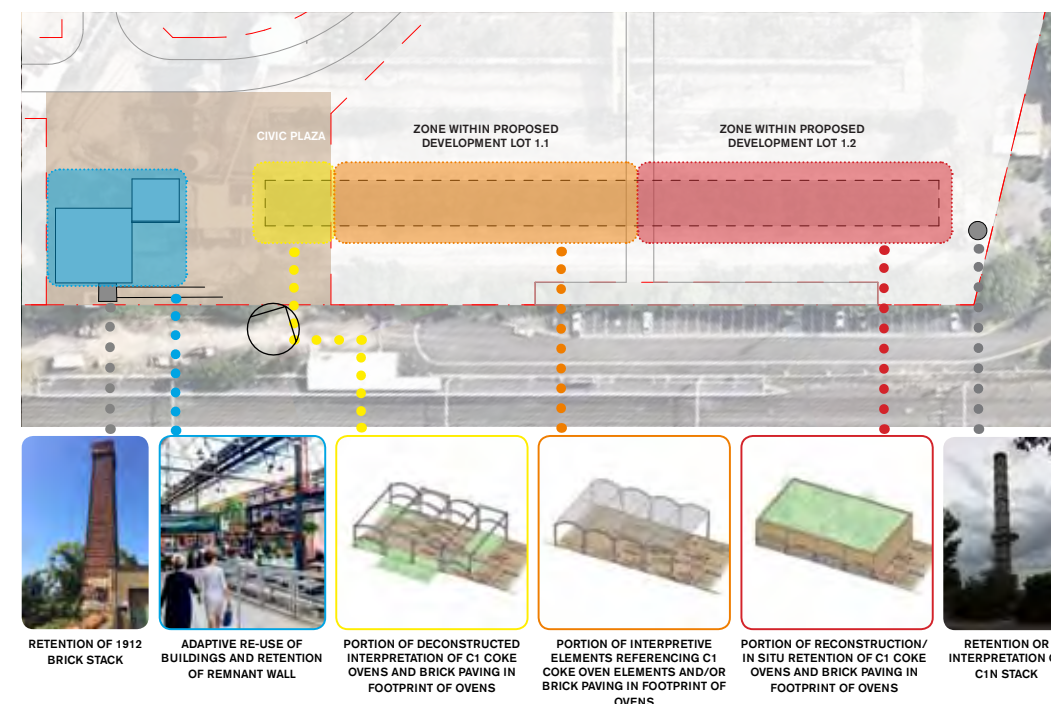
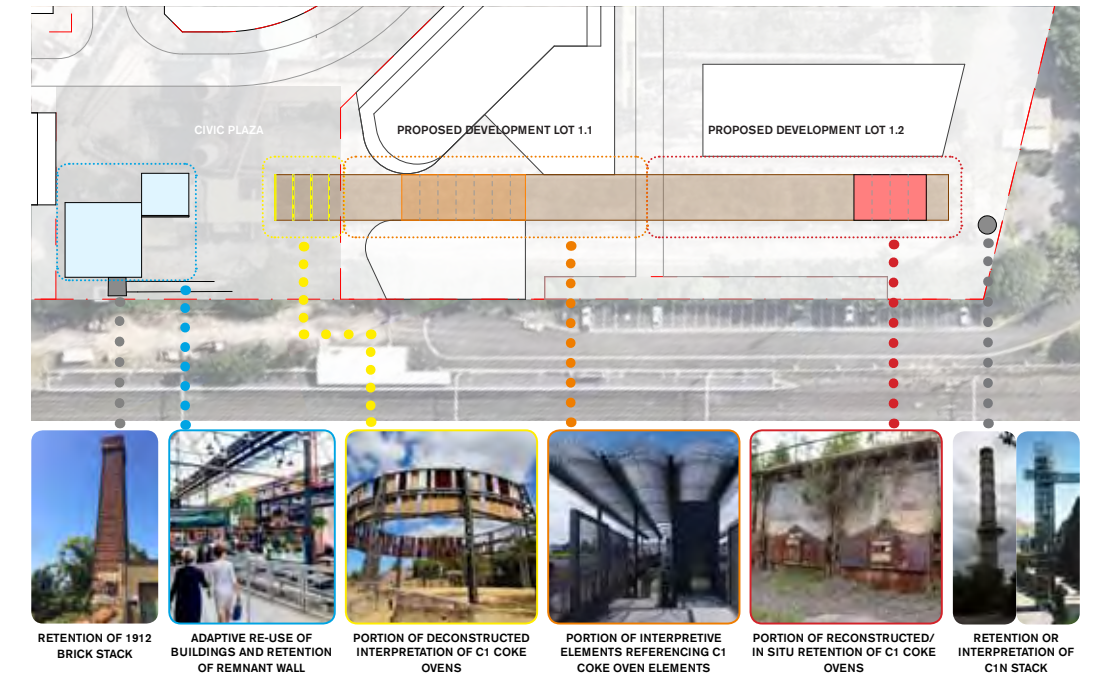
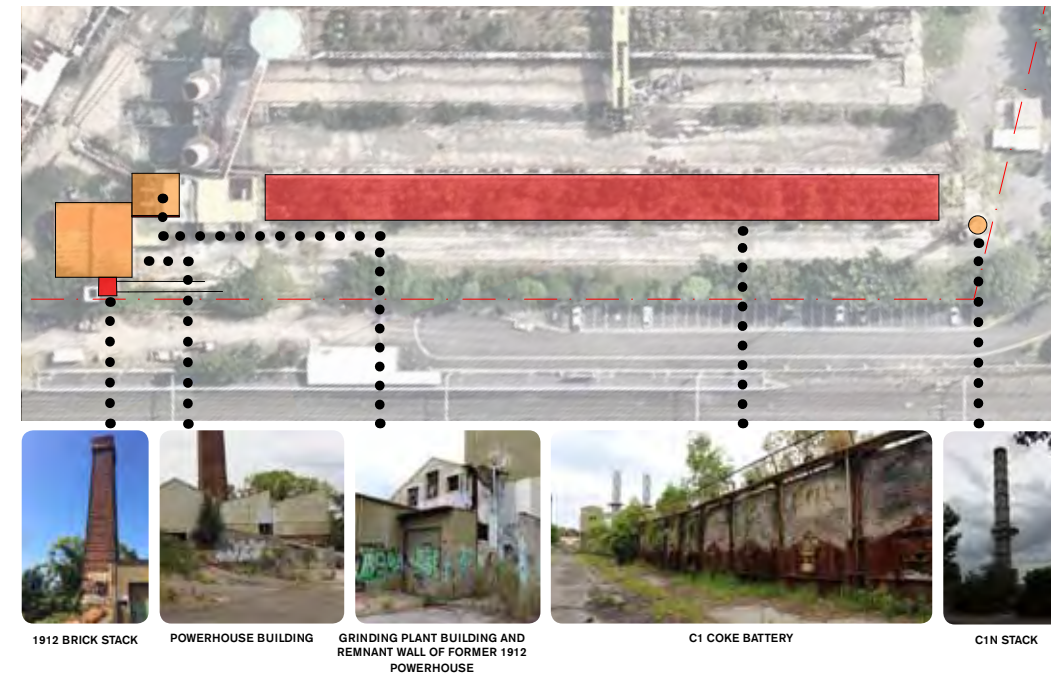
Included in the document is the Proposed Statement of Significance, which states that the Site, as the former Corrimal Coke Works, is “historically significant as a contributor to the development of the coke and steel manufacturing industry, in particular, in the Illawarra Region”. The former Corrimal Coke Works site generally demonstrates an industrial aesthetic, with a number of elements identified as being of aesthetic heritage significance including:

—1912 Brick Chimney

—C1 Coke Battery

The HIS contains a detailed description of the proposed built form interpretation, which will include a variety of interpretive devices for the identified high significance items and some moderate significance items.

The proposed built form interpretation strategy for the differing items is outlined in the adjacent diagrams.



The image features a dark, circular graphic element centered on a white background. Inside the circle is a grayscale aerial photograph of an industrial or urban landscape, showing various structures, roads, and green spaces. Overlaid on this circular image is the text 'Proposed Masterplan' in a large, white, serif font. Below this title, a thin white horizontal line spans the width of the circle. Underneath the line, the text 'CORRIMAL COKE WORKS WOLLONGONG' is written in a smaller, white, sans-serif font.

Proposed Masterplan

CORRIMAL COKE WORKS WOLLONGONG

VISION

The Corrimal Coke Works site vision is to integrate diverse housing and deliver a public plaza adjoining Corrimal train station to promote public-transit usage for residents and the broader community. The key principles incorporated into the indicative concept plan include:

- Realignment of the riparian corridor to deliver 11.1 ha of PMF-free contiguous developable land that maximises avoidance of significant ecological resources where practical.
- Delivering local open space resources and facilities along the realigned riparian corridor, connected with the coast and community and recreational resources.
- Maximising access to Corrimal train station and delivery of a public plaza adjacent to the station with interpretive heritage elements.
- Enabling neighbourhood and commuter services at the plaza such as child care, neighbourhood shops and services. However, ensuring limitations that prevent any undermining of the Corrimal town centre.
- Delivering apartment development at the north of the site where the greatest access to the train station is, with some further apartment development at the south of the site overlooking natural resources.
- Delivering a diversity of housing, particularly affordable under-represented housing typologies such as attached and semi-detached dwellings and low-scale residential flat buildings capitalising on connectivity.
- Providing a suitable buffer distance from the

rail and state road corridors adjacent to the site for noise attenuation.

- Outlining a potential green link to East Corrimal Beach to the east, Robert Ziems Park and community facilities to the west, and through to the north of the subject site.

THE ILLUSTRATIVE MASTER PLAN

The Illustrative Master Plan for the Site is derived from the Structure Plan presented later in the section. It provides a vision for the public domain, built form and key land use elements within the Site.

The Master Plan is based on a principle of delivering distinct stages of development with the flexibility to change over time to meet the needs of the community.

KEY FEATURES

Some of the key features of the Structure Plan are highlighted in the adjacent plan.

1	Railway Street Landscape Gateway
2	Ecological Stringybark Forest
3	Creek Parklands
4	Civic Plaza
5	Corrimal Station
6	Central Park
7	Blue-Gum Blackbutt Forest
8	Beach link connection



PROPOSED MASTERPLAN
RAILWAY STREET GATEWAY AND ENTRY BOULEVARDE PERSPECTIVE



REVISED STRUCTURE PLAN

The Structure Plan illustrates the planning and design framework for development on the Site.

The Structure Plan is intended as a guide to demonstrate how the vision, urban design objectives and key outcomes for the Site may be achieved.

It is recognised that over time there may be minor deviations from the Structure Plan where the vision, principles and key outcomes are still achieved.

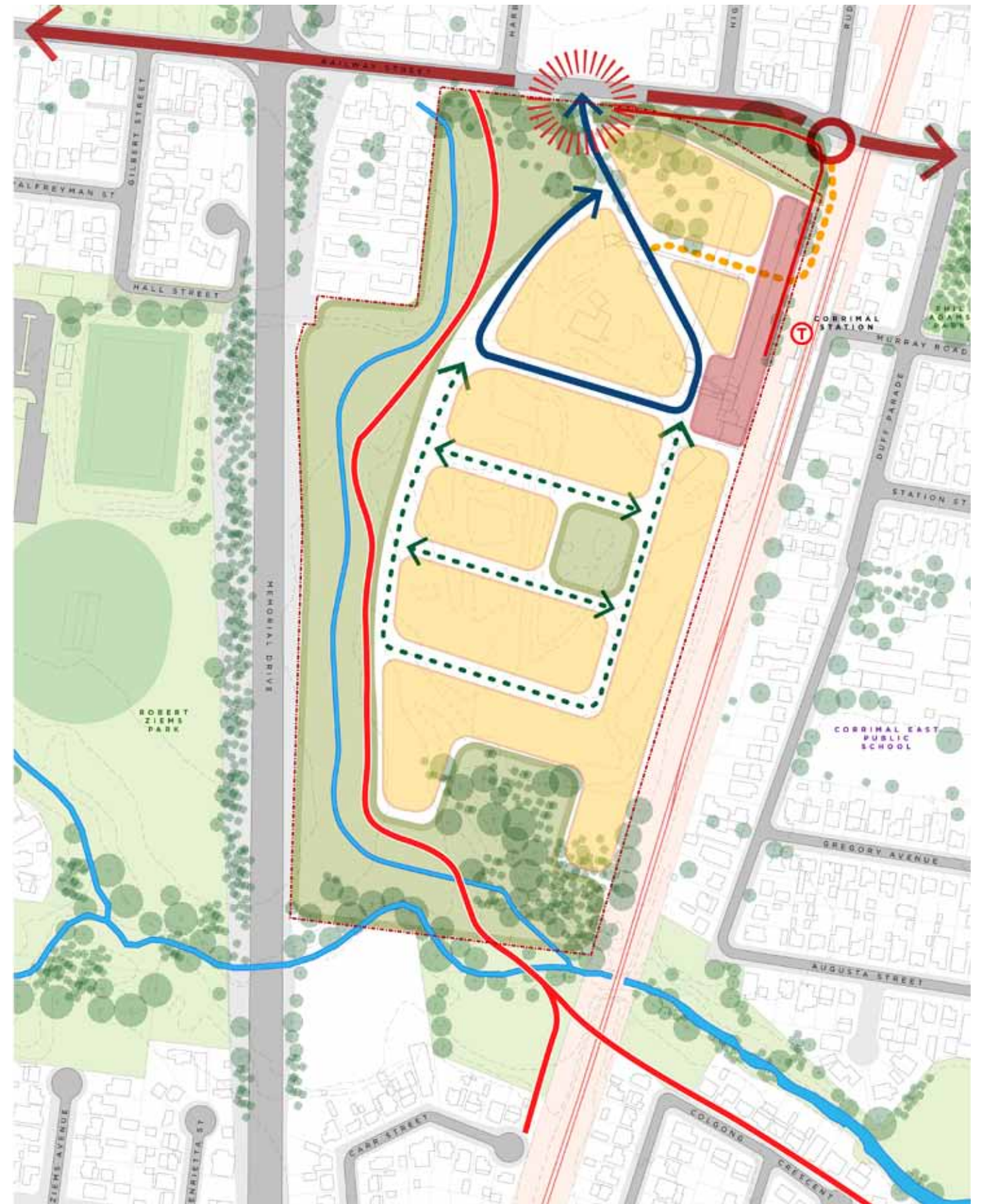
The over arching objectives of the Structure Plan are to:

- Support Corrimal as a key suburb for Wollongong as a regional city by providing a mix of housing, retail, services and open space.
- Ensure that development occurs in a coordinated manner consistent with the vision and development principles for the Site.
- Locate residential uses close to Corrimal Town Centre to optimise access to facilities and amenity.
- Provide a high quality public open space accommodating a realigned North Corrimal Creek and riparian corridor that connects to the broader public open space network.
- Develop a community hub adjacent to Corrimal Station to support the incoming population, focused around a Civic plaza with a mix retail of retail and services.
- Contribute to local active transport links consisting of connected and dedicated footpaths, shared paths and access to public transport stations and stops.

- Ensure key local infrastructure upgrades, such as the future grade separation crossing on Railway Street, can be accommodated adjacent to the Site.
- Provide high quality public domain and a range of streets.
- Provide for a range of flexible innovative housing.

Legend

- The Site
- ⊙ Main Gateway
- Secondary Gateway
- Railway Street
- Development Area
- Heritage Conservation and Interpretation
- Open Space
- Cycle Link
- Bus Loop
- - - - - Proposed Road Network
- - - - - Secondary Access





Structure Plan Design Components

CORRIMAL COKE WORKS WOLLONGONG

RIPARIAN CORRIDOR AND WATER SYSTEM

The realignment of North Corrimal Creek to the west is a key driver for planning and design on the Site. This change to the landform has a number of benefits including:

- The creation of a diverse 6.7 hectare public open space
- Addressing flooding.
- Providing space for stormwater treatment.
- Linking ecological communities.
- Providing a physical connection to adjacent public open space to the south.

WATER SENSITIVE URBAN DESIGN

The stormwater management concept is based around the creation of a Water Sensitive Urban Design (WSUD) ‘treatment train’ that progressively treats the water within North Corrimal Creek as well as stormwater run-off from the Site.

The stormwater strategy will be designed as a visible and seasonally changing element within the Site, engaging users and adding to the amenity and appreciation of the wider natural systems.

The WSUD outcomes will be in line with the WCC requirements.

FLOOD MITIGATION

The proposed realignment of North Corrimal Creek to run along the western site boundary consolidates the area impacted by a PMF event. The realignment maintains the two important vegetation communities as much as possible and negates the need for secondary site access points.



LANDSCAPE NETWORK AND PUBLIC OPEN SPACE APPROACH

The public open space approach identified in the Structure Plan provides a diverse range of new accessible public open space. It incorporates active and passive recreation, overland flow and water sensitive urban design measures, pedestrian and bicycle connections and social infrastructure.

The Structure Plan proposes a number of open space typologies including:

- Creek Parklands (5.4 hectares).
- Central Community Park (0.3 hectares).
- Civic Plaza (0.3 hectares).
- Ecological Area North (0.7 hectares).
- Ecological Area South (0.6 hectares).

The approach to open space ensures a high quality public domain with a diverse series of recreational, passive and active experiences for the community to enjoy.

The Public Domain Foundations section of the Report illustrates the intended character, materials, elements and programming for the public open space of the Precinct.



CHARACTER AREAS

The Structure Plan has a number of distinct character areas that respond to its particular location.

—Civic Hub

Located adjacent to Corrimal Station, this area provides a mix of uses and frames the Civic Plaza in high quality built form with a character that references the former industrial uses on the Site.

—Treetop Escarpment

This area forms an important interface with Railway Street and the wider urban area. It references the mature trees on the Site and the dynamic form of the Escarpment to the west.

—Green Edge

Running the perimeter of the riparian corridor, this area will have high quality built form that fronts open spaces, and delineating of the public and private domain. Landscape setbacks would reflect and contribute to the green landscape character and the built form would be designed to address the green outlook.

—Village Park

This area is focused around the central public park and contribute to an urban village feel. Development will address the local park and provide a clear urban edge that also ensures privacy.



INDUSTRIAL CHARACTER REMNANTS

The Structure Plan retains, enhances and interprets a number of structures associated with the former use of the site as a coke works.

The majority of these elements are located in the Civic Plaza.

1912 BRICK CHIMNEY STACK

Graded as having high significance, and the Master Plan will retain and conserve the structure.

COKE OVEN BATTERY

Graded as having high significance, but due to its poor condition, varying interpretation devices will be implemented:

- Retained or in situ reconstruction of portion of coke ovens
- Interpretation of coke ovens using steel structures
- Interpretation and demonstration of battery's spatial length with paving and view corridor

POWERHOUSE, GRINDING PLANT AND REMNANT 1912 WALL

The remnant wall of the former 1912 powerhouse building has high significance, with the adjacent power house and grinding plant buildings of moderate significance.

The remnant wall will be retained and conserved with the adaptive re-use of the grinding plant and powerhouse buildings.



INDICATIVE SUPERLOTS AND LAND USE

The Structure Plan provides for an indicative superlot layout to achieve the vision for the Site. The pattern of superlots and land use provide an indication of a possible future outcome.

Key features of the indicative superlot and land use pattern include:

- Non-residential uses focused around the Village Plaza adjacent to Corrimal station.
- Apartments and taller buildings in the north of the Site.
- Compact terraces and innovative housing forms in the centre of the Site.

The final configuration and location of land use may be subject to change to ensure that the needs of the future community develop.



LAND USE
Legend

- The Site
- Residential
- Mixed Use
- 150m Radius

EDGE CONDITIONS AND APPROACH

There are four approaches to the treatment on the Site edges:

1. Provide a green interface as a visual barrier - on the west and east.
2. Forming a landscaped urban interface on Railway Street.
3. Forming an extension to the local open space network in the south.
4. Linking to Corrimal Station.



ACCESS LOOP STREET NETWORK

The future street network defined by the Structure Plan is simple and legible providing high levels of accessibility for all superlots.

The main access loop will provide a generous road reserve and meet all access and servicing requirements.

It has been designed to link the main public domain elements of the Structure Plan, the Civic Plaza, North Corrimal Creek Parklands and the Central Local Park.

The design of the streetscape, verge widths, car parking location and vegetation changes according to the specific interface. The built form setbacks also change with location providing for a diverse and responsive key organising element.



ACTIVE TRANSPORT NETWORK

Ease of pedestrian movement and accessibility are key drivers for the proposed layout of the Structure Plan..

The Structure Plan includes a network of interconnected pedestrian and cycle paths providing links to destinations within and outside the Site.

Key features of the pedestrian and cycle network include:

- Active and passive trails along the realigned North Corrimal Creek.
- A key segment of the Corrimal Beach to each to Towradgi Creek link.
- A shared path along Railway Street frontage to Corrimal Station.
- Cycle parking in the Civic Plaza.



INTEGRATED PUBLIC TRANSPORT

The Site is located adjacent to Corrimal station, that provides services to Wollongong and Sydney. A range of local bus serves run along Railway Street and the Structure Plan provides the opportunity to enhance local access to Public Transport and provide a direct interchange between local bus and the railway within the Site.

The benefits of the proposed public transport network include:

- Direct and clear access to Corrimal station within the Site.
- Opportunities for transport interchange at the Civic Plaza.
- A bus loop within the north of the Site.
- A 'kiss and ride' bay within the Site close to Corrimal Station.
- The opportunity to re-configure the car parking adjacent to Corrimal Station providing more spaces within a well designed landscape and public domain setting.



ENTRY EXPERIENCE

The main access into the Site is from Railway Street in the north via the Entry Boulevard.

The alignment of the Entry Boulevard has been set to form a visual axis to the iconic 1912 Brick Chimney Stack which anchors the south side of the Village Plaza.

Passing through the retained vegetation in the north of the Site, with glimpses south to North Creek Parklands, the Site entry sequence commences as a green portal, transitioning to a urban street, that terminates in the Civic Plaza with retained industrial structures, framed by an active retail edge and a direct link to Corrimal station..





Access and Movement

CORRIMAL COKE WORKS WOLLONGONG

INTEGRATED TRANSPORT NETWORK

The adjacent diagram highlights the interface between the various modes of transport that serve the Site.

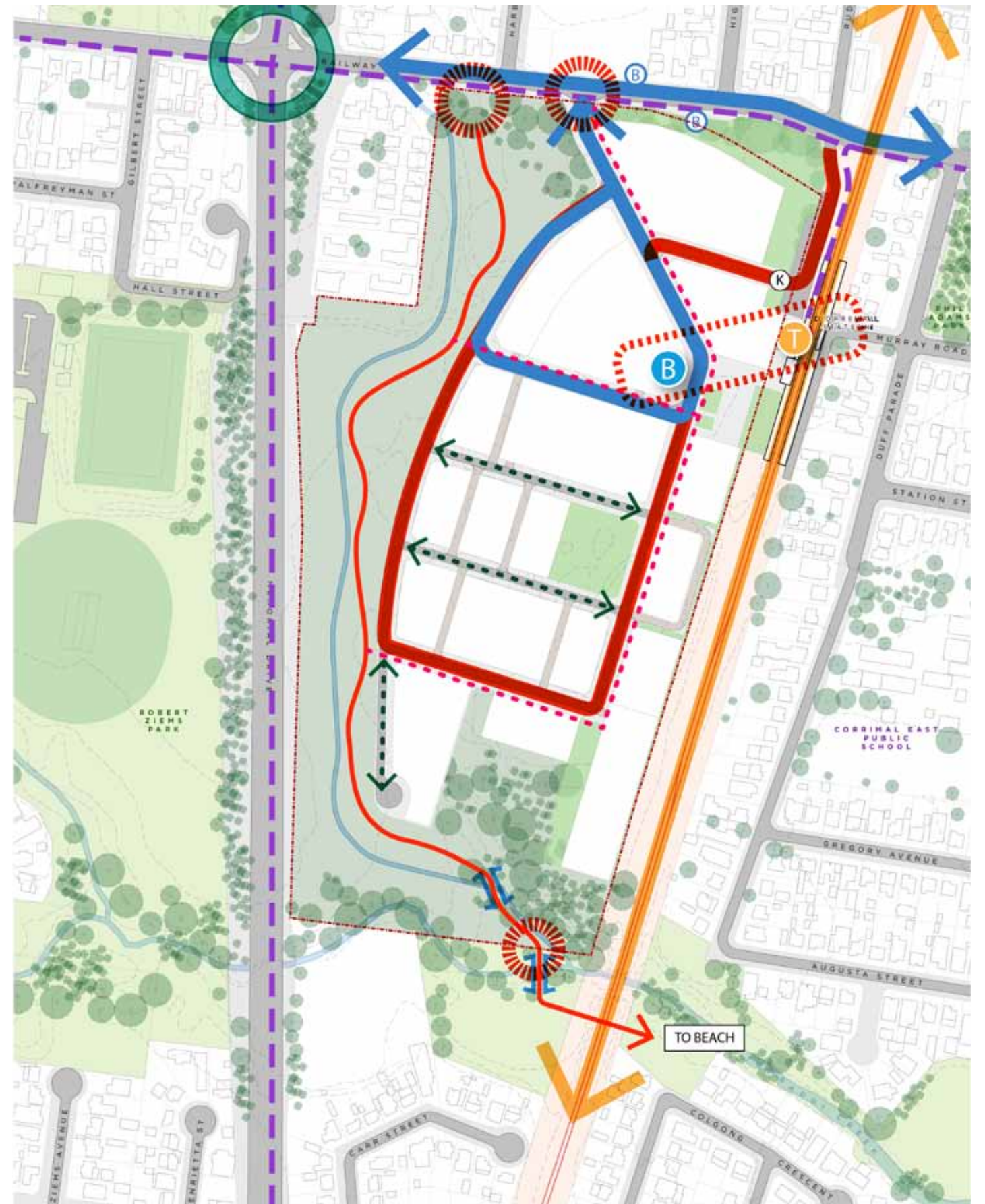
The Structure Plan has been developed to balance the needs of pedestrians, cyclists, buses, service vehicle and private cars.

This approach will ensure:

- A high quality and safe pedestrian environment.
- That the Site is well integrated with the local street and pedestrian network.
- That walking and cycling should be the principle mode of travel for local trips.
- That public transport is accessible and integrated.
- That service and emergency vehicles are accommodated on all necessary streets and roadways.

Legend

- The Site
- Site Access
- Key Pedestrian Route
- Key Cycle Route
- Council Regional Cycleway Network
- Proposed Intersection Upgrade
- Main Loop Road
- Secondary Roads
- Bus Route
- Train Line
- Bus Stop
- Train Stop
- Kiss and Ride
- Bridge (x2)

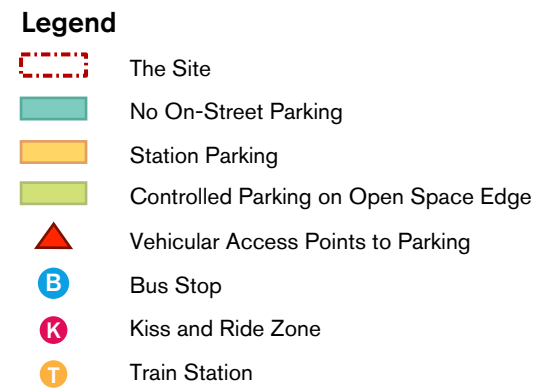


PARKING STRATEGY

Consideration of vehicle access has informed the Structure Plan, with a parking and servicing strategy developed to:

- Allow for adequate levels of parking reflective of the Site location and access to railway and road infrastructure.
- Minimise direct access to basement parking and service areas from main streets and pedestrian routes.
- Control the location of on-street parking.
- Allow for a kiss and ride area close to Corrimal Station.
- Reduce vehicular conflicts at key intersections and Site access points.
- Provide for flexibility in the types of small lot housing that are developed on the Site.

The preferred parking and servicing strategy is illustrated on the adjacent diagram



STREET TYPOLOGIES AND HIERARCHY

The street typologies approach identified in the Structure Plan reflect a refinement of street typologies unidentified in the WCC DCP.

The approach addresses current requirements of for footpath widths, carriageway widths, public open space interfaces, overland flow and water sensitive urban design measures, pedestrian and bicycle connections and transport infrastructure generally, combined with the vision that has been developed for the Site.

The Structure Plan provides for a number of street typologies that have been developed to ensure a high quality public domain.

Key objectives in the development of the street typologies include:

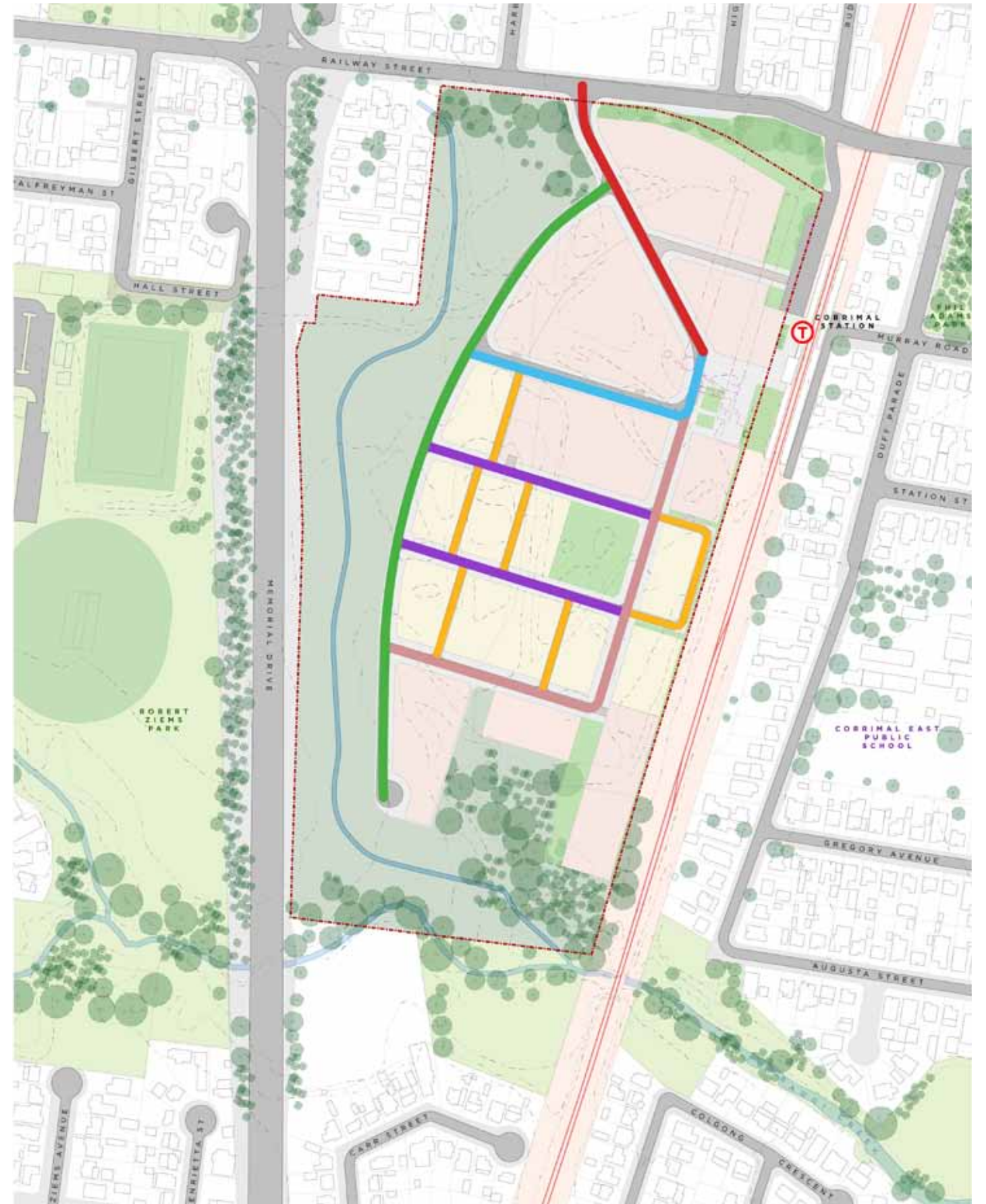
- Providing a high quality public domain.
- Integration of bus stops close to Corrimal Station.
- Providing a diverse series of experiences for the community.
- Responding to the particular conditions and features around the Site.
- Allowing for good access to the Site.
- Incorporating high quality pedestrian and active transport links into the Site.

The following section will illustrate the intended character, materials, elements and programming for the street typologies of the Site.

Legend



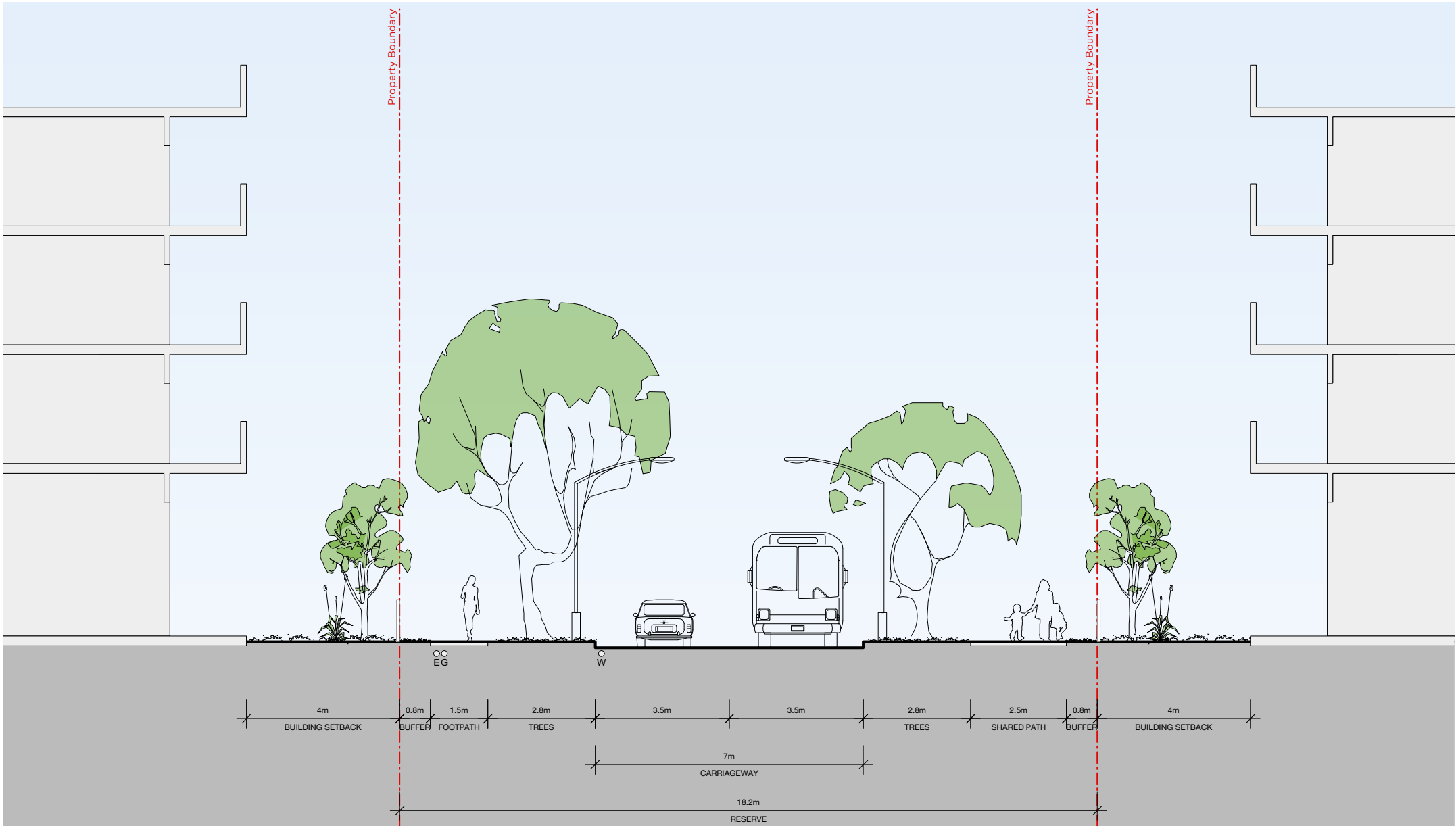
- Entry Boulevard - entrance and arrival
- Bus Loop Road - public transport access
- Main Loop Road - circulation
- Riparian Edge Road - activation of green space
- Local Street - typical street
- Laneway - private resident access



ENTRY BOULEVARD (TYPE A)

Reinforcing the Entrance and sense of arrival to the site

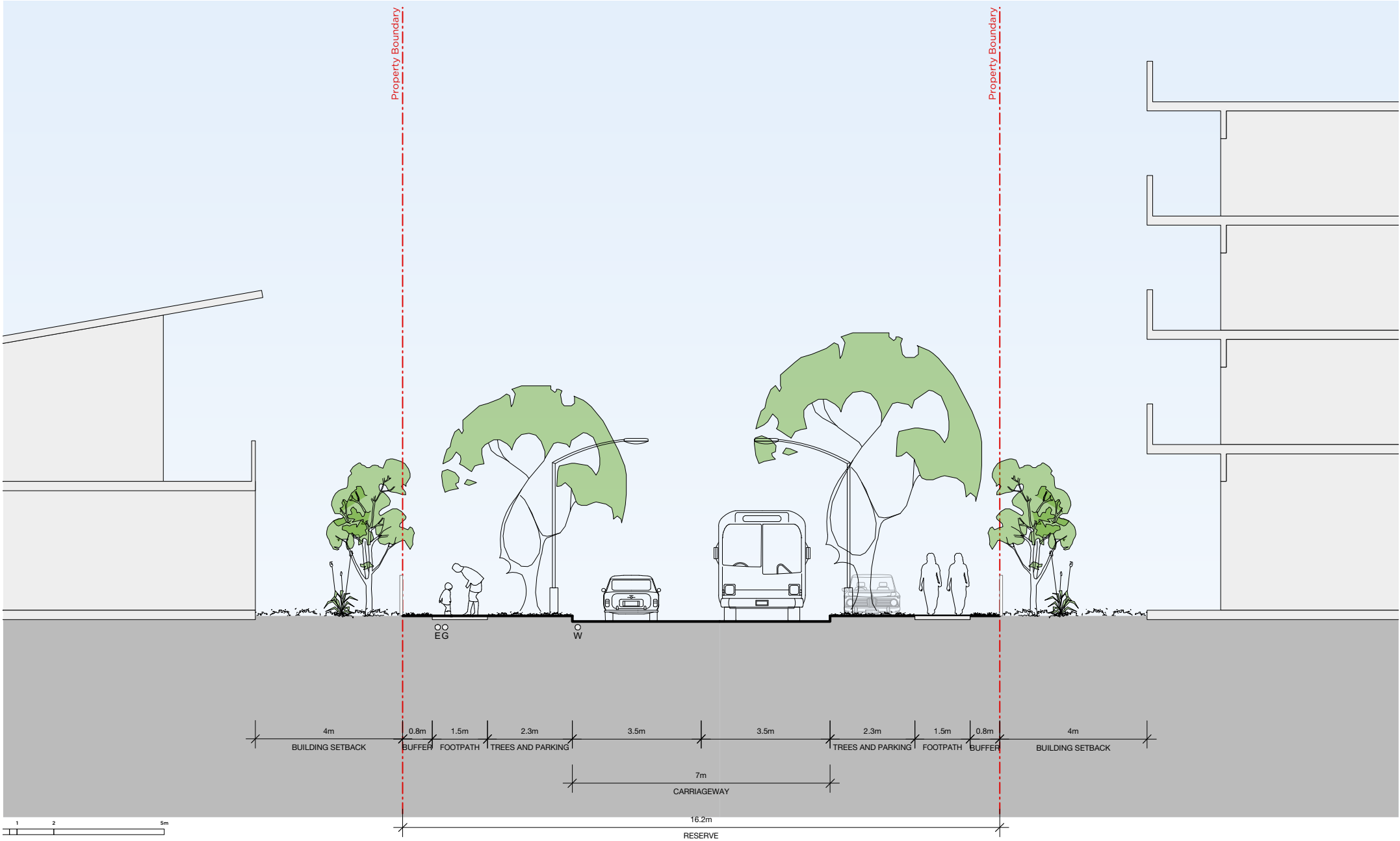
RESERVE	18.2 m
CHARACTER	Grand entrance feel providing clear, direct access to the station and Civic Plaza for vehicles, pedestrians and cyclists.
LANDSCAPE	Mature trees in rows with understory planting in verge.
PARKING	No dedicated parking



BUS LOOP ROAD (TYPE B)

Allowing for the circulation of a public transport loop.

RESERVE	16.2 m
CHARACTER	A clearly defined route for quicker circulation in and out of the Precinct and reinforcing the link between the Civic Plaza and larger usable open space in the Riparian corridor
LANDSCAPE	Mature trees in rows with understory planting in verge.
PARKING	Dedicated parking on both sides of the carriageway



MAIN LOOP ROAD (TYPE C)

Clearly defined main circulation loop around the precinct.

RESERVE	17.6 m
CHARACTER	Providing a clearly defined main circulation around the site for visitors and locals and flexible to accommodate differing ground level interfaces
LANDSCAPE	Mature trees in rows with understory planting in verge.
PARKING	Dedicated parking lanes on both sides of carriageway.



RIPARIAN EDGE ROAD (TYPE D)

Addressing and activating the edge of the Riparian Corridor and adjoining public open space.

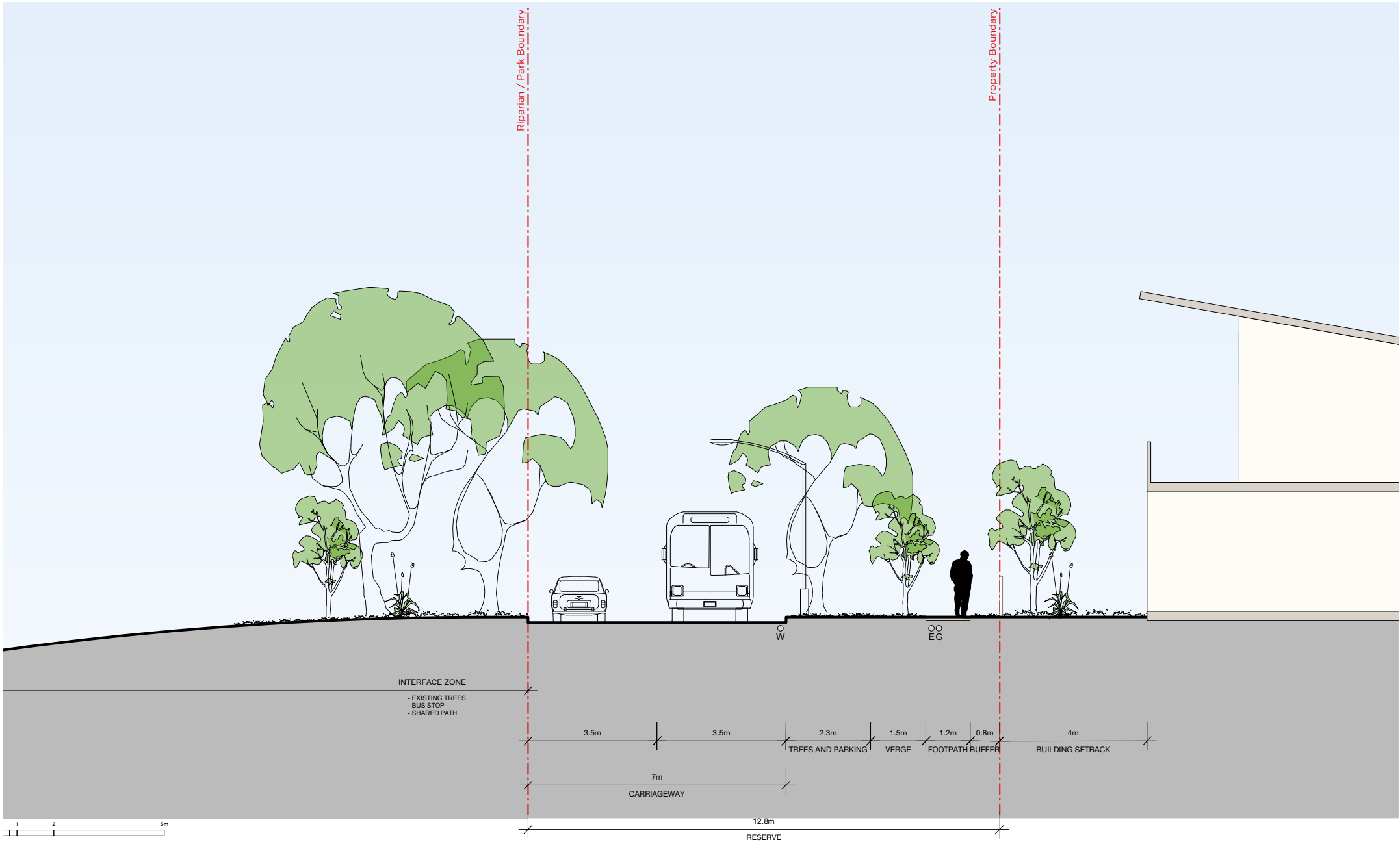
RESERVE	11.8 m
CHARACTER	Provides interface to public open spaces.
LANDSCAPE	Mature trees in rows with understory planting in verge.
PARKING	Single side of dedicated parking on the opposite side to creek edge.



RIPARIAN EDGE ROAD (TYPE E)

Adaption of the main Riparian Edge Road at the Northern end to accommodate bus circulation loop and relating carriageway needs.

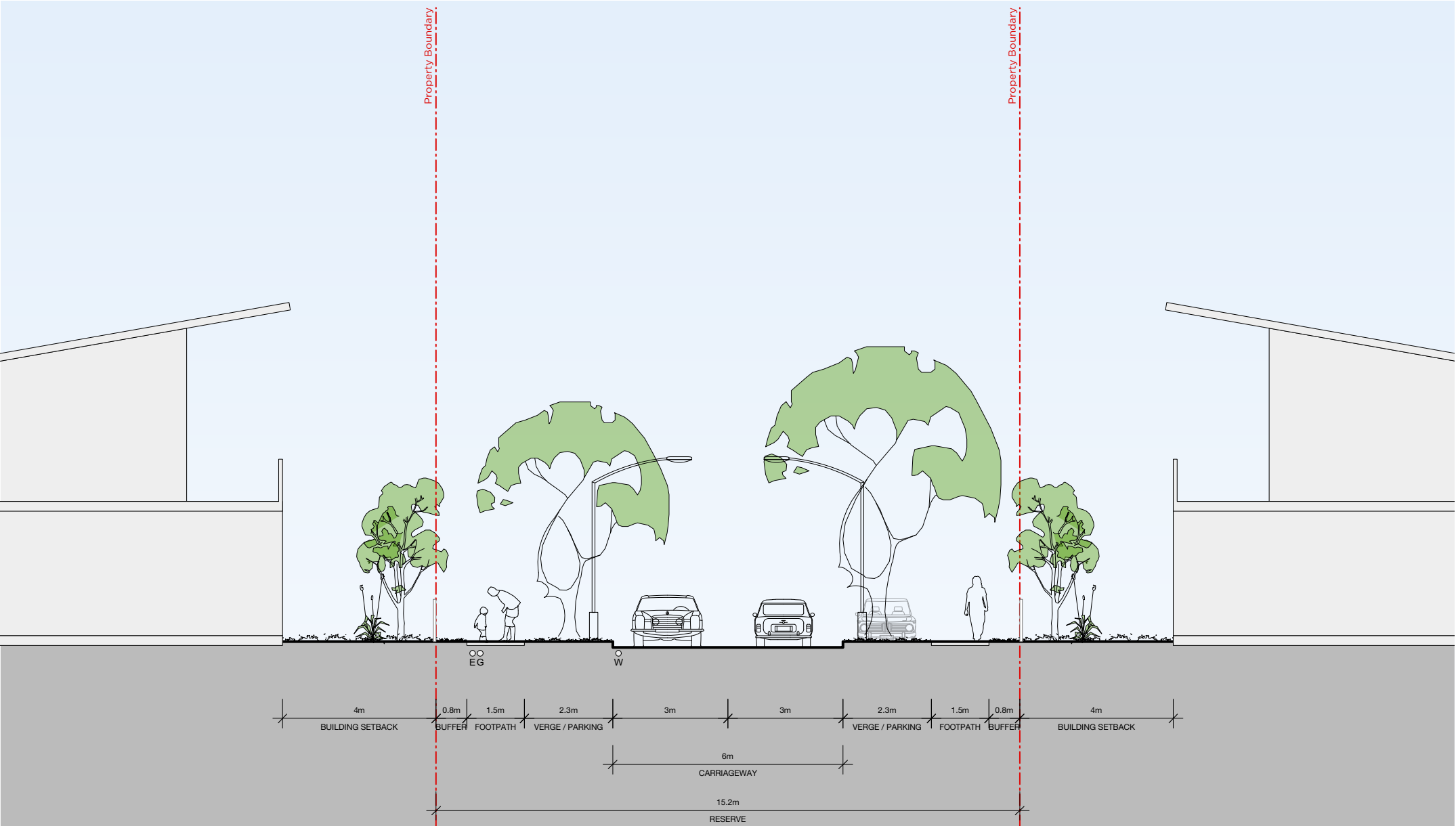
RESERVE	12.8 m
CHARACTER	Provides interface to public open spaces and bus circulation route along it
LANDSCAPE	Mature trees in rows with understory planting in verge.
PARKING	Dedicated parking on a single side of the carriageway opposite to the creek edge.



LOCAL STREET (TYPE F)

Catering for everyday use and providing pedestrian amenity.

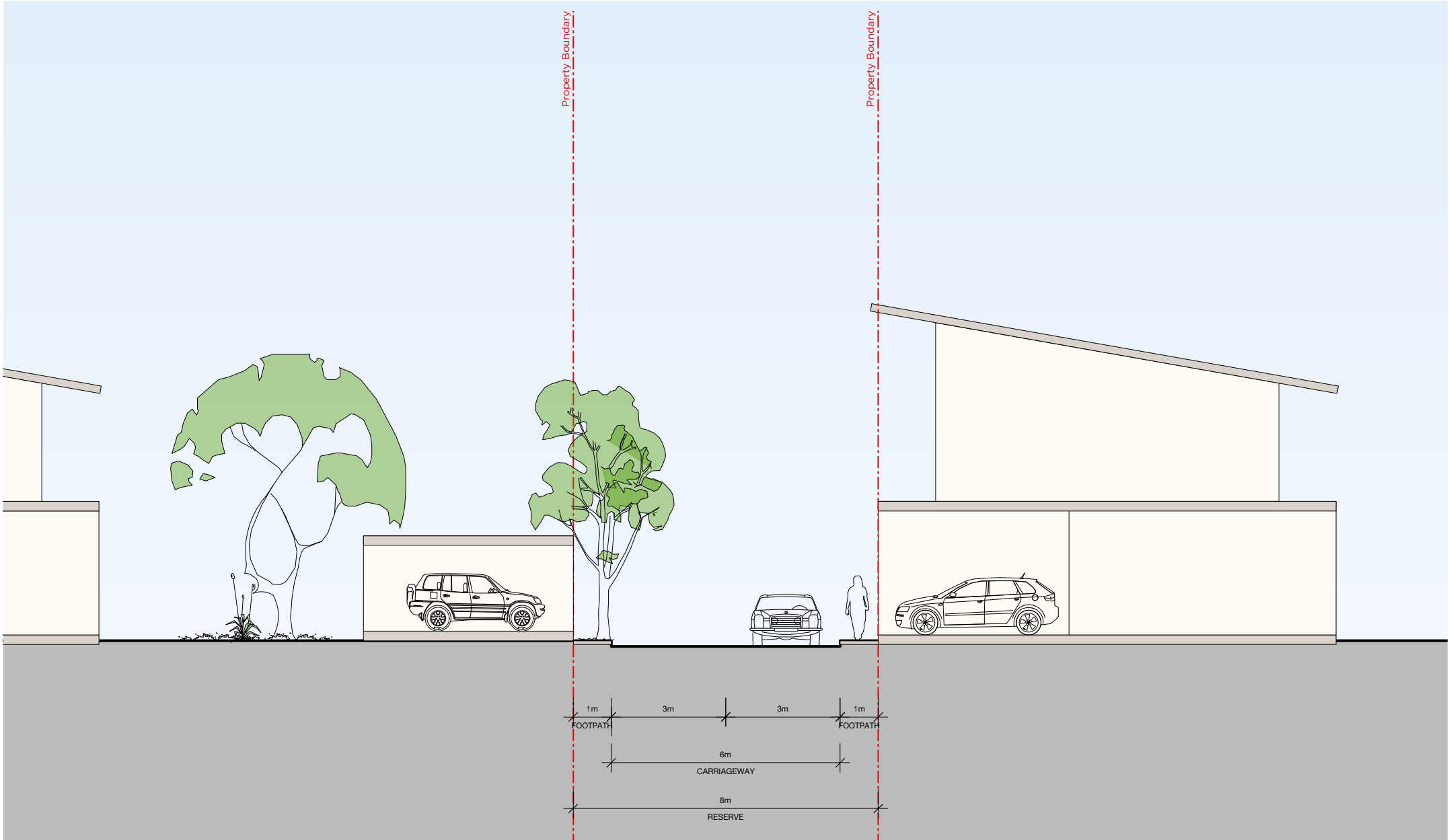
RESERVE	15.2 m
CHARACTER	Provide local connections between more major streets and public open space within the Precinct.
LANDSCAPE	Mature trees in rows with understory planting in verge.
PARKING	Dedicated parking lanes on both sides of carriageway.



LANEWAY (TYPE G)

Allowing for private residential access while still prioritising pedestrians.

RESERVE	8 m
CHARACTER	Highly activated mixed-use lane with pedestrian priority
LANDSCAPE	Small trees in rows with understory planting in verge
PARKING	Nil



STREET TYPOLOGIES DESIGN METRIC
AND FUNCTIONAL CRITERIA

The street typologies have been developed in accordance with the Wollongong City Council DCP and AusRoad Standards.

- The streets have been developed to provide:
- A high quality pedestrian environment.
 - Opportunities for active transport connectivity.
 - Zones for generous street tree and verge planting to mitigate the urban heat island effect.
 - Opportunities for WSUD, and sustainable management of stormwater flows.
 - To allow for buses, service vehicles and emergency vehicles.
 - To provide improved access to Corrimal Station.
 - To provide on-street parking in suitable locations.
 - To allow for the safe and efficient movement of vehicles around the Site.

The adjacent table sets out the key dimensions of the new road typologies within the Site.

Type	Road	Total Reserve	Verge			Carriageway	Verge		
			Buffer	Footpath/ shared path	Parking / Planting	Travel Lanes	Parking / Planting	Footpath/ Shared path	Buffer
A	Entry Boulevard	18.2 m	0.8 m	1.5 m	2.8 m	7 m	2.8 m	2.5 m	0.8 m
B	Bus Street	16.2	0.8 m	1.5 m	2.3 m	7 m	2.3 m	1.5 m	0.8 m
C	Main Loop Road	17.6 m	0.8 m	1.2 m	2.3 m	6 m	2.3 m	3.2 m	0.8 m
D	Riparian Edge Street	11.8 m	-	-	-	6 m	3.8 m	1.2 m	0.8 m
E	Riparian Edge Street - Bus	12.8 m	-	-	-	7 m	3.8 m	1.2 m	0.8 m
F	Local Street	15.2 m	0.8 m	1.5 m	2.3 m	6 m	2.3 m	1.5 m	0.8 m
g	Lane	8.0 m	1 m	-	-	6 m	-	-	1 m



Built Form and Architecture

CORRIMAL COKE WORKS WOLLONGONG

The opportunity of the proposed Master Plan is that it will be delivered as a holistic, integrated community which enables full control of the built form to ensure that the key place and development principles are met.

An architectural vision for the Site has been developed in parallel with built form controls to create a place that responds to, integrates and develops the Site's existing character.

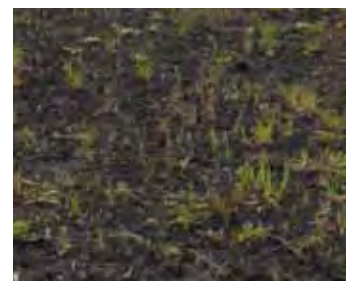
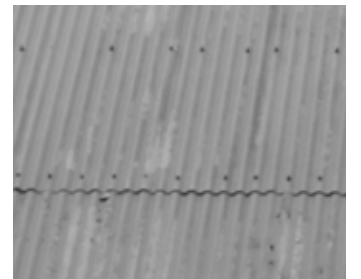
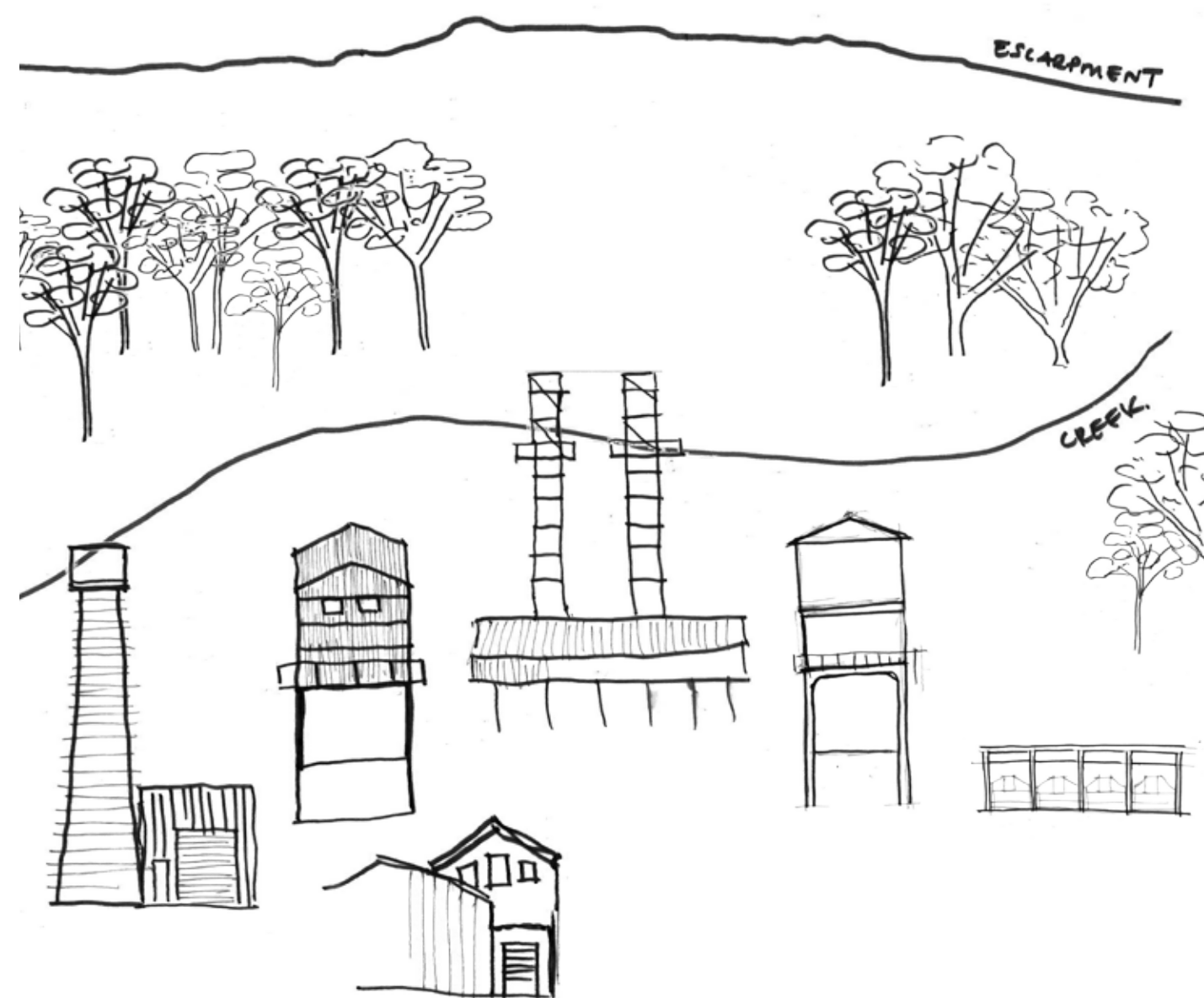
SITE CHARACTER ELEMENTS

The existing physical character of the Site is comprised of a number of elements, both built and natural which are dispersed in different ways across the large area

These elements are clustered in different parts of the Site, creating a series of changing characters. The remnant industrial structures are contained in the north-eastern corner, which have drama vertical structures. The existing creek, dams and riparian corridor divide the site and establish a strong sense of the movement of water. The Site is frame by lush established landscape, with the Illawarra escarpment establishing a dominant skyline behind the site.

SITE DERIVED MATERIALITY

The colours and materiality of both the natural and built environment of the site create a rich palette which the proposed architectural vision seeks to draw on. Materials and colours are proposed to be used in both a similar way but also interpreted in more modern means to ensure the Site speaks to the future and is timeless.



DESIRED FUTURE CHARACTER

The vision of the Master Plan is for Corrimal Coke Works to become a liveable and sustainable residential precinct with a mix of housing typologies including medium density and low to mid rise apartment buildings. The site's industrial heritage will play an important role in defining the future character, with key elements such as the 1912 brick chimney, old powerhouse building and coke ovens retained or interpreted as part of the development.

A liveable and contemporary precinct will be created around Corrimal train station, with quality public realm and neighbourhood retail services. The character is marked by a main entrance road creating a view axis to the brick chimney, and filtered by the retention of wooded trees along Railway Street.

Connectivity to both the Corrimal Town Centre, local community and recreation resources will be optimised for the new neighbourhood and for the benefit of the broader community.

The creek re-alignment will provide a valuable open space resource with north-south connections, linking to the Towradgi Creek corridor. Improved streetscapes will encourage active transport to and from Corrimal Station for residents and the wider community. Green linkages through the site will have the potential to connect to the broader pedestrian and cycling network.

CHARACTER AREAS

Development of built form in the Corrimal Coke Works will be defined by four new character areas: Civic Hub, Treetop Escarpment, Green Edge and the Village Park. Each precinct has a distinct desired character that is to be reflected in the design of built form and public domain to ensure there is no 'cookie cutter' approach with the same bland materiality, built form and public realm. All development at Corrimal Coke Works is to contribute to achieving the desired character within each precinct.

The desired character for each of the following areas are outlined in the following section of this report, and correlate with the proposed Site Specific DCP built form controls.

-  Civic Hub
-  Treetop Escarpment
-  Green Edge
-  Village Park





CIVIC HUB AREA

Character:

A more urban and active environment with taller buildings and a unique built form character which relates to and is derived from the retained heritage items. This character in combination with the area tying in with the Railway station strengthens it as the focal activity hub in the site.

The built form character is contributed to by active building frontages and buildings built to boundary. It will also be characterised by buildings with strong masonry bases and fine grain metal detailing which also seeks to interpret the character of the heritage elements.

Features:

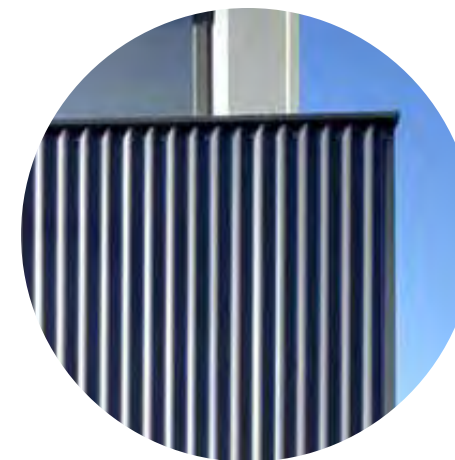
- Zero setback (G - L1) with 3m setback above
- Protected, recessed balconies
- On grade active frontages
- Awning over footpath
- Defined masonry base
- Metal cladding
- Finely detailed metal elements



Active Frontage



Metal Detailing



Metal Cladding

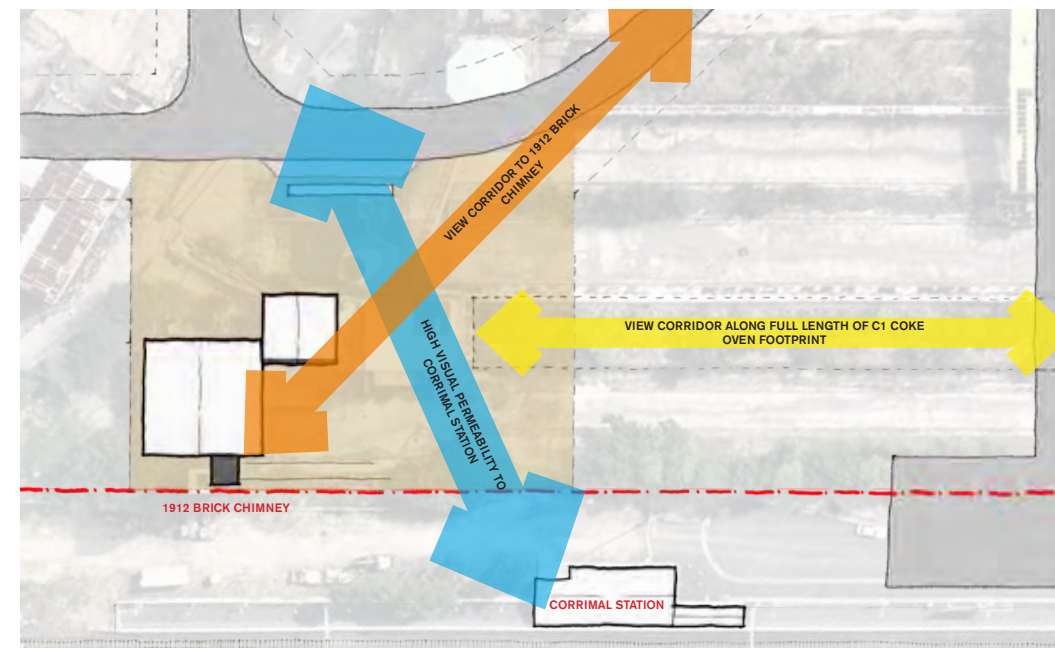


Masonry Base

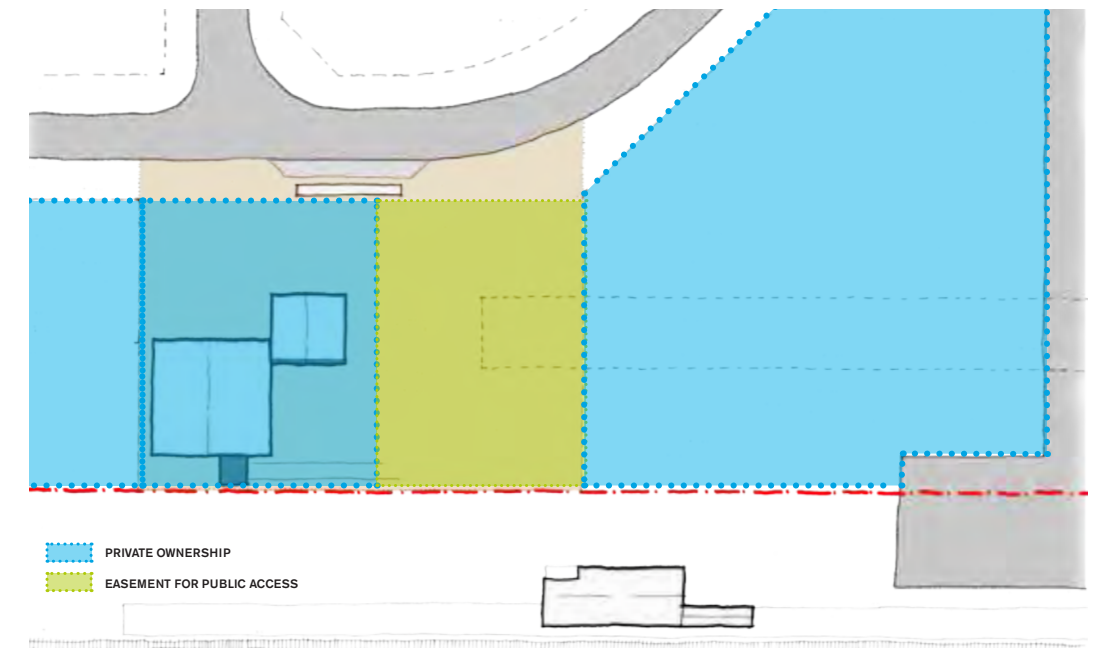
CIVIC HUB AREA - RETAIL PLAZA BUILT FORM STRATEGY

The Civic Plaza is the focal point of both the Civic Hub Character Area and the entire Master Plan. Specific built form controls are proposed to ensure that the heritage items and character of the space are respected and carried through, while supporting the space as an active community and retail hub. The proposed built form controls also ensure that public access to Corrimal Station is clear and direct, and that visual connections to the brick chimney are retained.

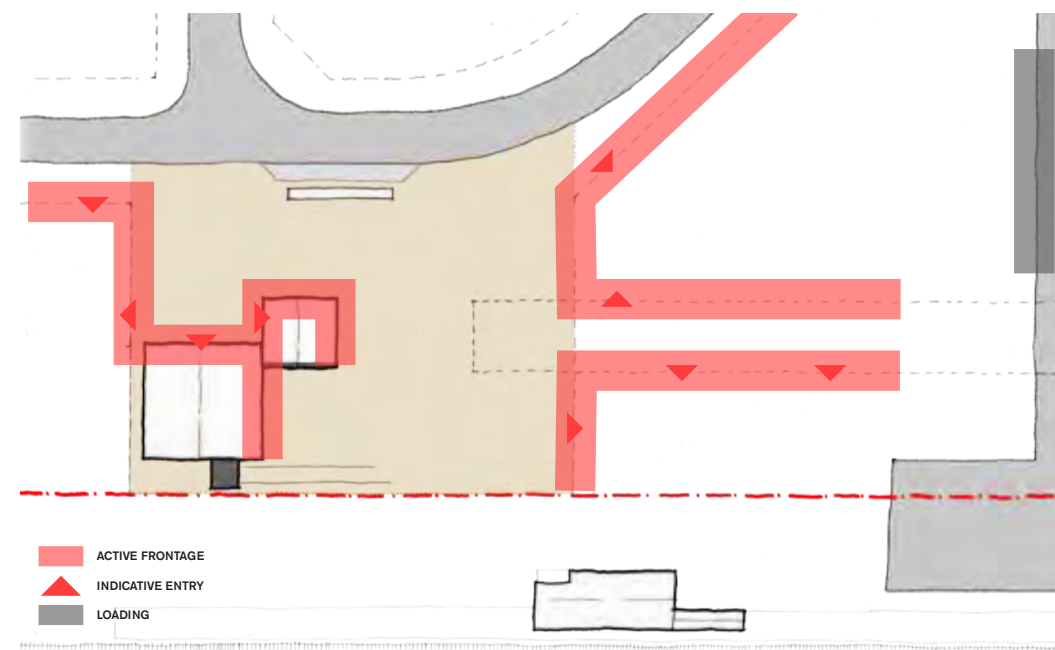
These development objectives and controls are outlined in the propose Site Specific DCP.



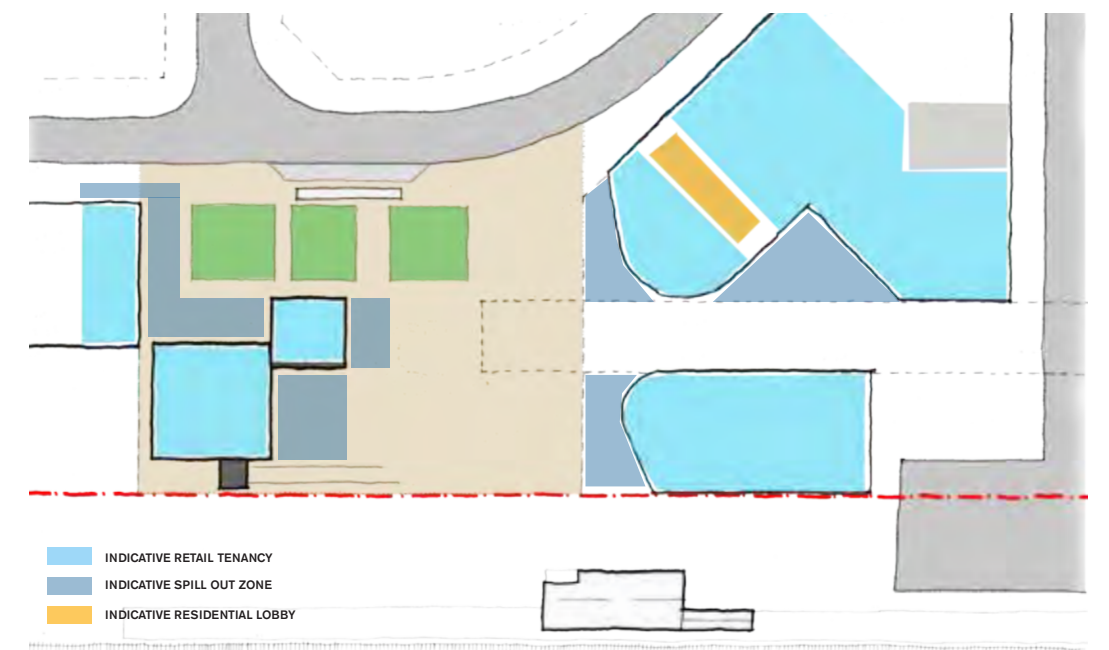
Visual Permeability and Sight Lines



Proposed Ownership



Active Frontages, Access and Loading



Indicative Plan and Spill Out Areas

BUILT FORM AND ARCHITECTURE
CIVIC PLAZA PERSPECTIVE





TREETOP ESCARPMENT AREA

Character:

Defined by built form with a refined character featuring elements strong vertical emphasis referencing the wooded interface and to break down the larger scale buildings. The area will also be marked by a dynamic roof line that “speaks to” the escarpment. Open balconies make use of the views and connection to the mature tree canopy, as well as to the escarpment and back to the coastline.

The area will also be characterised by landscape edges which assist to green the more urban street typologies and bleed into the adjacent generous setback to Railway Street and community garden. Ground floor apartments will be elevated above the street to define private space from the public domain instead of a reliance on visually dominant fencing, with the level change mediated by a stoop interface.

Features:

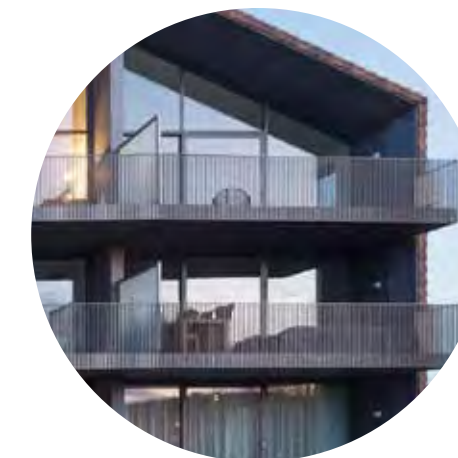
- Wide frontage balconies
- Landscaped and brick stoop ground interface
- Iconic roof form
- Architectural elements and massing providing a vertical emphasis



Stoop Interface



Dynamic Roof Form



Wide Frontage Balconies



Strong Vertical Elements

BUILT FORM AND ARCHITECTURE
ENTRY BOULEVARD SHOWING TREETOP ESCARPMENT





GREEN EDGE AREA

Character:

Comprised of a mix of built forms and scales, the character of the area seeks to engage with and reflect the lush riparian corridor which runs along its edge. Architecturally, built form will be simple with a more horizontal expression. It will be characterised by open facades and a light-weight feel featuring fine grain timber elements and screens and lightweight materiality. Generous upper level balconies make use of views of the riparian corridor and facilitating passive surveillance over the open space.

Buildings will have recessed ground floors to accentuate the sense of the upper levels of the buildings extending out to connect with the green edge. Built form setbacks are more generous to allow for an extension of the green space into front yards, with landscaping and low height, permeable fencing separating the public and private realms.

Features:

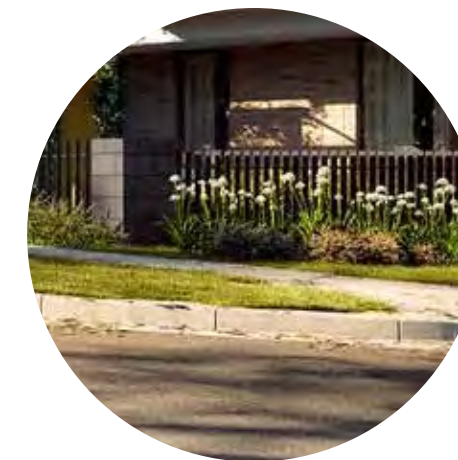
- Recessed lower level with protruding upper level balcony
- Low height front fence with landscaping and generous setback
- Predominantly rear-loaded product
- Simple Rectilinear form
- Lightweight materiality
- Fine grain timber look elements
- Pergolas and growing trellises



Rectilinear Form



Fine Grain Vertical Elements and Screens



Low Height Fence with Landscaping



Lightweight Materiality

BUILT FORM AND ARCHITECTURE
RIPARIAN EDGE PERSPECTIVE





VILLAGE PARK AREA

Character:

An urban village feel with mix of low rise dwelling forms which focus on the central park. The dwellings generally are in the form of attached, rear-loaded terraces with some front loaded compact houses which provide an alternative to apartment living. Dwellings are orientated to address the park and provide passive surveillance.

Defined front edge conditions with more solid fence lines and formal landscaping clearly differentiating between the public and private domains and ensuring privacy. The built form of the dwellings will be characterised by solid, finely grained architectural expression, with classic village elements including pitched roof forms and defined verandah porches and entry porticos.

Features:

- Defined entry portico/verandah
- Defined front masonry fence with landscaping
- Front-loaded and rear-loaded product
- Pitched roof element
- Masonry materiality with fine detailing
- Privacy screens



Pitched Roof Element



Defined Porches or Entry Porticos



Defined Front Fence



Masonry Materiality



SUMMARY OF BUILT FORM BUILDING
TYPOLOGIES

The Structure Plan provides a framework for a number of innovative housing typologies.

The adjacent table sets out a summary of key design metrics for each typology and the anticipated location within the Site where each type would be located.

Full built form controls are set out in the proposed Site Specific DCP.

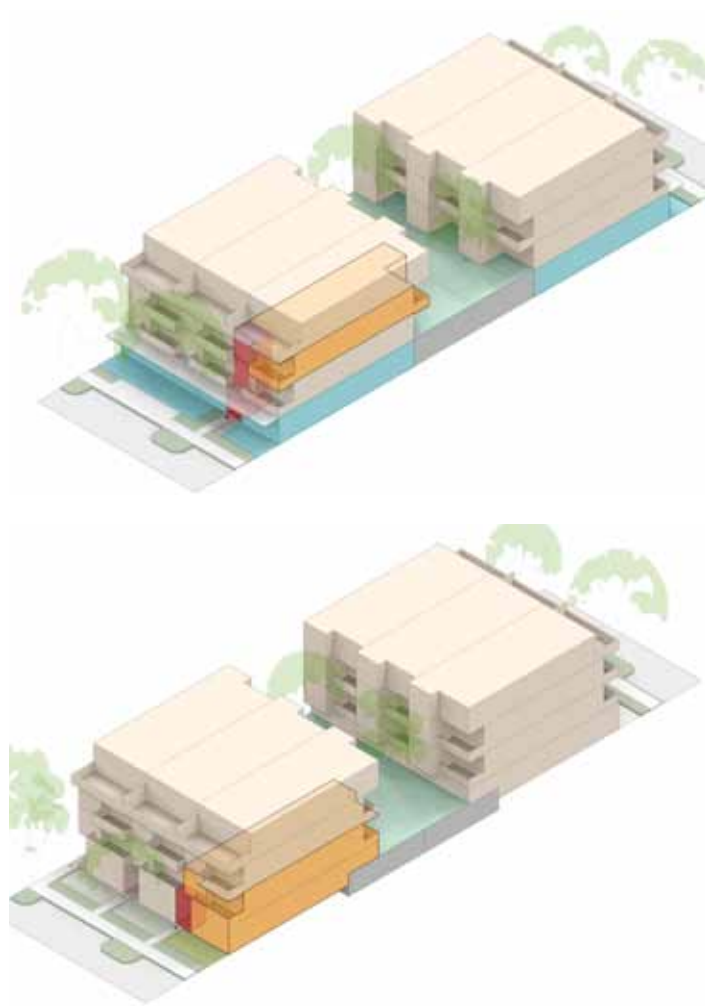
FRONT SETBACKS AND PUBLIC
DOMAIN INTERFACES

For each character are, differing front setback and public interface conditions (including secondary street setbacks) are proposed to reinforce the differing characters. This is outlined in full within the proposed Site Specific DCP.

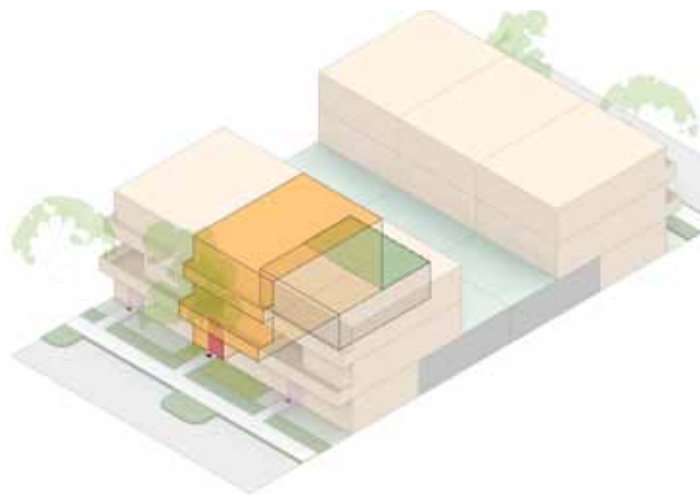
The following pages include the proposed front setback diagram for the Master Plan, as well as a selection of sections showing the different interface conditions.

Typology	Lot Width	Type				Side Setback	Rear setback	Minimum PPOS	Solar access to PPOS	Landscape Area	Location			
		Attached	Semi-Detached	Zero Lot	Detached						Civic Hub	Treetop Escarpment	Green Edge	Village Park
Shop Top Housing	N/A	N/A				SEPP 65 Applies					Y	N	N	N
Residential Flat Building	Minimum 30m	N/A				SEPP 65 Applies					Y	Y	Y	-
Multi Dwelling (Sleeved Townhouses)	N/A	N/A				3m	6m	Minimum 16m ² with a minimum dimension of 3m	For 70% of dwellings, 3hrs access to 50% of PPOS between 9am and 3pm June 21	30% of lot area	N	Y	Y	Y
Torres Title Medium Density	>4.5m	Y	N	N	N	0m, 3m between contiuous runs	Detached Garages: 0.5m to garage, 10m to facade line	Ground Level: minimum 16m ² with a minimum dimension of 3m		15% of lot area	-	-	Y	Y
							Attached Garages; 0.5m to garage, 2.5m to facade line	Upper Level/Rooftop: minimum 10m ² with a minimum dimension of 2.5m						
	7m-9m	Y	Y	Y	N	Attached: as above	If rear loaded: as above	Attached: as above		15% of lot area	-	-	Y	Y
						Semi-detached: 0.9m or 1.2m adjacent zero lot boundary	If front loaded: 4m to facade line (Ground Level) and 6m to facade line (Upper Level)	Semi-detached and Zero Lot: Ground level with minimum 20m ² with a minimum of 4m						
						Zero Lot: 0m or 1.2m								
	>9m>15m	-	Y	Y	Y	Semi-detached: as above	4m to facade line (Ground Level) and 6m to facade line (Upper Level)	Ground level with minimum 20m ² with a minimum of 4m		25% of lot area	-	-	Y	Y
						Zero Lot: as above								
						Detached: 0.9m or 1.2m adjacent zero lot boundary								
Secondary Dwellings and Studio Dwellings	N/A	N/A				N/A		Separate PPOS only required if Strata title: minimum 8m ² with a minimum dimension of 2m	3hrs access to 50% of PPOS between 9am and 3pm June 21`	N/A	-	-	Y	Y

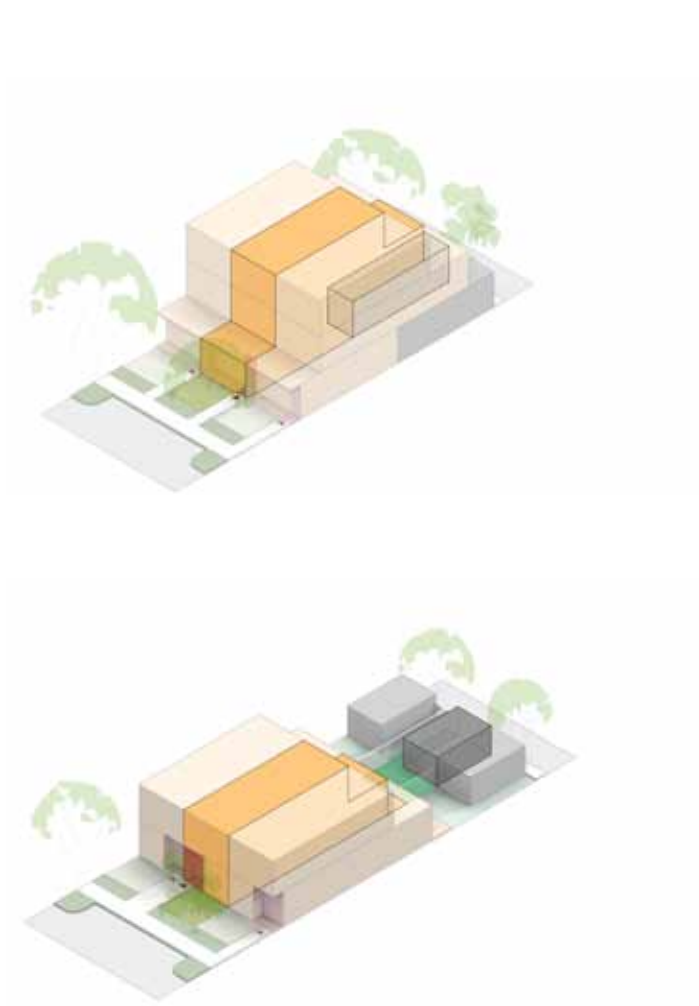
SHOP TOP HOUSING & APARTMENT
TYPOLOGIES



MULTI-DWELLING (SLEEVED
TOWNHOUSE) TYPOLOGY



3 & 2 STOREY REAR-LOADED TERRACE
TYPOLOGIES



2 STOREY FRONT-LOADED DETACHED
TYPOLOGY



Legend

	Living area		Carpark
	Access		Landscape buffer
	Front setback		Retail
	Rear garden/ terrace		Road

BUILT FORM GROUND LEVEL SETBACKS

The interface between the public domain and private lots is a key consideration in the development of successful urban master planned developments.








The proposed ground level setbacks within the Site reflect the different conditions, land use and built form situations.

Key features of the proposed ground level setbacks include:

- Deep soil landscape setbacks for the predominately residential areas, with setbacks developed according to building typology, scale and form.
- Generous landscape setbacks on Railway Street.
- Active frontages and zero lot setbacks in the village centre and retail areas to reflect the higher levels of activity.
- Provision for through-site links within the village plaza to provide a diversity of public domain experiences and opportunities for street-level activation.
- A setback to the railway corridor on the eastern edge of the Site to increase amenity.
- A public path interface along lots that directly adjoin public space.

The table below further describes each setback type and typical built form sections can be found on the following page.

Legend

-  The Site
-  A
 - Residential Flat Building: 5.0m
 - Multi-Dwelling: 4.5m
 - Medium Density: 4.5m with 3.5m articulation zone
-  B: 0.0m (Ground Level) and 3.0m (Level 1 and above)
-  C: 4.0m with 3.5m articulation zone
-  D: 15.0m
-  E: 3.5m with 2.5m articulation zone
-  I: 3.0m with 2.0m articulation zone



PROPOSED STREETWALL APPROACH
AND UPPER LEVEL SETBACKS

The Indicative Structure Plan proposes a series of street wall conditions to define enclosure of primary streets and the Civic Plaza.

The design intent is to frame public spaces, articulate the built form and create a human scale across the Site.

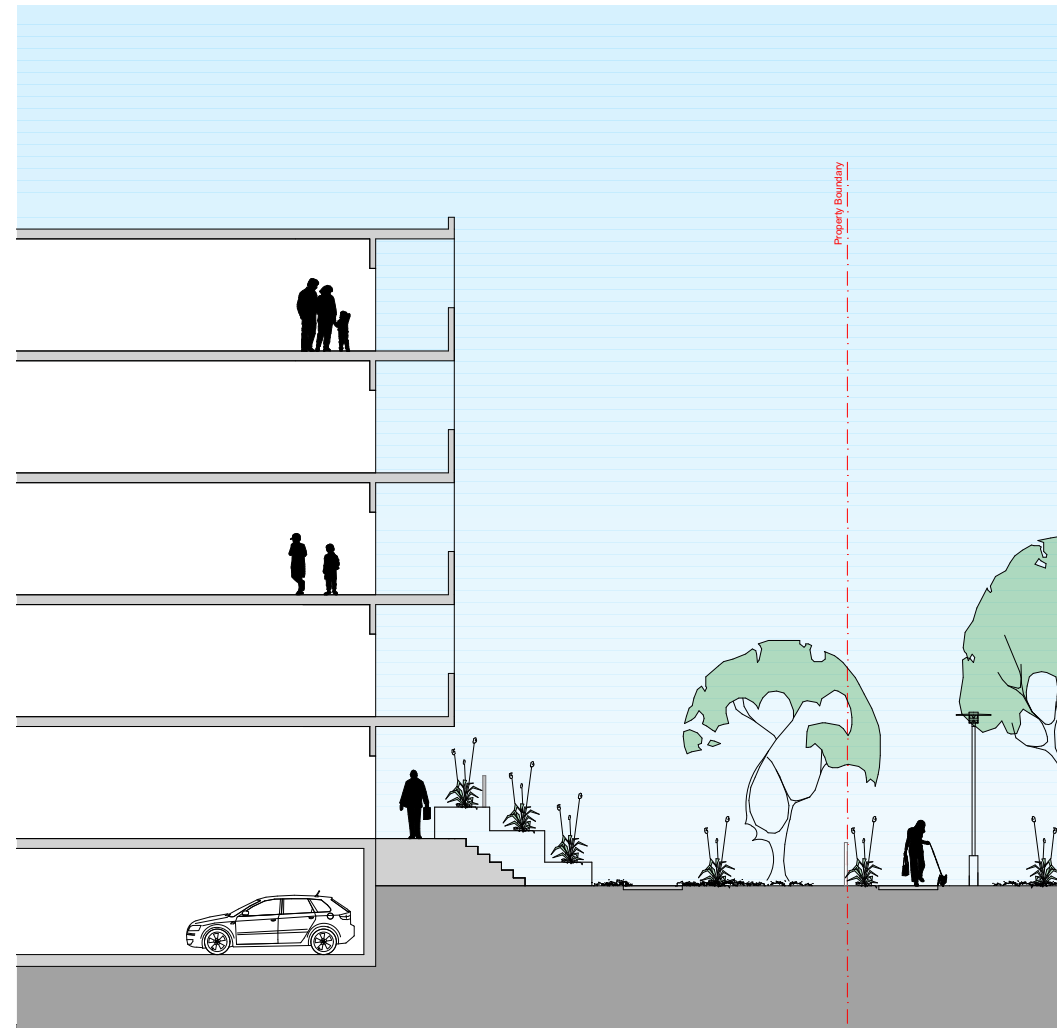
The street wall approach has been developed in tandem with the character area concepts to ensure a diverse architectural language unique to the Site and locality.

Legend

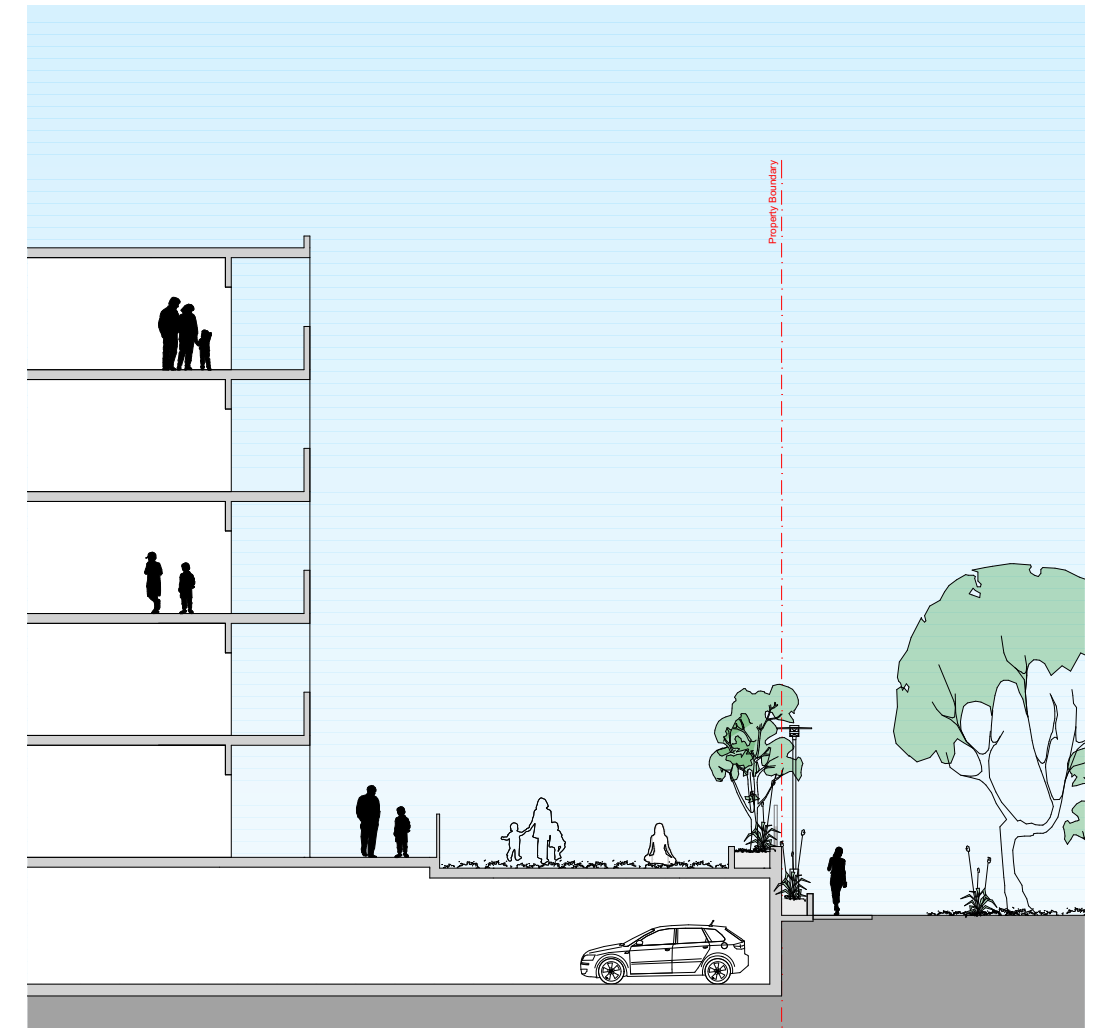
The Site

A: 3m from below facade line above 5 storeys

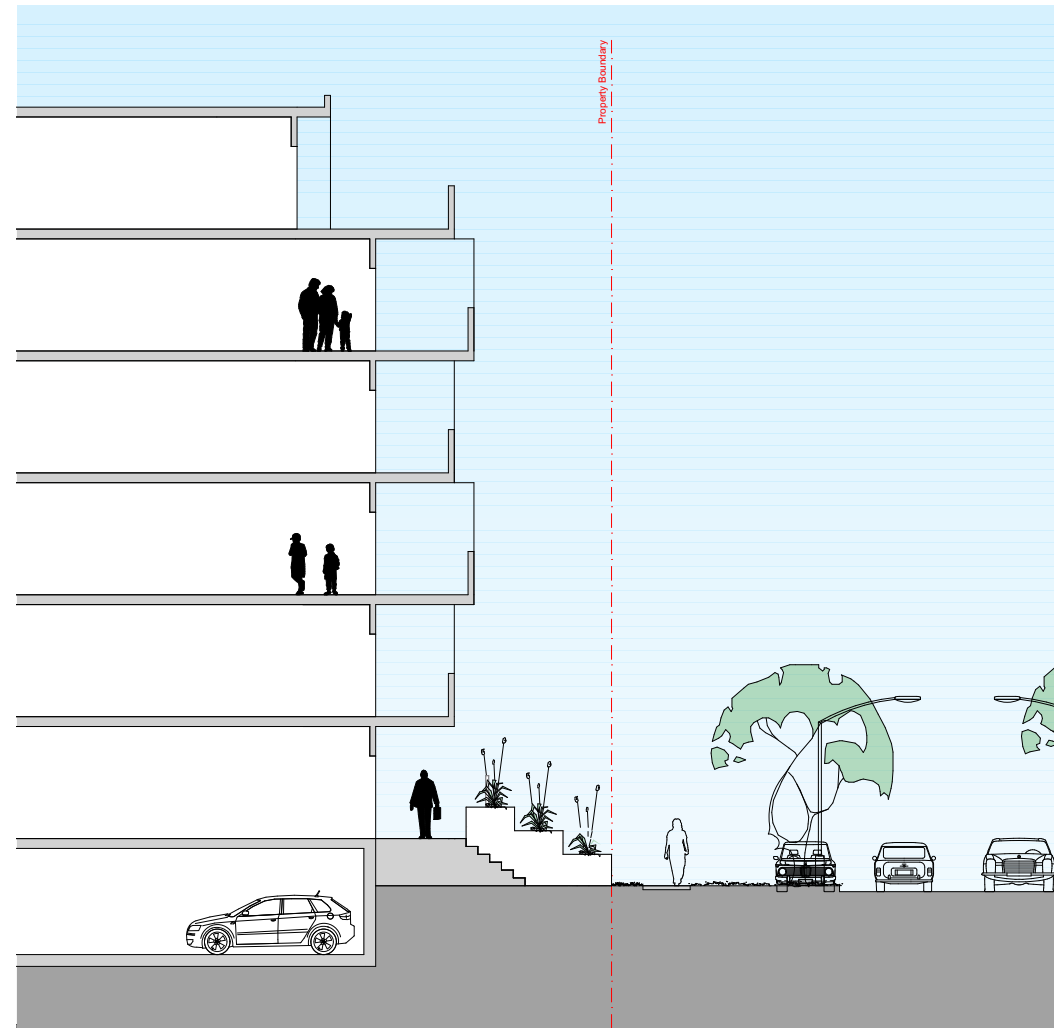




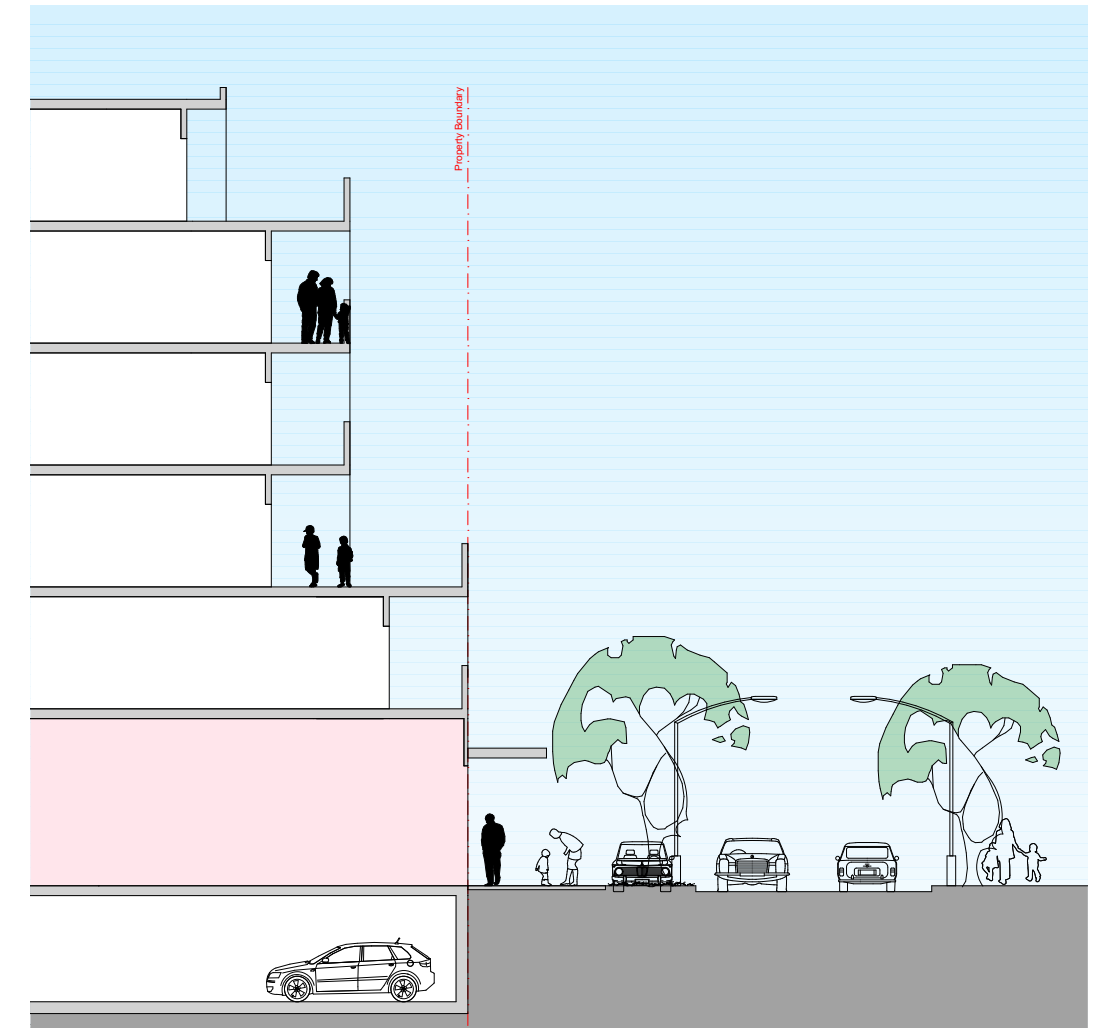
1. INDICATIVE SECTION - NORTHERN BUFFER INTERFACE



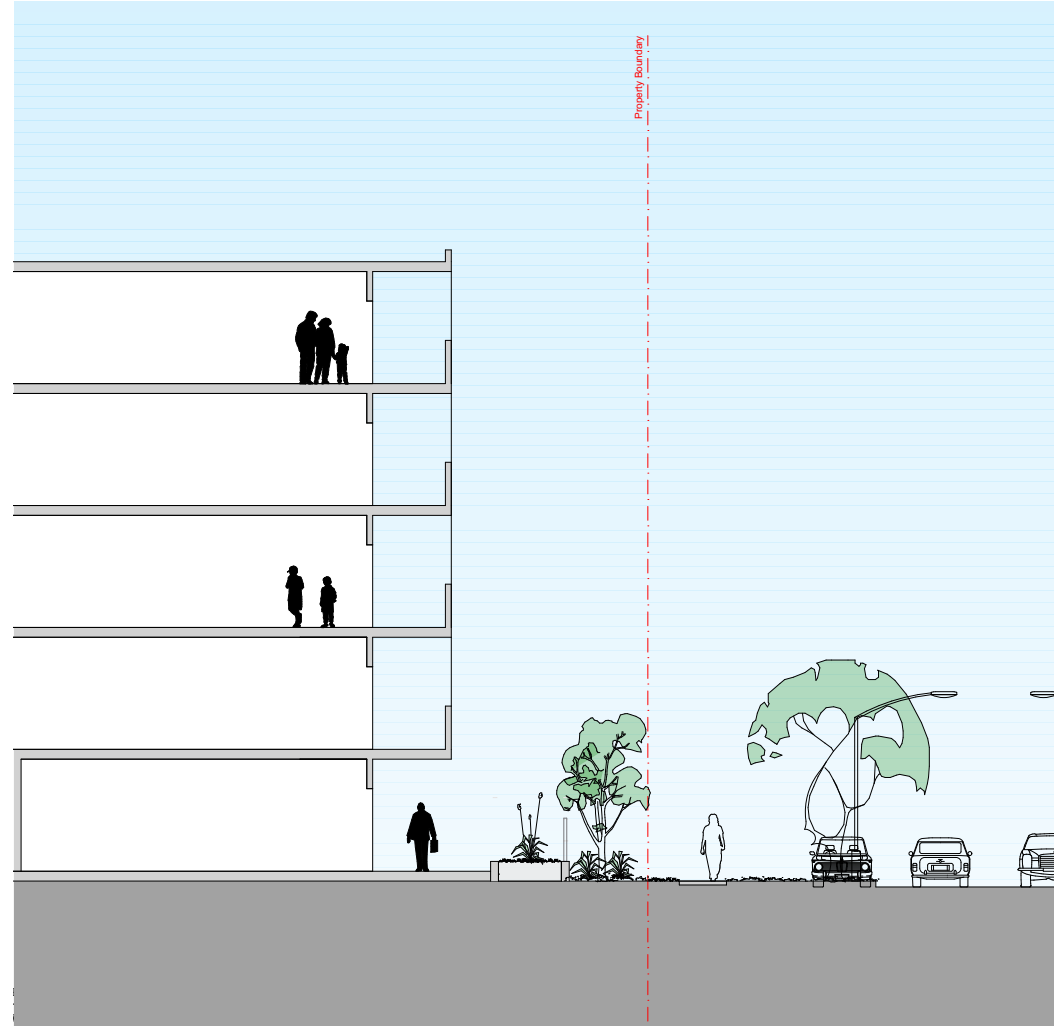
2. INDICATIVE SECTION - APARTMENT RIPARIAN REAR INTERFACE



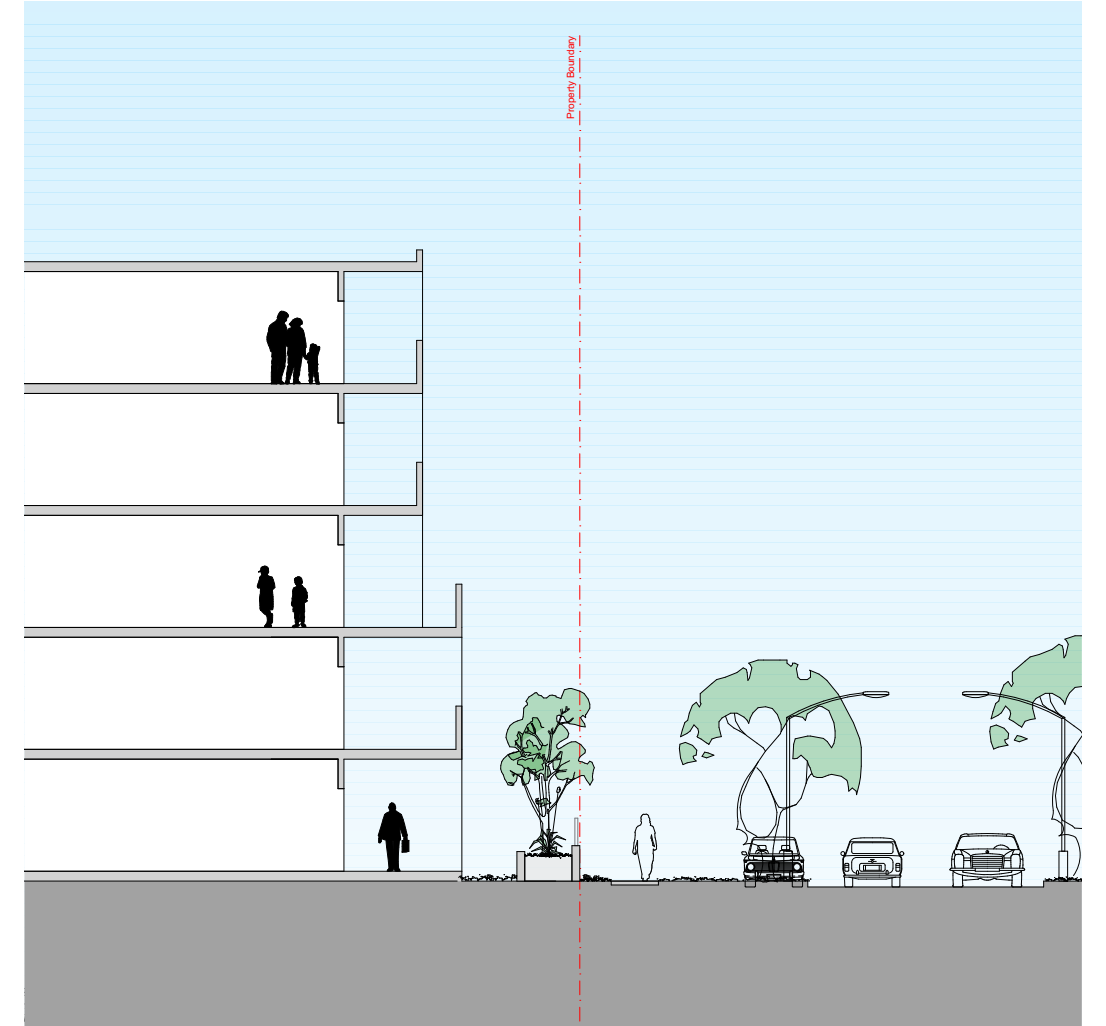
3. INDICATIVE SECTION - APARTMENT ENTRY
BOULEVARDE EDGE (1/2-IN 1/2-OUT BASEMENT CAR
PARK)



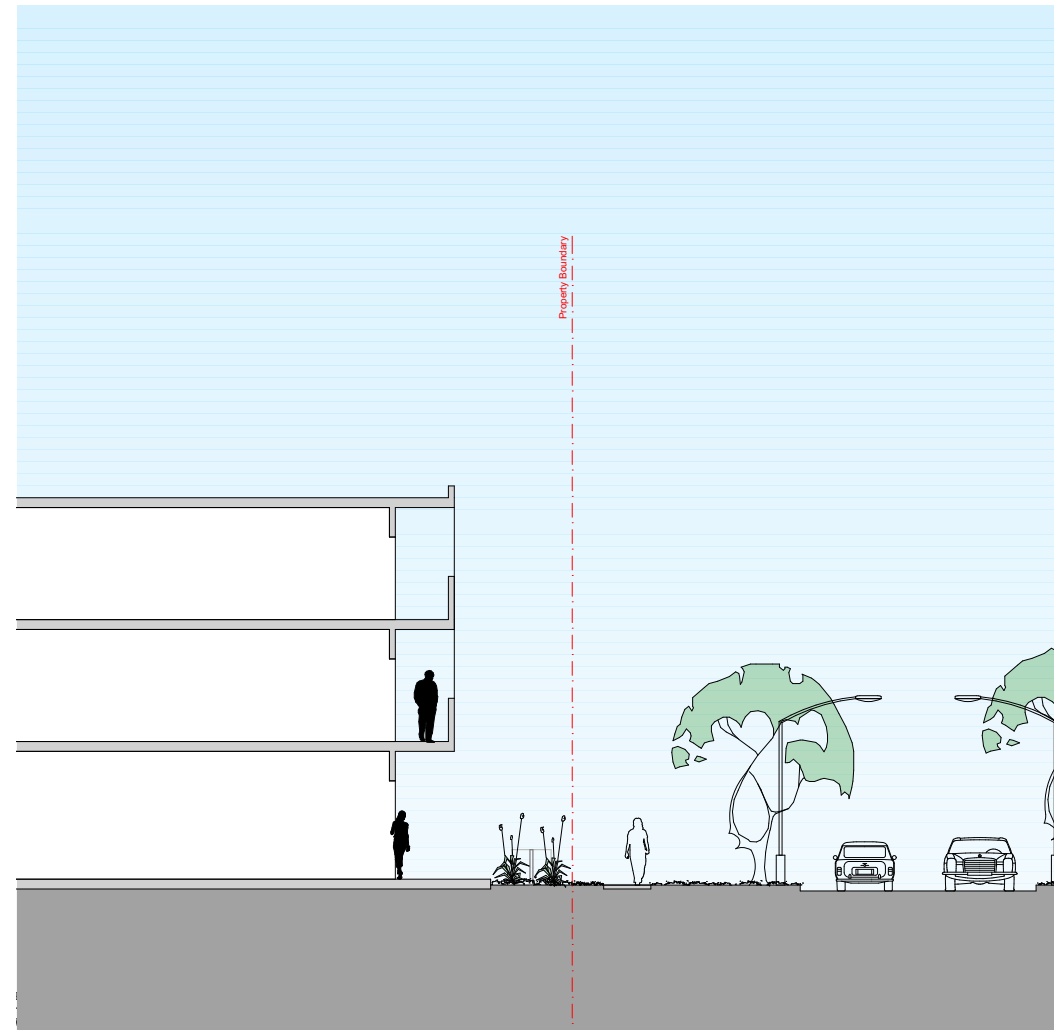
4. INDICATIVE SECTION - ZERO BOUNDARY ACTIVE
STREET FRONTAGE (BASEMENT CAR PARK)



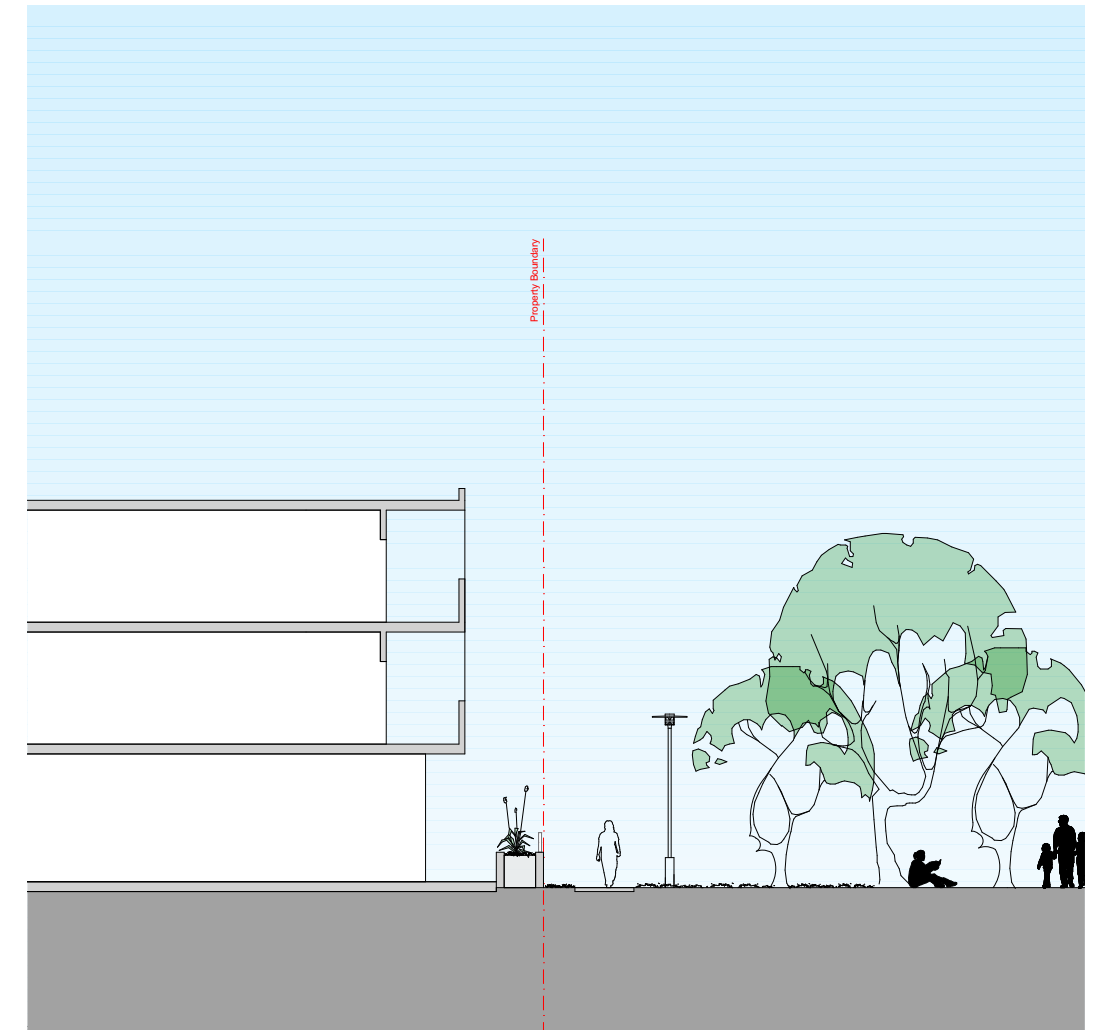
5. INDICATIVE SECTION - APARTMENT RIPARIAN INTERFACE (SLEEVED CAR PARK)



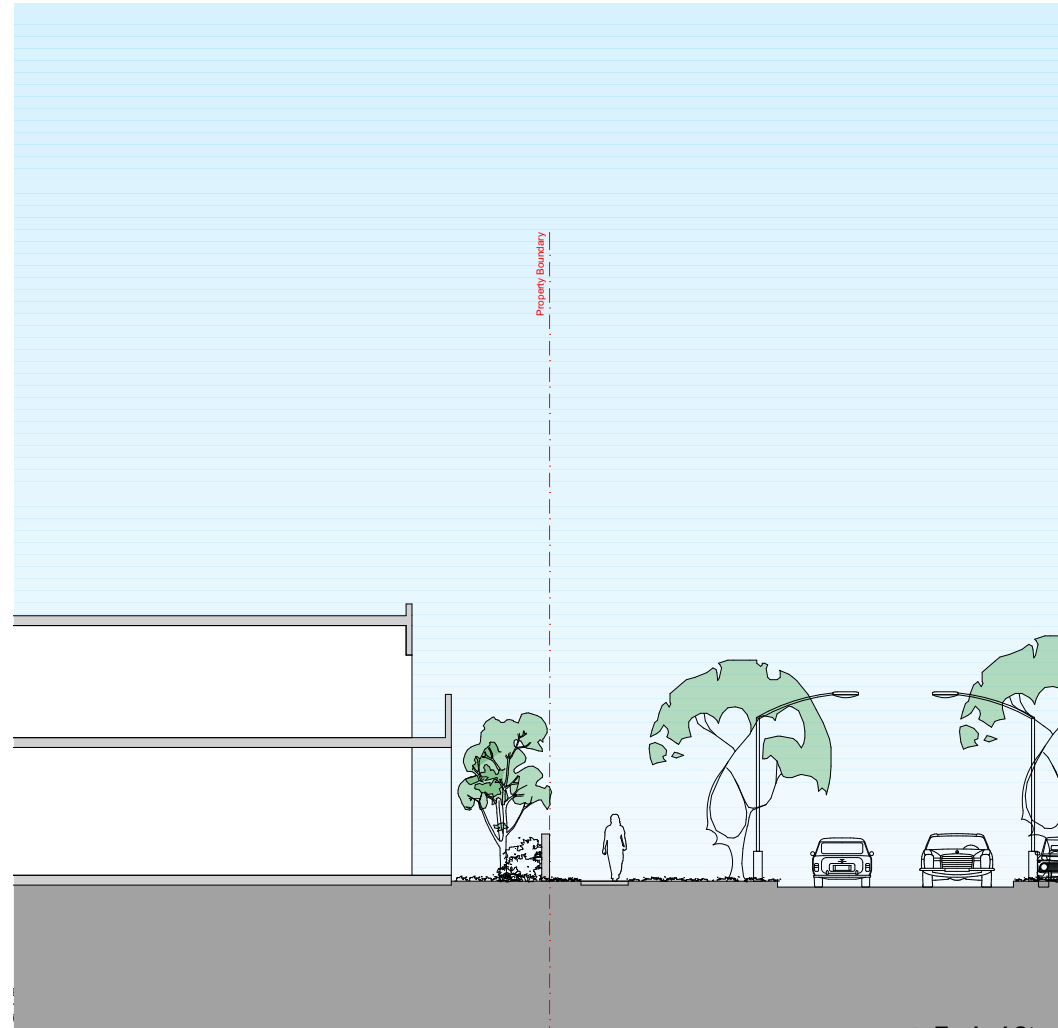
6. INDICATIVE SECTION - APARTMENT LOCAL STREET INTERFACE (SLEEVED CAR PARK)



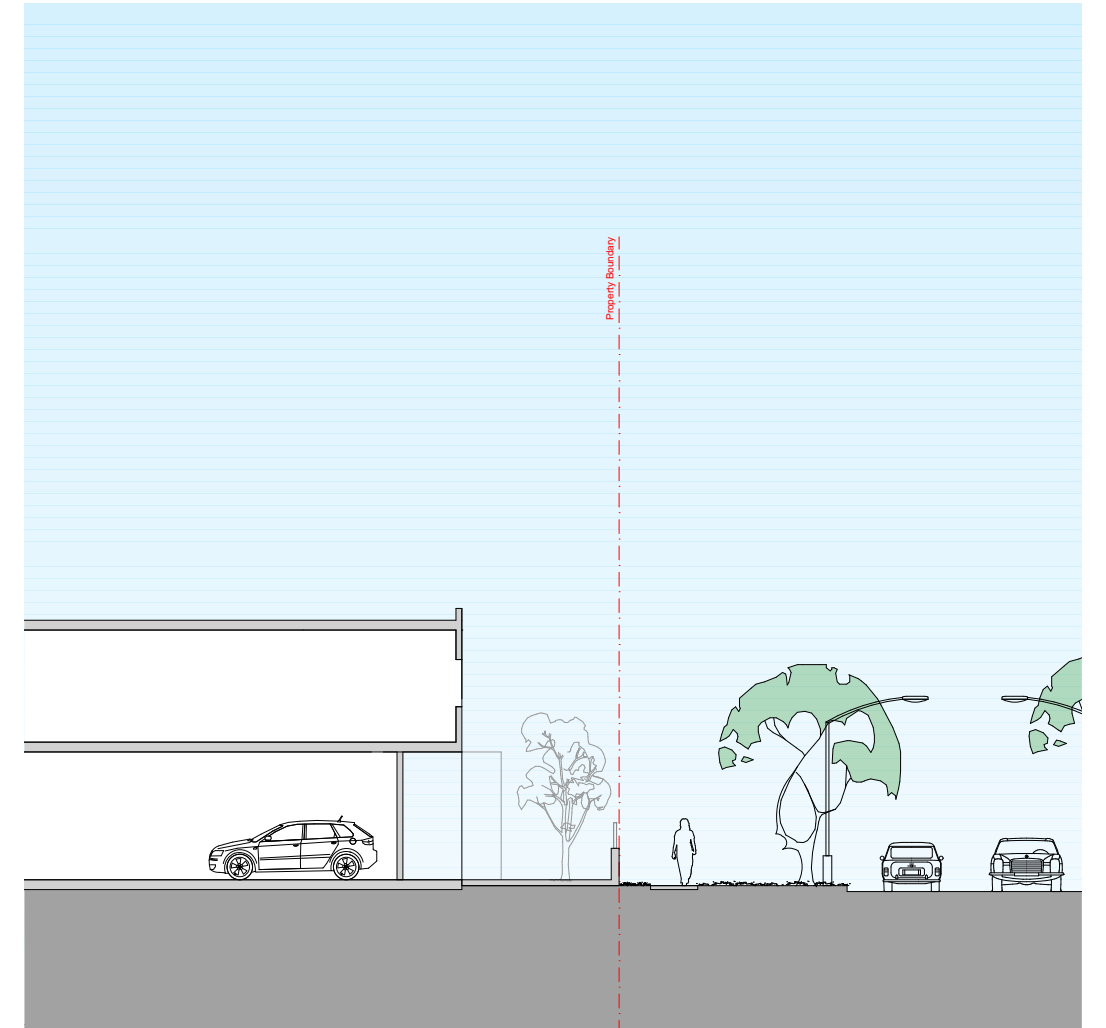
7. INDICATIVE SECTION - MEDIUM DENSITY RIPARIAN FRONT INTERFACE (REAR LOADED)



8. INDICATIVE SECTION - MEDIUM DENSITY PUBLIC PARK FRONT INTERFACE (REAR LOADED)



9. INDICATIVE SECTION - MEDIUM DENSITY MAIN LOOP INTERFACE (REAR LOADED)



10. INDICATIVE SECTION - MEDIUM DENSITY LOCAL STREET INTERFACE (FRONT LOADED)








PROPOSED BUILDING HEIGHT

The strategy for building heights has been developed in recognition of the current skyline, and ensures that the existing Northern Stack and the Brick Chimney Stack remain the tallest structures on the Site.

The strategy also addresses the interface on the East and West of the Site by setting development back so the long range views towards the Escarpment are still visible along public streets running perpendicular to the Site.

The adjacent plan sets out the proposed height zones and identifies the relative heights of the retained stacks in the north east of the Site.

Legend

-  The Site
-  J (9m)
-  N (13m)
-  Q (20m)
-  Q2 (16m)
-  S (24m)
-  Stack (37m)
-  Brick Chimney (28m)



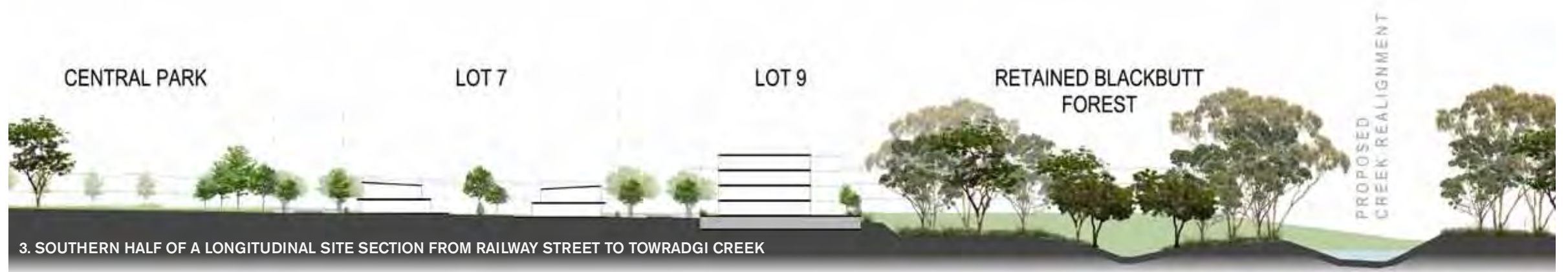
SITE SECTIONS



1. EASTERN ELEVATION OF CIVIC PLAZA AND SURROUNDING PROPOSED BUILDINGS

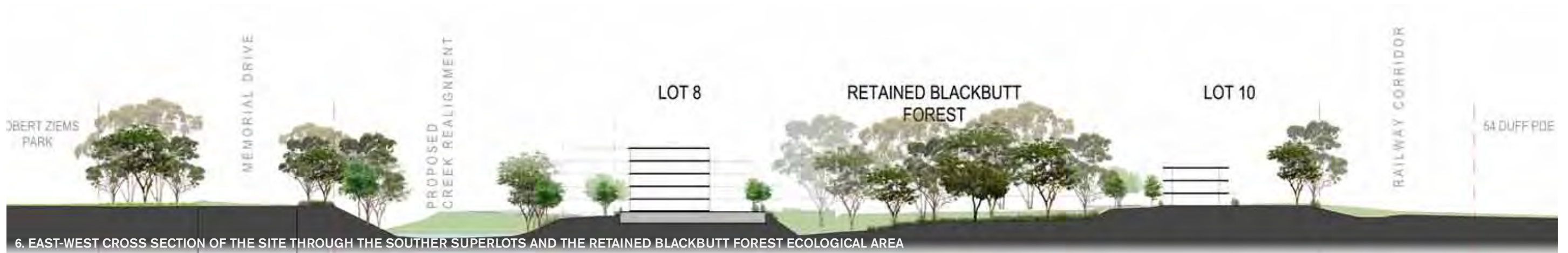
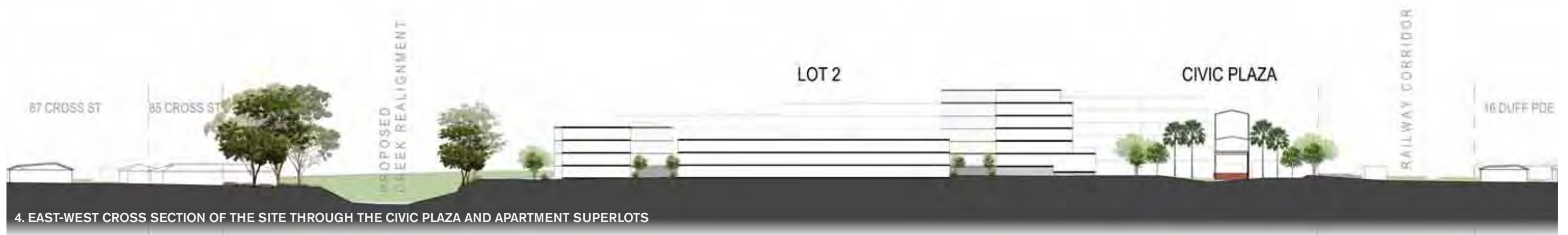


2. NORTHERN HALF OF A LONGITUDINAL SITE SECTION FROM RAILWAY STREET TO TOWRADGI CREEK



3. SOUTHERN HALF OF A LONGITUDINAL SITE SECTION FROM RAILWAY STREET TO TOWRADGI CREEK










INDICATIVE STAGING PLAN

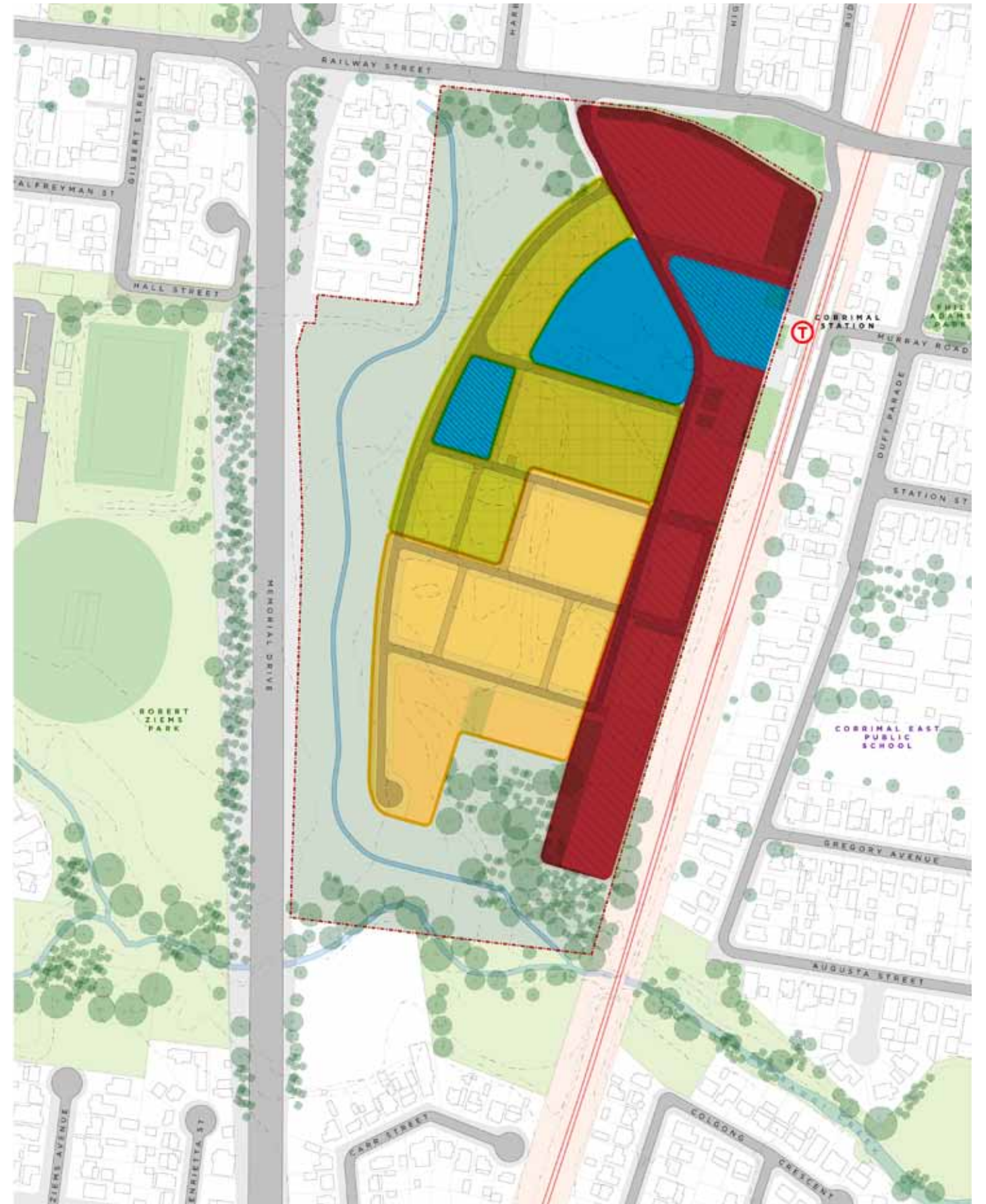
Development of the former Corrimal Coke Works will occur in stages over a number of years. The realignment of the creek and associated bulk earthworks is key to enabling development and will occur prior to delivery of built form. The indicative staging plan identifies the current approach to staging of the development and is based on the following objectives:

- Establish the key entry and spine road in the initial stage of the development in conjunction with the Civic Plaza, to provide early activation of Corrimal station
- Provide for development on the western/ riparian edge of the site to progress from South to North in order to minimise construction impacts
- Incorporate a mix of housing typologies within each stage
- Recognition that the majority of retail uses will not be viable until there is sufficient resident population on site

Ultimately the timing and staging of the development may evolve to respond to market conditions over the life of the project.

Legend

-  The Site
-  Stage 1
-  Stage 2
-  Stage 3
-  Stage 4







DKO

e8u

BG
&E



Clouston associates



Planning Proposal

Former Corrimal Coke Works

Client: Legacy Property & Illawarra Coke Company

Date: 06 August 2020

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Executive Summary

The Planning Proposal (PP) has been prepared on behalf of Legacy Property (Legacy) and the Illawarra Coke Company (ICC), for the rezoning of the 18.18 hectare (ha) former Corrimal Coke Works site, located at 27 Railway Street, Corrimal in the Wollongong Local Government Area (LGA). The former Coke Works ceased its operations in 2014, and now offers a substantial rezoning and redevelopment opportunity to provide much needed additional housing within Wollongong while becoming an exemplar of urban renewal.

The original draft PP was prepared and submitted to Council in October 2017. The PP was then placed on preliminary public exhibition from 23 October to 24 November 2017. Council prepared a preliminary assessment, which was endorsed (subject to further technical and design studies) on 3 April 2018 and forwarded to the Department of Planning, Industry and Environment (DPI&E) for a Gateway Determination on the 23rd April 2018.

The Gateway Determination was issued by the DPI&E on 20th August 2018, allowing the rezoning proposal for the Corrimal Coke Works site to proceed subject to conditions, with an 18 month timeframe for completion. The Gateway deadline was subsequently extended by 12 months on the 17 February 2020. This PP addresses and responds to Council's resolution, consultation outcomes with the community as well as the Gateway Determination.

The Proposal

The site is currently zoned IN3 Heavy Industrial and RE2 Private Recreation under Wollongong Local Environmental Plan (WLEP) 2009 and has previously been utilised for industrial works. This application proposes to rezone the site from IN3 and RE2 to R3 Medium Density Residential and RE1 Public Recreation, as well as providing appropriate controls relating to additional permissible uses, minimum lot size (MLS), height, and floor space ratio (FSR). The PP also request other minor amendments to the WLEP in relation to other "overlays". A small portion of the site (0.25ha) to the north east, is currently zoned SP2 Infrastructure - Road, for potential future acquisition. This land has been incorporated into the concept plan and will be retained as SP2.

The rezoning of the site will provide for a master planned community with approximately 700 – 750 dwellings, small scale local retail, business and community uses, and open space. Approximately 9.5ha, representing 52% of the site, will be provided as open space and riparian corridor.

The site provides a unique opportunity to provide a range of housing choices that meet the current and future needs of the community, including, apartments, strata titled townhouses, affordable housing and the potential for seniors living/aged care. Legacy and ICC are committed to providing 5% affordable housing, which is proposed for inclusion within a Voluntary Planning Agreement (VPA).

A heritage precinct will reflect the site's former use and contribute to the activation of Corrimal train station, with neighbourhood scale retail uses, flexible commercial space to support tele-working and local business start-ups, and a shared community space.

The site provides excellent proximity to existing services, schools and community facilities. A new pedestrian and cycling link will connect Railway Street in the north, through the site, and across Towradgi Creek to the south, forming a significant regional asset as part of the broader 'bush to beach' connection. The development will also significantly improve accessibility to Corrimal train station and support increased public transport use.

The proposed redevelopment of the site offers an exciting opportunity to deliver a new environmentally, socially and economically sustainable community that contributes to the revitalisation of Corrimal, delivering significant public benefits and represents a major investment in the Wollongong economy.

Strategic Context

The Corrimal Coke Works is nominated as a key site under Wollongong City Council's Corrimal Town Centre Plan 2015-2025 and Implementation Plan. The Town Centre Plan and Implementation Plan states "*Residential development may be suitable, where it offers housing diversity and improved connectivity*". R3 Medium Density Residential zoning is a logical extension of the same zoning to the north, north-east and east of the site, and permits a broad range of housing types including apartments, semi-detached housing and single dwellings.

Wollongong Council's Housing Strategy Discussion paper also identifies the need for medium density housing, as a housing type for the changing demographic.

Site Specific Context

The site is strategically well located, and suitable for rezoning, as it is:

- » directly adjacent to the Corrimal railway station, providing rail connection to Wollongong Central Business District (CBD) and Sydney
- » directly adjacent to existing bus stops on Railway Street
- » 350m walk to Corrimal town centre
- » 1.2km walk to Corrimal beach
- » In walking distance to community and recreational resources including Robert Ziems Park playing fields, Corrimal library, Corrimal swimming pool, East Corrimal Primary School and Corrimal High School

An analysis supported by extensive technical studies has identified the site as being suitable for development, due to the following site-specific opportunities:

- » Transport oriented development capitalising on existing public transport infrastructure
- » Integration via pedestrian and cycle links with the surrounding community, promoting active transport options
- » New access to the site to cater for traffic movement
- » Realignment and rehabilitation of North Corrimal Creek
- » Retention of key biodiversity areas
- » Protection of important sightlines to the Illawarra Escarpment
- » Provision of neighbourhood scale retail uses to activate Corrimal train station
- » Retention, interpretation and adaptive re-use of significant heritage items
- » Provision of a mix of housing types
- » Resolution of key flooding and drainage issues
- » Provision of infill development, minimising conversion of rural land for housing

The Concept Master Plan for the rezoning area is shown in **Figure 1**.

Design Values, Principles and Concept for the site

Corrimal Coke Works will become a community born of diverse people, places and stories that respects the site's past while showcasing Wollongong's future as the city of innovation.

The Master Plan for Corrimal has been based on the following place principles:

- » **Place Principle 1 - Made for Friendship:** We help people to meet, share and connect, building on and contributing to Corrimal's already genuinely friendly and supportive community. We're creating a safe and

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supportive place where people grow together, look out for each other and share special moments and events.

- » **Place Principle 2** - Designed for difference: We're building a community of many different ages, shapes and sizes. We champion difference and are designed to attract people at different stages of life. Our place is made up of a wide variety of landscapes, precincts, features, housing types and experiences, creating a vibrant and distinctive destination.
- » **Place Principle 3** - Bringing more to life: We invest in making people and places the best they can be, breathing new life into heritage and green space and helping people reach their potential. We make everyday life easier, so that people have more time and energy to enjoy what's important.
- » **Place Principle 4** - United through stories: We cherish our unique story. From our history, to our unique ecology, we're a place like no other. Together we will celebrate the stories of the past and build new stories, forging a strong sense of character and identity.

The Concept Master Plan provides for the delivery of housing within a strong public domain framework while ensuring that the site will be integrated into its existing context. Key features of the Concept Master Plan include:

- » Delivering a diversity of housing with the potential for low-scale apartment buildings, including affordable housing, strata titled townhouses and seniors living/aged care.
- » Providing 9.5ha of green open space to offer a range of community and recreational resources, including a 3,000sqm village park, 5,150 sqm southern recreation space and riparian corridor promoting walking and cycling.
- » Ensuring architectural diversity that responds to the local context through creation of a range of character precincts within the site.
- » Maintaining key views to the escarpment, including a significant view corridor along Murray Street, while also establishing new views within the development to key heritage features.
- » Maximising access to Corrimal train station and delivery of a public plaza adjacent to the station with retention of key heritage structures and also interpretive heritage elements.
- » Activating Corrimal train station and the heritage precinct with neighbourhood scale retail uses at a scale that does not detract from Corrimal Town Centre and East Corrimal shops.
- » Providing flexible community and business space within the heritage precinct, including the potential for tele-working and start-ups.
- » Realignment and rehabilitation of North Corrimal Creek to establish a new riparian corridor with enhanced biodiversity outcomes, while providing a contiguous area of PMF-free developable land.
- » Providing a regional walking and cycling path through the site, connecting Railway Street in the north across Towradgi Creek to the south.
- » Retention of key ecological areas within the southern section of the site and integrated with the riparian corridor, including provision of a suitable buffer to the existing grey headed flying fox camp.
- » Providing a suitable buffer distance from the rail and state road corridors and appropriate landscaping adjacent to the site for noise attenuation.
- » New site access from Railway Street with a proposed roundabout at the intersection of Harbinger Street.

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Figure 1 Corrimal Coke Works Master Plan



Source: DKO

The Concept Master Plan and supporting studies have informed the rezoning of the site.

Gateway decision

On 20 August 2018, the Executive Director, Regions at the DPI&E, delegate of the Minister for Planning, determined that that, under section 3.34(2) of the EP&A Act 1979, that an amendment to the WLEP 2009 should proceed, subject to the following conditions:

1. *To ensure consistency with 9.1 Directions 2.3 Heritage Conservation and 4.3 Flood Prone Land, the following studies are to be completed prior to public exhibition:*
 - *An Aboriginal cultural heritage assessment;*
 - *A conservation management plan that provides for the long-term conservation of significant coke work heritage components;*
 - *Revised flood study (including flood modelling); and*
 - *Geomorphological report*
2. *The following studies are also to be completed prior to public exhibition:*
 - *A revised ecological assessment;*
 - *A revised traffic impact assessment; and*
 - *A revised remediation action plan*
3. *Public exhibition is required under section 3.34(2)(c) and schedule 1 clause 4 of the Act as follows:*
 - *The planning proposal must be made publicly available for a minimum of 28 days; and*
 - *The planning proposal authority must comply with the notice requirements for public exhibition of planning proposals and the specifications for material that must be made publicly available along with planning proposals, as identified section 5.5.2 of A guide to preparing local environmental plans (Department of Planning and Environment 2016).*
4. *Consultation is required with the following public authorities/organisations under section 3.34(2)(d) of the Act and/or to comply with the requirements of relevant Section 9.1 Directions:*
 - *Roads and Maritime Services;*
 - *Department of Primary Industries – Water;*
 - *Environment Protection Authority;*
 - *Office of Environment and Heritage;*
 - *Sydney Water;*
 - *RailCorp;*
 - *Department of Education;*
 - *Heritage Council;*
 - *National Trust of Australia (Illawarra Shoalhaven Regional Branch);*
 - *Endeavor Energy;*
 - *Transport for NSW;*
 - *Department of Primary Industries – Fisheries NSW;*
 - *Catchment Management Authority; and*
 - *State Emergency Service*

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Each public authority is to be provided with a copy of the planning proposal and any relevant supporting material and given at least 21 days to comment on the proposal.

5. *A public hearing is not required to be held into the matter by any person or body under section 3.34(2)(e) of the Act. This does not discharge Council from any obligation it may otherwise have to conduct a public hearing (for example, in response to a submission or if reclassifying land).*
6. *The timeframe for completing the LEP is to be 18 months following the date of the Gateway determination.*

However, on 17 February 2020, the DPI&E amended Condition 6 from '18 months' to '30 months to allow a 12-month extension of time.

The relevant conditions required prior to public exhibition have been met and are addressed in this report.

Addressing and Achieving Council's requirements

Council's resolution of the 3rd April 2018 required that additional information be submitted before or during the exhibition period, to enable Council to consider the information prior to determining the Planning Proposal.

Below is a table outlining a summary of Council's resolution/requirements and a response as part of this Planning Proposal.

Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
a. A site specific DCP Chapter be developed and submitted in conjunction with the amended Urban Design Concept Plan	A Site Specific DCP has been prepared to support this Planning Proposal.
b. Advice from the NSW Heritage Council for comment in relation to: <ol style="list-style-type: none"> i. The Archaeological significance of the site ii. The potential for the site to be listed on the State Heritage Register 	<p>Heritage reports and a Conservation Management Strategy, along with subsequent peer reviews, have been prepared (refer to Appendices S, J, Q, U and V) which determine that the site has no archaeological significance, however does have local heritage significance.</p> <p>The archaeological investigations failed to identify widespread, <i>in situ</i> archaeological remains associated with the tramway which was known to run through the study area. As such, no heritage constraints were identified from within the area subject to historical archaeological testing (refer to Appendix T).</p> <p>Council has progressed a heritage listing for part of the site through a separate Planning Proposal.</p> <p>Council has also nominated the site for State Heritage Listing. This is a separate statutory process that has no direct bearing on this Planning Proposal.</p>
c. Further investigation of the relevant wastewater system capacity, including water cycle management/water services in the development site.	The Servicing Strategy Report Summary prepared by BG&E (Appendix X) confirms that the wastewater system can accommodate the proposed development.
d. Review of the local stormwater system	The Servicing Strategy Report prepared by BG&E (Appendix X) confirms that the stormwater system can accommodate the proposed development.
e. Hydrological review in relation to the rail corridor bridge to the south east of the site.	A Flood Study has been prepared and is included at Appendix I .

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Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
f. An amended Traffic Impact Assessment	An amended Traffic Impact Assessment has been prepared (Appendix R), which assesses the impacts of the development on the surrounding road network. The Railway Street/Memorial Drive intersection is proposed to be upgraded as part of the development. Although not strictly required, a roundabout at the intersection of Harbinger Street is proposed to provide an enhanced traffic management outcome for the development and existing residents to the north of Railway Street.
g. Review of Sydney Trains 33kV Aerial Line asset on the western side of the rail corridor	A review of the Sydney Trains 33kV Aerial Line was undertaken as part of the Servicing Strategy Report Summary by BG&E (Appendix X). Sydney Trains confirmed that feeder 787 does not currently have an easement as the station carpark and the access road is owned by RailCorp.
h. A Rail and Road Acoustic and Vibration Assessment be prepared	A noise and vibration assessment was undertaken by Renzo Tonin & Associates (Appendix R). The study found that noise and vibration issues do not present any constraint to rezoning the site for primarily residential uses.
i. An Aboriginal Cultural Heritage Assessment be prepared.	An Aboriginal Cultural Heritage Assessment was undertaken by Kelleher Nightingale Consulting Pty Ltd (Appendix S). Aboriginal community consultation has indicated that creek lines in the area were traditional fishing grounds and retain Aboriginal cultural value for their connection to this activity, however no specific or significant Aboriginal cultural features have been identified for the site. The site is heavily disturbed with limited potential for Aboriginal archaeological objects. One Aboriginal archaeological site comprising Aboriginal objects is located within the study area: low density artefact scatter FCCW AFT 1 (AHIMS 52-2-4505). Avoidance or mitigation to impacts of this archaeological were deemed as unnecessary.
j. A Conservation Management Plan for providing for the long-term conservation of significant Coke Works heritage components be prepared.	A Conservation Management Strategy has been prepared. Refer to Appendix Q .
k. Agreement to provide at least 5% Affordable Rental Housing within the development, and advice on the proposed management arrangements of the dwellings and the proposed housing needs sectors to be targeted.	Legacy and ICC are committed to providing 35 affordable rental housing units, representing 5% of an estimated 700 residential dwellings.

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Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
<p>l. A Revised Ecological Impact Assessment be prepared addressing the following:</p> <ul style="list-style-type: none"> i. Potential impacts to micro bats roosting on the site; and ii. Potential for Green and Golden Bell Frog habitat on the site. 	<p>An updated Flora and Fauna Assessment has been prepared by EcoLogical (Appendix M).</p> <p>Further investigations to determine the presence of the microchiropteran bat (microbat) habitat found no evidence found of microbats using any of the structures present on the subject site. No microbats were seen, or any clear evidence found of microbats having used the buildings at any time.</p> <p>The report also found that the study area was not considered to provide habitat for the Green and Golden Bell Frog.</p> <p>Although not specifically referenced in the Council resolution, additional monitoring and proposed management measures for the existing grey-headed flying camp have been prepared.</p>
m. A revised Remediation Action Plan	<p>A Remediation Action Plan (Appendix K) and a Remediation Action Plan Endorsement (Appendix L) has been prepared by Arcadis to provide the information required.</p>
n. Advice on the proposed facilities and / or infrastructure, including costings, that are proposed to be incorporated into a draft planning agreement.	<p>Legacy has provided a letter of offer for a VPA that details a range of public benefits that will be provided as part of the development. (Appendix D).</p>

Heritage Planning

The Council resolved in April 2018 to add part of the former Corrimal Coke Works site to Schedule 5 and add to the Heritage Map of the WLEP 2009, to identify the site as a heritage item of local significance and enable its conservation to be considered as part of any development proposal.

Council received a Gateway Determination from the DP&E on 26 February 2020. A draft PP was exhibited from 9th March to 8th April 2020 and on 29th June 2020 Council approved that part of the former Corrimal Coke Works site be listed as a local heritage item in the WLEP 2009. As such, this PP does not address a local heritage listing of the site.

Conclusion

This PP provides the justification for achieving the broader strategic planning framework that will support the delivery of the broader objectives for redevelopment of the rezoning area, focused on providing new housing in close proximity to Corrimal train station and Corrimal town centre, approximately 5.5km from Wollongong CBD.

The PP provides evidence to demonstrate that the site is suitable for the proposed zoning and the proposal provides development outcomes that appropriately balances economic, environmental and social considerations. The outcome from the review of the site's strategic context as well as the site-specific analysis and merit assessment, provides high confidence that the site has the capacity and suitability to be supported for a rezoning.

1 Introduction

1.1 Purpose of the Planning Proposal

This Planning Proposal (PP) has been prepared on behalf of Legacy Property (Legacy) and the Illawarra Coke Company Pty Ltd (ICC). The PP seeks to amend the Wollongong Local Environmental Plan (WLEP) 2009 to rezone the 18.18 hectare (ha) Corrimal Coke Works site (the site) located at 27 Railway Street, Corrimal, within the Wollongong Local Government Area (LGA). The site has frontages to Memorial Drive, Railway Street, Towradgi Creek and South Coast railway line.

Corrimal is well located, being adjacent to the railway line and Memorial Drive, which provides a direct road link south towards Wollongong and north to the Princes Highway, linking to Sydney. The site is located approximately 4 kilometre (km) north of the University of Wollongong and approximately 5.5km north of the Wollongong Central Business District (CBD) and 63.5km south of the Sydney CBD.

Corrimal town centre is a 350 metre (m) walk from the site, enabling easy access to major supermarkets, speciality retail, Corrimal Hotel, banks, dentists, pharmacy's, gyms, community centre, restaurants, cafes and a number of other retail uses. Land uses directly surrounding the site consist predominantly of low to medium density residential development, open space and transport infrastructure (road and rail corridors).

The site, in context, is shown in **Figure 2** below. An aerial image of the site is shown in **Figure 3**.

Figure 2 The site in context



Source: Nearmap 2020

Figure 3 Aerial view of the rezoning area



Source: Near Maps

The purpose of this PP is to amend the WLEP 2009 to rezone the existing 18.18ha Corrimal Coke Works site from RE2 Private Recreation, IN3 Heavy Industrial and SP2 Infrastructure to R3 Medium Density Residential and RE1 Public Recreation, to accommodate approximately 700 – 750 new dwellings. The portion of the site zoned SP2 Infrastructure will be retained. The site comprises Lot 1 in Deposited Plan (DP) 795791, Lot 5 in DP 749492, Lot 126 DP 598190 and Lot 11 DP 749492.

1.2 Background

The Corrimal site operated as a Coke Works from 1912 to 2014. The site was decommissioned in 2014 and since this time has been locked up and vacant. In considering its future, ICC engaged expert consultants to understand the opportunities and constraints of the site. Original concept planning identified the potential for a mix of attached or semi-attached dwellings and residential flat buildings under a R3 Medium Density Residential zoning. The remainder of the site was proposed to be rezoned to RE1 Public Recreation.

The original draft PP was prepared and submitted to Council in October 2017. The PP was then placed on preliminary public exhibition from 23rd October to 24th November 2017. Council prepared a preliminary

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assessment, which was endorsed by Council (subject to further technical and design studies) on 3rd April 2018 and forwarded to the Department of Planning, Industry and Environment (DPI&E) for a Gateway determination on 23rd April 2018.

The Gateway determination was issued by the DPI&E on 20th August 2018, allowing the rezoning proposal for the Corrimal Coke Works site to proceed subject to conditions, within a timeframe of 18 months. On 17th February 2020, the DPI&E amended Condition 6 from '18 months' to '30 months' to allow a 12-month extension of time. This PP addresses and responds to Council's resolution as well as the Gateway determination.

In December 2018, ICC and Legacy held a series of visioning workshops with key stakeholders from Council, Neighbourhood Forum 4, Corrimal Community Action Group (CCAG), Corrimal Region Action Group (CRAG) and the Corrimal Chamber of Commerce. These workshop outcomes informed the vision and principles included as part of the concept Master Plan for the PP. Further stakeholder meetings and a community information session were held in late March 2019, with 120 surveys completed as part of the community engagement.

An updated PP and supporting technical studies were submitted to Council in May 2019. Since this time there has been extensive dialogue to address Council queries, State agency feedback and further revisions to the PP and Concept Master Plan.

This PP now fully responds to all Council feedback and is submitted to Council for public exhibition, final endorsement and subsequent amendments to the WLEP 2009 by the DPI&E.

1.3 Summary of the Planning Proposal

The site is well placed to support 'in-fill' development, given its adjacency to existing residential development in Corrimal, its location next to Corrimal train station and its proximity to the Corrimal town centre.

As part of the PP, pre-exhibition consultation has been undertaken with the DPI&E, selected state agencies and Wollongong Council in order to inform the proposed planning controls on the site.

A summary table identifying the proposed outcomes of this PP are identified in **Table 1** below.

Table 1 Summary of the Proposal

Property Details	<p>The total site comprises 4 existing lots:</p> <ul style="list-style-type: none"> » Lot 1 in DP 795791 » Lot 5 in DP 749492 » Lot 126 DP 598190 » Lot 11 DP 749492
Area	18.18ha (181,754m ²)
Proposal	<p>Amend WLEP 2009 as follows:</p> <p>Land Zoning Map 024</p> <p>Remove the RE2 Private Recreation and IN3 Heavy Industrial zoning from the site and replace with:</p> <ul style="list-style-type: none"> » R3 Medium Density Residential » RE1 Public Recreation <p>The portion of the site zoned SP2 Infrastructure will be retained.</p> <p>Minimum Lot Size (MLS) Map 024</p> <p>Remove the 1.99ha MLS from the part of the site zoned IN3 and introduce a new MLS of 449 m² for the land zoned R3 Medium Density Residential.</p> <p>Height of Building (HOB) Map 024</p> <p>Remove the 9m height control from part of the site zoned RE2.</p>

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	<p>Insert a height control of 9m over all RE1 zoned land.</p> <p>Insert the following height controls over R3 zoned land - 9m, 13m, 15m, 18m and 22m</p>
	<p>Floor Space Ratio (FSR) Map 024</p> <p>No FSR control currently exists for the site.</p> <p>Insert the following FSRs over all R3 zoned land; 0.75:1, 1.2:1, 1.5:1, 2:1 and 2.5:1</p> <p>No FSR control is proposed for RE1 land</p>
	<p>Riparian Land Map 024</p> <p>Amend the location of the riparian corridor to reflect the creek re-alignment.</p> <p>Remove the foreshore building line from the map.</p> <p>Remove any land located below the foreshore building line from the map.</p>
	<p>Natural Resource Sensitivity Map 024</p> <p>Amend the Natural Resource Sensitivity map to include one area of biodiversity sensitivity in the southern area of the site.</p>
	<p>Key Sites Map 024</p> <p>Create a Key Sites Map to include the site as a key site in terms of Clause 7.18</p>
	<p>Urban Release Area 024</p> <p>Create a Urban Release Area Map to include the site and thus Part 6 of the LEP is to apply</p>
	<p>Part 7 Local Provisions – General</p> <p>Include two new local provisions clauses:</p> <p>The following clause will be included in Part 7 to allow increased heights, beyond the maximum height control of 22m, for C1 North Stack, Fine Coal Bin, and C1 Brick Chimney Stack, which currently sit at heights of approximately 36.8m and 29m respectively:</p> <p>7.20 Former Corrimal Coke Works</p> <p>Height of Development</p> <p>(1) <i>The height of any development on the former Corrimal Coke Works site may not to exceed the height limit shown on the Height of Buildings Map, with the exception of the following heritage items:</i></p> <p>a) C1 North Stack</p> <p>b) C1 Brick Chimney Stack</p> <p>c) C1 Fine Coal Bin</p> <p>(2) <i>The following height limits are permissible for the heritage items:</i></p> <p>a) C1 North Stack: 37m</p> <p>b) C1 Brick Chimney Stack: 29m</p> <p>c) C1 Fine Coal Bin: 25m</p> <p>The second local provisions clause will be included to provide certainty to the maximum building heights on the site, by allowing lift overrun or other ancillary uses to exceed the height outlined in the building heights map.</p> <p>7.21 Height of buildings at Corrimal Coke Works site</p>

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1. *The consent authority may grant development consent to development for the purpose of rooftop plant, lift towers, lift motor rooms, pergola and or communal open space and access to and structures associated with such space, that would exceed or causes a building to exceed, the height limits set by clause 4.3, but only if the consent authority is satisfied that the structures -*
 - a) *are for the purposes of equipment servicing the building, plant rooms, lift towers, lift motor rooms, fire stairs and other areas used exclusively for mechanical services or ducting; or*
 - b) *are for the purpose of communal open space and access to, and structures associated with that space; or*
 - c) *for both (a) and (b); and*
 - d) *are not an advertising structure; and*
 - e) *does not include floor space area and is not reasonably capable of modification to include floor space area; and*
 - f) *will cause minimal overshadowing; and*
 - g) *any such rooftop structure referred to in (1)(a) and 1(b) is fully integrated into the design of the building."*

Schedule 1 Additional permitted uses

The following clause will be included in Schedule 1 Additional Permitted Uses to ensure that convenience neighbourhood retail facilities and commuter services near Corrimal train station can be provided:

Use of certain land at the former Corrimal coke works site:

- (1) *This applies to land at the former Corrimal coke works site as mapped*
- (2) *Development for the purposes of food and drink Premises, shops, business premises and neighbourhood supermarket (1,000m²) is permitted with consent, but only if at ground floor and within 150m of the train station*
- (3) *Development for purposes of food and drink Premises, shops, business premises and and neighbourhood supermarket is permitted up to a maximum of a total GFA of 2,000m², with no one shop (except for the neighbourhood supermarket) to be more than 250m² in size.*

These outcomes are explained in further detail in the '**Planning Proposal Part 2 – Explanation of Provisions**' section of this report. A Site Specific Development Control Plan (DCP) has also been prepared to support the proposed rezoning.

In order to support the proposed rezoning, a Concept Master Plan has been developed to identify future potential land uses and design principles. The rezoning proposal is outlined in detail in Section 4 of this report.

1.4 Objective of this report

The objective of this report is the proposed amendment to the WLEP 2009 to facilitate the redevelopment of the site to accommodate primarily residential land uses intended to assist in meeting housing targets for the Wollongong LGA and the suburb of Corrimal.

The PP is consistent with the requirements of Section 55 of the Environmental Planning and Assessment Act 1979 (EP&A Act), in particular the guidelines issued under Section 55(3) of the EP&A Act and has been prepared having

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regard to the DPI&E's 'A guide to preparing planning proposals' (2016) and 'A guide to preparing local environmental plans.

The PP provides:

- » comprehensive details on the subject site and its surrounds;
- » responses to the Council resolution and the Gateway requirements (refer to **Section 2.5** below);
- » identification of the Environmental Planning Instruments (EPIs), non-statutory planning documents (such as Development Control Plan (DCPs)) and other strategies (planning or otherwise) applying to the subject site and/or the PP, as well as identification of the relevant controls and requirements contained within those EPIs, non-statutory planning documents and strategies;
- » a statement of the objectives and intended outcomes of the PP;
- » explanation of the provisions that are to be included in the proposed instrument;
- » thorough justification of the PP against the questions set out in the above-mentioned guidelines, demonstrating that the proposal is in the public interest, and is worthy of approval;
- » identification of the WLEP 2009 maps which would be amended under the PP;
- » details of the community consultation that has been/will be undertaken on the PP;
- » a project timeline detailing anticipated timeframe for the plan-making process.

The preparation of this report and supporting technical studies has involved the collaboration of a multi-disciplinary team to ensure all relevant issues have been addressed. The documentation submitted in support of the PP is identified in **Table 2** below. This PP should be read in conjunction with these technical reports.

Table 2 Supporting Technical Reports

Report	Prepared by/author	Date	Appendices
Master Plan and Site Visual Analysis	DKO		Appendix A
Landscape Master Plan Report	Clouston Associates		Appendix B
Gateway Determination and subsequent extension Letter	DPI&E	20 August 2018 17 February 2020	Appendix C
VPA Letter of offer	Legacy Property		Appendix D
Geotechnical Assessment	Douglas Partners	25 May 2017	Appendix E
Geomorphology Assessment	Soil Conservation Service	19 March 2019	Appendix F
Creek Realignment Stability Assessment	BG&E	19 March 2019	Appendix G
Supplementary Riparian Corridor Information	Clouston Associates	5 August 2019	Appendix H
Flood Study	Cardno	2 May 2019	Appendix I
Additional Environmental Assessment	Arcadis	12 April 2017	Appendix J
Remediation Action Plan	Arcadis	13 May 2019	Appendix K

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Report	Prepared by/author	Date	Appendices
Remediation Action Plan Endorsement	Zoic Environmental Pty Ltd	17 May 2019	Appendix L
Flora and Fauna Assessment	EcoLogical	May 2019	Appendix M
Flying Fox Camp Proposed Management Actions	EcoLogical	25 March 2020	Appendix N
Flying Fox Strategy Peer Review	Ecosure	26 March 2020	Appendix O
Threatened Microbat Habitat Assessment	EcoLogical	22 July 2019	Appendix P
Conservation Management Strategy	Urbis	30 July 2019	Appendix Q
Noise and Vibration Assessment	Renzo Tonin & Associates	20 May 2019	Appendix R
Aboriginal Cultural Heritage Assessment	Kelleher Nightingale Consulting Pty Ltd	17 May 2019	Appendix S
Archaeological Test Excavation Report	Austral Archaeology	13 March 2020	Appendix T
Heritage Interpretation Strategy	Urbis	7 May 2019	Appendix U
Historical Heritage Assessment	Biosis	1 August 2017	Appendix V
Structural Assessment of Existing Structures	BG&E	21 June 2019	Appendix W
Servicing Strategy Report Summary	BG&E	15 April 2019	Appendix X
Traffic Impact Assessment	Bitzios Consulting	29 April 2019	Appendix Y
Economic Impact Assessment	Hill PDA	19 September 2017	Appendix Z
Retail Market Demand and Economic Impact Assessment	Urbis	July 2019	AppendixAA
Illawarra – Shoalhaven: Housing Market Report for Corrimal Coke Works	Macro Plan Dimasi	22 February 2019	Appendix BB
Consultation Outcomes Report	Elton Consulting	1 May 2019	Appendix CC

1.5 Wollongong Council resolution

Council's resolution of 3 April 2018 resolved that additional information be submitted to enable Council to consider the information prior to determining the Planning Proposal. Below is a table outlining Council's resolution/requirements and a response to the issues raised in the right-hand column.

Table 3 Response to Council's Resolution

Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
a) A site specific DCP Chapter be developed and submitted in conjunction with the amended Urban Design Concept Plan	A Site Specific DCP has been prepared to support this Planning Proposal.
b) Advice from the NSW Heritage Council for comment in relation to: iii. The Archaeological significance of the site iv. The potential for the site to be listed on the State Heritage Register	<p>Heritage reports and a Conservation Management Strategy, along with subsequent peer reviews, have been prepared (refer to Appendices S, J, Q, U and V) which determine that the site has no archaeological significance, however does have local heritage significance.</p> <p>The archaeological investigations failed to identify widespread, <i>in situ</i> archaeological remains associated with the tramway which was known to run through the study area. As such, no heritage constraints were identified from within the area subject to historical archaeological testing (refer to Appendix T).</p> <p>Council has progressed a heritage listing for part of the site through a separate Planning Proposal.</p> <p>Council has also nominated the site for State Heritage Listing. This is a separate statutory process that has no direct bearing on this Planning Proposal.</p>
c) Further investigation of the relevant wastewater system capacity, including water cycle management/water services in the development site.	The Servicing Strategy Report Summary prepared by BG&E (Appendix X) confirms that the wastewater system is capable of accommodating the proposed development.
d) Review of the local stormwater system	The Servicing Strategy Report prepared by BG&E (Appendix X) confirms that the stormwater system is capable of accommodating the proposed development.
e) Hydrological review in relation to the rail corridor bridge to the south east of the site.	A Flood Study has been prepared and is included at Appendix I .
f) An amended Traffic Impact Assessment	An amended Traffic Impact Assessment has been prepared (Appendix R), which assesses the impacts of the development on the surrounding road network. The Railway Street/Memorial Drive intersection is proposed to be upgraded as part of the development. Although not strictly required, a roundabout at the intersection of Harbinger Street is proposed to provide an enhanced traffic management outcome for the development and existing residents to the north of Railway Street.

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Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
g) Review of Sydney Trains 33kV Aerial Line asset on the western side of the rail corridor	A review of the Sydney Trains 33kV Aerial Line was undertaken as part of the Servicing Strategy Report Summary by BG&E (Appendix X). Sydney Trains confirmed that feeder 787 does not currently have an easement as the station carpark and access road is owned by Railcorp.
h) A Rail and Road Acoustic and Vibration Assessment be prepared	A noise and vibration assessment was undertaken by Renzo Tonin & Associates (Appendix R). The study found that noise and vibration issues do not present any constraint to rezoning the site for primarily residential uses.
i) An Aboriginal Cultural Heritage Assessment be prepared.	<p>An Aboriginal Cultural Heritage Assessment was undertaken by Kelleher Nightingale Consulting Pty Ltd (Appendix S).</p> <p>Aboriginal community consultation has indicated that creek lines in the area were traditional fishing grounds and retain Aboriginal cultural value for their connection to this activity, however no specific or significant Aboriginal cultural features have been identified for the site.</p> <p>The site is heavily disturbed with limited potential for Aboriginal archaeological objects.</p> <p>One Aboriginal archaeological site comprising Aboriginal objects is located within the study area: low density artefact scatter FCCW AFT 1 (AHIMS 52-2-4505). Avoidance or mitigation to impacts of this archaeological were deemed as unnecessary.</p>
j) A Conservation Management Plan for providing for the long-term conservation of significant Coke Works heritage components be prepared.	A Conservation Management Strategy has been prepared. Refer to Appendix Q .
k) Agreement to provide at least 5% Affordable Rental Housing within the development, and advice on the proposed management arrangements of the dwellings and the proposed housing needs sectors to be targeted.	Legacy and ICC are committed to providing 35 affordable rental housing units, representing 5% of an estimated 700 residential dwellings.
l) A Revised Ecological Impact Assessment be prepared addressing the following: <ul style="list-style-type: none"> iii. Potential impacts to micro bats roosting on the site; and iv. Potential for Green and Golden Bell Frog habitat on the site. 	<p>An updated Flora and Fauna Assessment has been prepared by EcoLogical (Appendix M).</p> <p>Further investigations to determine the presence of the microchiropteran bat (microbat) habitat found no evidence found of microbats using any of the structures present on the subject site. No microbats were seen or any clear evidence found of microbats having used the buildings at any time.</p> <p>The report also found that the study area was not considered to provide habitat for the Green and Golden Bell Frog.</p> <p>Although not specifically referenced in the Council resolution, additional monitoring and proposed management measures for the existing grey-headed flying camp have been prepared.</p>

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Required additional information (shortened in this executive summary)	Response (shortened in this executive summary)
m) A revised Remediation Action Plan	A Remediation Action Plan (Appendix K) and a Remediation Action Plan Endorsement (Appendix L) has been prepared by Arcadis to provide the information required.
n) Advice on the proposed facilities and / or infrastructure, including costings, that are proposed to be incorporated into a draft planning agreement.	Legacy has provided a letter of offer for a VPA that details a range of public benefits that will be provided as part of the development. (Appendix D).

1.6 Gateway Determination

Gateway approval was received on 20th August 2018 by the DPI&E, with a 12-month extension granted on 17th February 2020. The approval was granted subject to conditions. These conditions, along with our responses, are identified in the table below.

Table 4 Response to the DPI&Es conditions

Condition	Response
1. To ensure consistency with 9.1 Directions 2.3 Heritage Conservation and 4.3 Flood Prone Land, the following studies are to be completed prior to public exhibition: <ul style="list-style-type: none"> An Aboriginal Cultural Heritage Assessment A Conservation Management Plan that provides for the long-term conservation of significant coke work heritage components Revised flood study (including flood modelling); and Geomorphological report 	These required reports have all been provided. Refer to Tables 2 and 3 above.
2. The following studies are also to be completed prior to public exhibition: <ul style="list-style-type: none"> A revised ecological assessment A revised traffic impact assessment; and A revised remediation plan 	These required reports have all been provided. Refer to Tables 2 and 3 above.

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Condition	Response
<p>3. Public Exhibition is required under section 3.34(2)(c) and schedule 1 clause 4 of the Act as follows:</p> <ul style="list-style-type: none"> a) The planning proposal must be made publicly available for a minimum of 28 days; and b) The planning proposal authority must comply with the notice requirement for public exhibition of planning proposals and the specifications for material that must be made publicly available along with planning proposals as identified in section 5.5.2 of A guide to preparing local environmental plans (Department of Planning and Environment 2016). 	<p>The Planning Proposal will be exhibited in accordance with these requirements.</p>
<p>4. Consultation is required with the following public authorities/organisations under section 3.34(2)(d) of the Act and/or comply with the requirements of relevant section 9.1 Directions:</p> <ul style="list-style-type: none"> • Roads and Maritime Services; • Department of Primary Industries – Water; • Environment Protection Authority; • Office of Environment and Heritage; • Sydney Water; • RailCorp; • Department of Education; • Heritage Council; • National Trust of Australia (Illawarra Shoalhaven Regional Branch); • Endeavor Energy; • Transport for NSW; • Department of Primary Industries – Fisheries NSW; • Catchment Management Authority; and • State Emergency Service <p>Each public authority is to be provided with a copy of the planning proposal and any relevant supporting material and given at least 21 days to comment on the proposal.</p>	<p>Selected consultation has occurred where necessary to address matters prior to public exhibition.</p> <p>Consultation with all specified authorities/organisations will be undertaken as part of the statutory exhibition period. Refer to Part 5 Community Consultation.</p>
<p>5. A public hearing is not required to be held into this matter by any person or body under section 3.34(2)(e) of the Act. This does not discharge Council from any obligation it may otherwise have to conduct a public hearing (for example, in response to a submission or if reclassifying land).</p>	<p>Noted.</p>
<p>6. The timeframe for completing the LEP is to be 30 months following the date of the extended Gateway determination.</p>	<p>Noted.</p>

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A review of the table above demonstrates that this PP has addressed the Gateway study requirements.

1.7 Heritage Planning

The Council resolved in April 2018 to add part of the former Corrimal Coke Works site to Schedule 5 and add to the Heritage Map of the WLEP 2009, to identify the site as a heritage item of local significance and enable its conservation to be considered as part of any development proposal.

In February 2020, a draft Planning Proposal for a local heritage listing was submitted to the NSW DPI&E for Gateway determination. A Gateway Determination was issued 26 February 2020 and the Heritage Amendment draft Planning Proposal was exhibited from 9 March to 8 April 2020. On 29th June 2020 Council approved that part of the former Corrimal Coke Works site be listed as a local heritage item in the WLEP 2009.

As such, this PP does not need to address a local heritage listing of the site.

2 Site Analysis

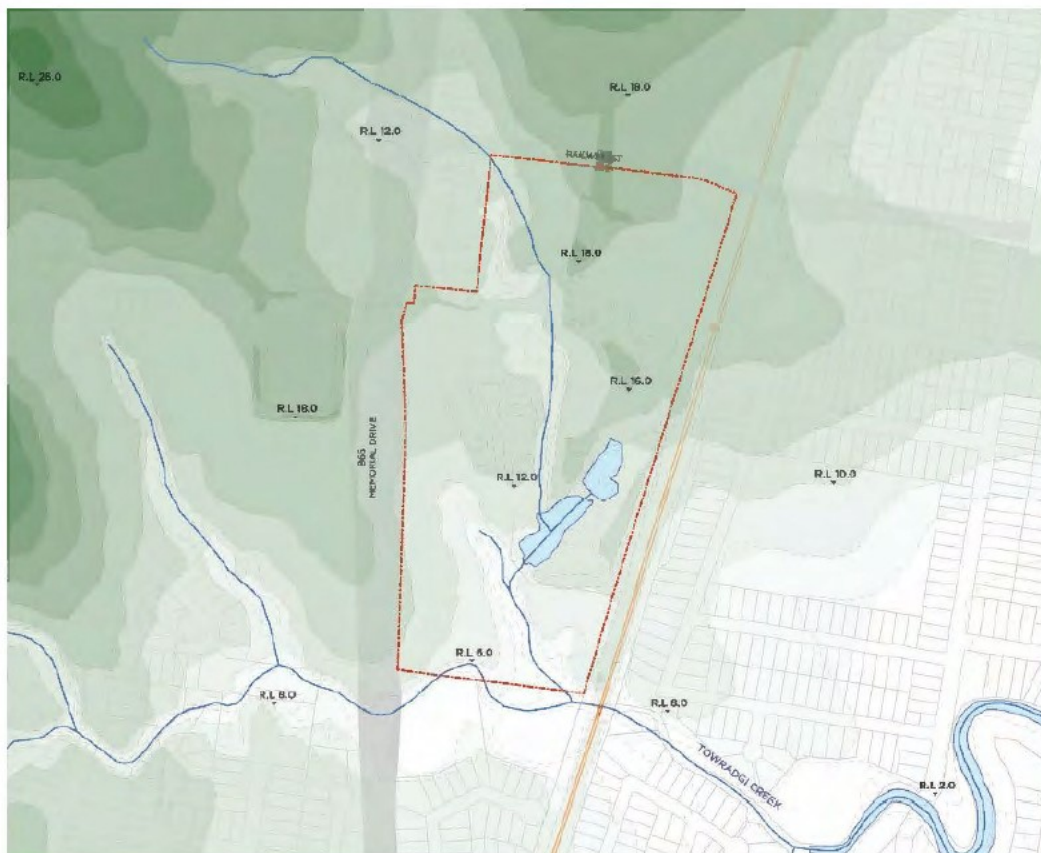
Legacy and ICC have undertaken extensive research and assessment to inform the suitability and capacity of the site for redevelopment. This section contains a summary of technical investigations relating to the rezoning area, including ecology, flooding and hydrology, land contamination, traffic and transport, infrastructure utilities and services and heritage.

This section of the report also provides an overview of the social infrastructure in the area and an analysis of the demographics and housing need. Further details of the analysis can be found in the supporting documentation accompanying this rezoning, outlined in **Table 2**.

2.1 Topography

The topography of the site is relatively flat, with the exception of the existing modified creek channel and various berms associated with former industrial operations. There is a minor fall in the landscape from north to south, with the low point of the site being associated with Towradgi Creek at the southern extent. **Figure 5** shows the existing topography of the site based on 2m GIS contours. Topography does not present a significant constraint to development.

Figure 4 Site Topography



Source: E8urban

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2.2 Soil

In accordance with the Acid Sulfate Soils (ASS) Assessment Guidelines, there is typically a requirement for assessment of the potential for estuarine ASS where the ground surface levels across the site are below (Reduced Level) RL 5.

The majority of the site is classified as 'no known occurrence of ASS – Class 5' with a small portion classified as 'Low Probability Class 3'. As part of future investigation, it is recommended that additional testing of soils be carried out in areas proposed for creek realignment or deep trench excavation for services.

Douglas Partners has indicated that even if ASS or acid soil conditions are encountered, this can be mitigated through engineering solutions.

2.3 Geology

The Wollongong – Port Hacking 1:100 000 Geological Series Sheet indicates that the site is underlain by the Erin Vale Formation (comprising fine to medium grained lithic sandstone) and Pheasants Nest Formation (comprising interbedded lithic sandstone, coal, carbonaceous daystone, siltstone and claystone). Almost all the site is mantled by alluvium (comprising quartz and lithic "fluvial" sand, silt and clay) associated with North Corrimal Creek and Towradgi Creek.

Geological investigations undertaken by Douglas Partners inferred the following geological model for the site:

- » bedrock comprising weathering-prone shales, siltstone and sandstone with minor included coalbands, possibly representing the Unanderra Coal Member
- » a highly variable thickness of clay-rich residual and alluvial soils underlying filling within areas previously affected by coke production and storage and directly underlying the surface within the mostly undisturbed, south-western section of the site. These natural soils range from very soft to hard conditions. The natural soils have been locally removed to provide foundation surfaces for sections of the coke works. Soils and weathered rock underlying or adjacent to the coke ovens have significantly reduced moisture contents and may have been "baked".
- » a capping layer ranging from 0.2m to in excess of 3.5m of mostly uncontrolled filling within areas previously affected by coke production and storage. The predominant filling materials are coke (from fines to cobble size), coal, coalwash and clay
- » a modified course of North Corrimal Creek underlying sections of former coke stockpile areas.

2.4 Geotechnical

Douglas Partners undertook a geotechnical assessment of the site, which indicates that the site can be made suitable for residential development from a geotechnical perspective. While no geotechnical conditions present a constraint to rezoning and development of the site, the following considerations will need to be incorporated into the design of earthworks to facilitate urban development:

- » existing uncontrolled fill (particularly that comprising coal and coke waste and associated waste fines) needs to be managed as part of future development. The degree of modification (if any) that will be required to provide a suitable fill material will be determined by future investigation;
- » minor constraints relate to erosion potential of disturbed materials, localised waterlogging, localised abnormal moisture content within the coke production area, sodicity and soil aggressiveness, which can be effectively handled during the earthworks design; and
- » as part of future investigations, an earthworks methodology will need to be prepared to ensure the reuse of existing site soils and optimised procedures for the site.

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Regarding specific issues, Douglas Partners outlined the key considerations to take into account, as shown in **Table 5**. These factors can be addressed through earthworks design.

Table 5 Geotechnical conditions and mitigations

Geotechnical condition	Mitigation
Erosion Potential - The geological mapping identified the development of minor erosion rills within existing filling of the previous stockpile areas. No bank erosion has been observed along the creek alignments nor is there any WCC record of major scour events within the subject sections of North Corrimal Creek or Towradgi Creek.	<p>The potential for erosion of exposed fill materials (particularly silt and sand size fractions) by concentrated flow is assessed as a minor constraint to development which can be addressed with good engineering practice. To minimise the constraints imposed by erosion potential, earthworks within the site should be undertaken in stages, with adequate erosion and sedimentation controls in place.</p> <p>Treatment of batters constructed as part of the future earthworks should include:</p> <ul style="list-style-type: none"> » filling using select materials (i.e. non-dispersive or erodible) placed under controlled conditions; » provision of temporary surface cover (e.g. pegged matting) during the period of stream floor or batter revegetation. » channel lining or piping of drainage paths where appropriate.
Waterlogging - waterlogging and softening of alluvial soils and underlying residual soils are expected within creek floors, areas of new batters, areas adjacent to current water ponds and areas requiring removal of deeper sections of existing filling prior to placement of engineered filling.	<p>Precautionary and remedial works for site preparation for residential development should include:</p> <ul style="list-style-type: none"> » improvement of surface drainage including the interception of overland flow. » installation of subsurface drainage to protect road subgrades. » allowance for placement of a granular bridging layer over stripped alluvial or residual soils where trafficability or placement of engineered filling is affected by soft surface conditions.
Uncontrolled Filling and Reuse particularly that dominated by coalwash and coke products, or new coalwash filling which may be proposed for importation to raise site levels, will be governed by the requirements of the EPA Resource Recovery Order and WCC Coal Washery Refuse in Subdivisions Policy. It should be noted that coke products are not specifically included in the EPA Resource Recovery Order. However, as a combustible material with associated waste derived from coal, it is considered appropriate to include this material for assessment similar to coal washery reuse, in accordance with Council guidelines.	<p>Further assessment will be required as part of the design process to categorise the fill and to determine its geotechnical suitability for reuse on the site. Earthworks design will need to consider WCC Coal Washery Refuse in Subdivisions Policy whereby:</p> <ul style="list-style-type: none"> » very coarse materials (>150 mm) or fine slurry materials (tailings) are to be rejected. » structures are to be slab on ground design. Other footing designs by a Structural/Geotechnical Engineer may be considered. » combustible contents to be determined from site sampling at specified regular frequency. » compaction to be in layers under full engineering control to at least 100% standard density. » combustible contents to be at a mean value not greater than 30% with the upper value not exceeding 40%. » inert fill should be used to backfill services trenches. » coalwash is to be covered by at least 300mm of inert cover. » proper site control to prevent run-off or dust nuisance. <p>This is achievable to ensure that the site can be developed.</p>
Abnormal Soil Moisture Content - Below the kiln flue, and likely the coke kilns, as well as the extensive concrete paving	<p>Following removal of the existing structures, it is anticipated that exposure to cyclic wetting and drying will result in greater soil swelling than nearby natural soils. Subject to inspection and testing</p>

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Geotechnical condition	Mitigation
about the production area is likely to preserve low moisture contents.	following removal of structures, over-excavation and moisture re-conditioning of materials may be required. This is not expected to be a constraint to development.
Aggressivity - The pH, chloride, Sulfate and resistivity of soil and groundwater samples were compared to the requirements of the AS 2159 – 2009 for exposure classification of concrete and steel piles in soil. Most of the results indicated non-aggressive exposure classification, with only two of seven pH results for soils indicating a "Mild" exposure classification for concrete piles.	Aggressivity of soils is not considered to be a constraint to development.

2.5 Geomorphology

The site is traversed by North Corrimal Creek, which runs roughly North-west to South-east through the site, and Towradgi Creek, which runs generally along the southern boundary. In addition, an unnamed drainage line enters the site on the western boundary from under Memorial Drive and discharges into North Corrimal Creek.

Based on topographic datasets, North Corrimal Creek is considered a 2nd order stream (Strahler) with an approximate catchment area of 1.6km² upstream of the site. The steep headwaters drain the escarpment through well vegetated bushland before joining at the boundary of developed urban areas near Cox Avenue.

The form and alignment of the existing North Corrimal Creek has been substantially modified in the past through site filling and construction of online dams. These past impacts have been more pronounced in the upstream and central sections of the site. Downstream of the dams, the creek flows generally along the original alignment.

The existing channel form through the site largely consists of a continuous low flow channel typically 3 – 5m wide inset within a broader macro channel, approximately 30m wide and 4 - 6m deep. Bounding sediments of the low flow channel consists of mud and silt and there are no significant accumulations of mobile bed load deposits.

The riparian zone upstream of the dam is dominated by exotic weed species, while downstream a mixture of native and exotic species is evident. The riparian corridor is proposed to be realigned as part of the development. The current design of the realignment consists of a concept level design of the macro-channel and involves:

- » Channel reshaping and enlargement generally along the existing alignment for approximately 170m at the upstream extent of the site.
- » Realigning the channel closer to the western boundary of the site over a distance of approximately 560m.
- » Maintaining the downstream 70m of the existing creek up to the confluence with Towradgi Creek.

As outlined in the technical reports, the limited evidence of bedload transport through the existing North Corrimal Creek system means that the realignment design does not need to account for the transport of bedload sediment in any significant quantity. As a result, further progression of the realignment design will need to focus on the identified areas of potential channel instability while also providing for a complex assemblage of potential habitats and community amenity.

The revised design of the creek mimics the existing creek channel from through the site albeit with the provision of a much wider base width for the macro-channel. The relatively wide base width of up to 30m will allow low to moderate flood flow events to dissipate energy across the inset benches within the macro-channel. Further, the width will provide for the opportunity to include a low flow channel based on natural channel design principles.

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2.5.1 Creek Realignment

A major component of the rezoning involves the realignment of North Corrimal Creek through the site. A preliminary creek channel design was prepared by BG&E and provides the following typical parameters:

- » An average longitudinal grade of 1%;
- » An average 11m wide micro channel;
- » A 50-70m wide macro channel;
- » Reduced batter slopes and provision of required offsets from top of bank and Peak Median Flood (PMF) levels with batter slopes are no steeper than 1V:2H;
- » Increased riparian edge road verges to allow more recreational opportunities overlooking the creek channel;
- » Offsets from private property as a high-risk flood precinct; and
- » Any retaining walls being located above the PMF level.

After discussion with Council, DPI&E and Transport for NSW (TfNSW), further design and assessment was undertaken to demonstrate the long-term stability of the realigned creek addressing:

- » Revised civil design and commentary for proposed creek channel profile;
- » Revised flood modelling including output of 1% Annual Exceedance Probability (AEP) and PMF flood levels, flow velocities and bed shear stresses;
- » Geotechnical stability assessment including slip plane failure analysis; and
- » Geomorphological review and assessment.

BG&E prepared a subsequent Creek Realignment Stability Assessment integrating civil, hydraulic, geotechnical and geomorphological expertise in relation to the long-term stability and robustness of the proposed creek design.

The assessment concluded that the proposed batter slopes, offsets and morphology of the proposed creek realignment are robust and provide high confidence for long-term stability. Provided design and construction is undertaken in accordance with geotechnical, civil and structural engineering accepted practice and appropriately designed, the realigned creek channel can withstand flood velocities and perform in events up to and including the PMF. The creek alignment incorporates and balances civil, hydraulic, geotechnical, geomorphology and landscape design outcomes, and will provide an enhanced riparian asset for the Corrimal community and Council.

Hydraulic modelling of the proposed realignment indicates that flow velocities along the bulk of the realignment do not exceed erosion thresholds for turfed surfaces for the more frequent, moderate magnitude 2 year and 10-year Average Recurrence Interval (ARI) events. Further, hydraulic modelling outputs for the 1% AEP event indicates that flow velocities along the realignment are similar to those experienced along the existing watercourse through the site. Given the existing watercourse does not exhibit any significant channel stability issues, the proposed realignment has a high potential to attain a long-term stable condition with the establishment of a structurally diverse native riparian vegetation community.

A typical section of the proposed channel outlines the following general setbacks from structures:

- » The proposed low flow channel for the creek bank to bank is 11m wide;
- » The floodway on the eastern side is 12m wide;
- » The eastern batter varies between 14m and 23m from the edge of the verge of the internal perimeter road and has a slope of approximately 1V: 4H, subject to final landscaping design which may include terracing, rock log retaining walls or other treatments;
- » The floodway on the western side is 5m wide;

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- » The western batter varies between 14m and 23m from the boundary to the edge of floodway and has a slope of maximum 1V: 2H which is subject to landscaping final concept which may include terracing, rock log retaining walls or other treatments
- » The top of bank of the creek on the western side generally aligns with the site boundary and is set off from the edge of bitumen of Memorial Drive between 10-12.5m.
- » These dimensions will ensure that the offset from Memorial Drive will comply with Brisbane City Natural Channel Design Guideline (as referenced by Council), being greater than the zone defined by 1V:3H from toe of bank.

The level of assessment undertaken was considered highly detailed for the rezoning stage and demonstrates that the proposed riparian width is adequate to achieve a robust and acceptable design solution in future development of the site. The assessment provides a range of design recommendations that would be addressed as part of more detailed design of the creek realignment for a future development application.

Council and TfNSW have confirmed their acceptance of the proposed design parameters.

2.6 Hydrology

The site is located within the lower reaches of the Towradgi Creek catchment, which is characterised by an extensively developed floodplain, with relatively underdeveloped steep upper slopes. Located approximately 5km north of the Wollongong CBD, the Towradgi Creek catchment has a total area of approximately 7.3km². The drainage network of Towradgi Creek catchment is comprised of the following tributaries:

- » Towradgi Creek
- » South Angels Creek
- » North Angels Creek
- » South Corrimal Creek
- » North Corrimal Creek
- » Carr Creek
- » Parker Creek

Towradgi Creek and North Corrimal Creek traverse the site. The majority of the site drains directly into the North Corrimal Creek, with the main production area generally draining south into a small on-site dam.

2.6.1 Hydrogeology

Groundwater is likely to be present within the deeper bedrock that consists of the Illawarra Coal Measures. The depth to the bedrock has been mapped at greater than 10m. Groundwater storage within the bedrock sequence would be dominated by fractures. The intensity, connectivity and orientation of the fracture network would determine the groundwater flow direction and velocity. The anticipated regional groundwater flow direction would be east towards the coast. The hydrogeology is summarised below:

"The standing water level within the existing wells onsite ranged between 3.03m to 4.69m below top of casing. The inferred local groundwater flow direction on the site is to the south to south east. This opinion is based on a review of the predominant slope of the natural topography to the south, the southerly surface water flow direction of North Corrimal tributary across the Site and the easterly flow direction of Towradgi Creek towards the coastline."

2.6.2 Flooding

The Corrimal Flood Study by Cardno found that significant flooding currently occurs on the low point of Railway Street, located to the north-west of the site. In the 100-year ARI event, flooding extends to the south,

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inundating the existing residential development at 29, 31 and 33 Railway Street. In the PMF event, areas of the existing Cross Street residential development are also inundated.

The proposed creek realignment has been designed to convey major flows up to PMF event within North Corrimal Creek through the site providing for creation of a flood free development area. The proposed creek works result in reduced flooding in the vicinity of Railway Street and within existing residential development on Cross Street for the 100-year ARI event and also the PMF event.

Maintaining flood plain storage in the 100-year ARI event is the main floodplain management constraint for the site. The realigned creek channel includes widening below the 100-year ARI flood level to maintain floodplain storage areas within the site.

The flood modelling demonstrates that development of the site will not result in any increased flooding either downstream or upstream of the site. As noted above, the development results in improvements to flooding in Cross Street and Railway Street.

2.7 Contamination

The land is not listed on the Environmental Protection Agency's (EPA) Contaminated Land Register. However, considering the former uses of land, comprehensive contamination assessments have been undertaken in order to understand the potential environmental liabilities associated with the historical uses of the site. These previous assessments include the following:

- » ADI Limited Stage 1 Site Investigation Report, Station Street, Corrimal (1996) Phase 1 and Phase 2 Contamination Reports
- » EnviroRisk Phase 1 Environmental Assessment, (2005)
- » EnviroRisk, Phase 2 Environmental Assessment, (2006)
- » ES Preliminary Site Investigation, 27 Station Street, Corrimal, March 2014
- » ES Detailed Site Investigation, 27 Station Street, Corrimal, July 2014
- » Arcadis Additional Environmental Assessment Works, 27 Station Street, Corrimal, April 2017
- » Arcadis Additional Environmental Assessment – Production Area, 27 Station Street, January 2018
- » Arcadis Flue, Stack and Powerhouse Assessment, 27 Station Street, March 2018
- » Arcadis, Remediation Action Plan (RAP) 13 May 2019

Previous environmental assessments have identified isolated areas of hydrocarbon impacted soil that have the potential to pose a risk to human health under the proposed land use. Hydrocarbon, copper and zinc are also present in soil and have the potential to pose a risk to ecological receptors. Asbestos fibres and fragments were identified in soil at isolated portions of the site.

There were minor exceedances of dissolved heavy metals and ammonia detected in shallow groundwater. These exceedances are considered typical of regional shallow groundwater conditions and are unlikely to pose a risk to human or local ecological receptors.

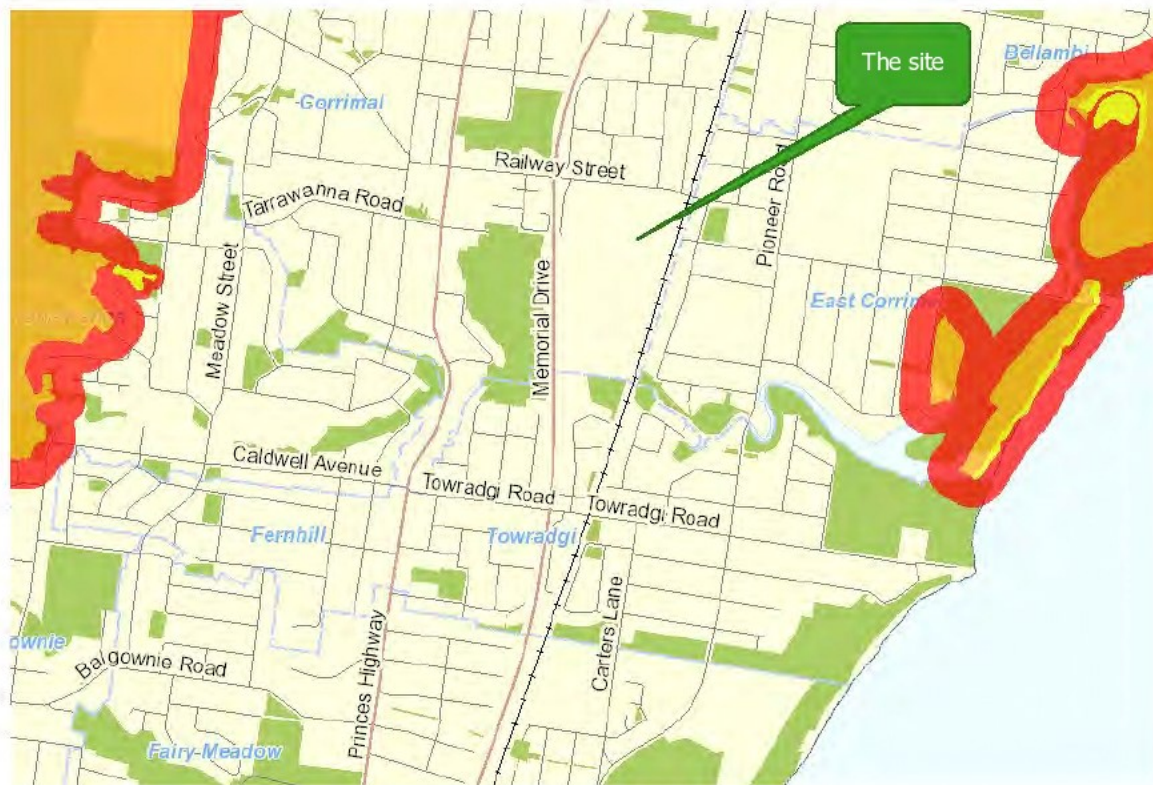
Although concentrations of contaminants of concern were detected above environmental assessment criteria, they were isolated and minimal compared to the wider site footprint. It is anticipated that through integration of the remedial strategies outlined in the RAP by Arcadis (refer to **Appendix K**) into the bulk earthworks civil design and construction stages of the project, the site can be suitable for the proposed use. The RAP has been endorsed by Zoic Environmental Pty Ltd (**Appendix L**).

The objectives of the onsite remediation will be achieved subject to the successful implementation of the actions contained in the RAP, which will enable the site to be made suitable for the proposed residential, commercial and open space uses.

2.8 Bushfire

The site is not identified as being Bushfire prone on the bushfire prone land map.

Figure 5 Bushfire Prone Land



Source: Wollongong Council

2.9 Ecological

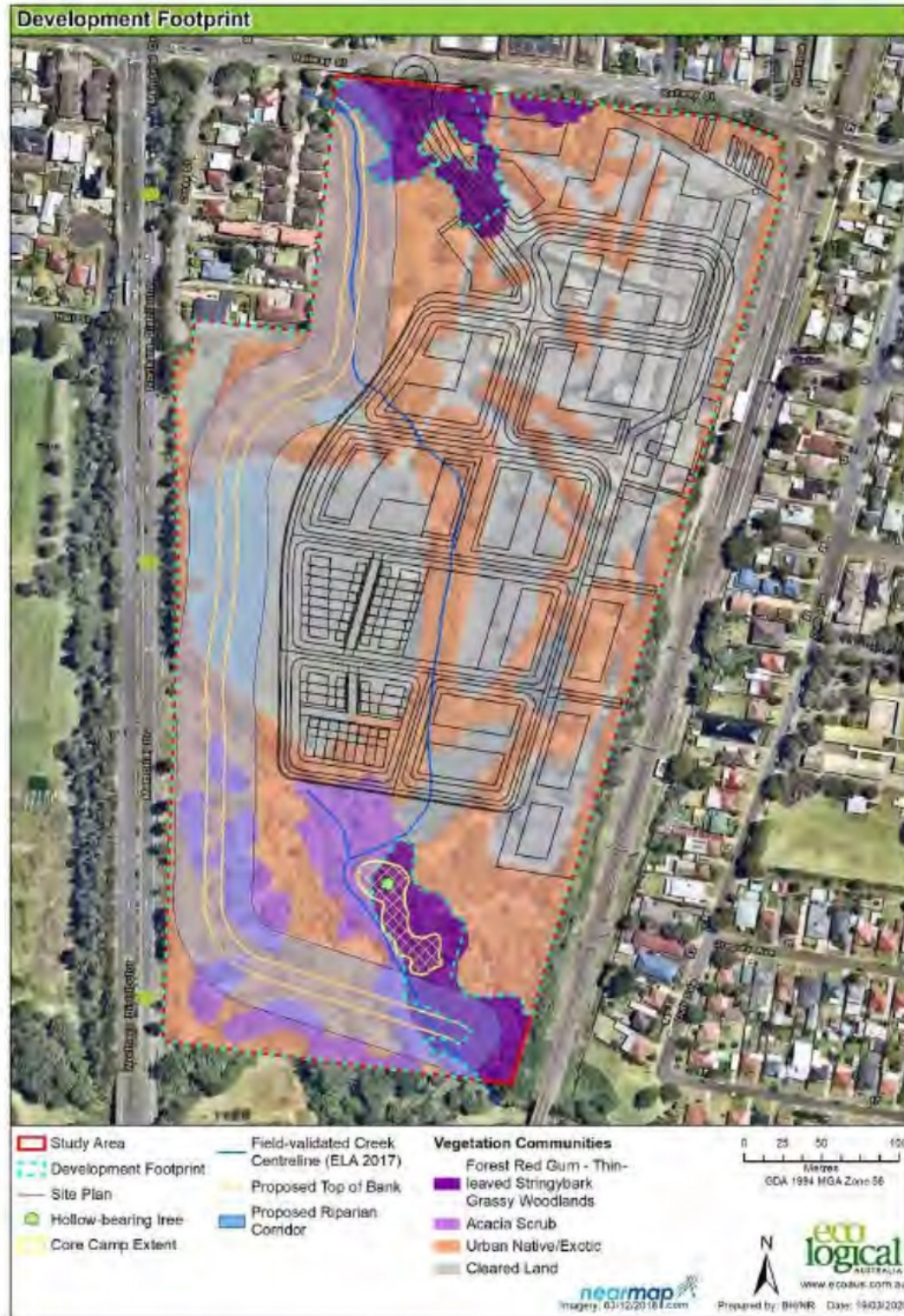
An assessment of the environmental impacts of the proposal was undertaken by EcoLogical (refer to **Appendix M**). The assessment determined that the proposed rezoning would result in the following:

- » realignment and revegetation of the riparian corridor along the western extent of the study area
- » removal of 8.28 ha of Urban Native and Exotic vegetation
- » removal of 0.58 ha of Forest Red Gum – Thin-leaved Stringybark Grassy Woodland
- » removal of 1.12 ha of Acacia Scrub

The proposed rezoning would retain 0.682 ha of Forest Red Gum – Thin-leaved Stringybark Grassy Woodland.

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Figure 6 Rezoning area



Source: EcoLogical

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2.9.1 Flora

One species listed under the Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act) and NSW Biodiversity Conservation Act 2016 (BC Act), *Eucalyptus scoparia* (Wallangarra White Gum), was identified within the study area. Within the study area, approximately eight individuals of this species were observed in a row adjacent to an internal road.

No other threatened flora species were recorded during the site inspections. Given the limited habitat available and its highly degraded condition, no other threatened flora species are considered likely to occur. A total of 108 species were identified during the field survey, including 53 exotic species, 48 endemic (locally native) species and five non-local planted natives.

Seven exotic flora species recorded within the study area are listed as requiring management consistent with the South East Regional Strategic Weed Management Plan (SERWMP) 2017.

Vegetation Communities

The vast majority of the study area accommodates areas of cleared land (including roads, infrastructure, water bodies) or weeds/exotics. Three native vegetation communities were identified within the study area:

- » Forest Red Gum Thin-leaved Stringybark Grassy Woodlands - occurred as two patches, comprising approximately 1.27ha.

The Forest Red Gum Thin-leaved Stringybark Grassy Woodland was identified as being in a highly modified and disturbed condition with high levels of exotic species, with planted non-local native species and modified landforms present. This vegetation community forms part of the 'Illawarra Lowlands Grassy Woodland in the Sydney Basin Bioregion' Endangered Ecological Community (EEC) listed under the BC Act. Neither of the patches present met the EPBC requirements. The proposed rezoning would retain and restore parts within the northern area and retain and restore the southern endangered ecological community.

The southern patch of this community (0.682ha) will be retained and rehabilitated. The northern patch will be largely cleared as a result of Council's request to relocate the site access point and provide a roundabout at the intersection of Harbinger Street.

- » Acacia Scrub – A total of 1.30ha occurred within the south of the study area in association with the tributary of Towradgi Creek which runs through the southern portion of the site.

This vegetation community occurred as a tall dense shrubland to low closed forest with the height and density of the canopy increasing in proximity to the tributary of Towradgi Creek. It is unclear whether these species were part of the original vegetation type within this area, or if they have colonised this area following the formation of a dense canopy of Acacias and exotic species.

- » Urban Native and Exotic - an area of approximately 8.28ha was mapped within the study area

Within and surrounding the most disturbed parts of the study area, stands of vegetation were comprised of exclusively exotic species or consisted of rows of non-local native planted trees with a disturbed predominately exotic understorey.

All vegetation communities across the site were identified as having been subjected to moderate to high levels of disturbance including vegetation clearing/thinning of the canopy layer, infestation of exotic species and modification to the landform and soil profiles.

The only areas of vegetation, beyond the areas which have previously been disturbed, which will be impacted as a result of the proposal is 0.588ha of Forest Red Gum Thin-leaved Stringybark Grassy Woodland and 1.12ha of Acacia Scrub. All efforts have been made during the Master Planning process to ensure the majority of the native vegetation communities are not impacted. The removal of these small areas of vegetation would not impact on the health of the overall community.

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2.9.2 Fauna

One threatened fauna species, *Pteropus poliocephalus* (Grey-headed Flying-fox) was identified in the study area during surveys. The Grey-headed Flying-fox is listed as vulnerable under the BC Act and EPBC Act and was identified as occupying the patch of Forest Red-gum Thin-leaved Stringy Bark Grassy Woodlands at the southern extent of the study area, adjacent to the dam and existing riparian corridor. The camp was estimated to support a typical population of 150-250 individuals, however this is subject to seasonably variations. More detail on this is provided below.

The remainder of the site is considered a low ecological constraint due to the presence of cleared lands, existing derelict infrastructure and Urban Native and Exotic Cover. The study area was not found to provide potential habitat for the Green and Golden Bell Frog (*Litoria aurea*) or Threatened microchiropteran bats.

Grey-headed Flying Fox

The study area contains a Grey-headed Flying-fox camp in the southern extent of the site. During 2017 – 2019, infrequent observations of the camp identified between 100 – 300 individuals occupying a small area of vegetation at the southern end of the site.

A regular monitoring program of the camp commenced in July 2019 to inform an understanding of its patterns of occupation and use. The camp was empty from July 2019 to early November 2019 when the population returned to a typical level. There was a spike in occupation to approximately 4,000 – 6,000 over a period in January – February 2020, which was attributed to significant bushfire activity in the region. The camp returned to a more typical population over March – April 2020 following rainfall events and the end of bushfire activity. Currently the camp is virtually vacant and showing similar trends as the last year.

The monitoring program will continue, however evidence to date supports the following understanding of the camp:

- » The typical population size is 100 – 500 individuals
- » The camp is occupied periodically, which appears to be seasonally related
- » No breeding has been observed at the camp
- » There is potential for the camp to expand in size, however this is constrained by the availability of suitable roosting and foraging species
- » Fly out direction is typically towards the escarpment to the west.

EcoLogical has developed Camp Management Actions to address the potential for future conflict between Grey-headed Flying-foxes and residential development on the site. The indicative management actions are consistent with the Flying-fox Camp Management Policy (OEH 2015) and include:

1. establishment of a physical buffer between the camp and future development (incorporating managed vegetations areas and road reserve)
2. creation of supplementary habitat, such as dams, at the southern boundary of the site
3. revegetation of portions of the site using known feed trees for the species (in areas away from the proposed development)
4. potential building design solutions (eg. acoustic)
5. communication and education for future residents

The proposed management actions have been peer reviewed and endorsed by Ecosure.

Discussions with Council and OEH occurred in relation to the camp buffer with extensive research, case studies and camp management process being considered. The proposed buffer has been increased to 100 metres from the core camp extent in response to feedback from Council and DPI&E.

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Microchiropteran Bats

EcoLogical undertook a site inspection of all buildings within the Corrimal Cokeworks, in order to determine the presence of the microchiropteran bat (microbat) habitat. The study found no evidence found of microbats using any of the structures present on the subject site. No microbats were seen or any clear evidence found of microbats having used the buildings at any time. In areas where thorough inspection occurred, it was concluded that the buildings were not being used by microbats, nor were there any signs of microbats.

However, some areas of the site were inaccessible, and it cannot be concluded that they were not being used by microbats at the time of survey. These areas were generally considered unsuitable microbat habitat. It is possible that there may be occasional use of buildings by individual microbats in other seasons of the year. The lack of suitable habitat, absence of signs of use or occupation and the lack of records from previous ultrasonic detection surveys, indicated that microbats rarely if ever use the structures and, if they ever do, it is only very occasionally and in small numbers.

2.10 Noise and vibration

Renzo Tonin & Associates conducted an environmental noise assessment of rail noise and vibration on the Corrimal site (refer to **Appendix R**) in order to assist in understanding any potential constraints to rezoning. The assessment was undertaken in accordance with NSW State Environment Planning Policy (Infrastructure) 2007 (ISEPP), the associated Development in Rail Corridors and Busy Roads – Interim Guideline, and other relevant vibration standards.

The findings of this study are:

- » Some facades of proposed residential buildings with exposure to road and rail noise will require acoustic facade treatments to meet the ISEPP criteria. The potentially affected building locations have been identified and indicative facade treatment recommendations provided.
- » Vibration impacts from the rail line have been found to be compliant with human comfort vibration criteria.
- » Ground borne rail noise during train pass-by is marginally compliant but should be confirmed during the design development of the residential buildings closest to the rail line.

In summary, noise and vibration issues do not present any constraint to rezoning the site for primarily residential uses. Any noise impacts can be suitably mitigated to achieve compliance through standard treatments to future residential development. These can be detailed at the development application stage.

2.11 Heritage

On 29th June 2020 Council approved that part of the former Corrimal Coke Works site be listed as a local heritage item in the WLEP 2009 and provided a curtilage for the site. For this reason, this PP does not need to include the proposed local heritage listing of the site.

This PP however includes information regarding the heritage aspects as it has been integrated into the design and logic of the Concept Master Plan.

The Corrimal Coke Works site was assessed by Urbis (refer to **Appendix U**), who also subsequently prepared a Conservation Management Strategy (CMS) (**Appendix Q**) to provide for the long-term conservation of significant elements on the site.

Overall, the site holds heritage significance to the local area, through its contribution to the growth of the steel industry in the Illawarra Region, the connection of the site to local collieries, the sites previous electricity generation and the historic connections of a major employer for over a century to the local community.

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Heritage Significance

The former Corrimal Coke Works are historically significant as a contributor in the development of the coke and steel manufacturing industry, in particular, in the Illawarra Region. The former Coke Works operated for just over 100 years, from 1912 to 2013. The former Corrimal Coke Works are closely associated with the nearby Corrimal Colliery, which fed the coke ovens coal from 1912 until 1985. Over its operational life, the site underwent substantial changes and upgrades, creating a layered industrial history.

The former Corrimal Coke Works provide an industrial landscape within the context of a residential suburb. The site as a whole, demonstrates an industrial aesthetic. These elements include:

- » C.1912 Brick Chimney – a handsome and historic landmark within the area.
- » Coke oven batteries, in particular C1 Coke Oven Battery. It is noted that the aesthetic of these ovens has been reduced due to decay.

Later added industrial elements including steel stacks, coke oven hoods, uptakes and ductwork contribute to the aesthetic qualities. However, these are not essential for understanding the heritage significance of the site.

Overall, the site holds heritage significance to the local area. This is demonstrated through its contribution to the growth of the steel industry in the Illawarra Region, the connection of the site to local collieries, the sites previous electricity generation and the historic connections as a source of employment to the local community.

The CMS recommends the following:

- » Adoption, implementation and review:
 - > Any works to the property should comply with appropriate legislation, policies and guidelines, as amended from time to time, including but not limited to the Heritage Act 1977, the Building Code of Australia (including the National Construction Code), the Australia ICOMOS Burra Charter (revised 2013) and relevant environmental planning documentation.
 - > The CMS should be subject to periodic review to ensure that the document remains relevant to ongoing change and use of the place, and statutory compliance and to incorporate updated information.
- » Managing heritage significance:
 - > The Statement of Significance of the former Coke Works, Corrimal, embodies the core heritage values of the place. All future decisions and works to the property must be guided by the statement of cultural significance and the identified significant spaces, fabric, and built elements identified in this CMS, together with any additional detailed research and assessment.
 - > Article 3 of The Burra Charter (revised 2013) indicates that conservation is based on a respect for the existing fabric of a place and should therefore involve the least possible physical intervention to prevent distortion of the evidence provided by the fabric. One of the key objectives therefore, of contemporary conservation practice is to retain as much of the significant original fabric as possible, in order to preserve the essential integrity of the heritage resource.
- » Conservation and Maintenance:
 - > Regular maintenance and scheduled conservation works are required to be implemented to conserve the heritage significance and identified significant fabric of the place. Ongoing maintenance should be undertaken in accordance with a cyclical maintenance plan.
- » Future use, alterations and new work:
 - > Ongoing sustainable and viable uses would encourage and facilitate the conservation and maintenance of identified elements within the recommended reduced heritage curtilage of the former Corrimal Coke Works, Corrimal NSW. New uses should be considered with a goal to conserve and enhance the identified heritage values of the site whilst providing for those uses.
 - > It is acknowledged that the site is large and contains many elements and built forms. It is not feasible, nor required, to retain all or complete elements. The recommendations below set out the minimum of which should be retained, conserved and/or interpreted.

2.11.1 Heritage retention

While new built forms may be included in any development of the site, the retention of tall, landmark elements will ensure the former industrial site is conserved and interpreted as an industrial element. Any internal access routes, road layouts, built envelopes and landscape elements should provide views and vistas to the reduced heritage curtilage. This could be achieved through view lines and interpretation of the existing railway/roadway and transportation in coke production. The Urbis Report provides a full detail of the elements to be retained and/or interpreted.

2.11.2 Curtilage

A curtilage, as part of a separate Planning Proposal determined by Wollongong Council in June 2020, has been included for the site, and therefore is not explained or justified in this report.

2.11.3 State Heritage

Wollongong Council has nominated the site for State heritage listing. The application is currently being considered by the Heritage NSW; this will follow the relevant statutory process separately to this PP.

2.11.4 Aboriginal Cultural Heritage

An Aboriginal Cultural Heritage Assessment has been prepared by Kelleher Knightingale. Aboriginal community consultation has indicated that creeklines in the area were traditional fishing grounds and retain Aboriginal cultural value for their connection to this activity, however no specific or significant Aboriginal cultural features have been identified for the site.

Extensive previous modifications and disturbance associated with former industrial land use have diminished or negated the archaeological potential of the majority of the study area. The parts of the study area used for the former cokeworks operations exhibited significant levels of disturbance that would have removed/displaced Aboriginal archaeological objects.

However, the archaeological assessment by Kelleher Nightingale identified one Aboriginal archaeological site comprising Aboriginal objects located within the study area: low density artefact scatter FCCW AFT 1 (AHIMS 52-2-4505). FCCW AFT 1 is located on an elevated floodplain landform in association with Towradgi Creek, in the southern portion of the study area. This site area would be impacted by the proposed redevelopment works and the assessment concludes that avoidance or mitigation to impacts are deemed as unnecessary. An Aboriginal Heritage Impact Permit (AHIP) issued by the former OEH under section 90(1) of the National Parks and Wildlife Act 1974 will be required prior to any impact on this site.

The proposed shared path through the site is expected to run in the vicinity of this identified site, so there is potential for signage/artwork detailing the cultural significance and stories associated with the area to be incorporated into the design of this element.

2.11.5 Archaeological

The Historical Heritage Assessment (Biosis 2017) identified that there was moderate potential for the presence of archaeological remains associated with the former tramway. As such, it was recommended that an approval under section 140 of the NSW Heritage Act 1977 will be required in order to excavate or disturb these potential relics associated with the tramway.

Historical archaeological excavations were subsequently undertaken by Austral Archaeology Pty Ltd under an Exception under S139(4) of the Heritage Act 1977 in order to identify structural remains associated with the 1890s tramway, assess the condition of the remains and confirm the potential alignment of the tramlines. The former tramway was constructed in the 1880s and was associated with Thomas Bertram's 'Brokers Nose Coal

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Company, which opened in 1884. The Brokers Nose Coal Company was taken over by the Corrimal Coal Company in 1889. The tramway continued to be utilised by the Corrimal Coal Company into the 21st century.

The aim of the archaeological excavations was to determine whether archaeological material associated with the previously identified 1890s tramway was present, and to establish the requirement for any heritage curtilage to manage any archaeological material.

The results of the archaeological investigations failed to identify widespread, in situ archaeological remains associated with the tramway. Where individual elements were identified which related to the tramway, these were all relatively shallow and within 250mm from the present ground level, suggesting that the tramline was removed following its decommissioning, with parts such as sleepers and railway tracks being reused elsewhere.

As such, no heritage constraints were identified from within the area that was subject to historical archaeological testing. There is no requirement to consider enlarging the heritage curtilage to include elements of the tramline as all such physical evidence is likely to have been destroyed, and no requirement for further archaeological investigations.

2.12 Services infrastructure

BG&E were engaged to update the previous Servicing Strategy Report by ADW Johnson (**Appendix X**). It has been confirmed that all utility services are available to the site, noting the following:

- » Potable Water, Gas and Telecommunication networks require no augmentation to service the overall development;
- » No funding or construction of extensive feeder infrastructure will be required for electrical servicing, with only minor augmentations to the existing Endeavour Energy network envisaged;
- » After further consultation with Sydney Water and a comprehensive MOUSE Modelling process, it has been confirmed that the wastewater network will be able to accommodate the full development yield; and
- » The wastewater servicing strategy involves the provision of additional storage capacity by upsizing the existing trunk main through the site, which will be undertaken in conjunction with a realignment of that trunk main during the bulk earthworks phase.

A summary is provided below.

2.12.1 Potable Water Servicing

Sydney Water Corporation (SWC) was consulted regarding servicing the proposed development with potable water and the associated impacts on existing SWC potable water infrastructure as a result of the increased potable water network demand imposed by the proposed development. SWC confirmed that the existing potable water main fronting the development along Railway Street has sufficient capacity to accommodate the proposed development.

2.12.2 Wastewater servicing

Results and findings from modelling and associated consultation with SWC has determined the existing Sydney Water wastewater network will be able to accommodate the proposed former Corrimal Coke Works development upon implementation of planned network upgrades and adjustment and upsize of the on-site wastewater trunk main.

2.12.3 Electrical servicing

Review of the existing electrical network shows that there are two 11kV feeders (CR1228 and CR1280) located opposite the development site along Railway Street, however both of these feeders have been estimated to have

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a total available capacity of approximately only 0.5MVA, which is not sufficient to accommodate the development site. However, Endeavor Energy (EE) noted that the overall development can potentially be serviced by Feeder RV1206 from Russell Vale Zone Substation, which is deemed to have the required available capacity to accommodate the mature load of the development.

EE confirmed that the development would not expect to fund or construct extensive feeder works in order to utilize the available capacity from Feeder V1206 and required augmentations would be limited to network switching and minor augmentation of the existing High Voltage (HV) distribution network by linking existing HV networks in various locations. It is envisaged that HV linkage points will be Underground to Overhead (UGOH) terminations on feeder CR1280, with one in both Railway Street and High Street. Details surrounding HV linkages and network configuration will be confirmed when a firm application for load is submitted to EE.

2.12.4 Telecommunications servicing

Design It Telco Pty Ltd was consulted about the capacity of existing National Broadband Network (NBN) and Telstra networks. It was confirmed that:

- » Current NBN and Telstra infrastructure could handle the overall development
- » As the development is over 100 lots it will automatically qualify for fibre to the premises (FTTP); and
- » NBN Co will supply a backhaul cable to cater for future lots.

It has been confirmed that the future development does not require any upgrades to telecommunications networks.

2.12.5 Gas servicing

A Technical Review Request was submitted to Jemena Limited, to assess the impacts of the proposed development and to confirm the gas network has adequate capacity to service the overall development. It was confirmed that Jemena has suitable gas mains located on Railway Street within the vicinity of this proposal which currently have adequate capacity to service the overall development at this time.

2.13 Traffic and transport

Existing transport infrastructure in the study area includes:

- » Memorial Drive (state road) which is a key strategic route through Corrimal carrying high volumes of north-south through traffic. Memorial Drive connects Corrimal to the Princes Highway at Bulli and the M1 Princes Motorway at Gwynneville via North Wollongong;
- » Railway Street between Memorial Drive and Pioneer Road, with intersections at Cross Street, Harbinger Street, High Street, Ruddock Street, Duff Parade and Park Road (all local roads);
- » signalised intersections at Railway Street/Memorial Drive and Railway Street/Pioneer Road;
- » a level crossing on Railway Street with boom gates and flashing lights across the Illawarra Railway Line, which carries both passenger trains (all stop and express services) and freight trains;
- » Corrimal Railway Station (located adjacent to the site), serviced by the South Coast Line between Kiama, Port Kembla and Sydney via Wollongong. Passenger trains generally run every 15-30 minutes during peak hour directly servicing Wollongong with 'all stop' services towards Sydney CBD; and
- » Bus stops along Railway Street in both directions, serviced by:
 - > Route 4: Bulli to Wollongong
 - > Route 92: Bulli to Wollongong
 - > Route 93: Bulli to Wollongong University

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- » The site provides direct access to the existing pathway network on Railway Street. There are very few formal bikeways located in close proximity to the development site.

Bitzios Consulting were engaged to provide a supplementary Traffic Impact Assessment (refer to **Appendix Y**) following Gateway Determination.

The Traffic Impact Assessment was prepared in close consultation with Council, TfNSW (including RMS) and Sydney Trains, and based on an agreed scope and methodology. The traffic modelling was subject to acceptance of calibration and validation in satisfaction of RMS guidelines.

The assessment concludes that there are no traffic and transport impacts associated with the development which cannot be mitigated through reasonable works within public land or on site.

The primary traffic impact from the development is confined to the intersection of Memorial Drive and Railway Street. This intersection requires upgrade to accommodate the future development traffic. The proposed upgrade has the benefit of also providing capacity to accommodate background traffic growth.

Other key conclusions drawn from the assessment of the traffic and transport impacts associated with the proposed development include:

- » The most appropriate location for development access is via Railway Street, approximately 60m east of Harbinger Street. A channelized T intersection with a right turn-in pocket and separate left turn-out and right turn-out lanes is sufficient to provide a safe and efficient access to the site.
- » A left in/out access off Memorial Drive is inconsistent with the function of Memorial Drive and may introduce a safety risk in the high speed, high volume environment in closed proximity to the Towradgi Road intersection. TfNSW also advised that they would not support this outcome.
- » The development will introduce 373 vehicles per hour (vph) into the network in the morning peak and 456 vph into the network in the evening peak with almost three-quarters of this traffic orientated towards the Memorial Drive intersection and the remainder orientated towards the Pioneer Road intersection with Railway Street. This increase in vehicles per hour can be catered for through the introduction of a channelised 'T' intersection at the sole site access intersection on Railway Street, approximately 60m east of Harbinger Street.
- » The additional traffic introduced into Railway Street by the development equates to one vehicle every 10 seconds (two-way) in the peak hour, which is insignificant on a distributor road such as Railway Street.
- » Railway Street will experience an approximate 17% increase in daily traffic. Its forecast volume of approximately 8,700 vehicles per day at the level crossing is well within the capacity of a single lane each way distributor road.
- » The development's impacts on the Railway Street level crossing due to additional traffic and pedestrian movement are insignificant. State Rail's ALCAM model calculated no increased risk at the level crossing as a result of the development and no works/upgrade to the level crossing are required.
- » The preferred route is along the southern side of Railway Street, which is consistent with the Wollongong City Council Bike Plan 2014-2018. The applicant has proposed to construct a shared path from Cross Street to the commuter car parking entrance as part of a VPA with Council.
- » Site specific parking rates are proposed taking into consideration the site is located adjacent to the Corrimal railway station and the localised catchment for retail, food and drink and restaurant uses.
- » The Concept Master Plan facilitates significant opportunities for transport oriented development including potential for bus circulation to interchange at Corrimal station, Kiss and Rides, taxi zones as well as significantly enhanced pedestrian and cyclist access. There is potential for the existing commuter car parking at Corrimal station to be incorporated into the new street network, however this would be subject to future approval from RailCorp and is not required for the PP.

Following subsequent discussions with Council and TfNSW, and at Council's request, it was agreed that a new roundabout access on Railway Street in line with Harbinger Street would be the preferred and best access option for the site. This has been incorporated into the PP and also benefits existing residents to the north of Railway Street by providing a managed intersection for access onto Railway Street.

2.14 Economic

2.14.1 Industrial suitability

An assessment of the viability of on-going industrial use of the site was undertaken by Hill PDA (**Appendix Z**). The report concluded:

- » There is sufficient supply of well-located industrial land in the Wollongong LGA without the subject site
 - > The Industrial Lands Audit in 2014 found that the Illawarra Region contained 3,110ha of industrial land. Of the total area of industrial land, 603ha was vacant (19%). The Wollongong LGA contains the highest proportion of vacant industrial land supply in the region, totalling 321ha (53% of the region). An examination of the take up of industrial land since the 2014 revealed that 45ha of land has been developed, however, 42ha of this was for the Prixcar processing, storage and transport facility.
 - > The majority of industrial land in the Wollongong LGA is in Unanderra, Kembla Grange and Port Kembla and offers lower cost and better access than Corrimal.
- » Increased demand in transport, storage and warehousing sectors, but the subject site is unsuitable for such uses
 - > Large-scale transport, storage and warehousing sector (or logistics) have locational needs that are determined by efficient supply chains, access to customers and suppliers, land availability and main road access. This describes firms that have specific land and infrastructure needs and potentially buffer distance requirements from residential or other sensitive land uses. Consequently, the site is not as attractive as locations such as Unanderra, in the Illawarra, for these industrial uses due to land use conflict issues due to the proximity of residential dwellings.
- » Conversion to new industrial uses is not viable
 - > The costs of demolition, remediation, land development, subdivision, external works and open space embellishments (around \$25m) would exceed the end value of the industrial parcels (around \$20m to \$22m based on an end sale value of \$200 to \$250/m²). Therefore, this option is not financially viable and would not be realised in the current climate or in the foreseeable future.

The Gateway Determination confirmed that the loss of employment land is justified in satisfaction of the relevant Section 9.1 Direction 1.1 Business and Industrial Zones.

2.14.2 Existing centres and supermarkets

Urbis undertook an assessment of the retail environment within Corrimal. Within the main trade area, in the Corrimal Town Centre, there is a Woolworths, Coles and ALDI supermarket. There is also a small shopping village in East Corrimal with a Food Works (approximately 300m²).

Fairy Meadow represents the key competition from south of the trade area. From the north, retail competition comes from Woonona, Bulli and Thirroul town centres. The current number of supermarkets and their floor space in surrounding town centres is shown in **Table 6** below.

Table 6 Supermarket provision

Town Centre	Supermarkets	Estimated supermarket Gross Lettable Area Retail (GLAR)
Corrimal	3	7,600m ²
Fairy Meadow	3	7,800m ²
Woonona	1	1,000m ²

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Town Centre	Supermarkets	Estimated supermarket Gross Lettable Area Retail (GLAR)
Bulli	1	3,200m ²
Thirroul	2	2,200m ²

The Corrimal town centre is the largest and closest retail area to the subject site, located around 350m to the west. Key retail includes:

- » Lederer Shopping Centre: a single-level centre with undercroft and at-grade car parking. The centre has multiple pedestrian access points from Railway Street and Princes Highway. The centre comprises:
 - > Woolworths (~3,500m²)
 - > Specialty retail (~3,100m²) comprising a mix of convenience retail, retail services, and fast casual dining and cafes
 - > Dan Murphy's
- » Corrimal Park Mall is a dated single-level centre at the northern end of the Corrimal town centre, comprising:
 - > Coles supermarket (~2,600m²)
 - > Nine specialty retail tenants (~1,200m²), orientated towards convenience retail
- » Strip retail along The Princes Highway and Railway Street:
 - > Around 60 retail specialty tenants (estimated 5,800m²), including strong provision of hair and beauty tenants (14), takeaway food (6), cafes (6), and massage (5)
 - > Some 36 non-retail tenants, including banks (5), employment and financial services (10), and real estate agents (4)
 - > An ALDI supermarket (~1,500m²) in a mixed-use development on Russel Street.

2.14.3 Retail demand

Urbis has undertaken an analysis (refer to **Appendix AA**) based on an assessment of key opportunities relating to the subject site and its competitive context, which demonstrate:

- » Retail at the subject site should target on-site residents first and foremost. Residents need on-site amenities to create a focal point for activity.
- » A small-scale independent local supermarket could serve a convenience retail role for on-site residents.
- » The critical mass of new residents on site could support a small convenience-based retail offer, potentially including the likes of a pharmacy, newsagent, and beautician, despite good provision of these in Corrimal town centre.
- » There is a noticeable gap in the local market for higher-quality sit-down dining venues and contemporary cafes. Such an offer at the subject site could draw weekend visitors from beyond the subject site.
- » Retail should be integrated with a strong public realm to establish the precinct as a small neighbourhood centre.
- » The new retail can leverage its location next to the train station and the historical reputation of the Corrimal Coke Works, well known throughout the region, to serve commuters and visitors to the Wollongong area.

Key findings of the retail assessment include:

- » Based on securing a market share of trade area food and grocery retail spending, the subject site could support a small supermarket targeted at the on-site market. Allowing for 10% of spending to come from beyond the catchment, and an allocation of 6% to general merchandising, the estimated sales potential of a

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supermarket facility at the subject site is \$5.2m in 2024, growing to \$6.2m by 2026. This aggregate sales potential could support a store in the range of 500 – 1,000m² (assumed 750m²), this translates to an average trading level of around \$6,900 per m² initially, stabilising at around \$8,300 per m² in 2026.

- » A small supermarket on site could be supported by some cafes and restaurants and a limited convenience offer.
- » The indicative composition that could support a small format supermarket on site, based on Urbis' understanding of key retail and design principles, includes:
 - > 2-3 cafes/restaurants, that also serve a takeaway role, and a specialty food store
 - > 2 non-food retail speciality stores, which could include a newsagency, small pharmacy or other leisure/general retail shops
 - > Personal services such as a hair salon, beautician, massage etc.
 - > A non-retail shopfront such as a real estate agent or allied health practice (dental, physiotherapy or chiropractor)

The above mix could occupy around 1,250m², taking total floorspace to around 2,000m².

Total retail sales are estimated at \$11.4m in 2024, growing to \$13.4m in 2026. These sales reflect an overall retail sales performance of \$6,350 per m² in 2024 and \$7,440 per m² in 2026.

Retail specialties are estimated to trade at around \$6,830 per m² by 2026, assuming an optimised tenancy mix.

- » The proposed retail at the subject site is estimated to achieve a very minor share of retail spending in the secondary trade area, and slightly higher share of retail spending in the primary trade area. The centre is estimated to achieve a 2.7% market share of main trade area retail spending, including a 3.5% market share of food spending. Across the combined primary trade areas, the centre is estimated to achieve a 10.6% market share, including 13.9% of food spending and 6.1% of non-food spending.

Within the primary on site trade area, the offer is estimated to achieve a 15.9% market share of total spending, including 20.9% of food spending and 9.0% of non-food spending. Around 10% of sales are estimated to be generated by residents from beyond the trade area.

The combined primary trade areas are estimated to account for 53% of total sales at the centre. The combined secondary trade areas are estimated to account for 37% of total sales.

The following benefits reflect the importance of retail facilities in supporting the residential component of the proposed development, by adding to the appeal of the overall precinct.

- » Improved public amenity and activation
 - > The proposed retail will be integrated with a new town square at the heritage precinct, adjacent to the train station. This town square will include public spaces and amenities, adding significantly to the liveability of the overall development.
 - > Activation of the heritage precinct with a small retail centre will deliver a high-quality urban design outcome with improved safety for residents and visitors.
- » Sense of place
 - > Creating a sense of place adds significantly to the vibrancy and appeal of a precinct.
 - > A modest retail offer at the town square on site can contribute to a sense of place if it is provided with:
 - Strong links to the outdoors, including good solar access
 - Generous and comfortable open space where people can sit and linger, particularly in areas close to the train station
 - A human-scale of street level pedestrian experience
 - Distinctive architectural or landscape features.

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- » Access to convenience retail
 - > The proposed 700 - 750 dwellings at the subject site will accommodate around 1,700 new residents upon completion. Retail at the subject site would be targeted to service the convenience needs of these residents, located centrally to dwellings and public transport.
 - > This would result in enhanced convenience for residents and train patrons, resulting in time savings and reduced travel costs.
- » Enhanced retail choice
 - > The inclusion of retail at the subject site will add to the overall quality and range of retailing provision within the trade area, resulting in an extension of choice for local and on-site residents.
 - > The extension of choice could potentially result in benefits for the consumer in terms of keener prices and better quality.

The proposed retail on site would generate an estimated 103 ongoing jobs during the operational period. Up to an additional 23 indirect jobs in supporting and supplying industries could also be provided, indicating that the development could support up to 126 total jobs in the region (full time, part time and casual).

This could result in a total of \$4.8m Gross Value Added (GVA) per year (in net present value terms) to the local region and broader state economy over a 20-year operating period. This consists of \$3.2m direct GVA per year generated by the daily operation and management of the proposed retail and \$1.6m indirect GVA per year over the same period.

In conclusion, the retail assessment found that:

- » The proposed 2,000m² of retail and non-retail floor space, comprising a small supermarket, supporting dining, convenience-based retail, and non-retail shopfronts will be well supported by market demand, driven by strong on-site population growth.
- » The modest retail offer is forecast to achieve turnover in 2024 that reflects:
 - > A minor market share of retail spending (3% of main trade area and 11% of primary trade area)
 - > Only a portion of the expected growth between 2018 and 2024 (28% of primary trade area growth and 16% of main trade area growth)
- » Residual spending and spending growth will continue to support existing and proposed retail centres.
- » The proposed retail at the subject site will also have significant benefits relating to providing valuable amenity for current and future residents of the immediate area, as well as employment, consumer and economic value benefits for the local region and state economy.

The assessment concludes that the proposed retail development should therefore be supported from an economic perspective.

2.15 Population and housing need

Population

In 2016, the Illawarra-Shoalhaven region had a population of 404,650. Wollongong LGA had the largest population, accounting for 52% of the region's total, followed by Shoalhaven LGA (25%), Shellharbour LGA (18%) and Kiama LGA (5%). Between 2001 and 2011 the Illawarra-Shoalhaven region grew at compound annual growth rate of 0.8%, 0.4% slower than Greater Sydney's growth of 1.2%, but faster than growth in regional NSW of 0.7%.

Growth in the Illawarra-Shoalhaven region between 2016 and 2036 will lift its population by 67,000, to 471,700 people. Within the Illawarra-Shoalhaven region, Shellharbour has experienced the fastest growth, in part reflecting supply with the presence of more greenfield options (vs Wollongong).

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Housing

The projected growth in population will translate to demand for a net addition to the dwelling stock of 45,000 dwellings or 2,250 per annum (pa) in the period between 2016 and 2036. Historically, detached housing has been the dominant form of housing in the Illawarra-Shoalhaven region, accounting for 76.4% of the housing stock in 2016. More recently, particularly in Wollongong but also in Shellharbour, there has been a higher share of growth in dwellings accommodated by in-fill developments, and by medium and high-density housing types. This in part reflects a long-term trend towards higher density.

The DPI&E's projections for 2015/16-2019/20 and 2020/21-2024/25 assume that detached housing will account for 50% of growth in the housing stock, with medium and high-density housing accounting for the other 50%. The DPI&E notes that 60-70% of demand might be met from greenfield developments and 30-40% from in-fill. In the past ten years to 2015, there has been 7,634 dwelling completions from in-fill (in the Illawarra-Shoalhaven region).

In 2016, the most common households within the Illawarra-Shoalhaven region were couples with children, which accounted for 28.7% of all households in 2016. This was followed by couple only households (26.7%) and lone person households (24.1%). Heading into the future the fastest growth is expected to be in couple only households (1.4%) and lone person households (1.4%) which include older households.

In order to cater to the region's changing household structures, diversification of dwelling structures is required. The downsizer market and single young professionals are likely to want smaller and more compact housing. While there has been growth in medium and higher density development, particularly in Wollongong CBD, provision of these housing types is still relatively under-served in relation to potential demand.

Housing diversity will also be important in maintaining housing affordability through the provision of a range of more compact dwelling types.

2.16 Social Infrastructure

2.16.1 Recreational/Community Infrastructure

Recreational/Community Infrastructure in the vicinity of the site includes:

- » Corrimal Memorial Swimming Pool (250m west from the western boundary)
- » Corrimal District Library & Community Centre (250m from the western boundary of the site)
- » Robert Ziems Park featuring multiple playing fields (66m west of the site)
- » Towradgi sporting fields, tennis courts and bowling greens at Moray Road, Towradgi (850m south-east of the site)

There is an opportunity to link all of these community recreational resources, the coast and the site via a green link aligning with the Towradgi Creek corridor. This would also effectively link with the Grand Pacific Walk, currently being developed in stages by Council, that will ultimately provide a continuous shared pathway from the Royal National Park to Lake Illawarra.

In addition, significant walking and cycling facilities are proposed as part of the concept plan for the site to promote active transport and public transport use.

2.16.2 Educational Infrastructure

Educational institutions surrounding the sites include:

- » Corrimal High School (375m to the east)
- » Corrimal East Public School (350m to the east)
- » Corrimal Public School (750m to the north-west)

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- » Bellambi Public School (1km to the north-east)
- » Wollongong High School of the Performing Arts & Keira High School (3.5km to the south-west)
- » TAFE NSW Wollongong (3.5km to the south-west)
- » University of Wollongong (4km to the south-west)
- » Saint Columbkille's School (822m to the north-west)
- » Towradgi Public School (1.2km to the south)
- » Specialist schools such as the Autism Association of NSW - South Coast School (850m to the north-west).

An assessment of likely capacity for schools has been performed by reviewing information on the 'My Schools' website. This is provided in **Table 7** below.

Childcare facilities are available surrounding the site, but there is also an excellent opportunity to provide childcare facilities on site, servicing both residents and commuters in particular.

Table 7 Maximum enrolment and current enrolments for relevant schools near the subject site

School	Enrolments		Estimated spare capacity
	Maximum	2016	
Corrimal High School (375m to the east)	496 (2008)	345	151+
Corrimal East Public School (350m to the east)	295 (2008)	202	93+
Corrimal Public School (750m to the north-west)	184 (2016)	184	Additional capacity unknown
Bellambi Public School (1km to the north-east)	256 (2008)	164	92+
Wollongong High School of the Performing Arts (3.5km to the south-west)	Additional capacity unknown		
Keira High School (3.5km to the south-west)	949 (2015)	938	9
Saint Columbkille's School (822m to the north-west)	422 (2016)	422	Additional capacity unknown
Towradgi Public School (1.2km to the south)	164 (2014)	160	4+
Fairy Meadow Public School (3.5km south)	421 (2008)	374	47+
Woonona High School (3.5km north)	787 (2008)	599	188+
Woonona Public School (3.5km north)	342	512	Additional capacity unknown
Woonona East Public School (3.5km north)	310 (2008)	222	88+

Source: www.myschool.edu.au

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2.16.3 Health Infrastructure

A brief assessment of health infrastructure would indicate that the subject site is well serviced. In Wollongong, (approximately 5.5km from the subject site) this includes:

- » Wollongong Hospital which is currently undergoing \$100m worth of capital works enhancements which include the construction of the Illawarra Elective Surgical Services Centre
- » South Coast Private Mental Health Hospital
- » Wollongong Day Surgery
- » Wollongong Private Hospital
- » Bulli Hospital (non emergency) is currently being upgraded/rebuilt
- » Numerous specialist centres.

In the local area surrounding the subject site, numerous primary care facilities are available with at least four medical centres and a community health centre.

Significant community, health, educational and recreational resources are available surrounding the site.

2.17 Suitability of the site

It is estimated that development of the site for residential land uses has the opportunity to provide for a diversity of housing in order to cater for the changing demographics of the Wollongong LGA. Importantly, the key factors that make the site suitable for residential development include:

- » Not viable for industrial use
 - > The coke works ceased operation in 2014
 - > A financial feasibility assessment has found that it would not be viable to redevelop the site for industrial purposes (considering the cost of redevelopment versus return)
 - > Significant industrial land resources are available in the Wollongong LGA that provide better access and less land use conflict than the subject site.
- » Planning
 - > The 18.18ha site is able to offer a mix of residential and recreational resources where site constraints have been investigated and effective mitigation measures or other strategies have been developed
 - > A large, contiguous area of generally flat, readily developable land adjacent to Corrimal train station offering the opportunity for transit-oriented development
 - > Logical extension of the R3 Medium Density Residential zoning to the north, north-east and east of the site
 - > The capacity to deliver a diverse range of housing within an R3 Medium Density Residential zone, delivering housing typologies that are in short supply in the Illawarra region.
 - > An R3 Medium Density Residential rezoning could also support small-scale neighbourhood and commuter services to deliver convenience for residents and the broader community
 - > Provision of significant open space resources within the site
 - > Residential use will allow preservation and interpretation of key heritage elements on the site
- » Connectivity
 - > Immediately adjacent to Corrimal train station and existing bus routes
 - > Rail and major arterial road access connect the site to the Wollongong CBD (5.5km) and Sydney CBD (63.5km)

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- > Ideally located within 350m of the Corrimal town centre
- > 8-minute bus ride to Wollongong University and the broader innovation employment zone
- > 500m or less from primary and secondary schools with over 10 schools within a 3.5km radius of the site.
- » Amenity
 - > 500m to recreational facilities including sporting fields, Corrimal pool, library and community centre
 - > 1.2km to patrolled beaches
 - > Proposed green link, cycle way/pathway to Corrimal Beach and adjacent sporting fields
 - > Superb views to the Illawarra Escarpment
 - > Potential views to the Wollongong coastline.

3 Statutory Planning Context

The rezoning has been prepared having regard to the existing planning framework in context of the WLEP 2009 and other applicable Environmental Planning Instruments (EPIs).

3.1 Wollongong Local Environmental Plan 2009

The aims of WLEP 2009, which guide the preparation of the PP for the site at Corrimal are as follows:

- a) *to provide a framework for land use management,*
- b) *to encourage economic and business development to increase employment opportunities,*
- c) *to encourage a range of housing choices consistent with the capacity of the land,*
- d) *to improve the quality of life and the social well-being and amenity of residents, business operators, workers and visitors,*
- e) *to conserve and enhance remnant terrestrial, aquatic and riparian habitats, native vegetation and fauna species,*
- f) *to conserve and enhance heritage,*
- g) *to ensure that development is consistent with the constraints of the land and can be appropriately serviced by infrastructure,*
- h) *to ensure that significant landscapes are conserved, including the Illawarra Escarpment, Lake Illawarra, the drinking water catchment and the coastline.*

The PP gives effect to these objectives, as outlined in **Part 1**.

3.1.1 Zoning

Under the WLEP 2009 the subject site is zoned IN3 Heavily Industrial, RE2 Private Recreation and SP2 Infrastructure (Road). The objectives of these zones are as follows:

- » **IN3:**
 - > *To provide suitable areas for those industries that need to be separated from other land uses.*
 - > *To encourage employment opportunities.*
 - > *To minimise any adverse effect of heavy industry on other land uses.*
 - > *To support and protect industrial land for industrial uses.*
 - > *To facilitate the ongoing sustainability of steel making and steel product manufacturing that will contribute to the economic and employment growth of Wollongong.*
- » **RE2:**
 - > *To enable land to be used for private open space or recreational purposes.*
 - > *To provide a range of recreational settings and activities and compatible land uses.*
 - > *To protect and enhance the natural environment for recreational purposes.*
- » **SP2:**
 - > *To provide for infrastructure and related uses.*

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- > To prevent development that is not compatible with or that may detract from the provision of infrastructure.
- > To provide for key transport corridors.

The zoning of the site is identified in **Figure 7** below.

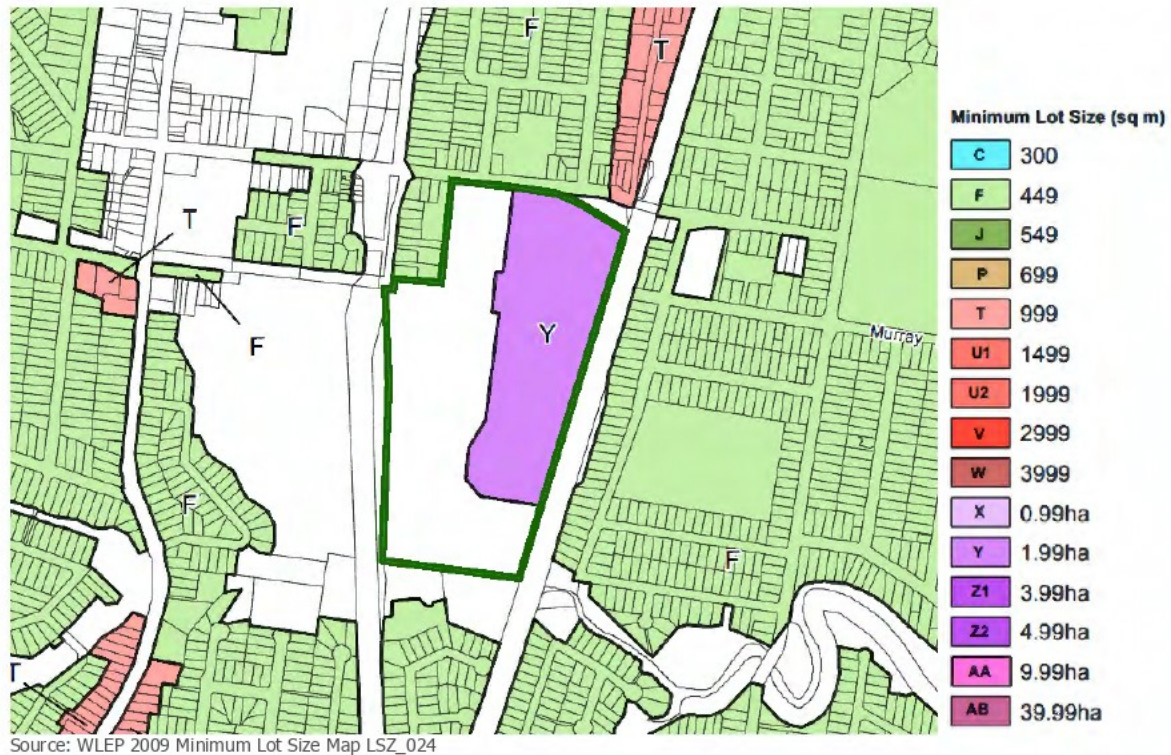
Figure 7 Current zoning map – WLEP 2009



3.1.2 Minimum lot size

Clause 4.1 of WLEP 2009 sets a MLS, as indicated in the associated MLS map. Part of the site has an MLS of 1.99ha, while the remaining part of the site is not subject to a MLS, as shown in **Figure 8** below.

Figure 8 Current MLS map – WLEP 2009



3.1.3 Height of Buildings

Clause 4.3 of the WLEP 2009 sets a maximum height limit for buildings, with heights shown on an associated Height of Buildings Map. Part of the site is not subject to a height control, while the remaining part has a 9m height control, as shown in **Figure 9** below.

Figure 9 Current height of buildings map – WLEP 2009



Source: WLEP 2009 Height of Buildings Map HOB_024

3.1.4 Floor Space Ratio

Clause 4.4 of the WLEP 2009 sets a maximum FSR, with FSRs shown on an associated FSR map. The subject site has no FSR control.

3.1.5 Land Reservation Acquisition

Clause 5.1A of WLEP 2009 aims to limit development on certain land intended to be acquired for a public purpose. The site contains Lot 126 DP 598190, which is intended to be utilised for the expansion of Railway Street, as shown in **Figure 10** below.

As per Clause 5.1 if there is an owner-initiated acquisition process the following applies:

5.1 Relevant acquisition authority

1. *The objective of this clause is to identify, for the purposes of section 27 of the Act, the authority of the State that will be the relevant authority to acquire land reserved for certain public purposes if the land is required to be acquired under Division 3 of Part 2 of the Land Acquisition (Just Terms Compensation) Act 1991 (the owner-initiated acquisition provisions).*

Note.

If the landholder will suffer hardship if there is any delay in the land being acquired by the relevant authority, section 23 of the Land Acquisition (Just Terms Compensation) Act 1991 requires the authority to acquire the land.

2. *The authority of the State that will be the relevant authority to acquire land, if the land is required to be acquired under the owner-initiated acquisition provisions, is the authority of the State specified below in relation to the land shown on the Land Reservation Acquisition Map (or, if an authority of the State is not specified in relation to land required to be so acquired, the authority designated or determined under those provisions).*

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Type of land shown on Map

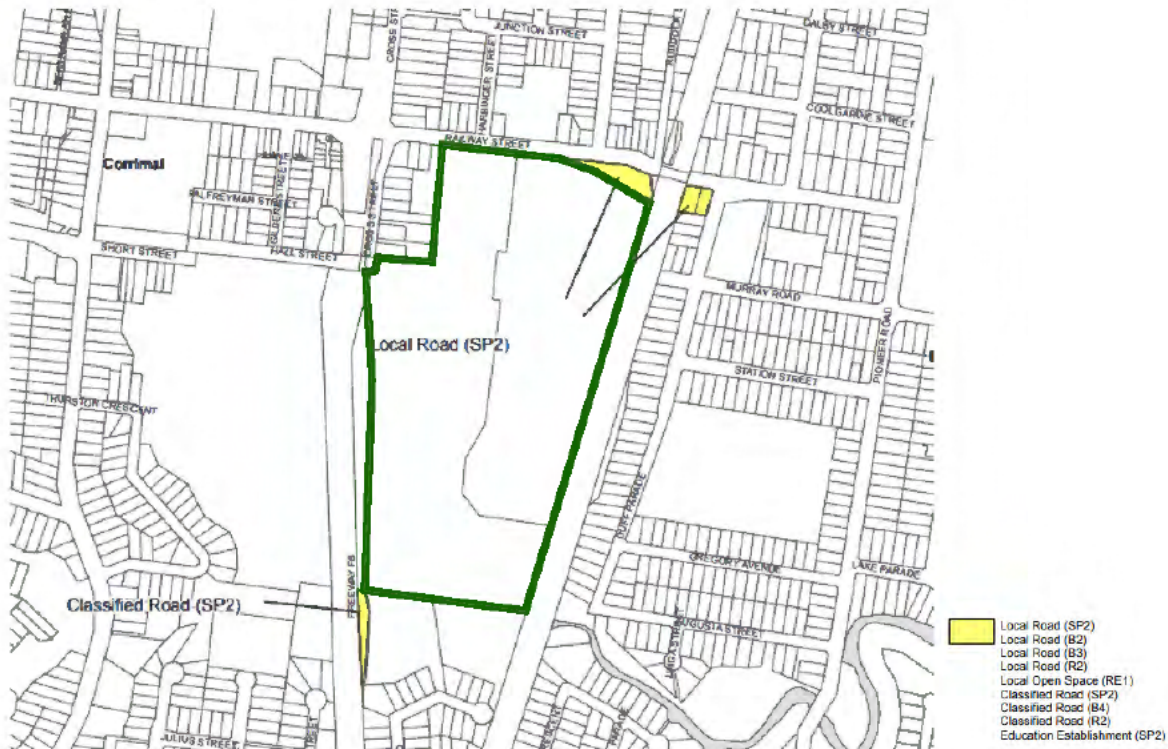
Authority of the State

Zone SP2 Infrastructure and marked "Local road"

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3. Development on land acquired by an authority of the State under the owner-initiated acquisition provisions may, before it is used for the purpose for which it is reserved, be carried out, with development consent, for any purpose.

Figure 10 Land Reservation Acquisition



Source: WLEP 2009 Land Reservation Acquisition Map LRA_024B

3.1.6 Heritage

Clause 5.10 of the WLEP 2009 sets controls for heritage items, heritage conservation areas, archaeological sites and Aboriginal objects or places. There is no heritage listed items of Commonwealth, State or local heritage significance within the site nor is the site located within a heritage conservation area.

On 29th June 2020 Council approved that part of the former Corrimal Coke Works site be listed as a local heritage item in the WLEP 2009. The WLEP Amendment has not yet been made.

3.1.7 Natural Resource Sensitivity

Clause 7.2 of the WLEP 2009 identifies controls for areas of natural resources sensitivity – biodiversity. As shown in **Figure 11** below, the site contains areas of natural resource sensitivity.

Extensive investigation into the biodiversity values of the site have been undertaken to ascertain the biodiversity values of the site. The areas of greatest significance do not reflect those provided in the LEP mapping.

The concept planning process has ensured the development is designed, sited and managed to avoid potential adverse environmental impacts and incorporates measures to minimise any impact.

Figure 11 Natural Resource Sensitivity – Biodiversity Map – WLEP 2009



Source: WLEP 2009 Natural Resource sensitivity – biodiversity Map NRB_010

3.1.8 Flood Planning

Clause 7.3 of WLEP 2009 is designed to ensure that:

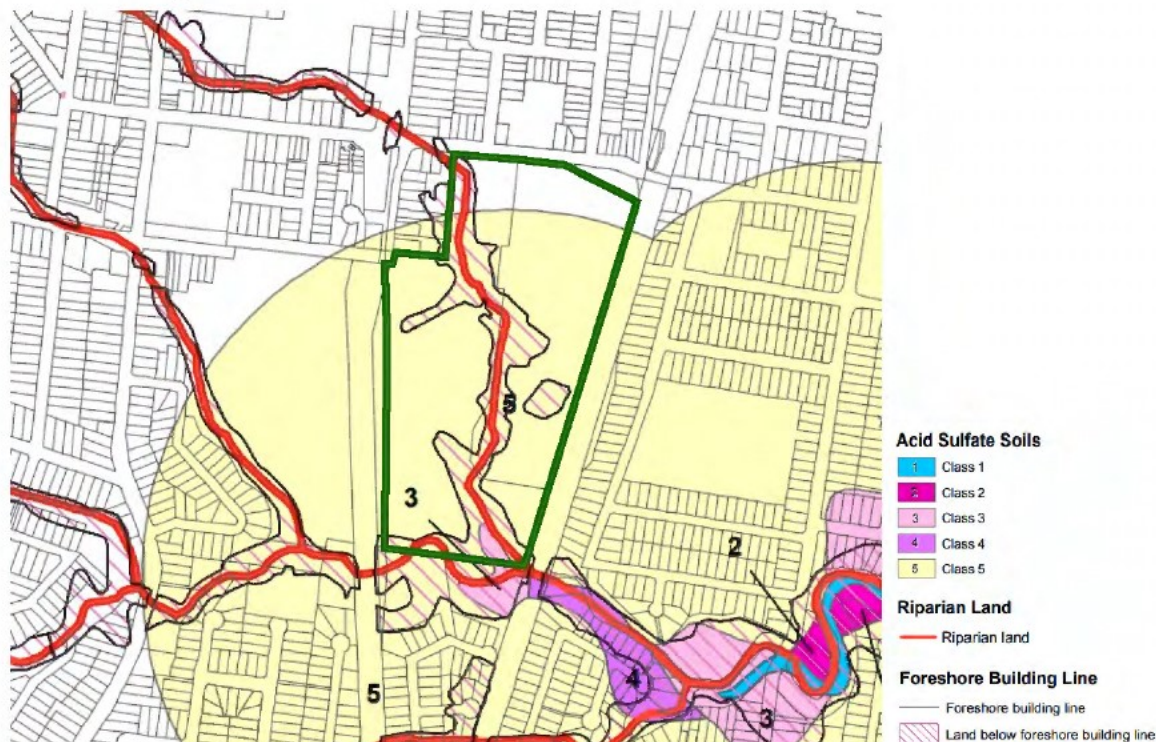
- to maintain the existing flood regime and flow conveyance capacity,
- to enable evacuation from land to which this clause applies,
- to avoid significant adverse impacts on flood behaviour,
- to avoid significant effects on the environment that would cause avoidable erosion, siltation, destruction of riparian vegetation or a reduction in the stability of river banks or watercourses,
- to limit uses to those compatible with flow conveyance function and flood hazard.

The subject site is identified on the accompanying Flood Planning Mapping as part of the WDCP 2009 Part E13 Towradgi Creek Flood Plain.

3.1.9 Acid Sulfate Soils

Clause 7.3 sets out controls for flood planning, Clause 7.4 identifies controls for Riparian Lands, while Clauses 7.5 and 7.7 identify controls for ASS and the Foreshore Building Line. The site contains Class 5 ASS and areas of riparian land and land below the foreshore building line, as shown in **Figure 12** below.

Figure 12 Acid Sulfate Soils, Riparian Land, Foreshore Building Line and Flood Planning Map – WLEP 2009



Source: WLEP 2009 Acid Sulfate Soils, Riparian Land and Foreshore Building line Map CL1_024

Clause 7.3 states that development consent must not be granted for land to which this clause applies unless the consent authority is satisfied in relation to all the following matters:

- all habitable floor levels of the development will be above the flood planning level,*
- the development will not adversely affect flood behaviour resulting in detrimental increases in the potential flood affectation of other development or properties,*
- the development will not significantly alter flow distributions and velocities to the detriment of other properties or the environment of the floodplain,*
- the development will not affect evacuation from the land,*
- the development will not significantly detrimentally affect the floodplain environment or cause avoidable erosion, siltation, destruction of riparian vegetation or a reduction in the stability of river banks or watercourses,*
- the development will not result in unsustainable social and economic costs to the community as a consequence of flooding,*
- if located in a floodway area—the development will not be incompatible with the flow conveyance function of, or increase a flood hazard in, the floodway area.*

Clause 7.4 aims to ensure that development does not adversely impact upon riparian lands. However, it should be noted that the existing riparian corridor is significantly degraded as a result of the previous industrial use of the land.

Clause 7.5 aims to ensure that development does not disturb, expose or drain ASS and cause environmental damage. The majority of the site is class 5 ASS. Under Clause 7.5 Class 5 ASS permits "works within 500 metres of adjacent Class 1, 2, 3 or 4 land that is below 5 metres Australian Height Datum and by which the water table is

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likely to be lowered below 1 metre Australian Height Datum on adjacent Class 1, 2, 3 or 4 land.” The ASS map also indicates a small section of the south-eastern corner of the site which is Class 3 ASS, however, this area of the site is not proposed to be developed.

Clause 7.7. provides foreshore building line controls. The clause aims to ensure that development in the foreshore area will not impact on natural foreshore processes or affect the significance and amenity of the area. However, as previously stated the riparian land and adjacent foreshore area is extremely degraded.

3.2 Wollongong Development Control Plan 2009

The site is also subject to the Wollongong Development Control Plan 2009 (WDCP 2009). The WDCP 2009 applies to the entire Wollongong LGA and includes general development guidelines. The main objective of the WDCP 2009 is to assist in the realisation of the aims of the WLEP 2009.

Details of the relevant provisions of the WDCP 2009 have not been undertaken as part of this PP due to the fact that a site specific DCP has been prepared for the Corrimal Coke Works and is submitted with this PP.

4 Proposed rezoning concept

4.1 Planning Proposal Description

The aim of the PP is to rezone the Corrimal Coke Works site to facilitate the development of housing, with local commercial services, public open space, and with a focus on delivering a public plaza adjoining Corrimal train station whilst retaining the sites industrial heritage.

4.2 Urban Design Concept

The Concept Master Plan, based on urban design principles, was undertaken to inform the PP. The Concept Master Plan provides for the delivery of housing within a strong public domain framework while ensuring that the site will be integrated into its existing context. Key features of the Concept Master Plan include:

- » Delivering a diversity of housing with the potential for low-scale apartment buildings, including affordable housing and the potential for seniors living/aged care, as well as options for strata titled townhouses.
- » Providing 9.5ha of open space to offer a range of community and recreational resources, including a 3,000sqm central park, 5,150 sqm southern recreation space and riparian corridor promoting walking and cycling.
- » Ensuring architectural diversity that responds to the local context through creation of a range of character precincts within the site.
- » Maintaining key views to the escarpment, including a significant view corridor along Murray Street, while also establishing new views within the development to key heritage features.
- » Maximising access to Corrimal train station and delivery of a public plaza adjacent to the station with retention of key heritage structures and also interpretive heritage elements.
- » Activating Corrimal train station and the heritage precinct with neighbourhood scale retail uses at a scale that does not detract from Corrimal Town Centre and East Corrimal shops.
- » Providing flexible community and business space within the heritage precinct, including the potential for tele-working and start-ups.
- » Realignment and rehabilitation of North Corrimal Creek to establish a new riparian corridor with enhanced biodiversity outcomes, while providing a large area of PMF-free contiguous developable area.
- » Providing a regional walking and cycling path through the site, connecting Railway Street in the north across Towradgi Creek to the south.
- » Retention of key ecological areas within the southern section of the site and integrated with the riparian corridor, including provision of a suitable buffer to the existing grey headed flying fox camp.
- » Providing a suitable buffer distance from the rail and state road corridors and appropriate landscaping adjacent to the site for noise attenuation.
- » New site access from Railway Street with a proposed roundabout at the intersection of Harbinger Street.

4.3 Place principles and values

The Master Plan for Corrimal has been based on the following place principles:

- » **Place Principle 1 Made for Friendship:** We help people to meet, share and connect, building on and contributing to Corrimal's already genuinely friendly and supportive community. We're creating a safe and

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supportive place where people grow together, look out for each other and share special moments and events.

- » **Place Principle 2 Designed for difference:** We're building a community of many different ages, shapes and sizes. We champion difference and are designed to attract people at different stages of life. Our place is made up of a wide variety of landscapes, precincts, features, housing types and experiences, creating a vibrant and distinctive destination.
- » **Place Principle 3 Bringing more to life:** We invest in making people and places the best they can be, breathing new life into heritage and green space and helping people reach their potential. We make everyday life easier, so that people have more time and energy to enjoy what's important.
- » **Place Principle 4 United through stories:** We cherish our unique story. From our history to our unique ecology, we're a place like no other. Together we will celebrate the stories of the past and build new stories, forging a strong sense of character and identity.

4.4 Concept Master Plan: The basis for the rezoning

The Master Plan is a progression of the 2017 Concept Plan which established the potential for the site to be rezoned from predominantly industrial uses to residential zoning. The 2017 Concept Plan established key design drivers including:

- » A generally regular street grid facilitating the creation of orderly development lots
- » A strategy to retain some former industrial elements of the Site including the 1912 Brick Chimney Stack.
- » The identification of ecological communities in the north and south of the Site.
- » The opportunity to position local services around Corrimal Station.
- » The opportunity to access the Site from Railway Street in the north.
- » The realignment of North Corrimal Creek to the west of the site to establish an enhanced green corridor, manage flooding and consolidate the developable area

In line with Wollongong Council's resolution and the Gateway Determination, it is proposed to rezone the site from IN3 Heavy Industrial and RE2 Private Recreation, under WLEP 2009, to R3 Medium Density Residential and RE1 Public Recreation. A small portion (0.25ha) of the subject site will retain its existing zoning as SP2 Infrastructure - Road. Appropriate controls relating to FSR, MLS, height, biodiversity and ASS are also proposed.

The rezoning of the site would accommodate approximately 700 – 750 dwellings, providing for housing diversity through the provision of a mix of residential apartments, strated titled townhouses, affordable housing and senior/aged care housing. It is also proposed to provide for small scale retail space in close proximity to Corrimal Station, and a village park nested within the residential area. The site will be connected to the wider Corrimal area through cycling and pedestrian links.

The riparian corridor, which is currently severely degraded, is proposed to be realigned to manage and reduce flood risks, offer a mix of passive and active recreational uses and support water quality management. The riparian corridor will link to the existing Towradgi Creek corridor and provide a shared cycle/pedestrian path from Railway Street to existing public open space at the south of the site. The southern ecological area is to be preserved, as well as the creation of a southern recreational park. The updated indicative Concept Master Plan for the site is shown in **Figure 13**.

A buffer of 100m has been included between the development and the grey-headed flying fox camp in the south of the site to avoid and minimise potential conflicts between future residents and flying foxes. The creation of a buffer is consistent with the level 2 camp management actions prescribed by DPI&E in the Camp Management Policy.

Figure 13 Proposed Master Plan



Source: DKO

Key features of the Master Plan include:

- » An expanded plaza adjacent to Corrimal Station.
- » Increased retention and interpretation of existing industrial structures, and establishment of key view corridors to heritage elements
- » Refinement of the realigned North Corrimal Creek corridor to provide opportunities for active and passive recreation while addressing long-term stability.

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- » A refined street network that facilitates bus access to Corrimal Station.
- » A new roundabout access on Railway Street at the intersection of Harbinger Street
- » An expanded area for local services around Corrimal station
- » A new, village neighbourhood park
- » A new, southern recreation park
- » The identification of a number of character precincts that respond to local conditions and the project vision
- » Further development of the built form strategy to allow for a diverse mix of innovative housing
- » An extensive network of active transport and pedestrian links to key destinations within and around the site
- » Updated staging and implementation planning

4.5 Key proposed land uses

4.5.1 Housing

Development of the site will provide approximately 700 – 750 new dwellings. This has been determined as an appropriate yield taking into account the opportunities and constraints of the site, the strategic hierarchy of Corrimal as a Major Centre, and through benchmarking a broad range of precedent infill sites.

The site offers ideal development potential for infill housing, adjacent to Corrimal train station and within walking distance of Corrimal town centre. The site is potentially the best location for new housing in the northern suburbs of Wollongong.

It is proposed to amend the land zoning on the site from IN3 and RE2 to R3 and RE1. The objectives of the R3 zone are as follows:

- » *To provide for the housing needs of the community within a medium density residential environment.*
- » *To provide a variety of housing types within a medium density residential environment.*
- » *To enable other land uses that provide facilities or services to meet the day to day needs of residents.*

The proposed R3 zoning has been selected, as:-

- » The site in essence has three “barriers” or edges:
 - > There is a large open space to the south of the site and separated by a creek, which does not impact any residents.
 - > Memorial Drive to the west is approximately 26 metres wide, and west of the site is further open space (Robert Ziems Oval), providing significant buffer to any dwellings in this direction.
 - > Dwellings in Cross Street will be separated from the site by the existing/future riparian corridor that will provide a significant green buffer.
 - > The railway line provides separation of approximately 40 metres to the nearest dwellings east of the site.
 - > North of the site is a changing environment, with some light industrial, new residential buildings and some dwellings. The proposed height and scale would not impact the amenity of the context to the north.

Further, it is a natural extension of the existing R3 zoned areas located directly to the north and east of the site. In addition, the R3 zoning will allow for the intended built form; being a mix of one, two and three-bedroom residential flat buildings, and even two storey apartments, affordable housing, senior/aged care housing as well as options for strata titled town houses. The rezoning of the site will increase housing choice and affordability within the area, supporting the current and future housing needs of the community.

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At present Medium Density Residential (R3) areas are significantly lacking within the Wollongong LGA and those that do exist are underperforming, in terms of supplying medium density outcomes. This has been recognised by Council in their recent Housing and Affordable Housing Strategy. A review of R3 Medium Density Residential zoned lands indicated these areas are operating not dissimilar to the R2 Low Density Residentially zoned land, and that there is not the capacity for the desired forms of medium density housing in these areas.

The data for both housing supply and affordable housing point to the need to increase the supply of smaller dwellings (1-2 bedrooms), as the Illawarra-Shoalhaven area has an increasing number of 1-2-person households due to an aging population. A more diverse housing mix is also required to respond to an increasing need for more affordable housing (as discussed below).

The proposed R3 zoning is considered appropriate for the site as it permits the following housing typologies under WLEP 2009:

Attached dwellings; Dual occupancies; Dwelling houses; Multi dwelling housing; Residential flat buildings; Shop top housing; Semi-detached dwellings; Seniors housing; Serviced apartments

4.5.2 Affordable Housing

Legacy and ICC are committed to ensuring that the site is rezoned and developed with the provision of 35 affordable rental dwellings, representing 5% of a forecast 700 dwellings. This affordable housing will be delivered in partnership with a registered Community Housing Provider. This is to ensure that a wide range of housing choice is provided, based on the belief that housing affordability is underpinned by providing a high level of housing diversity, including appropriate compact housing types and noting that the present housing stock of Corrimal is dominated by detached dwellings.

4.5.3 Open Space

52% of the site is proposed as open space, including the riparian corridor, the addition of two new parks and the heritage plaza area. This is more than originally proposed (43%), and provides a major asset and place making component on the site.

Open space is a key component of the site and provides connection and integration with the proposed development itself and with adjacent areas.

The open space and landscape have been integrated into design and derived from the following design principles:

- » Maintain the existing green buffer along Railway Street, and other site boundaries where possible
- » Create a new public parkland along the realigned North Corrimal Creek
- » Enhance proposed heritage public realm plaza adjacent to Corrimal station
- » Define a fine grain network of streets and pedestrian links
- » Frame internal views and vistas, while preserving key views from surrounding areas
- » Create additional parkland in the centre and to the south of the site
- » Create a system of cycle paths that link to key destinations and connect into wider network.

The addition of new parks is a central component of the proposal to provide an opportunity for local residents to informally gather and enjoy recreational facilities together. This open space will be zoned R3 so to enable the exact size and configuration to be resolved through the development application process, however a minimum area of 3,000sqm will be provided for local residents. To ensure that is implemented, the requirement for the village park and southern park will be included in the site specific DCP, as well as in Legacy/ICC VPA offer to Council.

The new riparian corridor, village park and southern recreation park reflect their value as a significant open space resources for the Corrimal community. They support the objectives of the RE1 zone namely:

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- » To enable land to be used for public open space or recreational purposes.
- » To provide a range of recreational settings and activities and compatible land uses.
- » To protect and enhance the natural environment for recreational purposes.
- » To cater for the development of a wide range of uses and facilities within open spaces for the benefit of the community.

To meet best practice in line with policies such as Greener Places Policy and Guideline and Green Grid, the design for the riparian corridor at Corrimal seeks to also integrate a range of functions:

- » Fulfilling its hydraulic function managing water quality and quantity without damage to property and the environment, especially in flood events
- » Enhancing the site and locality's ecosystem services, through a connected corridor of riparian native vegetation
- » Extending pedestrian/cycle access for residents of the site and the locality, through a creek corridor pathway, connecting into the wider district path network
- » Reducing Urban Heat Island Impacts by providing increased urban tree canopy
- » Offering opportunities for connection to nature for residents through recreational engagement with their local Creek
- » Encouraging water-oriented, nature-based play for children
- » Optimising passive surveillance by retaining views to the creek from the adjoining residential dwellings.

These functions have been carefully integrated to ensure that they are complementary, not exclusive of each other.

The proposed riparian corridor will be preserved and enhanced whilst also allowing it to be accessible for passive recreation by the residents of Corrimal. The proposed RE1 zoning will allow the permissibility of *"Environmental facilities; Environmental protection works; Recreation areas; Recreation facilities (outdoor)"*. The proposed RE1 zoning is in keeping with the zoning of surrounding areas, as shown in **Figure 14** below.

The intention of the corridor, village park and southern recreational park is to provide a range of recreational settings and compatible land uses while protecting and enhancing the natural environment for recreational purposes. Further:

- » The proposed RE1 zoning is consistent with upstream and downstream zoning of Towradgi Creek and there is no justification to change this approach.
- » The proposed riparian corridor area is currently zoned RE2 Private Recreation and is therefore already earmarked for recreational uses.
- » The proposed riparian corridor is not consistent with the objectives of either the E2 or E3 zones. In particular, the majority of the corridor is not of high ecological, scientific, cultural or aesthetic value. The portion of EEC that is within the riparian corridor precinct is being retained.
- » RE1 zoning is consistent with the Gateway Determination and there has been no new ecological information since this time to warrant a change to this approach

It is not proposed to include an Environmental Zone in the corridor as the space will:

- » Be owned and managed by Council and thus secured in public ownership
- » The biodiversity and vegetation will be managed and maintained by Council in the longer term
- » Other similar areas to the south and north with biodiversity have also been zoned RE1, and this will be consistent to form a prominent green linkage corridor.

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Figure 14 Surrounding RE1 land



Source: Elton Consulting

4.5.4 Proposed non-residential uses

It is proposed to accommodate a small neighbourhood precinct on the site to support activation of the rail station and heritage plaza, and meet the convenience needs of future residents. The inclusion of a local neighbourhood centre, with shops, restaurants and possible community facilities, was a key comment and vision that was an outcome of the community engagement process.

The precinct is expected to incorporate a small local supermarket, supported by some cafes and restaurants and limited convenience offer (such as newsagency or small pharmacy, personal services such as a hair salon, beautician etc.). The retail will be limited to a maximum GFA of 2,000m² within a 250m radius of Corrimal train station, to ensure that it does not detract for existing retail services in Corrimal Town Centre and East Corrimal. Any supermarket will be limited to 1,000sqm in size, while other shops are limited to 250sqm in size.

Subject to market demand, there is potential for a childcare centre to be included within this precinct.

It is also proposed to provide a shared community space and a flexible working space as part of the heritage precinct. These spaces are also intended to support local business start-ups, as well as providing opportunities for community groups and other local organisations. It is also proposed to include hairdressers, dry cleaners, internet access facilities or other business uses, to support local residents, and who may want to work close to the Corrimal Station. These type of uses were raised by communities during the community consultation. These uses are defined as business premises in the LEP.

These small-scale non-residential uses will be permitted through a local provisions' clause nominating additional permissible uses.

4.6 Land Use Character

The Master Plan for the rezoning area would result in a diverse range of building forms which respond to their individual location and context. The Master Plan also proposes a range of public realm and open space to provide for a range of needs for residents and visitors.

The Master Plan identifies different character areas to ensure the site respects heritage in appropriate locations and provides variety and interest in the design of buildings and the spaces.

The character area design controls have been included into the site-specific Development Control Plan.

4.7 Proposed road and public realm

4.7.1 Road networks

A new hierarchy of streets is proposed within the site to ensure easy and adequate access for residents and visitors. The key access point to the site will be from Railway Street, with the construction of a new roundabout at the intersection of Harbinger Street agreed following consultation with Council and TfNSW.

The traffic assessment has indicated that traffic from the proposed development can be accommodated by the existing network of streets and intersections, with the exception of the Memorial Drive/Railway Street intersection. An upgrade to this intersection is required to mitigate traffic impacts from the proposed development, consistent with the standards applied to all development through New South Wales.

The primary capacity issues with the existing intersection involve left turns from Railway Street onto Memorial Drive in the AM period, and right turns from Memorial Drive into Railway Street in the PM period. To address these issues and improve the capacity of the intersection, the upgrade is proposed to involve:

- » one of the two departure lanes on the eastern and western legs of Railway Street reassigned as a third approach lane (i.e. three approach lanes in each direction, one right turn pocket, one through lane and one left turn lane) in order to separate vehicle movements;

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- » extending the right turn pocket length in the southern approach along Memorial Drive from 95m to 150m to provide additional capacity and avoid queuing into the through traffic lanes; and
- » changing the signal phase times of the intersection to better flows and align with the geometrical changes.

Significantly, this upgrade will accommodate background traffic growth to 2026 as well as traffic generated by the development.

The detailed design of the intersection upgrade will be subject to further consultation and approval by TfNSW.

4.7.2 Pedestrian cycle network

Pedestrian and cycling connections are proposed throughout the site, linking Corrimal town centre to Corrimal train station, and further south through a shared cycle/pedestrian path from Railway Street across Towradgi Creek to the existing open spaces south of the site.

The existing footpath along Railway Street will also be upgraded to a shared path consistent with Council's Bike Plan.

4.7.3 Streetscapes

The concept Master Plan integrates a street hierarchy that meets vehicle, pedestrian and cycle circulation needs and enhances intuitive way-finding.

A palette of tree species has been selected to provide a clear identity for all streets and to maximise environmental and amenity benefits of tree canopy. The proposed species have been selected to draw on native species, many being locally endemic, and to respond to the WDCP 2009.

4.8 Proposed Built form controls

Based on the Concept Master Plan the following controls are proposed.

4.8.1 Minimum Lot Size

It is proposed that the R3 zoned area of the site be subject to a minimum lot size of 449 m². There is no proposed MLS for RE1 zoned areas.

Council previously endorsed a 149sqm minimum lot size as part of the Gateway process. The proposed minimum lot size is now greater than previously endorsed on that the basis that compact housing typologies, such as townhouses, will be delivered under strata title or through an integrated housing approval.

The proposed MLS is consistent with surrounding areas and will allow for the creation of development lots for future residential flat buildings.

4.8.2 Height of Buildings

The proposed building heights remain consistent with the height range endorsed through the Gateway Determination.

Following Gateway Determination, extensive engagement has occurred with Council officers to review building heights, massing and site levels to ensure that the scale and form of future development is appropriate. The distribution of building heights across the site has been developed through this process and is designed to:

- » Ensure that higher buildings of 5 and 6 storeys are located to have the least impact on existing residential areas around the site

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- » Avoid and minimise impacts on views from existing residential areas, particularly views to the Illawarra escarpment
- » Minimise building heights in proximity to key heritage elements on the site
- » Provide for some variation in building heights across the site

The proposed building heights have been benchmarked against a range of broadly comparable centres throughout Wollongong and Sydney to demonstrate that the range of heights are appropriate for the Corrimal Coke Works site. While 13m and 15m are established height zones in surrounding areas and Corrimal town centre, there are also precedents for greater heights in the northern suburbs including:

- » The Innovation Campus which provides building heights of 24m, 30m and 32m
- » 'Beaches Towradgi' which provides 6 residential storeys above a full level of above-ground parking

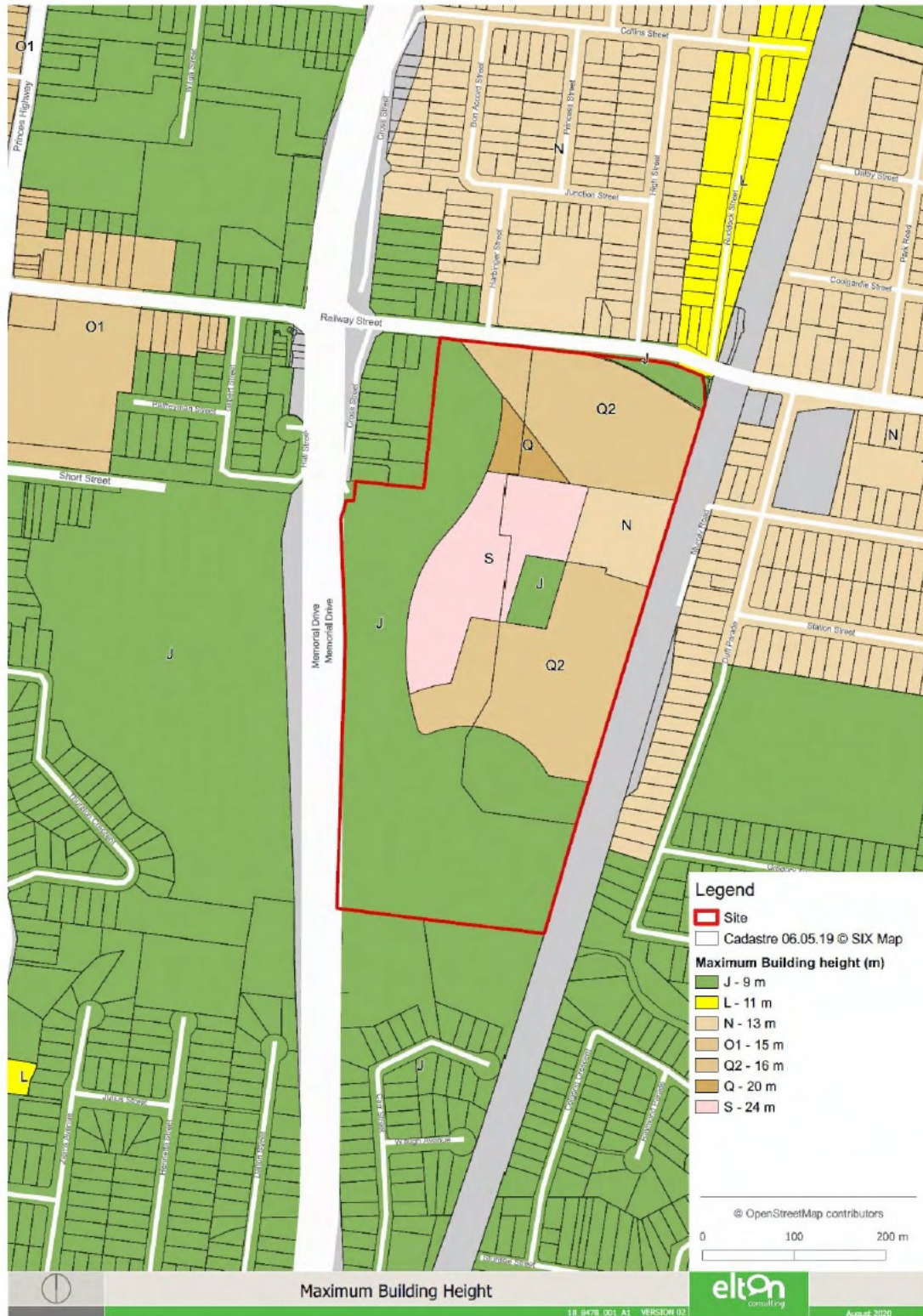
Both these locations do not benefit from the immediate adjacent of rail infrastructure that exists at Corrimal Coke Works. Further heights were assessed in areas such as West Dapto (vary between 11m- 30m) and in Warrawong (vary between 13m and 24m) as these centres do have a train station. These also demonstrated consistency of heights at stations.

The calculation of building heights also provides some minor flexibility to accommodate:

- » 'half in/half out' basement car parking (up to 1.2m above ground), and
- » new sustainable construction methods, such as engineered timber, that require increased floor to floor heights due to insulation requirements for BCA compliance.

Council requested Legacy to assess and consider two manners in which to define height at Corrimal. The first manner was that which is consistent with the definition in Clause 4.3 of WLEP. This proposed height is shown in the plan below and varies between 13m – 24m.

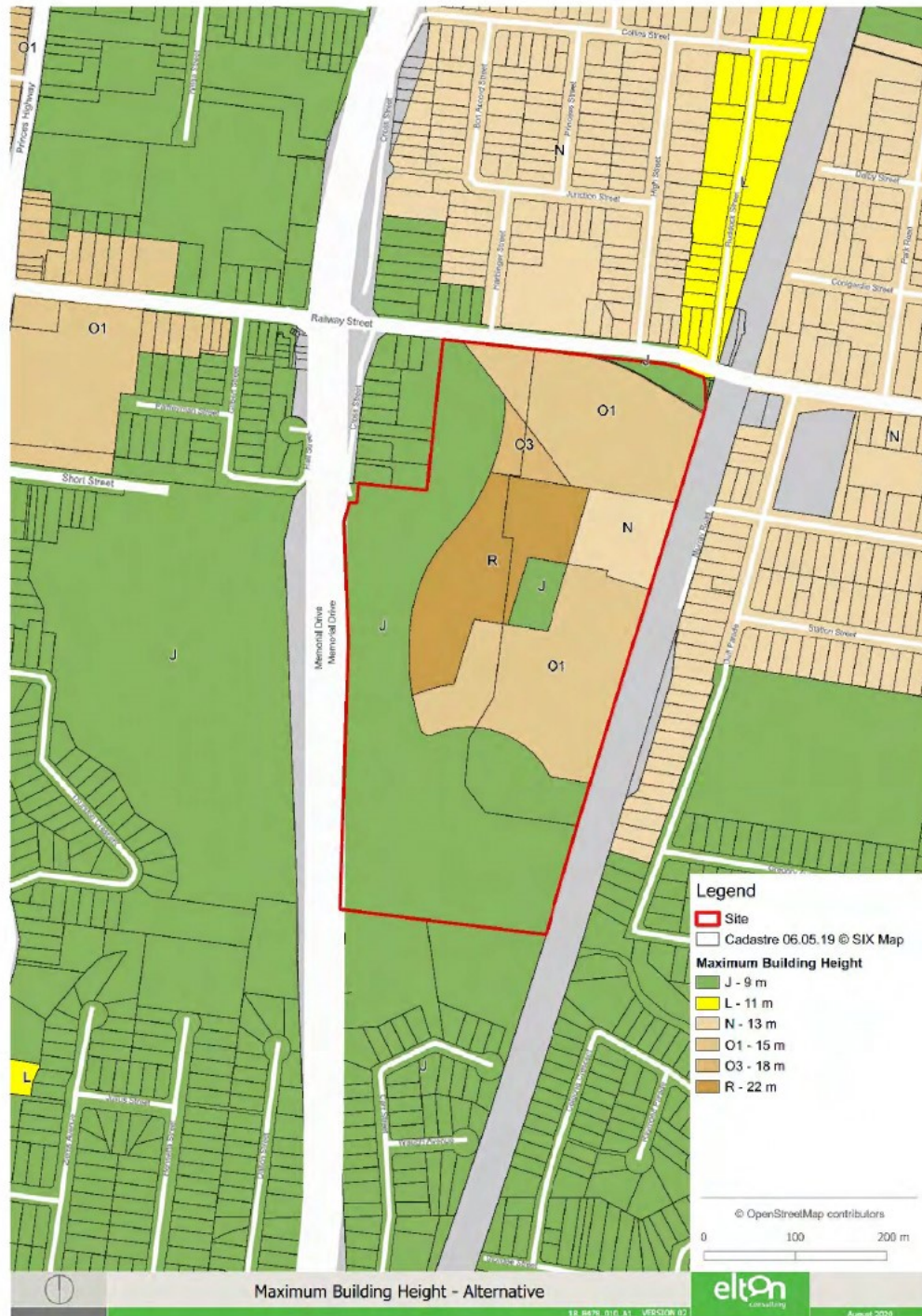
Figure 15 Height of buildings based on current definition



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The second manner to calculate height, is to exclude lift overruns, communal open space and other associated structures on roof tops. This proposed height is shown in the plan below and varies between 13m – 22m.

Figure 16 Proposed height based on the alternative height definition



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The building heights proposed in this PP for the site, therefore range from 13m to 22m. The proposed heights are:

- » 13m for proposed retail precinct adjacent to Corrimal train station, which serves to limit heights directly adjacent to key heritage elements
- » 15m on the northern side of the site, and that which is close to the 4-storey new building north of the site, as well as 15m adjacent to the railway line in the southern part of the site
- » 18 and 22m adjacent to the riparian corridor, towards the middle of the site.

The proposed height has also taken into consideration views and vistas of the brick chimney, as well as views of the escarpment from the east looking to the west.

The proposed heights within RE1 is 9m to allow for community facilities within open space areas.

The proposed building heights are therefore accompanied by a local provisions clause that adopts an alternative definition of building height to that of the standard LEP instrument. This clause is intended to provide greater certainty about the future scale of buildings by excluding elements such as lift-overruns, communal open space and pergolas from the definition of height.

Council have proposed clause 7.21 Height of Buildings at Corrimal Coke Works, as follows:

7.21 Height of buildings at Corrimal Coke Works site

1. *The consent authority may grant development consent to development for the purpose of rooftop plant, lift towers, lift motor rooms, pergolas, community facilities and or communal open space and access to and structures associated with such space, that would exceed or causes a building to exceed, the height limits set by clause 4.3, but only if the consent authority is satisfied that the structures -*
 - a) *are for the purposes of equipment servicing the building, plant rooms, lift towers, lift motor rooms, fire stairs and other areas used exclusively for mechanical services or ducting; or*
 - b) *are for the purpose of communal open space and access to, and structures associated with that space; or*
 - c) *for both (a) and (b); and*
 - d) *are not an advertising structure; and*
 - e) *does not include floor space area and is not reasonably capable of modification to include floor space area; and*
 - f) *will cause minimal overshadowing; and*
 - g) *any such rooftop structure referred to in (1)(a) and 1(b) is fully integrated into the design of the building."*

4.9 Floor space ratio

No FSR control currently exists for the site. The proposed floor space ratio (FSR) remain consistent with the FSR range endorsed by Council and in the Gateway Determination.

The proposed height and FSR controls reflect the concept master plan that has been developed based on further technical studies and in response to community consultation. The relationship between the proposed 22m height zone and 2.5:1 FSR is a common reconciliation of height and FSR outcomes, and simply reflects the master plan outcomes that have been demonstrated to Council

The FSR is appropriate for the site for the following reasons: -

- » The Corrimal town centre is located approximately 600-800m from the train station and therefore does not offer the same opportunity for transport-oriented development as the former Corrimal Coke Works site, which is located directly adjacent to Corrimal station. While Corrimal represents a context there are numerous precedents demonstrating that the density for the Corrimal Coke Works is appropriate and necessary to ensure proper utilisation of its unique opportunity.

Figure 17 Location of Corrimal Town Centre from the station



- » The Corrimal Coke Works site is within 800m of the town centre, plus directly adjacent to the town centre, which places it in a unique situation to have a floor space ratio density floor space ratio which is similar range to many housing diversity typologies in similar circumstances. The proposed FSR ranges also compare well to other scales and areas in Wollongong, but also other more regional or outer suburbs of Sydney, with an exert shown in the table below.

Comparable floor space ratio

Place	Current heights	Current FSRs	Applicability
Warrawong	24m	2.5:1	Lower hierarchy centre than Corrimal
Kiama	11m	2.1:1	Regional area
Edmondson Park	24m	2.5:1	Outer suburb of Sydney, with town centre to the south and a adjacent to a train station
Shell Harbour	15m	1.5:1	Regional Area

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Place	Current heights	Current FSRs	Applicability
West Dapto	20m	2.5:1	Wollongong LGA
Yagoona	20m	2:1	Outer suburb of Sydney
Putney, Ryde	18.5 and 21m	2.9:1	New development away from the station, close to town centre and adjacent to a major road and an infill site
Wyong	13-25m	Up to 2:1	Regional area

- » Corrimal is designated as an 'urban centre' within the Illawarra Shoalhaven Regional Plan. Benchmarking against broadly comparable 'local centres' within the Sydney Metropolitan Area identified a number of centres that illustrate generally comparable height and/or FSR outcomes located adjacent to train stations and a similar hierarchy of centre. This demonstrated evidence of the site's suitability for the FSR.
- » The highest FSR on the site is adjacent to the riparian corridor and away from the existing residential area. This there is no impact on the adjacent built form.
- » The FSR has been broken up across the site in order to achieve diversity in heights, scale and reflect the character areas of the site.
- » While the proposed FSRs controls range from 0.75:1 to 2.5:1, the resultant FSR across the entire site is approximately 0.5:1 demonstrating that the overall density is not significant.
- » The proposed FSR creates a resultant average FSR is 1.3:1, which is consistent with the broader floor space ratios in other benchmark studies as well as with Wollongong's city centre northern area FSR of 1.5:1.
- » The majority of the site has a proposed height similar FSR to Corrimal town centre or lower, being in the 1.5:1 FSR range or lower. As Corrimal is identified as one of the main urban centres in the Illawarra Shoalhaven Plan, and the site being directly adjacent to the train station highlights the suitability of the mix of FSRs across the site.
- » In terms of overall density, the site will achieve approximately 41 dwellings per hectare. This is consistent with the maximum density allowed in R3 zones in urban release areas across Sydney, demonstrating its general suitability. This also compares with other infill urban renewal sites such as:
 - Putney Hill (Ryde LGA) – 57 dwellings per hectare. This is an infill site in a suburban context with no immediate access to heavy rail.
 - Ashbury (Canterbury Bankstown LGA) – 119 dwellings per hectare. This is an infill site in a suburban context with no immediate access to heavy rail.
 - South Village, Kirrawee (Sutherland LGA) – 176 dwellings per hectare. This is a infill site in a generally suburban and industrial context, with close proximity to heavy rail, however Kirrawee has no designation within the strategic planning context.

The proposed FSRs for all R3 zoned land comprise the following; 0.75:1, 1.2:1, 1.5:1, 2:1 and 2.5:1. No FSR control is proposed for RE1 land.

4.10 Heritage

On 29 June 2020 the Council approved that part of the former Corrimal Coke Works site be listed as a local heritage item in the Wollongong Local Environmental Plan 2009. As such, this Planning Proposal does not address a local heritage listing of the site.

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Planning Proposal

This section of the PP provides justification and clarity in terms of the intended outcomes and the strategic merit of the proposal to enable the determining authority to approve the proposed amendment to the planning controls on the site, consistent with s3.33 of the EP&A Act.

In accordance with s3.33 of the EP&A Act and 'A Guide to Preparing Planning Proposal' (the guidelines), this section contains the basis of a PP for the WLEP 2009 amendment. It is comprised of:

- » Part 1 – objectives and intended outcomes
- » Part 2 – explanation of provisions
- » Part 3 – justification
- » Part 4 – mapping
- » Part 5 – community consultation

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Part 1 – Objectives or intended outcomes

The primary objective of this PP is to amend WLEP 2009 to facilitate the development of the site to deliver a residential development at the former Corrimal Coke Works site. The intended outcomes of the PP are:

- » create a residential site for approximately 700 - 750 dwellings for a mix of housing types
- » support local retail services adjacent to Corrimal train station to provide for activation of the train station and public domain, and local convenience retail services and facilities
- » retain and interpret key heritage elements of the site
- » deliver an open space network that provides a range of recreational opportunities
- » realign and rehabilitate the riparian corridor to manage flooding and enhance the biodiversity values on the site
- » support increase public transport use and active transport opportunities
- » provide improved connectivity through the site for pedestrians, cyclist and vehicles
- » manage traffic circulation from the development within the site and on the surrounding road network.

Part 2 – Explanation of Provisions

Part 2 of the PP is an explicit statement of how the objectives outlined in Part 1 are to be achieved through an LEP amendment.

The current IN3 Heavy Industrial and RE2 Private Recreation zoning on the site inhibits redevelopment. A rezoning of the site is necessary in order to create a high-quality residential development.

Viable development of the site can only be realised by amending WLEP 2009 to enable the type of development envisaged by the Master Plan.

4.11 Amendments to WLEP 2009 Mapping

The proposed outcomes will be achieved by an amendment to WLEP 2009 as follows:

- » **Amend the Land Zoning Map**
 - > Rezone the site from **RE2** Private Recreation and **IN3** Heavy Industrial to **R3** Medium Density Residential and **RE1** Public Recreation.
- » **Amend the Minimum Lot Size Map**
 - > On part of the land zoned **R3** specify a minimum lot size of 449 m²
 - > On all other land there will be no minimum lot size control
- » **Amend the Height of Buildings Map**
 - > On land zoned **R3** specify a height of 13m, 15m, 18m and 22m
 - > On land zoned **RE1** specify a height of 9m
- » **Amend the Floor Space Ratio Map**
 - > On land zoned **R3** specify FSRs of 0.75:1, 1.2:1, 1.5:1, 2:1 and 2.5:1
 - > On all other land there will be no FSR control
- » **Amend the Acid Sulfate Soils, Riparian Land, Foreshore Building Line Map, Flood Planning Map**
 - > Amend the location of riparian land to reflect the corridor realignment.
 - > Delete the foreshore building line.
 - > Delete land below foreshore building line.
- » **Amend the Natural Resource Sensitivity – Biodiversity Map**
 - > Remove the northern and add a southern area to the natural resource sensitivity – biodiversity of the site
- » **Amend the Key Sites Plan – Key Site 024**
 - > Include the site as a Key Site
- » **Urban Release Area 024**
 - > Create a Urban Release Area Map to include the site and thus Part 6 of the LEP is to apply

The objective and justification for the planning proposal amendments below, should be read in association with Section 4 above.

4.11.1 Land use zones

The proposed **R3** Medium Density Residential zoning facilitates a mix of higher density residential development on the site. This is consistent with the requirement for increased housing diversity within the Wollongong LGA and the vision for increased density housing surrounding train stations within the northern corridor of Wollongong (from Thirroul to Fairy Meadow).

It is envisaged that the development will include primarily residential flat buildings with potential for other forms of medium density housing, such as townhouses. The R3 zoned area of the site will also form an extension of the existing R3 zoned land to the north and the east.

A small-scale neighbourhood retail precinct is proposed adjacent to Corrimal train station, which will play an important role in activating the heritage precinct and associated public domain in this location.

The village park is intended to be located within the centre of the site. This park is proposed to be zoned R3 and will be included as a dedication in the VPA. In order to provide Council and the community with certainty that it will be delivered, the park will be included within the site specific DCP.

The **RE1** Public Recreation zoning is proposed for the passive and active open spaces provided within the development in order to ensure that there are designated areas for public recreation. The **RE1** land can be identified on a land acquisition map if necessary, to enable the land to be transferred to public ownership in the future.

4.11.2 Minimum lot size

The principle development standards for the MLS is proposed to be amended. At present the IN3 part of the site has a 1.99ha MLS. As part of the proposed amendments, part of the site zoned **R3** will have a 449m² MLS. The part of the site zoned **RE1** will have no MLS.

The designated MLS have been established to ensure that there is sufficient area to accommodate the proposed typologies of built development and to establish a character suitable to offer a diversity of housing choice.

4.11.3 Height of Buildings

The proposed instrument will amend the principle development standards for the HOB with an amended HOB map. At present the site has a HOB control of 9m over the part of the site zoned RE2 Private Recreation.

Building heights will range from 9m, over the part of the site zoned **RE1**, to heights of 13m - 22m over the part of the site zoned **R3**.

The proposed height zones across the site have been determined through an extensive master plan process and in close consultation with Council.

The proposed heights of the building are based on the alternative height definition as outlined in section 4.12 below. If Council, DPIE or NSW Parliamentary Council do not support this alternative definition to be inserted into the WLEP, the intention is that the heights would need to demonstrate the existing definition which is between 13-24m in height.

4.11.4 FSR

No FSR control currently exists for the site.

The proposed instrument will amend the principle development standards for the FSR with an amended FSR map. As part of the proposed amendments, the area to be zoned R3 will accommodate a range of FSRs from 0.75:1 – 2.5:1 in order to permit the diversity of development typologies and built form character envisaged for the site.

4.11.5 Acid Sulfate Soils, Riparian Land, Foreshore Building Line Map, Flood Planning

The proposed realignment of the riparian corridor will be reflected in the amended Acid Sulphate Soils (ASS), Riparian Land, Foreshore Building Line, and Flood Planning map.

The Foreshore Building Line and Land below the foreshore building land will also be deleted to reflect the fact that realignment will remove the probability of flooding on the site.

4.11.6 Natural Resource Sensitivity – Biodiversity Map

The Natural Resources Sensitivity Map will be amended to include one new area of natural resource sensitivity in the southern section of the site, in response to the biodiversity assessment undertaken by EcoLogical.

The current EEC in the northern area is required to be removed as a result of Council's request to incorporate site access via a new roundabout at the intersection of Harbinger Street be created.

4.11.7 Key Sites

The site is to be identified as a key site in a Key Sites Plan in order that design and built form excellence is demonstrated on the site, as per Clause 7.18 (2).

4.11.8 Urban Release Area

The site is to be identified in the Urban Release Area Map in order that Part 6 of the LEP is to apply, in particular that there should be a Development Control Plan (DCP) for the site prior to any development applications.

A site specific DCP is proposed for the site and is to be placed on exhibition and be considered by Wollongong Council simultaneous to the Planning Proposal, in order to provide further design controls related to the site.

4.12 Amendments to Part 7 Local Provisions – General

The following clauses is proposed to be included in Part 7 to allow increased heights, beyond the maximum height control of 22m, for C1 North Stack and C1 Brick Chimney Stack, which currently sit at heights of approximately 36.8m and 29m respectively:

7.20 Former Corrimal Coke Works

Height of Development

1) *The height of any development on the former Corrimal Coke Works site is not to exceed the height limit shown on the Height of Buildings Map, with the exception of the following heritage items:*

- a. C1 North Stack*
- b. C1 Brick Chimney Stack*
- c. C1 Fine Coal Bin*

2) *The following height limits are permissible for the heritage items:*

- a. C1 North Stack: 37m*
- b. C1 Brick Chimney Stack: 29m*
- c. C1 Fine Coal Bin: 25m*

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A second local provisions clause is proposed to be included in WLEP to manage the height of buildings on the site, but allowing rooftop plants, lift towers, motor rooms, pergolas, communal open space and associated structures above the maximum height proposed. The intention of this clause is to ensure that the maximum height of the building does not allow for additional floor area, but also to manage the height due to the necessary earthworks that is required to be undertaken on the site.

7.21 Height of buildings at Corrimal Coke Works site

2. *The consent authority may grant development consent to development for the purpose of rooftop plant, lift towers, lift motor rooms, pergola and communal open space and access to and structures associated with such space, that would exceed or causes a building to exceed, the height limits set by clause 4.3, but only if the consent authority is satisfied that the structures -*
 - a) *are for the purposes of equipment servicing the building, plant rooms, lift towers, lift motor rooms, fire stairs and other areas used exclusively for mechanical services or ducting; or*
 - b) *are for the purpose of communal open space and access to, and structures associated with that space; or*
 - c) *for both (a) and (b); and*
 - d) *are not an advertising structure; and*
 - e) *does not include floor space area and is not reasonably capable of modification to include floor space area; and*
 - f) *will cause minimal overshadowing; and*
 - g) *any such rooftop structure referred to in (1)(a) and 1(b) is fully integrated into the design of the building."*

4.13 Amendments to Schedule 1 Additional permitted uses

The following clause will be included in Schedule 1 Additional Permitted Uses to ensure that a full complement of neighbourhood and commuter services near Corrimal train station can be provided:

Use of certain land at the former Corrimal Coke Works site:

- 1) *This applies to land at the former Corrimal coke works site*
- 2) *Development for the purposes of food and drink Premises, shops, business premises and neighbourhood supermarket (1,000m²) is permitted with consent, but only if at ground floor and within 150m of the train station*
- 3) *Development for purposes of food and drink Premises, shops, business premises and and neighbourhood supermarket is permitted up to a maximum of a total GFA of 2,000m², with no one shop (except for the neighbourhood supermarket) to be more than 250m² in size.*

The above clause is proposed to accommodate a small supermarket on site, supported by some other small-scale retail shops as a limited convenience offer with a maximum GFA of 2,000m² and located within 200m of the railway station. In the future, some community uses may also be accommodated on the site.

As the site is close to the railway station and in proximity to Corrimal town centre, it is considered that the small commercial/retail neighbourhood uses should not be rezoned to a B1 Neighbourhood Centre zoning, but rather to retain the whole site as R3 to allow additional permissible uses to specifically apply to that part of the site which is within 200m of the railway station.

The intention of this clause is to provide a suitable amount of convenience retail and non-residential uses on the site while limiting the GFA to avoid competition and impacts on existing retail within Corrimal Town centre and East Corrimal.

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4.14 **Corrimal Coke Works Site Development Control Plan**

A Site-Specific DCP has been created for the Corrimal Coke Works site and is submitted with this PP.

4.15 **Voluntary Planning Agreement/Section 7.11 Contributions**

Discussions regarding a VPA are currently ongoing. A preliminary schedule of public benefits is provided at **Appendix D**. The proposed VPA will provide for offsets to local contributions due to the extent of work and public benefit being proposed.

Part 3 - Justification

Part 3 sets out the justification for the PP within its strategic planning context, considering the environmental, social and economic impacts of the proposal and the interests of the State and Commonwealth Governments.

In accordance with the guidelines, the level of justification is to be commensurate to the stage in the LEP making process. The potential impacts of the proposal have been identified in sufficient detail to confirm the suitability of the rezoning.

Section A – Need for the Planning Proposal

Q1 Is the Planning Proposal a result of any strategic study or report?

The Corrimal Town Centre Implementation Plan, which supports the Corrimal Town Centre Plan 2015-2025, includes Action 2.2, which relates to the former Corrimal Coke Works and states:

"Work with site owners to guide investigation of site renewal – Planning Proposal request to be prepared and submitted by owner and required to be strategically aligned with Town Centre Plan and other strategies."

The PP has been prepared as a result of the above action and the fact that the Corrimal Town Centre Plan 2015 – 2025 identifies the former Corrimal Coke Works as a 'key site'.

Q2 Is the Planning Proposal the best means of achieving the objectives or intended outcomes, or is there a better way?

In order to achieve the intended development outcomes and consistency with State Government strategic directions, the site requires rezoning. This is the only alternative to achieve the objectives of the strategic directions. The current controls allow for industrial development only.

Section B – Relationship to Strategic Planning Framework

Q3 Is the Planning Proposal consistent with the objectives and actions of the applicable regional, sub-regional or district plan or strategy (including any exhibited draft plans or strategies)?

This section of the report supplements the PP by addressing the provisions of the relevant regional and subregional plans and strategies.

Building Momentum: State Infrastructure Strategy 2018-2038 (2018)

Economic activity is growing around Wollongong, where the focus is on providing jobs and housing, growing the capacity of Port Kembla and driving greater economic diversity in priority sectors. Wollongong is recognised as a major city, key international gateway and significant economic activity centre.

By 2036 the Wollongong and Shellharbour area are likely to be home to more than half a million people. The area will drive the economic growth, employment and diversification of the broader Illawarra-Shoalhaven region, while also contributing to Greater Sydney's economy and labour force.

Wollongong will become increasingly connected to Greater Sydney by 2056, enabled by its proximity to Greater Sydney's jobs and services and improved road and rail connections. Improved connectivity between Wollongong and Port Kembla, the National Land Transport Network, the Western Sydney Airport and intermodal terminals in the Western Parkland City will also be important for the city's ongoing economic growth.

The NSW Government has committed to upgrading the Princes Highway at Albion Park Rail and the section between Berry to Bombaderry. TfNSW is now building an extension of the M1 Princes Motorway between Yallah and Oak Flats to bypass Albion Park Rail. The NSW Government is funding the \$630 million project. The bypass would complete the 'missing link' for a high standard road between Sydney and Bombaderry.

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This investment will improve access between the Shoalhaven and Wollongong and Sydney. Various other NSW Government and private investments will enhance Wollongong over the coming years, including an expansion of the Wollongong Hospital. Future investments should be focused on:

- » growing the amenity of Wollongong by providing good transport connections and local services
- » growing the capacity of the port at Port Kembla as an international trade gateway, enabled by dedicated rail connections
- » strengthening links between Wollongong, Port Kembla and Greater Sydney, with extra capacity for rail services and improved road connections across the Illawarra Escarpment and to the Western Parkland City
- » building on existing strengths and supporting economic diversity through growth in priority sectors including tourism, health, disability and aged care, ICT/knowledge services, education and training, and freight and logistics.

Future Transport Strategy 2056 (2018)

Future Transport strategy 2056 is a 40 year strategy supported by plans for regional NSW and for Greater Sydney. Future Transport 2056 outlines six state wide outcomes to guide investment, policy and reform and service provision. They provide a framework for planning and investment aimed at harnessing rapid change and innovation to support a modern, innovative transport network. These outcomes are as follows:

- » Customer focused: Customer experiences are seamless, interactive and personalised, supported by technology and data
- » Successful place: the liveability, amenity and economic success of communities and places are enhanced by transport
- » A strong economy: The transport system powers NSW's future \$1.3 trillion economy and enables economic activity across the state
- » Safety and performance: Every customer enjoys safe travel across a high performing, efficient network
- » Accessible services: Transport enables everyone to get the most out of life, wherever they live and whatever their age, ability or personal circumstances
- » Sustainable: The transport system is economically and environmentally sustainable, affordable for customers and supports emissions reductions

Planning and investment for Greater Sydney will focus around the three cities concept – the Western Parkland City, the Central River City and the Eastern Harbour City. Customers will be able to travel to one of these cities or to their nearest strategic centre within 30 minutes of where they live by public or active transport. This will give people better access to jobs, education and essential services.

An integrated network of corridors will support the efficient movement of people and goods throughout Greater Sydney. Future Transport 2056 is focused on three types of corridors that have been developed to align with the land use vision and to guide service levels (capacity, function and service frequencies) and infrastructure investment. The hierarchy of corridors in Greater Sydney include:

- » City-shaping corridors – major trunk road and public transport corridors providing higher speed and volume connections between our cities and centres that shape locational decisions of residents and businesses.
- » City-serving corridors – higher density corridors within 10km of metropolitan centres providing high frequency access to metropolitan cities/centres with more frequent stopping patterns.
- » Centre-serving corridors – local corridors that support buses, walking and cycling, to connect people with their nearest centre and transport interchange.

By 2056, economic and housing growth around Greater Sydney will drive integration across the city's hinterland, establishing areas such as Gosford and Wollongong as 'satellite cities'. Population and economic growth in these areas will require fast transit connections to Greater Sydney.

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In high demand areas including the satellite cities of Gosford and Wollongong, frequent, high capacity, city-shaping corridors will be provided to move the majority of people. These will be complemented by more flexible or ondemand services on city-serving and local corridors.

As part of the strategy, the following infrastructure upgrades are intended:

- » Regional NSW Initiatives for investigation (0-10 years):
 - > Sydney-Wollongong faster rail improvement
 - > Wollongong Rapid Bus Package
 - > Wollongong Place Plans
 - > Bus headstart for Wollongong

Future Transport 2056 - Regional NSW Services and Infrastructure Plan (2018)

The growth of Greater Sydney will directly influence the growth of surrounding regional cities resulting in the regional cities of Gosford and Wollongong becoming Satellite cities and a part of the Greater Sydney conurbation by 2056. The existing cities will evolve to strengthen critical linkages to jobs and services within Greater Sydney, due to their proximity and improved road and rail connections.

With its port and airport, Wollongong is considered to have future potential as an emerging Global Gateway. However there are constraints to its future growth due to the area's topography and proximity to Sydney, while over 80% of workers are employed locally.

In the next decade it is recommended that Faster Rail corridor infrastructure investment programs be focused on Satellite and Global Gateway cities to achieve significant travel time savings. For Wollongong and Gosford the aspiration is for a 60 minute journey time.

Investment in Faster Rail between Satellite cities (Wollongong, Gosford) through major investment in track straightening, signalling improvements to maximise the operational capabilities of the New Intercity Fleet and the Regional Rail Fleet Project.

This will be an improvement for Corrimall and the surrounding community.

Visitor Economy Industry Action Plan 2030 (2018)

The Visitor Economy Industry Action Plan 2030 sets targets for NSW to:

- » more than triple 2009 overnight visitor expenditure to 2030, aiming to achieve \$45 billion by 2025 and \$55 billion by 2030, and
- » achieve \$20 billion in regional overnight visitor expenditure by 2025 and \$25 billion by 2030, in alignment with the overall 2030 target.

To help drive growth in the Sydney and regional visitor economies, the Visitor Economy Industry Action Plan 2030 sets a direction for the NSW Government to invest in new ways to support the visitor economy through:

- » a new Team Tourism NSW Taskforce of industry leaders to improve visitor economy collaboration
- » increased funding and accountability for the Destination Networks
- » the development and release of a state-wide Destination Management Plan, nature based tourism strategy and tourism infrastructure strategy
- » a new industry portal, and
- » a new Visitor Economy Index designed to complement the expenditure targets by tracking industry conditions, perceptions and the future industry outlook over time.

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Future Transport 2056 – Tourism and Transport Plan (2018)

This plan provides a framework of customer outcomes for our visitors and initiatives to guide the work of the NSW Transport cluster over the next 10 years. These customer outcomes and initiatives are designed to harness emerging technology and service models as well as visitor trends. Customer outcomes and initiatives for investigation include:

- » Customer Outcome 1: Enhancing the visitor experience
 - > Improving the transport information available to visitors when they are planning their visit
 - > Improving access and travel for visitors, including people with disabilities, arriving and departing their NSW destination
 - > Creating places and experiences for visitors at their destination
 - > Providing opportunities for visitors to share their experience during and after their trip with WiFi access at key interchanges and on public transport and other ways to share their transport journeys on social media.
- » Customer Outcome 2: Greater access to more of NSW
 - > Improve global gateways
 - > Improving regional roads
 - > Tourist signposting
 - > Regional trains, coaches and air
 - > improving links to national parks, historic, artistic and cultural sites and events
- » Customer Outcome 3: Making transport the attraction
 - > Visitors consider riding a ferry or bicycle as some of the top activities to do while in NSW to experience the place they are travelling in. There are opportunities to use technology to create interactive experiences that highlight and provide information about destinations along transport routes.
- » Customer Outcome 4: A seamless experience
 - > Our collaboration and partnerships with transport service providers, tourism operators, across Government and with local councils all work to create a seamless experience for visitors.

Future Transport 2056 - NSW Freight and Ports Plan 2018 – 2023

The NSW Freight and Ports Strategy released in 2013 was the first long-term freight vision to be produced for NSW, which drove targeted investment in both metropolitan and regional transport networks. All freight transport operators who use our roads, our rail networks, our ports or our airports have felt the benefits the 2013 strategy has delivered. Projects such as WestConnex, the Cargo Movement Coordination Centre and upgrades to the Pacific, Newell and Great Western Highways are all fine legacies of that strategy. But with the NSW freight task set to grow by 28 per cent by 2036, a continued focus is required on the freight sector. The NSW Freight and Ports Plan 2018- 2023 is a call to action for government and industry to work together to make our freight system more efficient, more accessible, safer and more sustainable for the benefit of producers, operators, customers and communities across NSW. Over the next five years, we will:

- » Drive economic growth and deliver capacity enhancements
- » Increase efficiency, connectivity and access
- » Improve safety and sustainability

As a supporting plan to Future Transport 2056, this Plan is central to the NSW Government's long-term vision for transport in NSW. It aligns with the NSW Government's statewide land use and infrastructure plans – the NSW Regional Plans, Greater Sydney Commission District Plans and the State Infrastructure Strategy.

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The Plan includes over 70 initiatives to be delivered by 2023 ranging from infrastructure investment to trials of new technologies. These initiatives are focused on achieving five key objectives:

- » Objective 1: Economic growth
- » Objective 2: Efficiency, connectivity and access
- » Objective 3: Capacity
- » Objective 4: Safety
- » Objective 5: Sustainability

Illawarra – Shoalhaven Regional Plan 2036 (2015)

The Illawarra-Shoalhaven Regional Plan applies to the LGAs of Kiama, Shellharbour, Shoalhaven and Wollongong. By 2036, the population of the Illawarra-Shoalhaven is forecast to grow to 463,150, an increase of 60,400 from 2016. Population growth will result from natural increases as well as the sustained migration of young families and retirees.

The make-up of the population will change over the next 20 years. Growth will be moderate in most age groups, except in the 65-and-over group, particularly in Kiama and Shoalhaven. There will also be more one- and two-person households. Growth will necessitate at least 35,400 new homes.

This Regional Plan for the Illawarra-Shoalhaven provides the strategic policy, planning and decision-making framework to guide the region to sustainable growth over the next 20 years.

Within the Plan, Corrimal is marked as an urban centre, as shown in **Figure 17** below.

Figure 18 Corrimal Urban Centre



Source: Illawarra-Shoalhaven Regional Plan

The following key principles underline the planning framework for the plan:

- » identify and protect land with high environmental value and recognise cultural heritage values
- » support the sustainable use of land and water resources and build resilience to natural hazards and climate
- » support a strong, resilient and diversified economy that will enable the community to respond to environmental, economic and social challenges

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- » integrate transport and land use planning, and support improvements in active transport (walking and cycling), public transport and transport infrastructure (including freight)
- » take a balanced approach to housing that provides choice, affordability, and supports the orderly supply of land for development
- » increase housing density around centres that have access to jobs and transport and are already appealing to residents
- » encourage urban design that reduces car dependency, improves the public domain, promotes energy efficiency and supports healthier environments
- » improve infrastructure coordination

To achieve the vision for the Illawarra-Shoalhaven, a number of goals have been identified, as shown in **Table 8** below.

Table 8 Key Goals, Directions and Actions in the Illawarra-Shoalhaven Regional Plan 2015

Goals and Directions	Justification and response
Goal 1 – A prosperous Illawarra Shoalhaven	
DIRECTION 1.3 Grow regional strategic assets to support economic growth across the region	The subject site is not listed as being of strategic economic importance for continued industrial development in the plan. The Corrimal Centre is identified as an important Urban Centre servicing the needs of residents in the northern portion of the Wollongong LGA.
DIRECTION 1.4 Support new and expanded industrial activity by providing well-located and serviced supplies of industrial land	<p>There is a ready availability of better located heavy industrial land in the Wollongong LGA. In addition, the conversion of the land to light industrial is likely unfeasible due to remediation and other costs likely to exceed return on investment.</p> <p>The location of the site within a residential area also marks it as a less than ideal location to continue industrial activities due to land use conflict issues.</p>
Goal 2 – A variety of housing choices, with homes that meet needs and lifestyles	
DIRECTION 2.1 Provide sufficient housing supply to suit the changing demands of the region.	Within the Wollongong LGA, the projected housing need from 2016-2036 is 14,600 dwellings. The combined demands from tourism and the housing market, particularly in coastal towns, will require new housing developments. The changing demographics and market demand of the LGA will require a mix of housing.
DIRECTION 2.2 Support housing opportunities close to existing services, jobs and infrastructure in the region's centres.	<p>Ensuring additional housing development in existing urban areas is a sustainable option in terms of housing supply as it takes advantage of existing job markets infrastructure, commercial and retail opportunities, public transport and facilities for pedestrians and cyclists.</p> <p>Corrimal has been identified as a focus for increased housing activity within the Northern Corridor. An analysis of current planning controls in the region shows capacity for 24,100 new homes in existing urban areas, including townhouses and multi-unit dwellings.</p> <p>The Plan notes that additional residential development in locations such as Corrimal could also act as a catalyst to enhance their existing recreational and environmental features.</p> <p>The subject site presents an opportunity to provide a diversity of housing types adjacent to public transit, in close proximity to an existing urban centre. It offers strong recreational opportunities nearby and, in the Wollongong LGA, Northern Corridor generally.</p>

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Goals and Directions	Justification and response
GOAL 3 – A region with communities that are strong, healthy and well-connected	
<p>DIRECTION 3.2 Enhance community access to jobs, goods and services by improving connections between centres and growth areas.</p> <p>ACTION 3.2.2 Improve access to centres, particularly in the northern corridor, to encourage development</p>	<p>The proximity of the northern corridor (which encompasses Corrimal) to Sydney means that there is considerable scope to support commuters and also attract business to the region relocating from Sydney. Over 21% of the workforce in the northern corridor commutes to Sydney for work.</p> <p>In order to capitalise on improved public transport, new housing should be focused in and around centres in the rail corridor. The location of the site adjoining the Corrimal train station, linked with the Sydney and Wollongong CBDs, provides an excellent commuter location.</p> <p>The site also offers an opportunity to create local jobs in servicing the population such as childcare, seniors living and small-scale neighbourhood shops servicing local residents and commuters (all permissible under the proposed R3 zoning).</p> <p>A key objective in the concept plan is to make public transit more convenient and attractive for both residents of the development and the wider Corrimal community.</p>
<p>DIRECTION 3.2 Build socially inclusive, safe and healthy communities</p> <p>ACTION 3.3.2 Support Council-led revitalisation of centres</p>	<p>New neighbourhoods and centres should be designed to offer a high-quality lifestyle and to be environmentally sustainable, socially inclusive, easy to get to, healthy and safe. Opportunities should be taken to connect neighbourhood communities with the surrounding landscape.</p> <p>A key aim of the Corrimal redevelopment is to connect the site with the wider community through the establishment of a number of green links providing pedestrian and cycling paths.</p> <p>The Corrimal urban centre has been subject to investigation by Wollongong City Council through the Corrimal Town Centre Plan 2015-2025. The proposed development is in line with the vision for the revitalisation of Corrimal.</p>
<p>ACTION 3.4.1 Conserve heritage sites when preparing local planning controls</p>	<p>Council have recently approved the amendment to the WLEP to include the site in Schedule 5 as a local heritage item.</p>
GOAL 5 – A region that protects and enhances the natural environment	
<p>DIRECTION 5.1 Protect the region's environmental values by focusing development in locations with the capacity to absorb development</p> <p>ACTION 5.1.1 Avoid, minimise and mitigate the impact of development on significant environmental assets</p> <p>ACTION 5.1.4 Create a consistent approach to protect important riparian areas in planning and development controls</p>	<p>The regions 'high environmental value' land have been mapped. These include the Illawarra Escarpment. Due to the high value of this landscape, a key element of the proposed redevelopment of Corrimal has been to maintain view lines west towards the escarpment.</p> <p>The development will result in positive impacts for Corrimal by revitalisation a currently derelict industrial site to maximise public transport use and active transport opportunities.</p> <p>Portions of the subject site have been identified as being of high biodiversity value. Redevelopment of the site will ensure the management and maintenance of these assets, adding value to residents and the broader community alike. The majority of the biodiversity areas have been avoided in the development footprint.</p> <p>The proposal also includes re-alignment of the riparian corridor due to the fact it is currently significantly degraded and in light of the potential benefits realignment will have in terms of alleviating flooding on the site.</p>

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Goals and Directions	Justification and response
<p>DIRECTION 5.3 Improve the environmental outcomes for waste management and air quality</p> <p>ACTION 5.4.1 Protect sensitive estuaries and coastal lakes</p>	<p>The site presents an opportunity to support sustainable development given its public-transit access and proximity to the Corrimal town centre and recreational resources. The street network has been designed to deliver efficient waste management to work with the current regime in place in Wollongong.</p> <p>The establishment of a new, rehabilitated riparian corridor through the site will result in improved water quality outcomes for Towradgi Creek downstream from the site.</p>

Illawarra Regional Transport Plan (2014)

Population and employment in the Illawarra region are focused on the Wollongong metropolitan area and identified key urban centres, one of which is Corrimal.

Wollongong metropolitan area has a population of around 250,000 and is the focus for employment and residence in the Illawarra region, as well as for education and health care within the region. The following actions for Wollongong are identified within the Plan:

- » Action: Improve public and active transport access to Wollongong
 - > Deliver actions to increase the public transport share of commuter trips to and from Wollongong in peak hours to and from the CBD to 15% by 2016.
 - > Strengthen the role of rail in connecting local communities to Wollongong city centre.
 - > A network of bus services will provide local access with peak period frequencies supporting convenient access to work and education
 - > Local rail services integrated with high quality bus services, and supported by pedestrian and cycle networks
- » Action: Improve opportunities for walking and cycling
 - > Support the implementation of better facilities for walking and cycling, including the provision of cycle parking facilities at transport interchanges, centres, schools and hospitals.
- » Action: Invest in public transport infrastructure
 - > Identify opportunities to improve the infrastructure that supports public transport services, such as bus stops and shelters, terminal facilities and customer information.
- » Action: Deliver road upgrades
 - > Continue to improve the Princes Motorway (M1), Princes Highway (A1) and Mount Ousley Road to boost capacity, improve travel time, support public transport operations and provide efficient freight connections to Port Kembla

Q4 Is the Planning Proposal consistent with a Council's local strategy or other local strategic plan?

The PP is consistent with the aims and objectives of Council's local planning strategies, as discussed below.

Wollongong Local Strategic Planning Statement (LSPS) 2020

Council has adopted the Wollongong Local Strategic Planning Statement (LSPS) to provide a 20-year land use planning vision for the City. The LSPS states that the Wollongong LGA is expected to grow to a population of 257,450 by 2036 and 265,750 by 2041. This is an increase of 13,000 persons above the 2016 estimates contained in the Regional Plan for 2036. An additional 23,800 dwellings are therefore estimated to be required to house this population increase.

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By 2036 there will also be more people aged 70 years and above, with that age group increasing from 12.6% in 2016 to 14.8% by 2036, an increase in the number of couples without children, from 23.9% in 2016 to 27.6% in 2036 and a small increase in the proportion of people living alone, increasing from 26% in 2016 to 26.1% in 2036.

Future housing growth can be planned for by reviewing the controls in the City Centre, around Town Centres and transport nodes, including Corrimal, to increase housing density.

Discussion

Corrimal is identified by the LSPS as a major town and district centre and is the main shopping and commercial precinct for the northern suburbs, containing a range of retail, commercial and community services. As such, Corrimal is an ideal location for the provision of additional housing.

The Corrimal Coke Works site is arguably the best single opportunity to provide housing within Corrimal and the wider northern suburbs. The site is directly adjacent to Corrimal train station and provides easy access to the range of existing services in Corrimal. The site can play a major role in supporting diversity of housing choices, including small housing products, to cater for the aging population and changing community needs.

Draft Housing and Affordable Housing Options Paper (2020)

A Housing and Affordable Housing Options Paper was prepared to inform the development of a new Housing Strategy for the Wollongong LGA. The purpose of the paper was to present the key issues and planning priorities for housing, as well as options and key recommendations moving forward.

The Illawarra Shoalhaven Regional Plan 2016 estimates that by 2036 the Wollongong LGA will grow by 33,000 persons, who will require an additional 14,600 dwellings. The 2019 NSW Population Projections recently issued by the DPI&E indicate that Wollongong is now expected to grow by 45,700 persons by 2036 (257,450 population) who will require an additional 23,800 dwellings. The extra 12,700 persons and 9,200 dwellings is a significant increase from the 2016 data. However, the Housing and Affordable Housing Options Paper was prepared using the earlier projections.

On 17 July 2017 Council endorsed the 'Housing our Community discussion paper', the key findings of which were as follows:

- » Wollongong's population is growing
- » The proportion of older residents is increasing
- » Household types are changing and decreasing in size
- » The structure of dwellings has remained consistent and has not responded to the changing nature of households
- » Housing stress is increasing and is a major issue affecting a large proportion of the population; and
- » Employment challenges are affecting where people live

An increase in the number of new dwellings is required in order to house the projected increase in population. Projections for the type of housing have been developed using an analysis of demographics, lifecycles of neighbourhoods, and from reviewing how population and housing has evolved. Key findings include:

- » 1,000 – 1,200 additional dwellings are needed by 2021, at which time the annual dwelling need will reduce and stabilise between 800 – 1,000 until 2036
- » The types of dwellings needed will vary through time, however single dwelling houses are expected to dominate new housing stock
- » This additional housing will not be evenly distributed throughout the LGA. New dwellings in the form of apartments and higher density living will continue to be developed in Wollongong City Centre while new release areas will continue to be dominated by single dwelling houses and alternative types of low-density housing.

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Modelling undertaken by DPI&E under the 2017 planning controls indicated that the Wollongong LGA has widespread capacity to supply new housing and meet projected needs. This analysis excluded Urban Release Areas such as West Dapto. Key findings from the analysis include:

- » 44% of capacity was for semi-detached, medium density forms of housing (townhouses and villa development)
- » 45% of capacity was for higher density apartments (residential flat buildings)
- » 11% of capacity was for dual occupancy development
- » There is a mismatch between the zoning where capacity is available and the land use objectives of the zone as per the Wollongong LEP.

Although the West Dapto Urban Release Area and Wollongong City Centre are expected to absorb a large proportion of projected dwelling needs, there will be a shortfall of approximately 3,000 dwellings needed through to 2036.

Housing options

The data for both housing supply and affordable housing point to the need to increase the supply of smaller dwellings (1-2 bedrooms). The draft Housing and Affordable Housing Options Paper presents a number of options for the community and development industry to consider, based around the following three planning priorities for housing:

1. Increase housing stock diversity
 - Encourage the provision of more 1-2-bedroom dwellings
 - Review the location and planning controls for the R3 Medium Density Residential Zone to improve feasibility and take-up.
 - Change the dwelling mix in residential apartments, to increase the number of smaller dwellings from the current 10% requirement; this could include the introduction of incentive provisions, or the decoupling of car parking spaces from each unit.
 - Consider new smaller housing products, such as 'Fonzie Flats'.
2. Plan for future housing growth
 - Ongoing review and monitoring of housing supply, within greenfield development (eg West Dapto, Tallawarra), City Centre housing, town centres and around transport nodes
 - Review the planning controls in the City Centre, around Town Centres and transport nodes to increase housing density
 - Review the planning controls of constrained sites and locations
3. Increase supply of affordable rental housing stock
 - This is the key priority issue found in the options paper, and Council has already commenced a number of initiatives
 - An Affordable Housing Policy should be prepared, exhibited and adopted, and possibly an Affordable Rental Housing target be set
 - The inclusion in SEPP 70 Affordable Housing should continue to be pursued, through the preparation of an Affordable Contributions Housing Scheme, as the next step in the process, this may require an Affordable Housing Contribution to be paid
 - As part of residential up-zonings, an Affordable Housing Contribution will be expected, or a proportion of Affordable Rental dwellings to be provided on-site
 - Council work with NSW Land and Housing Corporation and Community Housing Providers to increase the supply of Affordable Rental Housing.

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Medium Density Housing

The WLEP 2009 defines 13 precincts for medium density residential development. These areas are located in Helensburgh, Thirroul, Bulli, Woonona, Corrimall, Fairy Meadow, Coniston, Unanderra, Kembla Grange, Dapto, Huntley, Avondale and Warrawong. These can also be termed R3 Medium Density Residential zoned land. Medium density residential housing typically includes townhouses, villas, terraces and smaller apartment buildings.

A land use survey of Wollongong's established medium density zoned lands was undertaken in 2014. This determined how well these zones were operating and issues affecting development. Key findings were:

- » Wollongong LGA had approximately 1,800 lots zoned for medium density development, representing 2.8% of the total of 64,000 residentially zoned lots throughout the Wollongong LGA;
- » 78% of housing in these medium density zones was single dwelling houses, which is not a desired medium density housing outcome
- » Other types of housing (dual occupancies, townhouses, villas) made up less than 5% each of the total housing stock
- » Although increased heights and permitted floor areas are generally permitted, 77% of existing development was single storey;
- » Overall, the established areas were far from full and their operation was similar to that of surrounding low density zoned land.

A review of DAs approved from 2010 – 2018 for medium density zoned land indicates that these areas are still attracting a mix of low-density housing types. Approvals for new development indicate:

- » 54% were for single dwelling houses, dual occupancies, or alterations and additions and an existing dwelling;
- » 14% were for multi dwelling housing (townhouses, villas)
- » 32% were for other approval types (strata subdivisions etc)

The review of R3 Medium Density Residential zoned lands indicated these areas are operating not dissimilar to the R2 Low Density Residentially zoned land, and that there is not the capacity for the desired forms of medium density housing in these areas.

Urban Feasibility modelling undertaken by the DPI&E indicated that there was minimal feasible capacity for housing in the form of townhouses in existing medium density residential zoned land. Evidence from Wollongong's development industry suggests Council's planning controls for such housing needs to change to facilitate diversity in housing. The Greater Illawarra: The Smart Growth Agenda (2015) suggests changes to planning controls that will facilitate housing that achieves the medium density residential zone objectives.

Preliminary review of land and environmental constraints throughout Wollongong LGA indicates there is opportunity to expand medium density zoned land. This may include expansion of existing medium zoned precincts or the creation of new medium density zoned lands where there is currently low-density zoned land. The Wollongong Residential Density Study (2014) nominated some areas suitable for additional medium density housing. The Illawarra – Shoalhaven Urban Development Program Update 2016 also identifies centres 'as having capacity for additional dwellings through higher densities and appropriate zonings. This includes land in Woonona, **Corrimall**, Fairy Meadow, Wollongong, Figtree, Unanderra, Warrawong and Dapto.

There are three options available to address the shortfall in R3 housing. These are discussed in detail in the draft paper. In particular the Paper indicates:

Option 1 – No change

R3 Medium Density Residential zoned land can continue to operate under current planning controls and conditions. This will likely see dwelling houses and dual occupancy housing being developed inconsistent with the zoning objectives.

Option 2 – Adjust controls

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Review planning controls in existing areas zoned Medium Density Residential with the intent to increase residential density. A second approach, which may be considered, is to review and refine planning controls in existing Medium Density Residential zoned land to target density and growth. This may include review of permissibility of single dwelling houses in Medium Density Residential zoned land, or FSRs, heights and lot widths for all permissible residential development. The primary reason is that the analysis showed that single dwelling houses are still being constructed in this zone. The land use table could be amended to remove new dwelling houses as being permitted, which will better encourage land to be redeveloped into two or more dwellings.

Option 3 – Expand R3 Medium Density zones

Planning for urban growth in medium density residential areas and in and around town and village centres: this approach would include a review and refinement of planning controls in existing Medium Density Residential zoned land, as well as business zones (town or village centres) in the vicinity, and adjacent areas transitioning into surrounding low density residential zone.

This option is called planning for growth or defining urban growth areas. Such review could result in changes to height limits, FSR, minimum lot widths and/or permissibility of development types, to facilitate a density consistent with zoning objectives. It may also look to expand to Medium Density zoned land.

Council specifies that the preferred approach is a combination of Options 1 and 2.

Review of R3 Medium Density residential precincts

There are 1,811 lots located within ten precincts throughout the City that are zoned R3 Medium Density Residential under WLEP 2009. During 2013-14 a review of the built form within the R3 Medium Density Residential precincts occurred. The purpose of the review was to examine the take up of the Medium Density precincts and determine the remaining capacity for additional housing within those precincts.

The review found that there has been limited development in the precincts, with some 78% of lots being single dwelling houses, 2% dual occupancies, 6% town houses, 4% villas and 4% residential flat buildings. This is due to medium density development not being financially feasible, bank lending policies, the planning control settings (eg FSR or lot width controls) not encouraging development, the value of improvements in many single dwellings, and existing owners not willing to sell for the amount offered.

Review of land and environmental constraints throughout Wollongong indicate that there is opportunity to expand medium density zoned land. This may include expansion of existing medium zoned precincts or the creation of new medium density zoned lands where there is currently low density zoned land.

Discussion

The draft Housing and Affordable Housing Options Paper identifies three planning priorities for housing as follows:

- » Increase housing stock diversity
- » Plan for future housing growth
- » Increase supply of affordable rental housing

The draft Housing and Affordable Housing Options Paper documents forecast housing supply and demand. The data suggests that Council's existing planning controls and strategies will cater for the projected demand and that there is sufficient feasible supply capacity to meet the projections forecasted in 2016. The housing supply includes the West Dapto, Calderwood and Tallawarra urban release areas, development in the Wollongong City Centre and other centres, and infill development in the suburbs.

However, the analysis indicates that a greater supply of smaller dwellings (1-2 bedrooms) is required to meet the increasing demand, as a consequence of the ageing population, lone person households and for very, low, low and moderate income households. The draft Housing and Affordable Housing Options Paper considers the housing needs of various sectors of the community and Council's role. It also outlines and justifies the need for more housing diversity through the provision of smaller lots.

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Corrimal is predicted to require 563 single dwelling houses, 130 terrace, townhouse, villas, 1-2 storey flats and apartments and 93 flats and apartments, 3 storeys and larger, by 2036 and can therefore assist in meeting the LGAs requirements for smaller housing typologies.

Draft Sustainable Wollongong 2030: A Climate Healthy City Strategy (2020)

The vision for sustainable Wollongong is *"together we're creating a healthy and sustainable future for all. Wollongong is a thriving low waste, low emissions city which is resilient, liveable and has high biodiversity value."*

Creating a Sustainable Wollongong means:

- » protecting our ecosystems,
- » reducing greenhouse gas emissions,
- » reducing natural resource consumption,
- » becoming resilient to the impacts of climate change,
- » increasing active transport and walkability of our city,
- » designing and constructing buildings to minimise their environmental impact,
- » connecting with and appreciating our natural environment.

In response to the Paris Agreement, the Australian Government has set an emissions reduction target of 26-28% of 2005 emissions by 2030.

The goals of the Sustainable Wollongong Strategy are as follows:

- » *Environmental and climate leadership underpins Council decision-making and service delivery, and inspire the same in others*
- » *Protect our environment, reduce emissions and increase resilience to climate change*
- » *Achieve net zero emissions by 2030 for Council operations, and together we will achieve net zero emissions by 2050 for the city.*
- » *Our ecosystems and waterways are enhanced, our urban areas are cooler and greener, and our community is connected to our environment*
- » *Our people only take what they need, reuse and recycle what they can, and are aware of the resources that they consume*
- » *Our infrastructure and community can adapt to the impacts of a changing climate and water is valued as a vital natural resource*

Discussion

The development of Corrimal site will include sustainability measures, by: -

- » including and increasing pedestrian paths and cycle paths,
- » significantly enhancing the riparian corridor
- » including sustainable building construction
- » increasing opportunities for active transport through the site and to the surrounding area. This will reduce the use of vehicles, thereby assisting in the reduction of emissions.

The future development also aims to incorporate sustainable design.

Draft Wollongong Cycling Strategy 2030 (2020)

Since 2013 Council have expanded and upgraded 27km of the cycling and shared path network. This has improved the quality and safety of the cycling routes while also increasing the distances available to the community for cycling.

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The draft Wollongong Cycling Strategy 2030 works toward a 10-year vision where cycling is a preferred option for transport. By 2030 Council seeks to:

- » *Provide an additional 50km of on-road cycling routes and 35km of off-road cycling routes, through partnering with State and Federal partners. This will expand the present shared path and cycling network from approximately 130km to a total of nearly 215km*
- » *Work with the State Government to fund and deliver education programs and significant cycleways alongside regional and state roads, including the Princes Highway, Crown Street and Lawrence Hargrave Drive*
- » *Connect residential areas to the City Centre and towns and villages, by delivering missing cycling links*
- » *Work with partners to develop positive community perceptions of bike riding and enable the development of a local cycling culture that encourages bike riding alongside other forms of sustainable transport*
- » *Develop shared infrastructure that is inclusive and allows for safe access by those who are unable to cycle*
- » *Support tourism, business and innovation through Council's planning and support mechanisms to seek opportunities to support and enable cycling events and community engagement in the LGA.*

This Strategy describes the priority actions needed to fulfil Council's commitment to encouraging and enabling cycling as a sustainable transport option for short and medium-length trips throughout the city. In addition, a greater uptake of cycling will support meeting Council's adopted emissions reduction targets.

By 2030 Council intends to:

- » Increase weekly cycling participation, from 12.9% to 20%
- » Increase cycling journey to work trips, from 0.7% to 2%
- » Increase the number of transport cycling trips, from 20.6% to 25%
- » Reduce the number of cycling related crashes reported from 156 to 70 per 5-year period
- » Increase weekly female participation, from 10.9% to 15%.

The strategy will guide Council in achieving its visionary targets for increasing participation across Wollongong.

The following priority areas form the basis for the Strategy:

- » *Safe: invest in safe cycling infrastructure and educate our community on safe behaviour.*
- » *Convenient: prioritise infrastructure that supports convenient cycling as a sustainable and well-integrated mode of transport.*
- » *Planned: actively plan and implement strategies to improve cycling in Wollongong to facilitate increased participation in the future.*
- » *Business, tourism and events: promote and support cycling in Wollongong as a fun, attractive and sustainable way of life.*
- » *Innovation: actively engage and support in innovative solutions that inform and encourage greater participation in riding.*

Discussion

The redevelopment of the Corrimal Coke works site will include an internal cycling network, providing cross connections and links through the site to the surrounding area. The site will promote cycling from the site to the station and to the Corrimal town centre, assisting with Council's vision for cycling being a preferred transport option by 2030.

City of Wollongong Pedestrian Plan 2017 - 2021

Over the past 20 years there has been a steady decline in walking as the only method used in a commute trip (around a 20%). As many public transport journeys are usually accompanied by significant walks at either or both ends, data related to public transport use is also an important indicator of how much walking occurs in an area.

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The latest available travel data shows about a 30% drop in train travel and close to a 40% reduction in bus travel between 1991 and 2011.

The vision of the strategy is as follows:

- » Walking is the preferred means of transport for short trips in the city and adjoins a public transport trip for longer journeys.
- » The walking network is accessible to all and is a safe, quick and pleasurable way to move to and through centres and other popular destinations in the city.
- » High quality walking information including wayfinding and trip planning is readily available to the travelling public.
- » The wide-ranging benefits of walking are recognised and valued in the community.

The Plan sets out a range of strategies to address key walking issues facing the city, and facilitate Council's contribution toward its vision for walking and the following community goal established in the Wollongong 2022 Community Strategic Plan. Specific goals of the strategy are as follows:

- » Goal 1 - Encourage Walking
- » Goal 2 - Create Pedestrian Friendly Places
- » Goal 3 - Make Walking Safe
- » Goal 4 - Make Walking Easy and Convenient
- » Goal 5 - Work Effectively to Implement the Pedestrian Plan

The Plan identifies key activity centres including Corrimal. One of the aims is to investigate opportunities for 'continuous footpath' treatments and pedestrian permeability in town centres such as Corrimal.

Discussion

The proposed vision for Corrimal is to create a community base don active transport. Pedestrian and cycle links will form an integral part of the development, allowing connectivity both through the site and to the wider Corrimal area. Increasing the opportunities for walking and cycling will reduce reliance on motor vehicles and consequently less traffic congestion within the area and subsequent air pollution. The physical activity would, meanwhile, improve public health.

Wollongong 2028 Community Strategic Plan (2018)

The Wollongong 2028 Community Strategic Plan outlines the Wollongong community's priorities and aspirations, providing directions for the provision of key projects and services. Relevant directions are included in **Table 9** with an outline of how the PP for the site responds to these directions.

Table 9 Relevant Strategic Plan directions and justifications and response

Strategic Plan Directions	Justifications and response
Goal 1: We value and protect our environment	
Objective 1.1 Our natural environment, waterways and terrestrial areas are protected, managed and improved	
1.1.1 The community is actively involved in the expansion and improvement of our green corridors and other natural areas connecting the escarpment to the sea.	The proposed redevelopment of the site will include pedestrian and cycling connections providing improved access to Corrimal train station and facilitating a regional connection from Railway Street across Towradgi Creek to the south.
1.1.2 Manage and effectively improve the cleanliness, health and biodiversity of creeks, lakes, waterways and oceans.	The riparian corridor on the site is proposed to be realigned and rehabilitated. At present the corridor is severely degraded

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Strategic Plan Directions	Justifications and response
<p>1.1.3 The potential impacts of natural disasters, such as those related to bushfires, flood and landslips are managed and risks reduced to protect life, property and the environment.</p>	<p>and the restoration of the corridor will result in improved biodiversity outcomes while also managing flooding.</p> <p>Realignment of the riparian corridor would significantly reduce the potential of flooding on the site and improve drainage outcomes for adjacent neighbours.</p> <p>The site is not bushfire prone land.</p>
<p>Objective 1.2 We practice sustainable living and reduce our ecological footprint</p>	
<p>1.2.1 Reduce our ecological footprint, working together to mitigate the impacts of climate change and reduce waste going to landfill.</p>	<p>The development footprint of the PP has been designed so as to avoid the majority of areas considered to have high biodiversity values.</p>
<p>Objective 1.3 The sustainability of our urban environment is improved</p>	
<p>1.3.1 Manage land uses to strengthen urban areas and improve connectivity close to train stations and key transport routes.</p>	<p>The proposed development incorporates pedestrian and cycling links to improve connectivity between surrounding areas and Corrimal train station, and through the site.</p> <p>The development will result in this large site being connected into to the broader Corrimal area.</p>
<p>Objective 1.4 We recognise and celebrate our heritage</p>	
<p>1.4.1 Programs and projects that achieve proactive heritage management, education and promotion are developed and implemented.</p>	<p>Council have recently approved the amendment to the WLEP to include the site in Schedule 5 as a local heritage item.</p>
<p>Goal 5 We have a healthy community in a liveable city</p>	
<p>Objective 5.1 There is an increase in the health and well-being of our community</p>	
<p>5.1.4 Urban areas are created to provide a healthy and safe living environment for our community.</p>	<p>The development will provide a network of new open spaces as well as walking and cycling connections to facilitate community interaction and promote active transport opportunities.</p>
<p>Objective 5.2 Participation in recreational and lifestyle activities is increased</p>	
<p>5.2.1 Provide a variety of quality public spaces and opportunities for sport, leisure, recreation, learning and cultural activities in the community.</p>	<p>The proposed development incorporates a large riparian corridor and several public recreation spaces including an amphitheatre as well as green links through the site, enabling active and passive recreation. Interpretation/learning installations will be integrated referencing the site's history and ecology.</p> <p>The site does not warrant provision of sporting facilities, however Robert Ziems Oval and Corrimal swimming pool are located in dose proximity to the west of the site.</p>
<p>Objective 5.3 Residents have improved access to a range of affordable housing options</p>	
<p>5.3.1 Housing choice in the Wollongong Local Government Area is improved, taking into account population growth, community needs and affordability.</p>	<p>Development of the site would provide an opportunity to increase the amount and variety of housing in the LGA from townhouses to residential flat buildings. The LGA is currently in need of higher density housing to accommodate for the</p>

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Strategic Plan Directions

Justifications and response

increasing number of 1 -2 person households. The diversity of housing proposed will also increase affordability options.

Goal 6 We have sustainable, accessible and affordable transport

Objective 6.1 Wollongong is supported by an integrated transport system

6.1.2 Work with partners to decrease car dependency and facilitate sustainable transport to provide convenient movement throughout the city, with sustainable transport modes such as walking and cycling.

The proposed development has been centred around decreasing car usage of future residents and increasing the patronage of train services from Corrimal station, as well as promoting active transport through pedestrian and cycling connections.

6.1.4 Integrated communities close to public transport and local services and facilities focused around existing train stations and town and village centres are planned for and encouraged.

The site is located adjacent to the Corrimal train station and 350m from the Corrimal Town Centre representing an opportunity to develop a diverse integrated community close to a major transport link and significant urban centre.

Places for People: Wollongong Social Infrastructure Planning Framework 2018 – 2028 (2018)

Places for People, Wollongong's Social Infrastructure Planning Framework is a blueprint for long term strategic planning and management of Council's social infrastructure. It is the vision of Wollongong Council that all residents, workers and visitors will have access to quality, sustainable social infrastructure that meets their needs and reflects Wollongong's role as a leading regional city.

Wollongong is divided into ten planning areas, based on population size, common topography, historical and affiliative perceptions of 'place' and patterns of people movement along road and rail systems. Corrimal is located within Area 2/3 of the Northern Catchment. It is expected that the Northern Catchment will continue to grow, but the overall share of the population will drop from 37% to 33% by 2036. However, the population of the Northern Corridor is still expected to expand from 78,425 people in 2016 to 81,811 people in 2036.

Within the planning framework Corrimal is identified as a key urban centre within the Northern catchment. The current social infrastructure provision by catchment area indicates that the northern catchment has a total foundation GFA of 9,183m², a total of 15 foundation facilities, 14,482m² of total supporting GFA and a total of 40 supporting facilities.

Key outcomes for the Northern catchment are:

- » Consider the role of Social Infrastructure in Council's town centre planning for Corrimal – possible impact of proposed higher density residential developments for social infrastructure.
- » Recognise availability of two, existing 'District level multi-purpose community centre and library facilities at Thirroul and Corrimal
- » Recognise limited availability of dedicated, creative, cultural social infrastructure within the Northern catchment, while noting that Thirroul and Corrimal community centres and libraries offer spaces that can be used for exhibition and performance.

Discussion

There is no identified requirement for additional Council social infrastructure on the site. However, it is envisaged that a shared community space will be incorporated into the development and managed as part of the local retail facilities.

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Diasability Inclusion Action Plan 2016-2020

The purpose of the Disability Inclusion Action Plan is to set out the strategies and actions that Council will deliver in the next four years to enable people with disability to have greater access to Council information, services and facilities. The Plan includes actions for all areas of Council and will provide guidance for making services and facilities more inclusive.

The Plan is underpinned by the following principles which support the United Nations Convention on the Rights of Persons with Disabilities (2006):

- » Focusing on abilities and not disabilities.
- » Fundamental rights for all people.
- » Genuine dialogue and participation.
- » Improving access and inclusion for all.
- » Prudent use of resources.
- » Recognising the benefits of collaboration.
- » Principles of Universal Design.
- » Access is everyone's business.

The aims of the plan are as follows:

- » Create liveable communities:
 - > Improve our accessible public toilets
 - > Increase number of accessible parking spaces
 - > Increase the number of accessible paths of travel to key destinations
 - > Improve access to our spaces and streetscapes
 - > Increase the number of accessible bus stops and shelters
 - > Increase access to our recreation services and facilities
 - > Increase access to library, community, youth and cultural services
 - > Increase access to buildings
 - > Improve policy and planning tools to create better access
- » Improve access to our systems and processes:
 - > Increase access to information
 - > Increase participation in our community engagement activities
 - > Increase awareness about our services to support access
 - > Increase our capacity to provide accessible services and facilities
 - > Improve our systems and processes to deliver better outcomes
- » Promote positive attitudes and behaviours:
 - > Raise awareness about the contribution people with disability make to our community
 - > Increase participation in our events, festivals and activities
 - > Undertake programs to promote access and inclusion
- » Promote access to meaningful employment:
 - > Increase employment opportunities
 - > Increase participation opportunities

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- > Incorporate workforce diversity as part of everyday Council business
- > Increase retention and development opportunities
- > Increase awareness about working with people with disability

Corrimal Town Centre Plan (including Memorial Park) 2015 – 2025 (2015)

The plan provides a strategic direction for the Corrimal town centre. The plan identifies the former Corrimal Coke Works site as being a 'key site', adjacent to the railway station and in walking distance to the Corrimal Town Centre services and noted that the following matters should be considered in future planning for the site:

1. Investigations into flooding and contamination constraints would be required in order to inform the development capacity of the site
2. A quality east-west linkage from the town centre to the site and the railway station and the beach would need to be established and any development would need to have an active street frontage with Railway Street
3. Any future development would need to be complementary to the town centre
4. Investigations would be required into the archaeological and cultural significance of the site
5. Future works would need to be cognisant of flood impacts and in accordance with the existing Vegetation Management Plan.

The Corrimal Town Centre Implementation Plan, which supports the Corrimal Town Centre Plan 2015-2025, includes Action 2.2, which relates to the former Corrimal Coke Works and states:

"Work with site owners to guide investigation of site renewal – Planning Proposal request to be prepared and submitted by owner and required to be strategically aligned with Town Centre Plan and other strategies."

Consequently, the PP has been prepared as a result of the above action and the fact that the Corrimal Town Centre Plan 2015 – 2025 identifies the former Corrimal Coke Works as a 'key site'.

Discussion

In response to the above:

1. Investigations into contamination on the site have revealed that there are a limited number of locations that require remediation and the site can be made suitable for residential and recreational uses.
2. The concept plan will deliver a significant improvement to the streetscape connecting the Corrimal town centre and Corrimal train station. Both road edge and through site routes will be created, particularly concentrating on pedestrian and cycling links in order to increase active transport. Pedestrian and cycling links will connect with a potential green link along the Towradgi Creek corridor to Corrimal Beach. This would also link with the Grand Pacific Walk and the community and recreational resources west of Memorial Drive.
3. The proposed development would be complementary to the town centre in that it would result in an increased population, subsequently increasing economic activity in Corrimal. A R3 Medium Density zoning is proposed for the developable area of the site. This is consistent with surrounding zoning to the north, north-east and east of the site and will result in the delivery of housing diversity.

Some retail and commercial development is intended to be located adjacent to Corrimal station. However, the low-scale nature of these will not undermine the efficacy or further fragment to the core retail area of the Corrimal town centre.

4. Council has progressed a local heritage for the part of the site separate to this PP and the development will provide for the retention and interpretation and key heritage elements.
5. An Aboriginal Cultural Heritage Assessment has been undertaken and, while consultation has indicated that creeklines in the area were traditional fishing grounds and retain Aboriginal cultural value for their connection to this activity, no specific or significant Aboriginal cultural features have been identified for the site. The site is heavily disturbed with limited potential for Aboriginal archaeological objects, with only one Aboriginal

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archaeological site identified on the site. Avoidance or mitigation to impacts of this archaeological were deemed as unnecessary.

6. The proposed realignment of North Corrimal Creek will manage flooding on the site and provide a flood-free development area, while also reducing some flood impacts upstream of the site and avoiding any change to downstream flood levels. At the same time, this will also deliver a rehabilitated riparian corridor with enhanced biodiversity values.

Q5 Is the Planning Proposal consistent with the applicable State Environmental Planning Policies?

The PP considers the State Environmental Planning Policies (SEPPs) which are applicable for the proposed urban development of the site, as identified in **Table 10** below.

Table 10 Applicable SEPPs

SEPP	Applicable	Consistency
SEPP No 1 – Development Standards	No	N/A
SEPP No 14 – Coastal Wetlands	No	N/A
SEPP No 19 – Bushland in Urban Areas	No	N/A
SEPP No 21 – Caravan Park	No	N/A
SEPP No 26 – Littoral Rainforests	No	N/A
SEPP No 30 – Intensive Agriculture	No	N/A
SEPP No 33 – Hazardous and Offensive Development	The site has previously been utilised for industrial uses, however operations ceased 6 years ago.	N/A
SEPP No 36 – Manufactured Home Estates	No	N/A
SEPP No 44 – Koala Habitat Protection	The site does not represent 'potential koala habitat' or 'core koala habitat' as defined in SEPP 44. Accordingly, there is no requirement to prepare a Koala Plan of Management.	Consistent
SEPP No 50 – Canal Estate Development	No	N/A
SEPP No 55 – Remediation of Land	A Contamination Assessment has been prepared for the site in accordance with the SEPP requirement. A Remediation Action Plan has been prepared for the subject site and endorsed by a Site Auditor.	Consistent
SEPP No 62 – Sustainable Aquiculture	No	N/A
SEPP No 64 – Advertising and Signage	This proposal allows future development to meet the requirements of the SEPP	Consistent

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SEPP	Applicable	Consistency
SEPP No 65 – Design Quality of Residential Flat Development	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP No 70 – Affordable Housing (Revised Schemes)	No	N/A
SEPP (Building Sustainability Index: BASIX) 2004	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Housing for Seniors or People with a Disability) 2004	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Infrastructure) 2007	The proposal is supportive of planned and required upgrades to services, facilities and infrastructure which will support the development of the site.	Consistent
SEPP (Mining, Petroleum and Extractive Industries) 2007	No	N/A
SEPP (Miscellaneous Consent Provisions) 2007	No	N/A
SEPP (Exempt and Complying Development Codes) 2008	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Affordable Rental Housing) 2009	The proposal allows future development to meet the requirements of this SEPP	Consistent
SEPP (Coastal Management) 2018	<p>The site is within the Coastal Area Use Area Mapping associated with the Coastal Management SEPP.</p> <p>Division 4 applies to this area and the most relevant provision is that development <i>has taken into account the type and location of the proposed development, and the bulk, scale and size of the proposed development.</i></p> <p>The proposal can comply with the requirements of the SEPP.</p>	Consistent

Q6 Is the Planning Proposal consistent with applicable Ministerial Directions?

The PP gives consideration to the relevant Ministerial Directions issued under Section 117 (now 9.1) of the EP&A Act. The Minister for Planning and Environment issues Local Planning Directions that councils must follow when preparing a PP. The directions cover the following broad categories:

- » employment and resources,
- » environment and heritage,
- » housing, infrastructure, and urban development,
- » hazard and risk.

Table 11 provides information for consideration with regard to the consistency of the PP with regard to the relevant ministerial directions.

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Table 11 Section 9.1 Directions

Section 9.1 Direction	Comment
Employment and Resources	
<p>1.1 Business and Industrial Zones</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will affect land within an existing or proposed business or industrial zone (including the alteration of any existing business or industrial zone boundary).</p>	<p>A key objective of this ministerial direction is to protect business and industrial zones. The site is not considered suitable for the use of any further heavy or light industrial land uses.</p> <p>The response to this Direction was accepted as part of the Gateway Determination.</p>
<p>1.2 Rural Zones</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will affect land within an existing or proposed rural zone (including the alteration of any existing rural zone boundary).</p>	<p>Does not apply as the land is not zoned rural and does not apply in the Wollongong LGA.</p>
<p>1.3 Mining, Petroleum Production and Extractive Industries</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that would have the effect of:</p> <p>(a) prohibiting the mining of coal or other minerals, production of petroleum, or winning or obtaining of extractive materials, or</p> <p>(b) restricting the potential development of resources of coal, other minerals, petroleum or extractive materials which are of State or regional significance by permitting a land use that is likely to be incompatible with such development.</p>	<p>To date no mining activity has been approved or undertaken below the site.</p>
<p>1.4 Rural Lands</p> <p>This direction applies when:</p> <p>(a) a relevant planning authority prepares a planning proposal that will affect land within an existing or proposed rural or environment protection zone (including the alteration of any existing rural or environment protection zone boundary) or</p> <p>(b) a relevant planning authority prepares a planning proposal that changes the existing minimum lot size on land within a rural or environment protection zone.</p>	<p>Not applicable as the land has not been used for rural purposes and does not apply in Wollongong City Council LGA.</p>
Environment and Heritage	
<p>2.1 Environmental Protection Zones</p> <p>This direction applies when a relevant planning authority prepares a planning proposal.</p>	<p>It is noted that this Ministerial Direction applies when a relevant planning authority prepares a PP.</p> <p>The PP does not seek to reduce the environmental protection standards affecting the site.</p>

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Section 9.1 Direction	Comment
<p>2.3 Heritage Conservation</p> <p>This direction applies when a relevant planning authority prepares a planning proposal.</p> <p>The objective of this direction is to conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.</p>	<p>Council has progressed a separate planning proposal to list part of the site as a local heritage item. This satisfies the requirements of this Direction.</p> <p>This PP is supported by a Conservation Management Strategy and Heritage Interpretation Plan.</p>
Housing, Infrastructure and Urban Development	
<p>3.1 Residential Zones</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will affect land within:</p> <p>(a) an existing or proposed residential zone (including the alteration of any existing residential zone boundary),</p> <p>(b) any other zone in which significant residential development is permitted or proposed to be permitted.</p>	<p>The proposal will provide diverse housing choices with a mix of medium density housing typologies in close proximity to public transit infrastructure. The proposed provision of approximately 700 - 750 new dwellings will help ensure that Wollongong City Council deliver a diverse range of housing options as an infill project.</p> <p>The proposed development will make efficient use of existing and proposed infrastructure, promote active living and seek to minimise the potential impacts of housing on the environment.</p>
<p>3.3 Home Occupations</p> <p>This direction applies when a relevant planning authority prepares a planning proposal.</p>	<p>This Ministerial Direction requires a PP to contain 'home occupations' as a use that is permissible without consent in dwelling houses.</p> <p>'Home Business' and 'Home occupation' as additional permitted uses are permitted through SEPP Exempt and Complying Development Code.</p>
<p>3.4 Integrating Land Use and Transport</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will create, alter or remove a zone or a provision relating to urban land, including land zoned for residential, business, industrial, village or tourist purposes.</p>	<p>The PP integrates with the adjacent Corrimal train station to offer an opportunity for transit-oriented development.</p> <p>Improved pedestrian and cycling infrastructure will also reduce car dependence for the broader Corrimal community.</p> <p>Commuter-related services, without undermining the efficacy of the Corrimal town centre, can be offered under the PP such as neighbourhood shops, food and beverage premises and childcare to offer convenience for both residents and commuters.</p>
<p>4.1 Acid Sulphate Soils</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will apply to land having a probability of containing acid sulfate soils as shown on the Acid Sulfate Soils Planning Maps.</p>	<p>The site is identified as having a probability of acid sulphate soils. The majority of the site is Class 5 with a small portion Class 3 (however, this is not proposed to be developed).</p> <p>Assessment has shown it is not a constraint to development, but further investigation and management of acid Sulfate soils will need to be undertaken.</p> <p>The response to this Direction was accepted as part of the Gateway Determination.</p>
<p>4.2 Mine Subsidence and Unstable Land</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that permits development on land that:</p> <p>(a) is within a mine subsidence district, or</p>	<p>It is understood that the site is not located within a mine subsidence district.</p>

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Section 9.1 Direction	Comment
<p>(b) has been identified as unstable in a study, strategy or other assessment undertaken:</p> <p>(i) by or on behalf of the relevant planning authority, or</p> <p>(ii) by or on behalf of a public authority and provided to the relevant planning authority.</p>	
<p>4.3 Flood Prone Land</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that creates, removes or alters a zone or a provision that affects flood prone land.</p>	<p>The site is flood affected. Mitigations are proposed as part of this PP to ensure that all land proposed to be zoned R3 will be PMF-free and assist in alleviating flooding issues for surrounding properties. All other land that will remain flood affected will be zoned RE1.</p> <p>The Gateway Determination required further information to demonstrate consistency with this Direction. Numerous reports and information have been provided to satisfy this Direction and Council has indicated that it is satisfied with the approach.</p>
<p>4.4 Planning for Bushfire Protection</p> <p>This direction applies when a relevant planning authority prepares a planning proposal that will affect or is in proximity to land mapped as bushfire prone land.</p>	<p>This direction applies when a relevant planning authority prepares a PP that will affect or is in proximity to land mapped as bushfire prone land. The subject site is not classed as bushfire prone by Wollongong City Council. The proposal will be delivered in accordance with this direction and Planning for Bushfire Protection 2006.</p>
Regional Planning	
<p>5.1 Implementation of Regional Strategies</p> <p>This direction applies when a relevant planning authority prepares a planning proposal.</p>	<p>The site is not within any areas included in the Regional Strategies listed in this Ministerial Direction.</p>
<p>5.2 Sydney Drinking Water Catchments</p> <p>This Direction applies when a relevant planning authority prepares a planning proposal that applies to land within the Sydney drinking water catchment.</p>	<p>The site is not within a Sydney drinking water catchment area as provided in mapping associated with State Environmental Planning Policy (Sydney Drinking Water Catchment) 2011.</p>
<p>5.8 Second Sydney Airport: Badgerys Creek</p> <p>Planning proposals must not contain provisions that enable the carrying out of development, either with or without development consent, which at the date of this direction, could hinder the potential for development of a Second Sydney Airport.</p>	<p>The development of the site will not hinder the development of a second Sydney Airport.</p>
<p>5.10 Implementation of Regional Plans</p>	<p>The PP is consistent with the Illawarra-Shoalhaven Regional Plan 2015.</p>
<p>6.1 Approval and Referral Requirements</p>	<p>The proposal does include provisions that will require concurrence regarding development adjacent to a state road and rail line. The creek re-alignment and development within areas designated under the draft SEPP (Coastal Management) 2016. These concurrences are likely to be required.</p>

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Section 9.1 Direction	Comment
6.2 Reserving Land for Public Purposes	The proposal includes provisions and dedication of land for public services and facilities.
6.3 Site Specific Provisions	A Site Specific DCP has been prepared to support this PP.

Section C – Environmental, Social and Economic Impacts

Q7 Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal.

A Flora and Fauna Assessment was undertaken by EcoLogical Australia (refer to **Appendix M**) in order to provide a description of the biodiversity values of the site and the impact and proposed outcomes associated with the PP.

The vast majority of the study area accommodates areas of cleared land (including roads, infrastructure, water bodies) or weeds/exotics. However, three vegetation communities were found on the site:

- » Forest Red Gum Thin-leaved Stringybark Grassy Woodlands - occurred as two patches, comprising approximately 1.27ha.
The Forest Red Gum Thin-leaved Stringybark Grassy Woodland was identified as being in a highly modified and disturbed condition with high levels of exotic species, with planted non-local native species and modified landforms present. This vegetation community forms part of the 'Illawarra Lowlands Grassy Woodland in the Sydney Basin Bioregion' EEC.
- » Acacia Scrub – A total of 1.30ha occurred within the south of the study area in association with the tributary of Towradgi Creek which runs through the southern portion of the site.
This vegetation community occurred as a tall dense shrubland to low closed forest with the height and density of the canopy increasing in proximity to the tributary of Towradgi Creek. It is unclear whether these species were part of the original vegetation type within this area, or if they have colonised this area following the formation of a dense canopy of Acacias and exotic species.
- » Urban Native and Exotic - an area of approximately 8.28ha was mapped within the study area
Within and surrounding the most disturbed parts of the study area, stands of vegetation were comprised of exclusively exotic species or consisted of rows of non-local native planted trees with a disturbed predominately exotic understorey.

All vegetation communities across the site were identified as having been subjected to moderate to high levels of disturbance including vegetation clearing/thinning of the canopy layer, infestation of exotic species and modification to the landform and soil profiles.

The only areas of vegetation, beyond the areas which have previously been disturbed, which will be impacted as a result of the proposal is 0.588ha of Forest Red Gum Thin-leaved Stringybark Grassy Woodland (northern area) and 1.12ha of Acacia Scrub. The Forest Red Gum Thin-leaved Stringybark Grassy Woodland vegetation community forms part of the 'Illawarra Lowlands Grassy Woodland in the Sydney Basin Bioregion' which is an EEC listed under the BC Act. The proposed rezoning would retain and restore a majority of this EEC.

One flora species listed under the EPBC Act and BC Act, *Eucalyptus scoparia* (Wallangarra White Gum), was identified within the study area, where approximately eight individuals of this species were observed in a row adjacent to an internal road. No other threatened flora species were recorded during either of the site inspections and given the limited habitat available and its highly degraded condition, no other threatened flora these species are considered likely to occur.

All efforts have been made during the Master Planning process to ensure the majority of the native vegetation communities are not impacted.

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In terms of fauna, one threatened fauna species, *Pteropus poliocephalus* (Grey-headed Flying-fox) was identified in the study area during surveys. The Grey-headed Flying-fox is listed as vulnerable under the BC Act and EPBC Act and was identified as occupying the patch of Forest Red-gum Thin-leaved Stringy Bark Grassy Woodlands at the southern extent of the study area, adjacent to the dam and existing riparian corridor. The camp was estimated to support a typical population of 100-300 individuals, however this is subject to seasonably variations. More detail on this is provided below.

The remainder of the site is considered a low ecological constraint due to the presence of cleared lands, existing derelict infrastructure and Urban Native and Exotic Cover. The study area was not found to provide potential habitat for the Green and Golden Bell Frog (*Litoria aurea*) or Threatened microchiropteran bats.

Grey-headed Flying Fox

The study area contains a Grey-headed Flying-fox camp in the southern extent of the site. During 2017 – 2019, infrequent observations of the camp identified between 100 – 300 individuals occupying a small area of vegetation at the southern end of the site, immediately adjacent to the dam wall.

A regular monitoring program of the camp commenced in July 2019 to inform an understanding of its patterns of occupation and use. The camp was empty from July 2019 to early November 2019 when the population returned to a typical level. There was a spike in occupation to approximately 4,000 – 6,000 over a period in January – February 2020, which was attributed to significant bushfire activity. The camp returned to a more typical population over March – April 2020 following rainfall events and the end of bushfire activity. Currently the camp is virtually empty and showing a trend like the last years.

The monitoring program will continue, however currently support the following understanding of the camp:

- » The typical population size is 100 – 500 individuals
- » The camp is occupied periodically, which appears to be seasonally related
- » No breeding has been observed at the camp
- » There is potential for the camp to expand in size, however this is constrained by the availability of suitable roosting and foraging species
- » Fly out direction is typically towards to escarpment to the west.

As a result, a buffer from the core of the camp has been established.

EcoLogical has developed Camp Management Actions to address the potential for future conflict between Grey-headed Flying-foxes and residential development on the site. The indicative management actions are consistent with the Flying-fox Camp Management Policy (OEH 2015) and include:

- » establishment of a physical buffer between the camp and future development (incorporating managed vegetations areas and road reserve)
- » creation of supplementary habitat, such as dams, at the southern boundary of the site
- » revegetation of portions of the site using known feed trees for the species (in areas away from the proposed development)
- » potential building design solutions (eg. acoustic)
- » communication and education for future residents

The proposed management actions have been peer reviewed and endorsed by Ecosure.

The proposed buffer has been increased to 100 metres from the core camp extent in response to feedback from Council and DPI&E.

Microchiropteran Bats

EcoLogical undertook a site inspection of all buildings within the Corrimal Cokeworks, in order to determine the presence of the microchiropteran bat (microbat) habitat. The study found no evidence found of microbats using

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any of the structures present on the subject site. No microbats were seen or any clear evidence found of microbats having used the buildings at any time. In areas where thorough inspection occurred, it was concluded that the buildings were not being used by microbats, nor were there any signs of microbats.

However, some areas of the site were inaccessible, and it cannot be concluded that they were not being used by microbats at the time of survey. These areas were generally considered unsuitable microbat habitat. It is possible that there may be occasional use of buildings by individual microbats in other seasons of the year. The lack of suitable habitat, absence of signs of use or occupation and the lack of records from previous ultrasonic detection surveys, indicated that microbats rarely if ever use the structures and, if they ever do, it is only very occasionally and in small numbers.

Q8 Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed?

Additional environmental investigations were undertaken to inform the PP. The following table provides a summary of the key findings of these investigations.

Table 12 Environmental investigation area and outcome of findings

Environmental Factor	Site Implication
Contamination and Remediation	<p>Site investigations have revealed minimal contamination considering the former heavy industrial uses. A Remediation Action Plan by Arcadis (May 2017) was completed for the site pre-Gateway and has subsequently been updated in response to conditions in the Gateway Determination. The investigations summarised in the report revealed that only six soil samples that exceeded acceptable levels of human health assessment criteria considering future residential and open space use. Three locations are contaminated with asbestos, two with lead and one with TEQ BaP. The locations are isolated in nature and will require delineation sampling to determine the lateral extent prior to remediation.</p> <p>The RAP has been endorsed by a Site Auditor and future remediation works will be subject to validation and issue of a Site Audit Statement.</p>
Geotechnical	<p>Douglas Partners concluded that all geotechnical conditions can be managed to facilitate development. As part of future investigations, an earthworks methodology will need to be prepared to ensure the reuse of existing site soils and optimised earthworks and procedures.</p> <p>Considerations in redevelopment of the site will include:</p> <ul style="list-style-type: none"> » uncontrolled fill assessed and processed for suitability for redevelopment » further test and management of any Acid Sulphate Soils in earthworks, design and construction » minor constraints relate to erosion potential of disturbed materials, localised waterlogging, localised abnormal moisture content within the coke production area, sodicity and soil aggressiveness, which can be effectively handled during the earthworks design.
Bushfire	<p>The site is not identified as bushfire prone.</p>
Overland flow and riparian corridors	<p>A flood study undertaken by Cardno concludes that flooding issues can be effectively managed, and that development of the site will not increase flood impacts upstream of downstream of the site.</p> <p>The proposed creek realignment has been designed to convey major flows up to PMF event within North Corrimal Creek through the site, allowing for a flood free development area.</p>

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Environmental Factor	Site Implication
Heritage	The Heritage Report undertaken by Urbis confirms that there are no existing statutory heritage listings on the site, nor in the vicinity of the site. The report recommends a reduced heritage curtilage which enables significant heritage elements of the site to be retained in situ and/or interpreted. It is recommended that these individual elements and or curtilage be incorporated as items of local significance within Schedule 5 of the WLEP 2009
Visual	A visual impact analysis was performed by e8urban pre-Gateway to inform typology and building heights in the concept plan. The view to the Illawarra Escarpment from development to the east of the subject site was considered of primary importance. Further analysis of views has been undertaken as the master plan has evolved, and the distribution of building heights has been developed in close consultation with Council.
Traffic serviceability	The Traffic and Transport Assessment revealed that the development does not result in traffic or transport impacts that cannot be mitigated. The primary impact is confined to the intersection of Memorial Drive/Railway Street, which will require an upgrade to mitigate additional traffic from the development. These upgrade works are proposed to be undertaken as part of the development.
Servicing	<ul style="list-style-type: none"> » ADW Johnson initially, and BG&E more recently, have assessed that the site is capable of being serviced with gas, electricity, sewer, potable water and NBN telecommunications » Consultation with various providers is underway to optimise servicing arrangements
Mining and mining subsidence	Review of available mine subsidence maps indicates that the site is not located in a mine subsidence area.

Q9 Has the planning proposal adequately addressed any social and economic effects?

Economic Effects

The urban development of Corrimal is forecast to generate \$761M in additional economic activity (Hill PDA). Construction and indirect jobs will be supported during the delivery of the project. The relevant social and economic effects of the proposed development include:

- » Delivery of a range of public benefits associated with the development
- » Location in proximity to current transport services
- » Building capacity within the local community to increase housing and lifestyle diversity, employment, economic viability, social activity and opportunity.

A detailed assessment of the economic impacts of the PP was undertaken by Hill PDA pre-Gateway. Hill PDA assessed the land as not being viable for on-going industrial purposes due to the high cost of remediating the land, compared to the return of investment for industrial purposes. Hill PDA has also outlined the large supply of better located land for industrial purposes readily available in the Illawarra.

Redevelopment of the site will provide economic benefits in terms of jobs and value added. While the R3 Medium Density zoning does not specify employment-generation as a specific intended use, and the development will be primarily residential, there are a number of permitted uses and proposed additional permitted uses that are expected to generate employment within a 200m location of Corrimal train station.

Key permitted uses that are likely to generate jobs are:

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- » Neighbourhood shops (but not large-scale retail which would undermine the Corrimal town centre)
- » Food and Beverage Premises
- » Child Care Centres
- » Seniors Housing

Urbis has undertaken an Economic assessment post-Gateway which concludes the following:

- » The proposed 2,000m² of retail, comprising a small supermarket and supporting dining and convenience-based retail, will be well supported by market demand, driven by strong on-site population growth
- » The modest retail offer is forecast to achieve turnover in 2024 that reflects:
 - > A minor market share of retail spending (2% of main trade area and 9% of primary trade area)
 - > Only a portion of the expected growth between 2018 and 2024 (25% of primary trade area growth and 14% of main trade area growth)
- » Residual spending and spending growth will continue to support existing and proposed retail centres.
- » The proposed retail at the subject site will also have significant benefits relating to providing valuable amenity for current and future residents of the immediate area, and employment, consumer and economic value benefits for the local region and state economy.

Urbis has undertaken an analysis (refer to **Appendix U**) based on an assessment of key opportunities relating to the subject site and its competitive context. Key findings of the retail assessment include:

- » Based on securing a market share of trade area food and grocery retail spending, the subject site could support a small supermarket targeted at the on-site market. Allowing for 10% of spending to come from beyond the catchment, and an allocation of 6% to general merchandising, the estimated sales potential of a supermarket facility at the subject site is \$5.2m in 2024, growing to \$6.2m by 2026. This aggregate sales potential could support a store in the range of 500 – 1,000m² (assumed 750m²), this translates to an average trading level of around \$6,900 per m² initially, stabilising at around \$8,300 per m² in 2026.
- » A small supermarket on site could be supported by some cafes and restaurants and a limited convenience offer.
- » The indicative composition that could support a small format supermarket on site, based on Urbis' understanding of key retail and design principles, includes:
 - > 2-3 cafes/restaurants, that also serve a takeaway role, and a specialty food store
 - > 2 non-food retail speciality stores, which could include a newsagency, small pharmacy or other leisure/general retail shops
 - > Personal services such as a hair salon, beautician, massage etc.
 - > A non-retail shopfront such as a real estate agent or allied health practice (dental, physiotherapy or chiropractor)

Total retail sales are estimated at \$11.4m in 2024, growing to \$13.4m in 2026. These sales reflect an overall retail sales performance of \$6,350 per m² in 2024 and \$7,440 per m² in 2026.

Retail specialties are estimated to trade at around \$6,830 per m² by 2026, assuming an optimised tenancy mix.

- » The proposed retail at the subject site is estimated to achieve a very minor share of retail spending in the secondary trade area, and slightly higher share of retail spending in the primary trade area. The centre is estimated to achieve a 2.7% market share of main trade area retail spending, including a 3.5% market share of food spending. Across the combined primary trade areas, the centre is estimated to achieve a 10.6% market share, including 13.9% of food spending and 6.1% of non-food spending.

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Within the primary on site trade area, the offer is estimated to achieve a 15.9% market share of total spending, including 20.9% of food spending and 9.0% of non-food spending. Around 10% of sales are estimated to be generated by residents from beyond the trade area.

The combined primary trade areas are estimated to account for 53% of total sales at the centre. The combined secondary trade areas are estimated to account for 37% of total sales.

The proposed retail on site would generate an estimated 103 ongoing jobs during the operational period. Up to an additional 23 indirect jobs in supporting and supplying industries could also be provided, indicating that the development could support up to 126 total jobs in the region (full time, part time and casual).

This could result in a total of \$4.8m GVA per year (in net present value terms) to the local region and broader state economy over a 20-year operating period. This consists of \$3.2m direct GVA per year generated by the daily operation and management of the proposed retail and \$1.6m indirect GVA per year over the same period.

Social Effects

The PP has the following intended social effects:

- » Delivery of approximately 700 - 750 dwellings with excellent access to public transit
- » Provide a diversity of housing to meet affordable price points for residents in the Illawarra
- » Provide neighbourhood and commuter services to deliver convenience for residents and commuters (e.g. neighbourhood shops, childcare facilities, food and beverage premises) promoting the use of public transit
- » Support tele-working and start-ups with the provision of a flexible working space
- » Deliver improved connections through the site to encourage active transport to and from Corrimal train station
- » Provide a public plaza at Corrimal station to provide a forum for social activity and celebrate the heritage of the site
- » Capitalise on the proximity to health, education, recreational and community resources near the site, including Wollongong Hospital that is undergoing an upgrade, schools within walking distance that are believed to have additional capacity, and the significant community resources such as Corrimal pool and library nearby.

Section D - State and Commonwealth Interests

Q10 Is there adequate public infrastructure for the Planning Proposal?

A Servicing Strategy Report Summary was prepared by BG&E (refer to **Appendix X**). The report confirms that all utility services are available on the site, noting the following:

- » Potable Water, Gas and Telecommunication networks require no augmentation to service the overall development;
- » No funding or construction of extensive feeder infrastructure will be required for electrical servicing, with only minor augmentations to the existing Endeavour Energy network envisaged; and
- » After further consultation with Sydney Water and a comprehensive MOUSE Modelling process, it has been confirmed that the wastewater network will be able to accommodate the full development yield. The wastewater servicing strategy involves the provision of additional storage capacity by upsizing the existing trunk main through the site, which will be undertaken in conjunction with a realignment of that trunk main during the bulk earthworks phase.

The likely infrastructure upgrade requirements are summarised in **Table 13** below.

Table 13 Infrastructure requirements

Infrastructure requirements	
Electricity	<p>Endeavor Energy (EE) estimated that the load of the overall development is approximately 2.6MVA (Mega Volt Amp) based on 3.5kVA (Kilo-Volt-Amp)/unit Average Daily Maximum Demand (ADMD) for apartments.</p> <p>Review of the existing electrical network shows that there are two 11kV feeders (CR1228 and CR1280) located opposite the development site along Railway Street, however both of these feeders have been estimated to have a total available capacity of approximately only 0.5MVA – not sufficient to accommodate the development site. However, EE noted that the overall development can potentially be serviced by Feeder RV1206 from Russell Vale Zone Substation (ZS) which is deemed to have the required available capacity to accommodate the mature load of the development.</p> <p>It is envisaged that HV linkage points will be Underground to Overhead (UGOH) terminations on feeder CR1280, with one in both Railway Street and High Street. Details surrounding HV linkages and network configuration will be confirmed when a firm application for load is submitted to EE.</p>
Telecommunications	<p>Design It Telco Pty Ltd was consulted about the capacity of existing NBN and Telstra networks on 9 November 2018, which confirmed:</p> <ul style="list-style-type: none"> » Current NBN and Telstra infrastructure could handle the overall development (750 dwellings); » As the development is over 100 lots it will automatically qualify for fibre to the premises (FTTP); and » NBN Co will supply a backhaul cable to cater for future lots. <p>It has been confirmed that the future development does not require any upgrades to telecommunications networks.</p>
Gas	<p>Jemena Ltd confirmed that there are suitable gas mains located on Railway Street within the vicinity of this proposal which currently have adequate capacity to service the overall development at this time.</p>
Water	<p>Sydney Water was consulted at a meeting dated 20 December 2017, regarding servicing the proposed development with potable water and the associated impacts on existing SWC potable water infrastructure as a result of the increased potable water network demand imposed by the proposed development.</p> <p>A letter from SWC dated 8 March 2018 confirmed that the existing DN200 CI/CL potable water main fronting the development along Railway Street has sufficient capacity to accommodate the proposed development.</p>
Sewer	<p>Results and findings from modelling and associated consultation with SWC has determined the existing Sydney Water wastewater network will be able to accommodate the proposed development upon implementation of planned network upgrades and adjustment and upsize of the on-site wastewater trunk main.</p>
Roads	<p>Modelling and impact assessment by Bitzios concluded that there are no traffic and transport impacts associated with the development which cannot be mitigated through reasonable works within public land or on site.</p> <p>The primary traffic impact from the development is confined to the intersection of Memorial Drive and Railway Street. This intersection requires upgrade to accommodate the future development traffic. The proposed upgrade has the benefit of also providing capacity to accommodate background traffic growth.</p> <p>Following discussions with Council and TfNSW, it was agreed that a new roundabout access on Railway Street would be the preferred and best access option for the site. This has been incorporated into the PP and also benefits existing residents to the</p>

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Infrastructure requirements

north of Railway Street by providing a managed intersection for access onto Railway Street.

Rail

Sydney Trains – Transport NSW was consulted regarding the existing high voltage 33kV overhead electrical network (Feeder 787) located outside of the eastern boundary of the proposed development, traversing Corrimal Station carpark.

It was confirmed that Feeder 787 does not currently have an easement, as the station carpark and access road is owned by RailCorp. However, it was noted that the electrical poles were previously located against the development boundary but were relocated to their current alignment many years ago, with the old easement (Y254942) being rescinded on gazette notification.

It is not envisaged that this electrical feeder will pose any significant constraint to rezoning and development of the site.

Q11 What are the views of state and Commonwealth public authorities consulted in accordance with the Gateway determination

Consultation with DPI&E

Various meetings have been held with Council and DPE to discuss and obtain comment on the planning provisions and the completion of the studies outlined in the Gateway. DPI&E have also assisted and facilitate meetings with other state agencies.

Consultation with Transport for NSW (now including RMS), Sydney Trains and RailCorp

The relevant transport agencies were closely consulted in preparing the updated Traffic Impact Assessment following Gateway Determination. This included a number of face to face meetings, provision of information from the agencies, validation and calibration of the traffic modelling (in accordance with RMS guidelines) and presentation of findings.

There has been ongoing communication with TfNSW in relation to the concept design for the proposed upgrade of the Memorial Drive/Railway Street intersection.

During discussions and meetings with TfNSW Sydney Trains they indicated that the car parking should not be impacted, or redesigned or be included into the PP. They commented that there was no plans at this stage to provide any station or pedestrian upgrades. For this reason the PP has excluded the area in the PP.

Consultation with DP&IE - Environment

Consultation and a few meetings with Environment were held to discuss the biodiversity outcomes and management of the grey-headed flying fox camp.

Consultation with Council

Consultation with Council has occurred on a regular basis, with meetings held often at Council's offices or on line. The aim of these meetings has been to progress the PP and master plan, obtain comments and input, and discuss and draft the new Corrimal Site Specific DCP.

A substantial number of amendments have been included into the PP based on council feedback.

Part 4 – Mapping

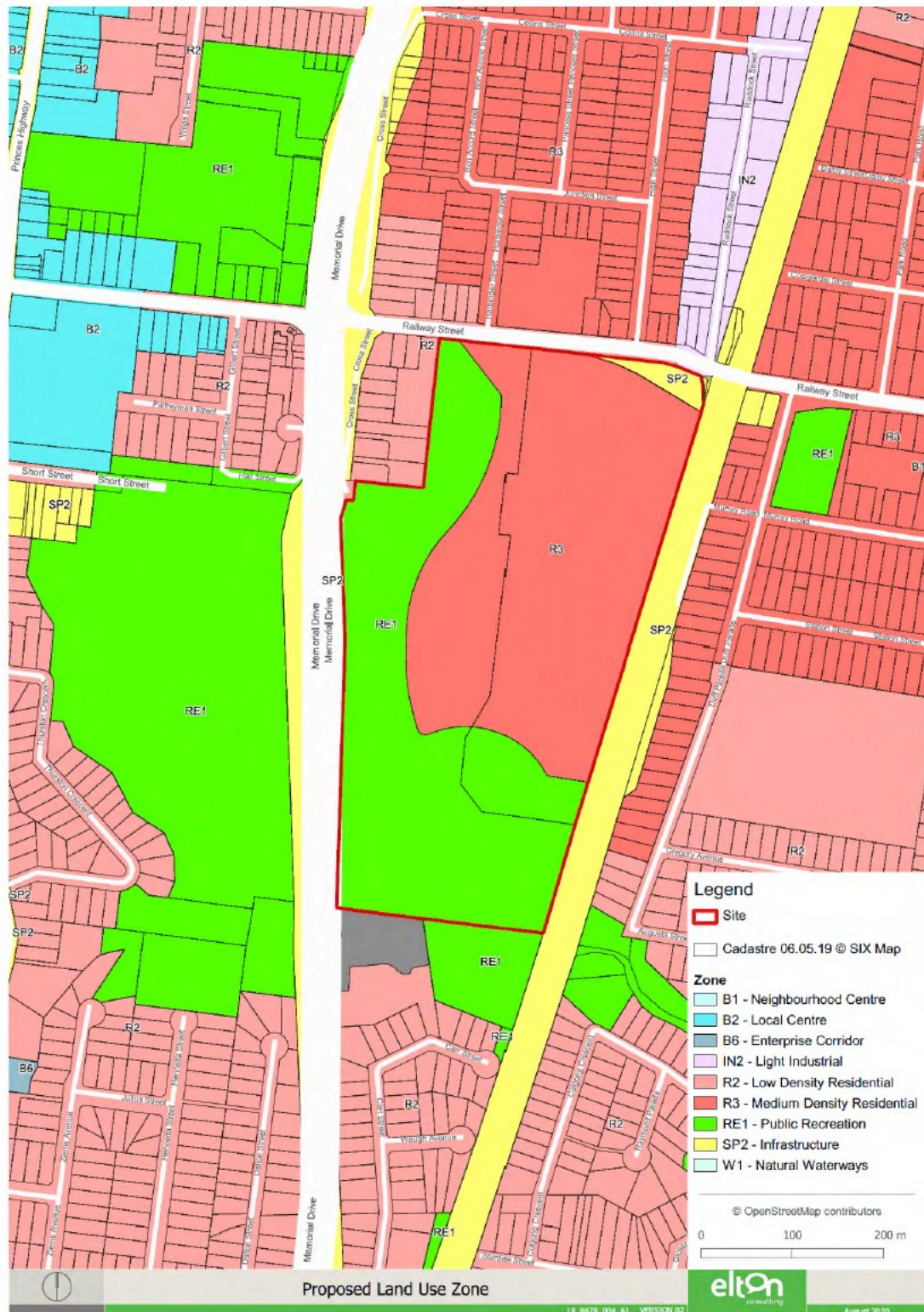
Part 4 presents clear and accurate mapping depicting relevant aspects of the PP. The subject PP seeks amendments that relate to land use zoning, height of buildings, minimum lot size, natural resource sensitivity and acid sulfate soils map, riparian land map & foreshore building line mapping.

Thus, the mapping amendments of the WLEP 2009 required are listed below:

- » Land zoning
 - > Sheet LZN_024
- » Minimum lot size
 - > Sheet LSZ_024
- » Height of Buildings
 - > Sheet HOB_024
- » Floor Space Ratio
 - > Sheet FSR_024
- » Natural Resource Sensitivity - Biodiversity
 - > Sheet NRB_010
- » Acid Sulfate Soils Map Riparian Land Map Foreshore Building Line Map
 - > Sheet CL1_024
- » Key Sites Plan
 - > Sheet KSP 024
- » Urban Land Release Plan
 - > Sheet URP 024

The proposed WLEP map amendments are identified in **Figures 18 – 24** below. The maps will be provided in the format prescribed by the technical guidelines for LEP maps published by the DPI&E for exhibition purposes as required.

Figure 19 Proposed Land Zoning Map



Figure

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Figure 20 Proposed Minimum Lot Size Map



Figure 21 Proposed Height of Buildings Map



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Figure 22 Proposed Floor Space Ratio Map

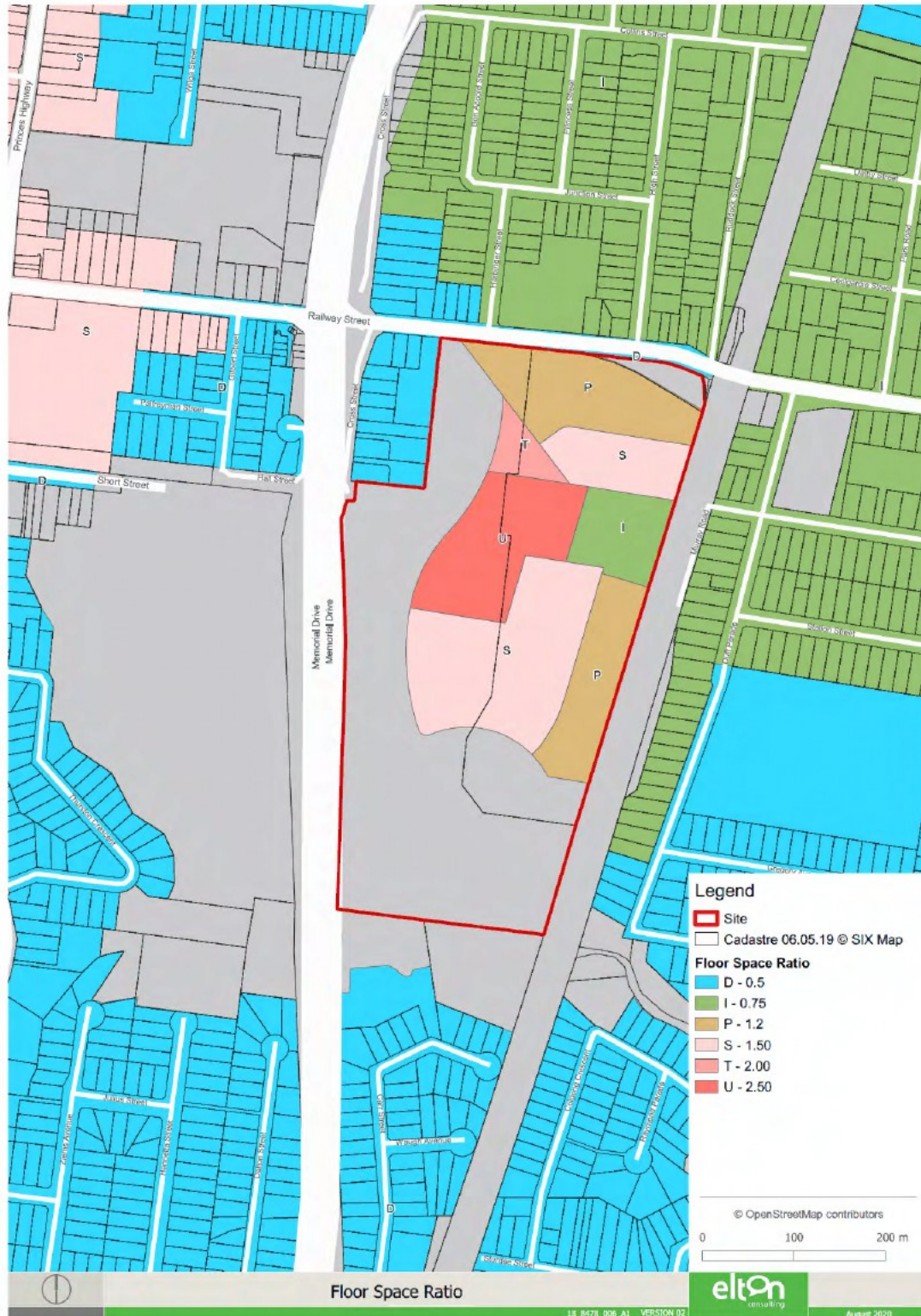


Figure 23 Proposed Natural Resource Sensitivity – Biodiversity Map





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Figure 25 Key Sites Plan



Figure 26 Urban Land Release Map



Part 5 – Community Consultation

Legacy Property and ICC are committed to transparent and proactive communication with the communities in and around this project to ensure all parties are informed about the plans and have the opportunity to have their say.

At Corrimal, this has meant:

- » Being open, clear and sensitive in all dealings with directly affected landowners, stakeholders and the community
- » Making the greatest possible effort to ensure community members are aware of the proposal
- » Providing opportunities for participation and collaboration

Initial community consultation commenced 6 years ago with ICC holding workshops with key community groups recommended by WCC including Neighbourhood Forum 4, the Corrimal Chamber of Commerce and the Corrimal Revitalisation Action Group. A booth was also set up at the Spring into Corrimal 2015 event to answer questions raised by the public. In addition, a dedicated website was launched in 2017 (www.corrimalcokeworks.com.au) to provide information on the project and to answer specific questions from the public.

Community newsletters are produced on an ongoing basis and distributed to approximately 1,500 residences and business in the Corrimal area.

Since obtaining Gateway approval, ICC and Legacy Property have conducted a range of more formal consultation activities as outlined below with a specific focus on residents immediately adjoining the rezoning area and in surrounding areas.

Consultation has also occurred with the Registered Aboriginal Parties.

4.16 Statutory consultation requirements

Sections 56 and 57 of the EP&A Act sets out statutory community consultation requirements for PPs. It is envisaged that, at a minimum, this will involve the public exhibition of the PP and supporting information for a period of 28 days. Council has previously resolved to exhibit the PP for 60 days. Notification of the PP will likely occur:

- » On Wollongong City Council's website;
- » In the applicable local newspaper(s);
- » In writing to the owners of adjacent and nearby land parcels, relevant stakeholders and neighbourhood/community groups and the surrounding community in the immediate vicinity of the site; and
- » In writing to relevant Government agencies.

4.17 Consultation workshops

In December 2018, ICC and Legacy Property held a series of visioning workshops with key stakeholders from Council, Neighbourhood Forum 4, Corrimal Community Action Group (CCAG), Corrimal Region Action Group (CRAG) and Corrimal Chamber of Commerce. This followed the issuing of a Gateway Determination in August 2018.

ICC and Legacy Property advised participants in December 2018 that the project team would consider the feedback received and other outputs of the sessions and come back to each stakeholder group by the end of March 2019 to detail how their feedback had been incorporated into detailed development of the proposal.

These follow-up sessions, as well as information sessions for the broader community, were held in late-March 2019. The information session invitation was letterbox dropped to approximately 1,000 residences in the

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surrounding area and was shared with key stakeholders who in turn shared with their networks. Digitally, the invitation was emailed to all those who had registered interest in the project online and was further posted on the official website as a pop-up box as well as a static notice on the homepage.

As well as the information sessions, pop ups were held on Friday and Saturday 30/31 March 2019 in order to ensure a broad cross section of the community had the opportunity to provide their views on the proposed development. 122 surveys were completed as part of this consultation process and demonstrated broad community support for the proposal.

The scale of that consultation has significantly exceeded what is mandated for a rezoning process, demonstrating a genuine commitment to an inclusive and transparent process that meaningfully engages the community and key stakeholders.

A consultation outcomes report can be found at **Appendix CC**.

4.18 Additional consultation methods

Other methods of consultation ICC and Legacy Property have undertaken include:

- » Regular distribution of newsletters to approximately 1,500 residences and businesses in the Corrimal area. 12 newsletters have been distributed between October 2017 and May 2020.
- » Developing a website that provides wide range of information about the proposal, including frequently asked questions, and the opportunity to register for ongoing updates

4.19 Response to Submissions

A number of submissions have been received by Council (and provided to the Applicant) since the Gateway determination was made, in response to the proposed development. These submissions are summarised, and responses provided in **Table 14** below.

Table 14 Residents/stakeholder Concerns and Responses

Concerns	Response
Road Network	
Traffic	
Increased traffic	The proposed development would result in an increase in traffic, however the traffic assessment undertaken by Bitzios indicates that any significant traffic impacts are isolated to the intersection of Memorial Drive/Railway Street and that an upgrade to this intersection is required to mitigate the development's impacts. ICC and Legacy propose to upgrade this intersection in accordance with this recommendation.
A Master Traffic Plan is required	A Traffic Impact Assessment has been prepared for the proposed development by Bitzios Consulting.
Engagement with RMS should occur to examine mitigation strategies for the Memorial Drive/Railway Street intersection	Consultation with RMS and TfNSW has occurred on an ongoing basis. The Memorial Drive/Railway Street intersection will be upgraded in accordance with the recommendations made by the Traffic Impact Assessment.
RMS need to undertake a fully independent assessment that's available to the public	RMS has been consulted and will continue to be consulted regarding this project. Any recommendations which are made by RMS or TfNSW will be incorporated into the proposed development scheme.

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Concerns	Response
Traffic generation should be calculated on dwellings types with height traffic generation	Traffic generation has been based on dwelling types, including terraced/semi-detached, 1, 2 and 3 bedroom apartments, Seniors Living apartments, affordable housing dwellings and Aged Care beds.
The intersection of Memorial Drive and Station Street data for SIDRA is from 2014 and an inaccurate baseline	The data utilised for traffic modelling is considered to be the most accurate and up to date data available. The traffic modelling has been subject to calibration and validation acceptance by RMS.
Distribution at the junction of Harbinger Street and Railway Street should be included in traffic distribution	Traffic distributions at the intersection of Railway Street/Harbinger Street are assessed as part of the Traffic Impact Assessment. A new roundabout is proposed at the intersection with Harbinger Street, providing access to the site. This will result in improved traffic outcomes for residents north of Railway Street.
Parking	
Not enough parking	Adequate parking will be provided on site in accordance with the requirements of the Site Specific DCP.
Undercroft parking is unsuitable, will act as a heat sink, reduce parking area, attract vandalism, prefers basement parking.	The parking typologies included onsite will form part of the later DA stage. However, any proposed development will be supported by a Crime Prevention Through Environmental Design (CPTED) report in order to determine the crime prevention features to include in the proposed development.
Access	
Direct access to Memorial Drive should be provided	Consultation has been undertaken with RMS and TfNSW to discuss the possibility of access from Memorial Drive, however this was not supported. It was also tested in modelling and shown to have very limited benefit.
The development proposes limited road access	Road access from Memorial Drive was discussed with RMS and TfNSW but was not supported. Therefore, one access from Railway Street is proposed.
There needs to be a turning lane into the site	Access to the site is proposed from a new roundabout at the intersection of Railway Street and Harbinger Street.
More points of ingress and egress are required	See above.
Access for emergency services will be difficult due to increase traffic	An adequate access route will be provided for emergency services.
Community	
Community Facilities	
Increased pressure on public services such as hospitals and schools	An assessment has been undertaken and the conclusion made, in consultation with Council, that there is no need to provide for any social infrastructure on the site. The Department of Education was contacted, with the outcome that there is likely to be adequate classrooms for the future volume of children resulting from the proposed development.
Should contain more facilities for the broader community	As stated above, Council has determined that there is no need to provide for any social infrastructure on the site. However small-

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Concerns	Response
	<p>scale retail land uses are proposed surrounding Corrimal train station, which will be accessible to the future population of the site as well as the broader community.</p> <p>The development will provide a range of new parks and open spaces for the benefit of the broader community.</p> <p>The development is also proposed to include a shared community space and flexible working space as part of the heritage precinct.</p>
Demographics	
The development will attract a younger demographic, and this is going to have a negative effect on the current elderly demographic	The proposed development is to provide for a diversity of housing and a diversity of demographics in order to create an inclusive community linking with the existing surrounding Corrimal community.
Environment	
Flooding	
Making sure the development does not increase the risk of flooding downstream	The Flood Study demonstrates that flooding issues can be effectively managed, and that development of the site will not increase flood impacts upstream or downstream of the site.
How will waterways be measured and monitored for water levels and contamination ongoing	The waterways will be monitored during creek alignment works. Refer to below regarding contamination.
Too many hard surfaces reducing rainfall absorption	See above.
Creek should remain following natural course.	<p>The existing creekline and riparian zone has been subject to high levels of modification and disturbance associated with the cokeworks operations.</p> <p>The proposed creek realignment has been designed to convey major flows up to PMF event within North Corrimal Creek through the site and provides improved flooding outcomes.</p> <p>This also allows for restoration of a riparian corridor to provide enhanced biodiversity outcomes.</p>
Ecology	
Coke works one of the last natural areas in the suburb and should not be taken away	<p>The site has previously been used for industrial purposes and therefore is not considered to be a natural area.</p> <p>An area of EEC in the southern portion of site with highest ecological values will be retained and rehabilitated.</p>
Tree canopy should remain, and mature trees should be kept	<p>An area of EEC in the southern portion of site with highest ecological values will be retained and rehabilitated.</p> <p>The existing significant trees along Railway Street will be substantially retained.</p>
Lack of green space	Approximately 9.5 hectares of new open space will be provided, representing 52% of the total site area.
Protection of bat habitat	The development retains the existing grey-headed flying fox camp and provides a significant buffer to future residential dwellings.
Riparian zone along the creek should be retained	The existing creek corridor is significantly modified and degraded, with limited biodiversity values.

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Concerns	Response
	North Corrimal Creek is being realigned through the majority of the site and will be restored as new riparian corridor with enhanced biodiversity outcomes.
Retain ponds for the ecology	The onsite dams were not found to contain any threatened species or threatened species habitat. These are man-made dams and not considered suitable to retain.
Contamination	
Concerned about contamination being properly assessed and dealt with	<p>Previous environmental assessments have identified isolated areas of hydrocarbon impacted soil that have the potential to pose a risk to human health under the proposed land use. Hydrocarbon, copper and zinc are also present in soil and have the potential to pose a risk to ecological receptors. Asbestos fibres and fragments were identified in soil at isolated portions of the site.</p> <p>There were minor exceedances of dissolved heavy metals and ammonia detected in shallow groundwater. These exceedances are considered typical of regional shallow groundwater conditions and are unlikely to pose a risk to human or local ecological receptors.</p> <p>Although concentrations of contaminants of concern were detected above environmental assessment criteria, they were isolated and minimal compared to the wider site footprint. It is anticipated that through integration of the remedial strategies outlined in the RAP by Arcadis (refer to Appendix K) into the bulk earthworks civil design and construction stages of the project, the site can be suitable for the proposed use. The RAP has been endorsed by Zoic Environmental Pty Ltd (Appendix L).</p> <p>It is considered that the objectives of the onsite remediation will be achieved subject to the successful implementation of the actions contained in the RAP, which will enable the site to be made suitable for the proposed residential, commercial and open space uses.</p>
Independent contamination report must be done	The Remediation Action Plan has been endorsed by an accredited Site Auditor. This approach is consistent with the legislative framework for managing contaminated sites in NSW.
Water testing needs to be conducted	Testing of water in the existing dams as well as groundwater on the site has occurred and did not identify any significant issues.
Heritage	
There should be a museum on site for both aboriginal and industrial heritage of the site	Development of the site as a residential community provides a significant opportunity to celebrate the heritage of the site. This will occur through retention and interpretation of key existing structures, as well as a broad range of interpretative measures.
Need to respect aboriginal heritage	Consultation with Registered Aboriginal Stakeholder has occurred in accordance with the relevant guidelines. While this consultation has indicated that creeklines in the area were traditional fishing grounds and retain Aboriginal cultural value for their connection to this activity, no specific or significant Aboriginal cultural features have been identified for the site. The site is heavily disturbed with limited potential for Aboriginal archaeological objects, with only one Aboriginal archaeological site identified on the site. Avoidance or mitigation to impacts of this archaeological were deemed as unnecessary.

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Concerns	Response
	However, there is opportunity to recognise Aboriginal heritage values with interpretative elements included as part of the shared walking and cycling path that will be located in the general area of the Aboriginal archaeological site.
Built form	
Density	
Building height and density too high compared to surrounds. Should be low density village.	<p>The general scale of proposed development has been supported through the Gateway Determination.</p> <p>There has been extensive consultation with Council to evolve the master plan and provide appropriate density and distribution of building heights.</p> <p>The overall density, as well as building heights and FSRs, have been benchmarked against a range of broadly comparable locations and precincts.</p> <p>The development, as proposed would result in revitalisation of the site and up to 750 additional residences, contributing to the housing needs of Corrimal and the wider Wollongong area.</p> <p>The development of a low-density village would represent a significant underutilisation of the site and is inconsistent with broader strategic planning objectives for Corrimal.</p>
Zoning	
The zoning should remain the same and the land should be employment land	<p>The rezoning of the site to support residential use has been supported by Council and DPI&E through the Gateway Determination.</p> <p>An economic assessment has demonstrated that it is not feasible to redevelop the site for industrial/employment uses.</p>
Need retail included	Small scale retail land uses have been included within the proposed development in close proximity to Corrimal train station.
It should be a mixed used zone	<p>See above. Although the zoning is for R3 Medium Density Residential, the site is proposed to accommodate some small-scale residential land uses surrounding Corrimal train station.</p> <p>A mixed-use zone would create potential for an excessive amount of non-residential floor space, potentially detracting from Corrimal town centre.</p>
Detailed Design	
Development should only take place on the dumpsites and cleared areas	The proposed development will be located on areas which have been previously cleared and were previously utilised for industrial activities. Minimal clearing of areas of ecological value is proposed.
Apartment size is too small	All future apartments will be designed to be compliant with SEPP 65 and the ADG.
The artistic impressions of the housing type are not detailed enough	The development is currently only at the rezoning stage. Additional detail regarding housing typologies will be provided at the DA stage.
Should be world class design	The proposed development is intended to be of a high-quality design and an exemplar of urban renewal.

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Concerns	Response
Needs to be age friendly	It is expected that the development will support a wide demographic with a broad spectrum of ages.
Green development	
Needs to include bike paths and improve pedestrian pathways	<p>The proposed development will include a wide network of pedestrian and cycling connections, linking the site with the wider Corrimal community.</p> <p>A regional pedestrian and cycling path will be provided from Railway Street through the site and across Towradgi Creek at the southern end of the site.</p> <p>The existing footpath along the frontage of the site will be upgrade to a shared path.</p>
Building should be a green building, solar panel and water catching	The proposed development is proposed to include sustainable elements; however, these will be detailed at the future DA stage.
Views	
Not impact on residences view of escarpment	<p>The proposed development intends to retain views to the escarpment by proposing heights which are lower than the heights of the existing brick and steel towers.</p> <p>In consultation with Council a view from the east along Murray Street axis has been retained to ensure connection to the west and also views to the escarpment.</p> <p>The master plan has been subject to extensive consultation and review with Council to ensure impacts of views to the escarpment are avoided and minimised. A detailed view analysis has been undertaken and outlined in a view analysis report.</p>
Planning process	
Planning process must be transparent	The PP application has been undertaken in accordance with all legislative requirements. The proponent has conducted consultation that exceeds legislative requirements.
All documents should be made publicly available	All documents will be made publicly available during the exhibition period.
An independent assessment of all reports should be undertaken	Council and the DPI&E will undertake an assessment of the PP as well as all associated documentation.

Appendices

- A Master Plan and Site Visual Analysis
- B Landscape Master Plan Report
- C Gateway Determination and subsequent extension Letter
- D VPA Letter of offer
- E Geotechnical Assessment
- F Geomorphology Assessment
- G Creek Realignment Stability Assessment
- H Supplementary Riparian Corridor Information
- I Flood Study
- J Additional Environmental Assessment
- K Remediation Action Plan
- L Remediation Action Plan Endorsement
- M Flora and Fauna Assessment
- N Flying Fox Camp Proposed Management Actions
- O Flying Fox Strategy Peer Review
- P Threatened Microbat Habitat Assessment
- Q Conservation Management Strategy
- R Noise and Vibration Assessment
- S Aboriginal Cultural Heritage Assessment
- T Archaeological Test Excavation Report
- U Heritage Interpretation Strategy
- V Historical Heritage Assessment
- W Structural Assessment of Existing Structures
- X Servicing Strategy Report Summary
- Y Traffic Impact Assessment
- Z Economic Impact Assessment
- AA Retail Market Demand and Economic Impact Assessment
- BB Illawarra – Shoalhaven: Housing Market Report for Corrimal Coke Works
- CC Consultation Outcomes Report

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A **Master Plan and Site Visual Analysis**

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B Landscape Master Plan Report

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C Gateway Determination and subsequent extension Letter

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D VPA Letter of offer

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E Geotechnical Assessment

ELTON CONSULTING

F Geomorphology Assessment

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G Creek Realignment Stability Assessment

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H **Supplementary Riparian Corridor Information**

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I Flood Study

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J **Additional Environmental Assessment**

ELTON CONSULTING

K Remediation Action Plan

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L Remediation Action Plan Endorsement

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M Flora and Fauna Assessment

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N **Flying Fox Camp Proposed Management Actions**

ELTON CONSULTING

O Flying Fox Strategy Peer Review

ELTON CONSULTING

P **Threatened Microbat Habitat Assessment**

ELTON CONSULTING

Q **Conservation Management Strategy**

ELTON CONSULTING

R Noise and Vibration Assessment

ELTON CONSULTING

S **Aboriginal Cultural Heritage Assessment**

ELTON CONSULTING

T **Archaeological Test Excavation Report**

ELTON CONSULTING

U **Heritage Interpretation Strategy**

ELTON CONSULTING

V Historical Heritage Assessment

ELTON CONSULTING

W **Structural Assessment of Existing Structures**

ELTON CONSULTING

X **Servicing Strategy Report Summary**

ELTON CONSULTING

Y Traffic Impact Assessment

ELTON CONSULTING

Z Economic Impact Assessment

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AA Retail Market Demand and Economic Impact Assessment

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BB Illawarra – Shoalhaven: Housing Market Report for Corrimal Coke Works

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CC Consultation Outcomes Report







Corrimal Coke Works

Draft Development Control Plan

Client: Legacy Property & Illawarra Coke Company

Date: 11 October 2020

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1 Introduction

1.1 Introduction

This locality Chapter is intended to supplement the land use planning controls applied by part B of the WDCP 2009 to guide any future development at the former Corrimal Coke Works. This Chapter of the DCP should also be read in conjunction with Part A, B, C, D and E of WDCP 2009 and the relevant Local Environmental Plan and SEPPs. In the event that the provisions contained in this DCP chapter are inconsistent with the provisions of WDCP 2009, the provisions of this DCP shall prevail.

This chapter recognises the unique qualities of the Corrimal Coke Works, and the ability to deliver unique character precincts, local retail facilities heritage preservation and interpretation, and new, high quality open spaces.

1.2 Aims and objectives

The aim of this chapter is to provide planning controls to facilitate urban renewal of the Corrimal Coke Works site in a manner that is consistent with the Wollongong Local Environmental Plan 2009 (as amended) and the Corrimal Coke Works Master Plan.

The key objectives of this chapter are to:

1. Ensure that development of the site results in an open and welcoming urban environment to facilitate integration and connection between future residents and the wider community.
2. Create a liveable and sustainable residential precinct that provides quality housing with contemporary built form and materials to respond to the character.
3. Ensure that future development respects and celebrates the heritage of the site.
4. Deliver a transport-oriented development adjacent to Corrimal station that promotes increased public transport use.
5. Provide for a neighbourhood scale retail precinct to meet the convenience retail needs of local residents and to activate Corrimal station and the Heritage Plaza.
6. Create a high quality, safe and accessible public domain that provides a range of recreational and community benefits.
7. Define the subdivision structure, character and built form controls for the site.
8. Provide a permeable network of streets in order to integrate the site with the wider Corrimal area and Corrimal train station.
9. Encourage walking and cycling by providing high quality walking and cycling paths, including a broader regional connection through the site.
10. Manage and where appropriate, enhance, the important environmental attributes of the site to ensure future sustainability and management of the site.
11. Ensure that development of the site is sensitive to surrounding areas, including the protection of key view corridors to the Illawarra Escarpment.

1.3 Definitions

For all definitions refer to **Appendix 4: Definitions of the Wollongong Development Control Plan 2009.**

1.4 Relationship to Other Chapters of the DCP

This chapter of the DCP provides the development controls for the site. This Chapter of the DCP should also be read in conjunction with Part A, B, C, D and E of WDCP 2009 and the relevant Local Environmental Plan and State Environmental Planning Policies (SEPPs). In the event that the provisions contained in this precinct plan are inconsistent with the provision of WDCP 2009, the provisions of this precinct plan shall prevail.

In essence, this DCP overrides the DCP chapter as outlined in **Table 1** below due to the specific aspects of the site.

Table 1 DCP Chapters that are not applicable to this site

Current chapter	Controls that will not apply to this site
B1 – Residential Development	The entire chapter
D1 – Character Statements	The entire chapter
E3 – Car Parking Access Servicing Loading facilities	<p>7.1 Car Parking, Motor cycle, Bicycle Requirements and Delivery /Servicing Vehicle Requirements</p> <p><i>1. The car parking, motorcycle and bicycle requirements for specific land uses / developments are contained in Schedule 1 to this chapter of the DCP.</i></p> <p>7.7 Car Parking Layout and Design</p> <p>7.8 Basement Car Parking</p> <p>7.13 Car Parking and Access Requirements</p>
E10 – Aboriginal Heritage	4.3 Preparation of the Aboriginal Archaeological and Cultural Heritage Assessment Report
E11 – Heritage	<p>11. Subdivision</p> <p>12. Alterations and additions to heritage listed buildings or buildings within heritage conservation areas</p> <p>13. Infill development</p> <p>15. Adaptive reuse of a heritage building</p>
E23 – Riparian Land Management	Section E23 Section 6

If there is a conflict between this chapter and the other chapters of the WDCP 2009, this chapter prevails.

2 Land to which this DCP applies

This chapter applies to land identified as the Corrimal Coke Works site, located off Memorial Drive and Railway Street, Corrimal (Lot 1 DP 795791, Lot 5 DP 749492, Lot 126 DP 598190 and Lot 11 DP749492), as identified in the figure below.

Figure 1 Land to which this plan applies



3 Vision and Development Concept

A concept master plan for the Corrimal Coke Works has been prepared to demonstrate the holistic vision, key outcomes and principles that will guide the future development of the site.

Figure 2 Corrimal Coke Works Master Plan



3.1 Vision

Corrimal Coke Works will become a community born of diverse people, places and stories that respects the site's past while showcasing Wollongong's future as the city of innovation.

The site will be revitalised as a place for people that provides a mix of housing choices within a landscape and heritage setting, while leveraging its accessibility to services and public transport.

3.2 Key planning and design principles

The key principles can be interpreted in the following way:

- » Deliver a public open space network that establishes the character of the site and provides a range of recreational opportunities.
- » Provide diversity in the design of built form and public realm to ensure the character of the site reflects a varied cohesive group of design elements and different character precincts.
- » Integrate the site's heritage through the retention, interpretation and adaptive re-use of key elements so to respect the history of the site.
- » Design a safe and functional road network within the site including the provision of adequate on street parking and opportunities to access the station.
- » Establish a new access to the site from Railway Street to connect with the surrounding Corrimal community and facilitate improved access to Corrimal station.
- » Deliver a site that is highly permeable to pedestrians and cyclists, linking the precinct to surrounding amenities, services and facilities, public transport, recreational opportunities and beach and broader regional pedestrian/cycle path network.
- » Create a residential community with housing diversity, neighbourhood hub and commuter services near the train station for added convenience, but at a scale that does not detract from the Corrimal town centre.
- » Conserve, manage and improve existing native vegetation and biodiversity located in the south of the site, as well as facilitate a new realigned riparian corridor.
- » Allow for earthworks to improve and manage the drainage across the site, and realign North Corrimal Creek to improve flood management and ultimately enhance the biodiversity values on the site.
- » Deliver serviced development with appropriate infrastructure services.
- » Maintain key views to the escarpment from the east, particularly the ridgeline and along key streets
- » Establish key view corridors through the site, in particular along the Murray Street alignment and views towards the iconic brick chimney.
- » Locate higher buildings away from the station area in order to ensure the heritage character and significance around the brick chimney is respected.
- » Ensure that public spaces receive appropriate levels of solar access, are not excessively overshadowed, and are safe and accessible.
- » Manage flooding and stormwater, and incorporate water sensitive urban design principles
- » Enable the built form and the natural environment to co-exist, including appropriate buffers from the core of the occasional flying fox camp.
- » Ensure that future development complies with universal design and Crime Prevention Through Environmental Design (CPTED) principles.

3.3 Elements of the Master Plan

The key elements that have been incorporated into the indicative master plan include:

- » Delivering a diversity of housing with the potential for low-scale apartment buildings, including affordable housing and the potential for seniors living/aged care, as well as opportunities for strata titled townhouses.
- » Providing approximately 9 hectares of open space to offer a range of community and recreational resources, including a 3,000sqm Village Park, 5,150 sqm Southern Recreation Park and riparian corridor promoting walking and cycling.
- » Ensuring architectural diversity, including articulation and modulation of built form, that responds to the local context through creation of a range of character precincts within the site.
- » Maintaining key views to the escarpment, including a significant view corridor along Murray Street, while also establishing new views within the development to key heritage features.
- » Maximising access to Corrimal train station and delivery of a public plaza adjacent to the station with retention of key heritage structures and interpretive heritage elements.
- » Activating Corrimal train station and the heritage precinct with neighbourhood scale retail uses at a scale that does not detract from Corrimal Town Centre and East Corrimal shops.
- » Providing flexible community and business space within the heritage precinct, including the potential for tele-working and start-ups.
- » Realignment and rehabilitation of North Corrimal Creek to establish a new riparian corridor with enhanced biodiversity outcomes, while providing a large area of PMF-free contiguous developable area.
- » Providing a regional walking and cycling path through the site, connecting Railway Street in the north across Towradgi Creek to the south.
- » Retention of key ecological areas within the southern section of the site and integrated with the riparian corridor, including provision of a suitable buffer to the occasional grey headed flying fox camp.
- » Providing a suitable buffer distance from the rail and state road corridors and appropriate landscaping adjacent to the site for noise attenuation.
- » New site access from Railway Street with a proposed roundabout at the intersection of Harbinger Street.

4 Character statement

4.1 Existing character

The site currently contains areas of derelict industrial structures, large former stockpiling areas and more natural areas of native and exotic vegetation to the south. The site is not accessible to the public and presents as a derelict former industrial site.

Existing industrial structures are clustered in the north-east corner of the site and reflect the site's past industrial character. There has been widespread vandalism across the site since industrial operations ceased and many of the existing buildings and structures are in poor condition.

The site is highly modified as a result of its past industrial uses, including realignment and damming of North Corrimal Creek to supply water for the coke works, and establishment of many large berms often containing discarded building materials.

The southern part of the site is less disturbed by industrial activities and has a more natural, vegetated character including an area of Illawarra Lowlands Grassy woodland.

The site is generally characterised by mature trees that present a natural green buffer on the boundaries of the site.

4.2 Desired Future Character

4.2.1 Overview

Corrimal Coke Works will be developed as a contemporary urban village, providing medium density housing within a high quality public domain and reflecting the unique heritage of the site.

Corrimal train station and the adjacent heritage precinct will be activated with neighbourhood scale retail uses, as well as flexible community and business space. This will create a high quality environment with improved accessibility that promotes public transport use. Provision for a bus loop through the site will further encourage reduced car use.

The development will encourage walking through pleasant streets and the provision of key off-road shared paths, to Corrimal Station and also connecting through the site to the Towradgi Creek corridor. Importantly, the site will be open and permeable to invite the broader community to use its public spaces and facilities.

The environmental values of the site will be restored and enhanced, including establishing a new riparian corridor with increased biodiversity, and management of retained vegetation areas.

4.2.2 Character Precincts

Corrimal Coke Works will be developed with five character precincts that relate to key features of the master plan and are intended to ensure diversity and interest in the future built form.

Figure 3 Character Areas



Civic Hub

The Civic Hub will provide an urban and active character, reflecting the concentration of heritage elements and its proximity to Corrimal station. Buildings are built to the street boundary to activate the street, while the architecture will reflect the industrial history of the site through extensive use of masonry combined with finer grain metal elements.

The Heritage Plaza will provide a high quality public domain at Corrimal station and a focal point for the retention, interpretation and adaptive re-use of key heritage structures.

Retail uses are located within the Civic Hub to activate the Heritage Plaza, Corrimal station and provide amenity for local residents.

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Treetop Escarpment

The Treetop Escarpment precinct forms the entrance to the site from Railway Street, with buildings screened by the established green buffer that is retained in a generous landscape setback. The landscape character will extend with new trees planted between building forms and within private courtyards of residential apartment buildings.

The built form is designed with strong vertical elements to emphasise the pattern of surrounding trees, while rooflines vary in order to reflect the character of the Illawarra Escarpment. A simple palette of materials complements the natural context of this precinct.

Green Edge

The Green Edge precinct fronts the vast riparian corridor and green open space at the south of the site, providing a key interface between the natural and built elements of the site. New buildings will respond to this natural interface, providing a sense of openness and characterised by a lightweight materiality including fine grain timber elements and screens. Buildings will promote passive surveillance, while creating appropriate privacy for residential dwellings.

The higher buildings on the site are located in this precinct, providing significant separation to existing residential areas outside of the site. The upper levels of buildings will be setback to minimise overall scale and designed with different materials and colours.

Village Park

The Village Park precinct is focused around the neighbourhood Village Park and provide a place for local connections at the heart of the site.

The built form reflects an urban village character, reflecting some of the character of the Civic Hub but utilising more contemporary materials and architecture.

Riparian Precinct

The Riparian Precinct provides a natural and functional green edge to the site, providing a significant connection with the natural environment.

Its natural biodiversity qualities will be retained, restored and enhanced, while offering opportunities for passive recreation and providing a significant walking and cycling connecting through the site. These outcomes are achieved while satisfying functional flood management and engineering requirements.

The Southern Recreation Park provides a dedicated recreation space for community enjoyment, while providing an appropriately defined interface to more ecologically sensitive areas.

5 Development controls and guidelines

This chapter of the DCP provides the subdivision and built form controls for the Corrimal Coke Works.

Objectives

1. Supplement existing controls for residential development provided in WDCP 2009.
2. Provide design controls that define setbacks and articulation to create an appropriate street interface.
3. Ensure good quality and innovative architectural designs that reflect the character precincts across the site and avoid a bland and monotonous architectural style.
4. Provide for adequate amenity and services for the residential dwellings.
5. Support activation of Corrimal station and the adjoining Heritage Plaza with appropriate retail/business uses.
6. Provide for adequate car parking on the site for various uses, while reflecting the opportunity for transport-oriented development because the site is directly adjacent to the Corrimal railway station.
7. Ensure appropriate access is provided for each of the buildings on the site whilst minimising the impacts on the public domain from parking structures.
8. Minimise on street parking for residents in order to allow visitors to use on street parking.

5.1 Views and Vistas

Development Controls

New development will establish or maintain key view corridors as indicated in **Figure 4**.

1. The Brick Chimney is of significance and a landmark and therefore views to and from the stack should be conserved or prominently seen from Corrimal Railway Station, Railway Street and Towradgi Road overpass.
2. Ensure key view axis from Railway Street to the Brick Chimney are retained to ensure heritage items are prominent, as well as visual axis aligned with Murray street (to the east) and a new east west road within the site to view the Brick Chimney as shown in Figure 4.
3. New development (not in existing structures) adjacent to the Brick Chimney within the heritage curtilage should be stepped back in its form, or have an appropriate setback/buffer zone
4. Buildings adjacent to the rail corridor shall utilise a natural colour and material palette to blend into the backdrop of the Illawarra Escarpment when viewed from East Corrimal.
5. Create a green link corridor through the site in order to link the southern Park to the northern tree lined edge.
6. New commercial buildings should be positioned and be of a two level scale or set back so to ensure views along the visual axis from Railway Street to the Brick Chimney

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Figure 4 Views and vistas



MAINTAINED VIEW CORRIDORS

Legend

- The Site
- View Corridor to Heritage
- Murry Road New Corridor
- Western View Corridors to Riparian Corridor and Escarpment
- Central View Axis

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5.2 Residential Development

5.2.1 Setbacks

Development controls

1. Building setbacks are to comply with **Figure 5**.
2. Upper level front setbacks shall be generally in accordance with SEPP 65 and **Figure 6**.
3. Up to 70% of the articulation zone can be occupied by architectural elements. Articulation zone means a zone forward of the Main façade line that may include architectural features such as pergolas, roofs elements, bay windows, sun shading and verandahs.

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Figure 5 Built Form Setback



Figure 6 Built Form Upper Setbacks



UPPER LEVEL SETBACK

Legend

- The Site
- A: 3m from below facade line above 5 storeys

5.2.2 Architectural Diversity and Building Design

Development controls

1. The design of new buildings is to achieve architectural diversity reflecting the character precincts in accordance with **Table 2**.

Table 2 Building design

	Treetop Escarpment	Civic Hub	Village Park	Green Edge
Primary Façade	Render	Masonry	Render / cladding	Cladding
Base Element	Masonry	Masonry	Masonry / render	Masonry / render
Feature Elements	Timber / metal	Metal	Timber / cladding elements	Metal screens / Façade greening
Balustrade	Glass	Metal / masonry	Glass / solid	Glass
Roof Form	Varied	Flat	Flat	Varied
Rhythm	Vertical	Horizontal	Vertical	Horizontal
Colour Palette	White / Neutral	Brown / Grey	Natural / colour highlights	Natural

2. The design of new development must have particular regard to the topography of the site to minimise the extent of cut and fill associated with building construction.
3. Retained heritage items must be integrated into the design of the built form. Any retained coke ovens and the remnant wall of the powerhouse, in particular, must be incorporated into the overall design.
4. Design of new buildings should minimise the overall sense of bulk and scale, with maximum building length to comply with the Apartment Design Guide. The use of landscaping, articulation, balconies, sun shading devices and awnings will help reduce the apparent bulk of buildings.
5. Provide for a mix of built form styles, whether north south orientated, courtyard type style or L-shaped, or articulation in the built form in order to reduce monotony in built form.
6. Entrances must be visible at eye level from the street and well lit.
7. Buildings directly adjoining open space, such as the Village Park or Riparian Corridor, must be designed with an appropriate interface and frontage to the open space.
8. Establish a variety of built form heights, with lower heights closer to the railway station, and higher towards the creek corridor, and varying heights within the northern and southern parts of the site.
9. Buildings adjoining the rail corridor are to be designed with consideration of their presentation to the rail corridor. The use of landscaping to screen these buildings and create a green edge to the rail corridor is encouraged.

5.2.3 Rail and Road Noise

Development Controls

1. Acoustic treatments shall be provided, where required, in accordance with the Environment Noise and Vibration Assessment (Renzo Tonin, 2019) to address noise impacts from the rail corridor and Memorial Drive. Site specific acoustic assessments may be undertaken for any development application where alternative measures or treatments are proposed.

5.3 Non-residential development

Development Controls

1. Non-residential land uses, primarily retail and business uses, are to be located in close proximity to Corrimal station and the Heritage Plaza, as in accordance with **Figure 7**.
2. Buildings fronting the Heritage Plaza shall comprise of predominantly non-residential uses on the ground floor to ensure activation and surveillance of the public space.
3. Non-residential uses may spill out into the Heritage Plaza as generally shown in **Figure 8** and be managed by the owner.
4. Any neighbourhood supermarket should be located and designed to minimise the extent of blank and non-active frontage to public streets and open space. The indicative location for a neighbourhood supermarket is shown on **Figure 7**.
5. Development within the Heritage Plaza shall integrate with the heritage of the site, through the interpretation of the heritage buildings as well as through landscaping.
6. Awnings should be included in developments associated with the proposed retail uses, however are not necessary when a current heritage structure is being interpreted and adapted for retail and business purposes.
7. Signage on heritage structures adapted for the reuse should integrate and reflect the heritage character of the structure.
8. Access areas into buildings, including for loading, shall be generally in accordance with that shown on **Figure 9**.
9. All loading and unloading activities shall take place wholly within the designated loading bay, at all times, except for the uses associated with the adaptive re-use of heritage structures within the Heritage Plaza.
10. The designated loading / unloading area for the local retail use shall be kept free for that purpose, at all times.
11. All loading dock facilities must satisfy on-site manoeuvring areas for trucks in accordance with the Australian Standard AS 2890.2 Design Vehicular and Turning templates.
12. Loading / unloading facilities shall be located or screened so they are not directly visible from the adjoining residential area and do not transmit excessive noise onto any adjoining residential area.
13. An acoustic assessment report may be required at the discretion of Council with a Development Application where loading dock facilities are proposed to be positioned in proximity to any adjoining noise sensitive land uses including residential dwellings.

If required, the acoustic assessment report must address predicted maximum noise levels from loading and unloading activities conducted within the loading dock facility of the development and provide recommendations on acoustic attenuation measures required to mitigate any unacceptable noise impacts. The noise impact assessment report should apply the NSW EPA's "Noise Policy for Industry" maximum noise level assessment when assessing sleep disturbance.

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Figure 7 Indicative Land Use



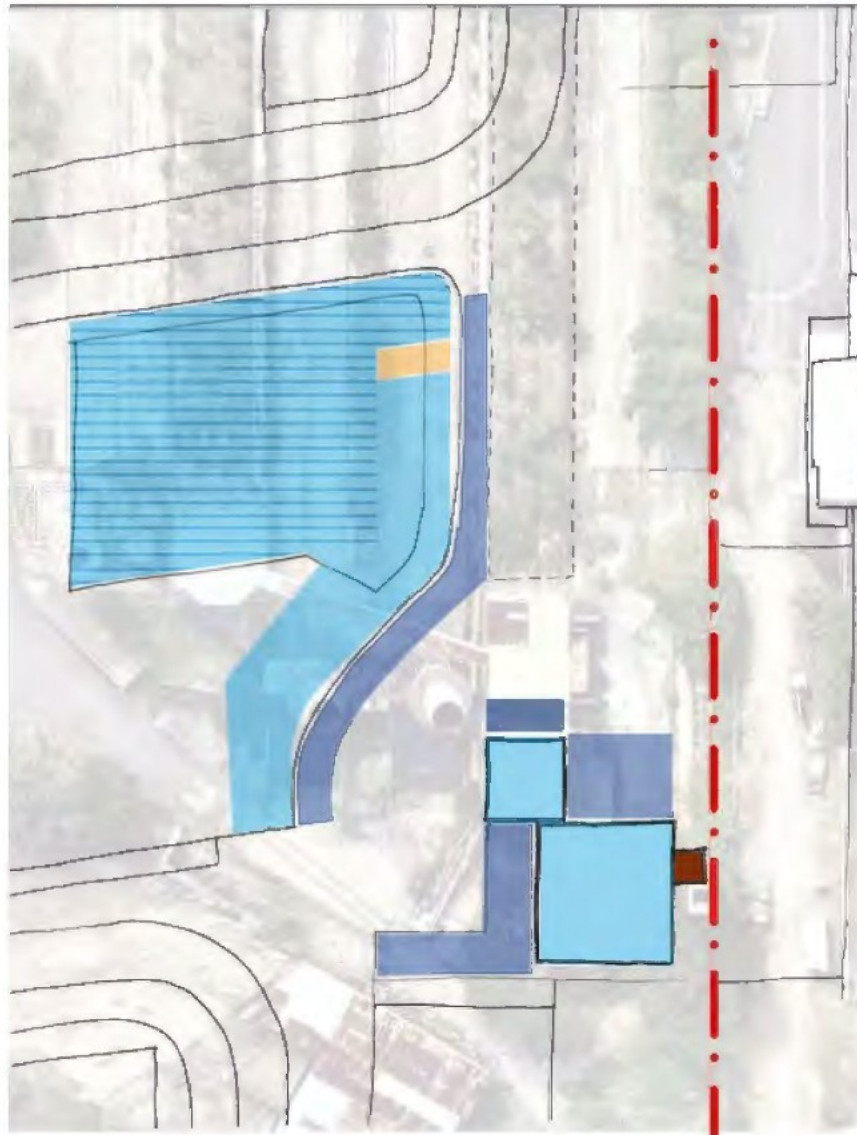
INDICATIVE LAND USE

Legend

- The Site
- Residential
- Non-residential Uses

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Figure 8 Indicative Uses and Spill out zone



INDICATIVE USES AND SPILL OUT ZONES
Legend



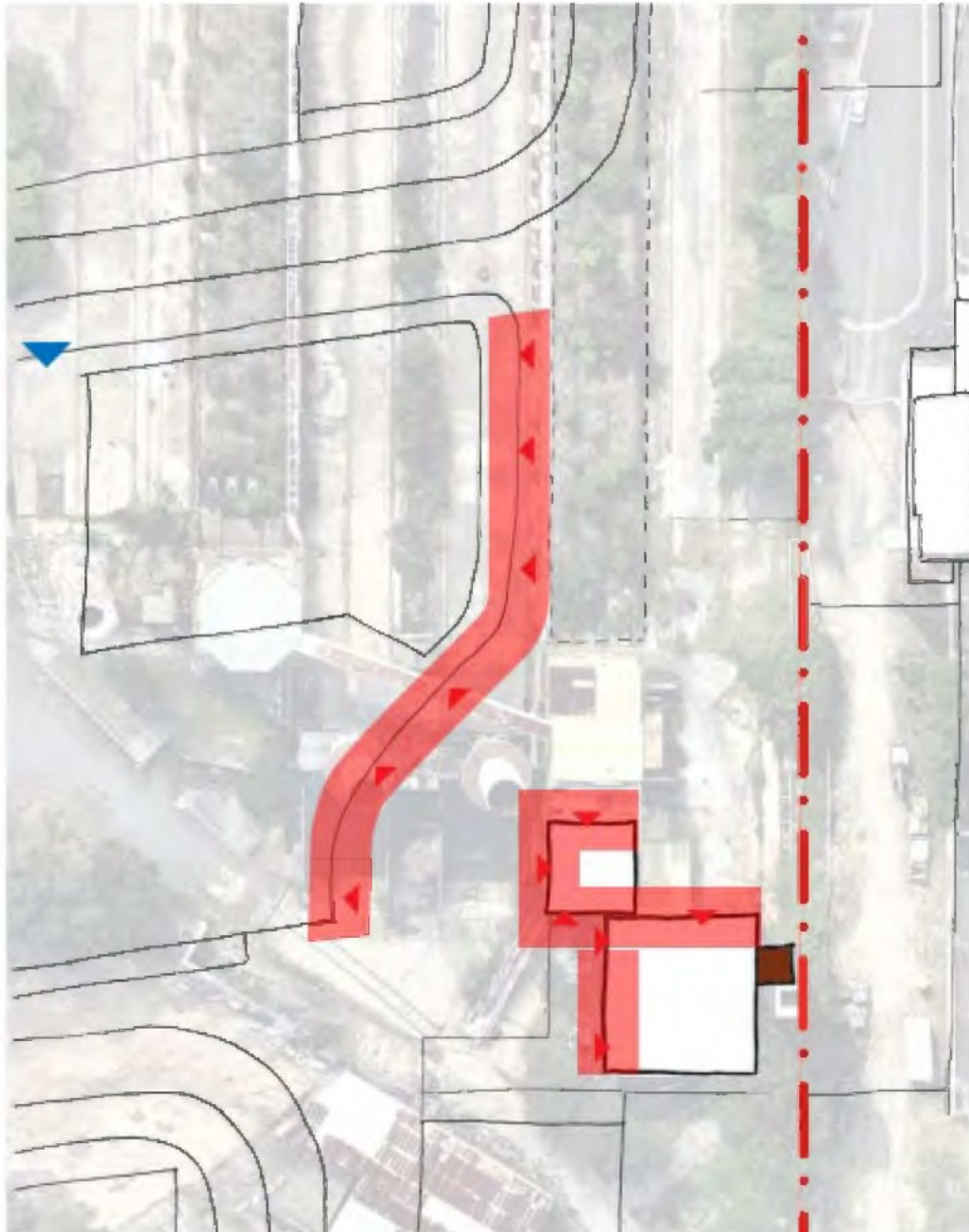
- | | | |
|---|---|---------------------------------|
|  |  | INDICATIVE RETAIL TENANCY |
| |  | INDICATIVE SPILL OUT ZONE |
| |  | INDICATIVE LOBBY |
| |  | INDICATIVE SUPERMARKET LOCATION |

Figure 9 Active frontages and loading



ACTIVE FRONTAGES AND LOADING

Legend

- ACTIVE FRONTAGE
- INDICATIVE ENTRY
- INDICATIVE LOADING ACCESS

5.4 Car parking and vehicular access

Development Controls

1. Vehicular access to development lots should be integrated with the design of the building and located to minimise impacts on adjoining streets.
2. Car parking for residential flat buildings may be provided in the form of a full basement, 'half in half out' basement or sleeved above ground.
3. On site car parking must be positioned to minimise impacts on the streetscape. Car parking must be located behind the building setback and be screened from view with well-designed structures and vegetation.
4. Car parking in residential flat buildings areas should be designed to conveniently, efficiently and appropriately serve residents and visitors of the site. This can be achieved in the following ways:
 - a) Ensuring that car parking areas are located close to entrances and access ways.
 - b) Car parking areas to be secure yet easily accessible for all residents.
 - c) Have clearly defined areas for visitor parking and disabled parking.
5. Parking for cars, motorcycles and bicycles other than outlined in **Table 4** must be provided and designed in accordance with the requirements contained in Traffic, Access, Parking and Servicing Chapter contained in Part E of the DCP.
6. Parking is to be provided for the site in accordance with **Table 4** below.

Table 4 Parking Controls

Land use		Car Parking Rates	Motorcycle Parking	Bicycle spaces
Residential Flat Building, and shop top housing	1 bedroom	1 space per dwelling (<70m ²)	1 motorcycle space per 15 dwellings	1 bicycle space per 3 dwellings
	2 bedroom	1.5 car space per dwelling (70-110m ²)		
	3 bedroom	2 car spaces per dwelling (>110m ²)		
	visitors	0.2 spaces per dwelling for visitors	N/A	1 bicycle space per 12 dwellings
Retail		1 space per 25m ²	1 motorcycle space per 25 car parking spaces	1 bicycle space per 750m ² GFA for staff plus 1 space per 1,000m ² GFA for shoppers
Restaurant		1 space per 25m ²	-	-
Food and drink premises		1 space per 25m ²	-	-

7. The layout of all residential flat building and non-residential car parking areas shall be in strict accordance with Australian Standard AS 2890 and the following additional requirements:
 - a) Parking areas must be designed so any vehicle which uses the area will be able to enter and leave the site in a forward direction.
 - b) Stacked or tandem parking may be permitted in residential flat building or non residential buildings in the following circumstances:
 - i. The applicant must demonstrate that there is a need for stacked parking and that the provision of stacked parking will not adversely affect the safe, efficient and effective use of the site;
 - ii. No more than two cars are parked in a stacked arrangement;
 - iii. Stacked parking should be limited to a maximum of 50% of all parking
 - iv. Provision shall be made on site for shifting cars without the movement of vehicles onto public streets;

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- v. Residential: only permitted where both spaces are utilised by the same dwelling and such spaces do not interfere with common manoeuvring areas;
8. On street parking should be subject to time restrictions in accordance with **Figure 10 and 11**. This is intended to avoid the potential for all-day commuter parking and promote the turnover of on street parking spaces for visitors.

Figure 10 Parking

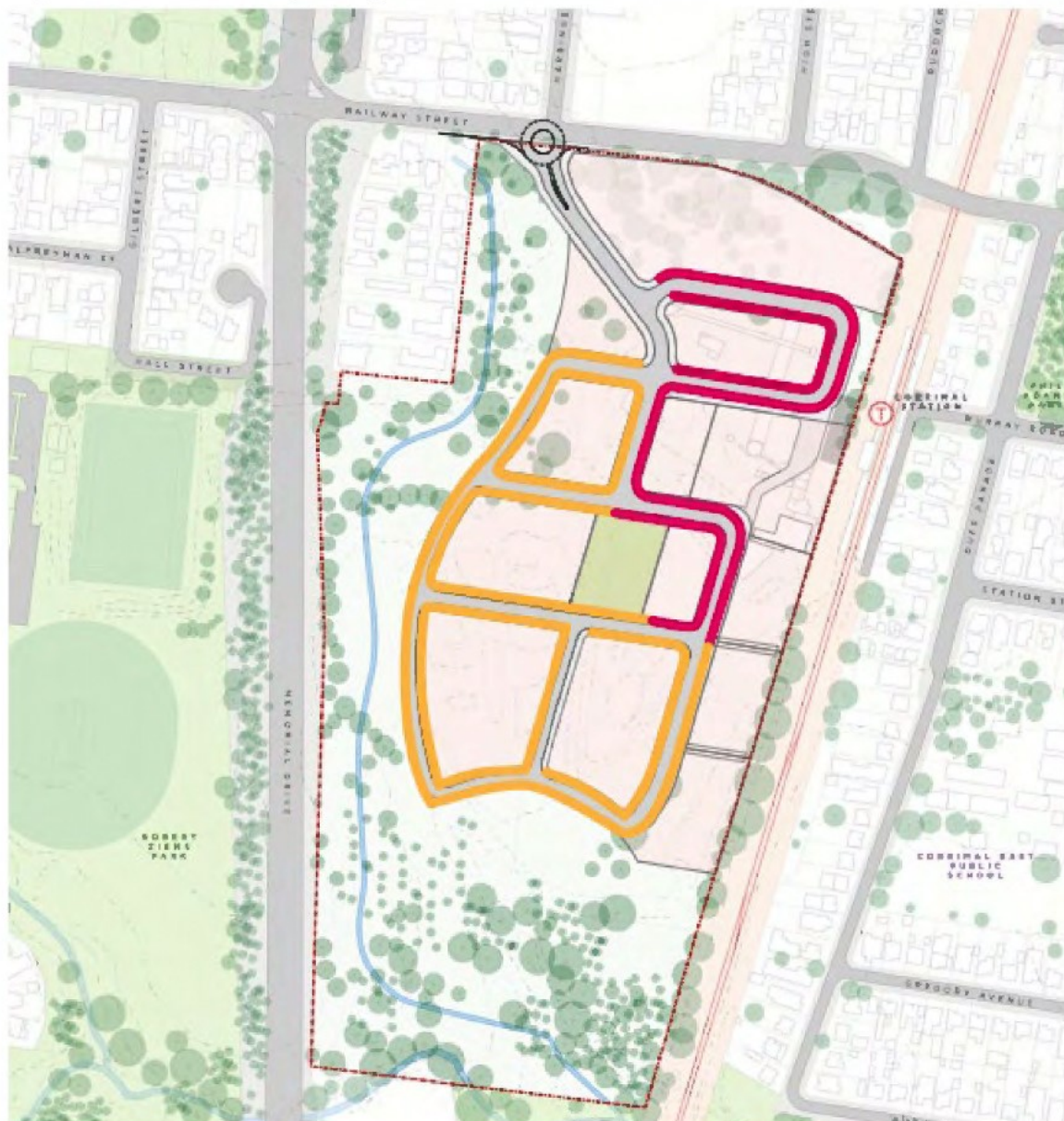


PARKING AND ACCESS
Legend

- The Site
- No On-Street Parking
- Station Parking
- Controlled Parking on Open Space Edge
- B Bus Stop

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Figure 11 On street car parking



PARKING AND ACCESS

Legend

- The Site
- 1 Hour Retail Precinct Parking Limit
- 4 Hour Parking Limit

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9. The design of basement car parking should support the use of natural ventilation where possible.
10. The design of basement car parking should be integrated with the overall design of the development and limiting the extent to which the podium extends beyond the building footprint will minimise the impact of the basement parking areas on the streetscape.
11. Any basement or part basement that is above ground level should not be greater than 1.2m above finished ground level. On sites with significant slope it may be acceptable to exceed this height provided appropriate design measures are included to minimise the impact on the streetscape.
12. The following setbacks from side and rear boundaries apply to basement podiums:
 - a) Where the height of the basement podium (measured to the top of any solid wall located on the podium) is less than 1.2m above natural or finished ground level (whichever distance is greater), the basement podium may extend to the property boundary. A minimum 1.5m wide landscaped planter must be provided on the perimeter of any section of the basement podium which is located on a side or rear property boundary. Such planter must prevent direct access to the outer edge of the podium, to minimise direct overlooking of adjacent dwellings and open space areas.
 - b) Any portion of the basement (measured to the top of any solid wall located on the podium) which exceeds 1.2m above natural or finished ground level (whichever distance is greater) must be setback from the property boundaries by a ratio of 1:1 (height: setback). A minimum setback of 1.5m applies in this instance, with this area to be landscaped.
13. Ventilation structures/openings/exhausts for basement parking and air-conditioning units must be orientated away from windows of habitable rooms and private open space areas on the subject site as well as adjoining sites. They must be designed to minimise any visual or amenity impacts on adjoining public domain. Ventilation grills must be integrated into the design of the façade of the building to minimise their visual impact.
14. The visual impact of all basement walls must be minimised through the use of various design techniques including well-proportioned ground level articulation and relief, mixed finishes and materials, terracing and/or dense landscaping.
15. Waste collection vehicles may enter building basements from rear lanes to collect waste and/or recyclables subject to the following requirements:
 - a) Compliance with Australian Standard AS 2890.2 Parking Facilities: Off-Street Commercial Vehicle Facilities;
 - b) The height to the structural members and upper floor ceiling should allow for collection vehicle travel height/operational height, consistent with the type of vehicle nominated as the waste collection vehicle;
 - c) Adequate provision of space clear of structural members or vehicle parking spaces to allow a typical three-point turn of collection vehicles or alternatively, provision should be made for a truck turn table within the basement car parking area; and
 - d) The basement floor should be of industrial-type strength pavement and designed for a maximum wheel loading of seven tonnes per axle to accommodate garbage and recycling collection vehicles.

6 Heritage

This chapter should be read in conjunction with Chapter E11 of the DCP.

Objectives

1. Integrate and reflect the heritage significance of the site through conservation, interpretation and management within the planning and development of the curtilage area of the site.
2. Ensure that any development undertaken within the heritage curtilage is undertaken in a manner that is sympathetic and responds to the heritage character of the site.
3. Celebrate and interpret the heritage significance of the site in the design of buildings and open space in a manner that contributes to a broader understanding of the site's history and function.
4. Provide opportunities for public access to, and appreciation of, retained heritage items within the curtilage.
5. Create opportunities for adaptive reuse or re-purposing of significant heritage items.
6. Encourage innovative approaches to the conservation and interpretation of the heritage items on the site.
7. Maintain and establish significant views to the Brick Chimney, and ensure the Brick Chimney is a key built form on the site.
8. Provide for interpretation of Aboriginal heritage values in development of the site.

Development Controls

1. Retention, interpretation and removal of existing structures on site shall generally be in accordance with the Conservation Management Strategy and Heritage Interpretation Strategy (Urbis 2019)
2. Retention and re-use of existing structures will have regard to the condition of those structures and their suitability within a residential context in relation to safety, visual and physical connectivity and potential vandalism.
3. The salvage and re-use of materials from existing structures shall be included in future development outcomes, where possible.
4. The physical remains of the remnant tramway do not need to be retained, however interpretation of its historical value should be considered in the design.
5. A Heritage Impact Statement shall be submitted with development applications within the heritage curtilage area. The Heritage Impact Statement should be accompanied by a structural engineering report on the condition of items being addressed.
6. The existing industrial structures on the site shall be recorded through a photographic archival recording in accordance with NSW Heritage Council guidelines.
7. Identified Aboriginal heritage shall be managed in accordance with the Aboriginal Cultural Heritage Assessment (Kelleher Nightingale Consulting, 2019). Interpretation of identified Aboriginal heritage is encouraged and should occur in consultation with Registered Aboriginal Parties.
8. Further archaeological assessment associated with existing industrial structures may be required as part of future Development Applications where relevant.

7 Public Domain

The Corrimal Master Plan provides a range of new parks and open spaces for the community, integrated with the riparian corridor, biodiversity, cycle and pedestrian connections, proposed residential development and local retail hub.

These open spaces and public realm will cater for a variety of informal recreational areas, community activities and spaces for people, including older and younger residents and visitors.

Objectives

1. Provide a range of open space as identified in the master plan to create a high quality public domain and cater for a wide range of user needs.
2. Establish attractive and safe walking and cycle links across the site that also provide opportunities for regional linkages.
3. Incorporate heritage interpretation or public art to enhance place making.
4. Retain the existing tree line character along Railway Street as a green buffer to the development.
5. Establish a new riparian corridor that provides a significant natural edge to the development and provides a range of passive recreational opportunities.
6. Ensure that new public spaces are safe and accessible.
7. Provide for appropriately managed access to the site from Railway Street
8. Define a fine grain network and hierarchy of streets, lanes and pedestrian links to ensure connections within and out of the site.
9. Facilitate the potential for transport-oriented development and interchange at Corrimal train station, including providing a high quality public domain at the station.
10. Provide a conservation outcome for the occasional Grey Headed Flying Fox camp that avoids and minimises the potential for conflict with future residential development on the site

7.1 Public open space areas

Development Controls

1. New open spaces are to be provided in accordance with **Table 5**:

Table 5 Proposed park typologies

Open Space	Total Area (ha)	Ownership
Heritage Plaza	0.3ha	Private
Village Park	0.3ha	Council
Southern Recreational Park	0.5ha	Council
Riparian Corridor	7.7ha	Council

2. A landscaping plan is to be submitted with the development application for any of the open space and public realm areas, and generally be consistent with the landscape concept plan below.
3. The design of new open spaces shall have regard to ongoing maintenance requirements, universal design and CPTED principles.

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4. Locational and interpretive signage, and appropriate lighting shall be provided as part of the open space system
5. All open spaces shall provide high quality and robust park furniture to allow the functional use of the open space by the community.
6. Ensure that any servicing facilities are located to minimise impacts on the public domain. Any substation located within the public domain is to be positioned to minimise its functional and visual impact, and appropriate screened.

Figure 12 Indicative Landscape Plan



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7.1.1 Heritage Plaza

The Heritage Plaza is intended to have an urban character and function. The primary functions of the Heritage Plaza are to:

- Create a safe and accessible public domain that provide connectivity to Corrimal train station
- Provide a community space that can be activated by adjoining neighbourhood retail uses
- Celebrate existing heritage features through appropriate retention and interpretation

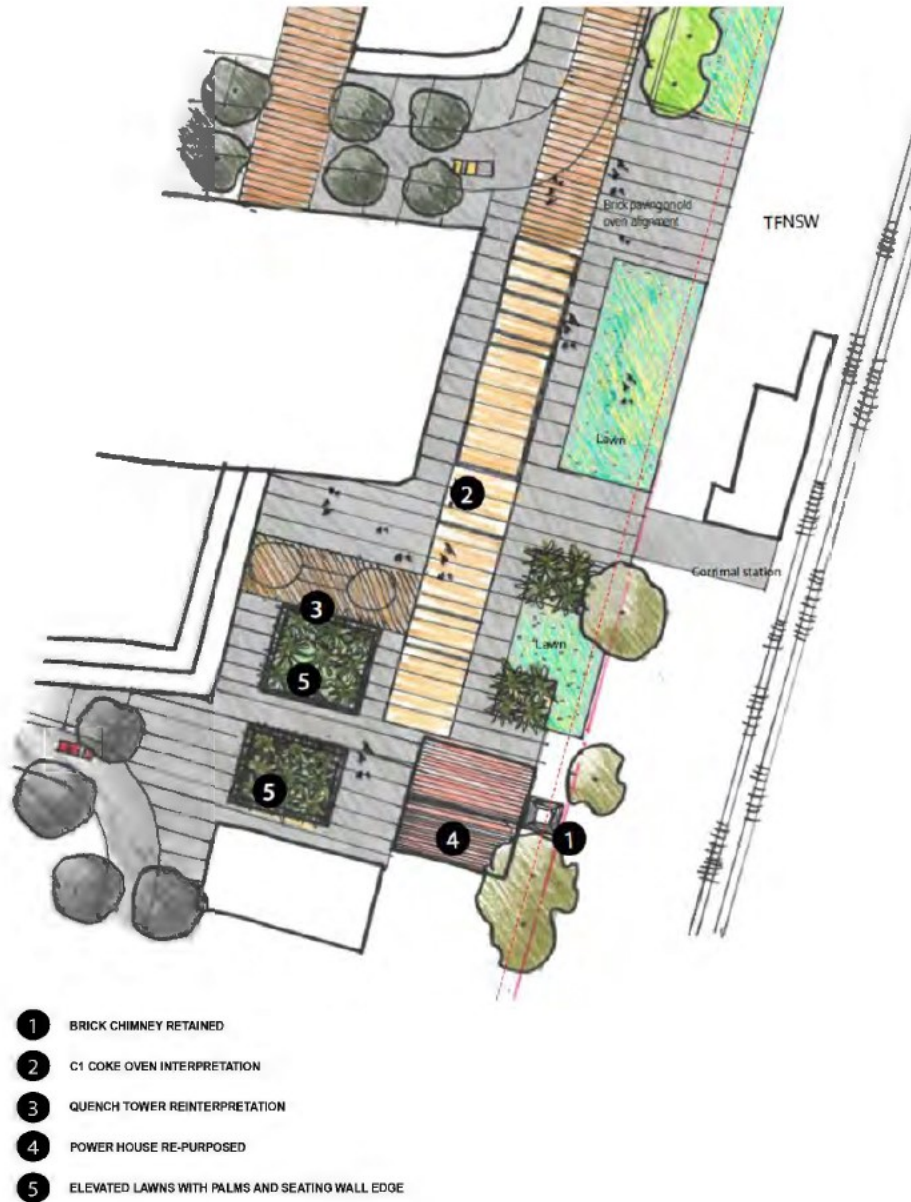
Development Controls

Key design controls for the Heritage Plaza include:

1. The design of the Heritage Plaza shall generally be in accordance with the concept provided in **Figure 13**.
2. The design of the Heritage Plaza is to be appropriately integrated and reflects key heritage elements, in particular retaining and interpreting elements of the former industrial use and materials as part of the public realm in accordance with the Conservation Management Strategy and Heritage Interpretation Strategy (Urbis, 2019).
3. Provide a high level of physical and visual permeability to facilitate access to Corrimal station and activation of the Heritage Plaza, generally in accordance with **Figure 14**.
4. Facilitate direct access to Corrimal train station, noting that any works within the rail corridor will require the approval of RailCorp.
5. Provide appropriate lighting to promote a sense of safety.

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Figure 13 Indicative concept design – Heritage Plaza



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Figure 14 Permeability



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7.1.2 Village Park

The Village Park is a neighbourhood scale recreational space that is intended to encourage community interaction and facilitate unstructured recreation activities for a range of demographics. The key functions of the Village Park are to:

- Provide open lawn areas suitable for unstructured recreation activities and encourage community interaction
- Incorporate a defined play space

Development Controls

1. A minimum 50% of the park is to be provided as open lawn area.
2. Incorporate a designated playground area with an appropriate range of equipment as well as formal seating areas, as generally indicated in **Figure 15**.
3. Provide a separate access path for any residential dwellings that front onto the park, with appropriate landscape screening to create appropriate privacy but simultaneously ensuring surveillance of the park.
4. The Village Park shall receive at least 4 hours of solar access to at least 70% of its area all year round. Solar Access Studies showing the extent of overshadowing on the Southern park between 9am and 3pm on 21st June are to accompany all development applications.

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Figure 15 Indicative concept design – Village Park



7.1.3 Southern Recreation Park

The Southern Recreation Park is an informal recreational area intended to provide a transition area between the occasional flying fox camp core and the residential built form by encouraging integration and respect of the environment. The primary functions of the park are to:-

- » Provide open lawn areas suitable for unstructured recreation activities and encourage community interaction
- » Incorporate a defined play space

Development Controls

1. A minimum 30% of the recreation area is to be provided as open lawn area.
2. There shall be a southern edge defining element, whether a fence, a pathway or the like, to provide adequate distance to the natural bushland and respond to the topography of that part of the site. A viewing platform may be incorporated on the park edge.
3. Recreational area should be designed to have view of a design feature or view of the natural bushland from the central north-south local road.

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4. The Southern park shall receive at least 4 hours of solar access to at least 70% of its area all year round. Solar Access Studies showing the extent of overshadowing on the Southern Park between 9am and 3pm on 21st June are to accompany development applications.

7.1.4 Riparian Corridor

The Riparian Corridor is established through a realignment of North Corrimal Creek to create an opportunity to establish a new green corridor with improved ecological, flooding and recreation outcomes. The primary functions of the re-aligned Riparian Corridor are to:

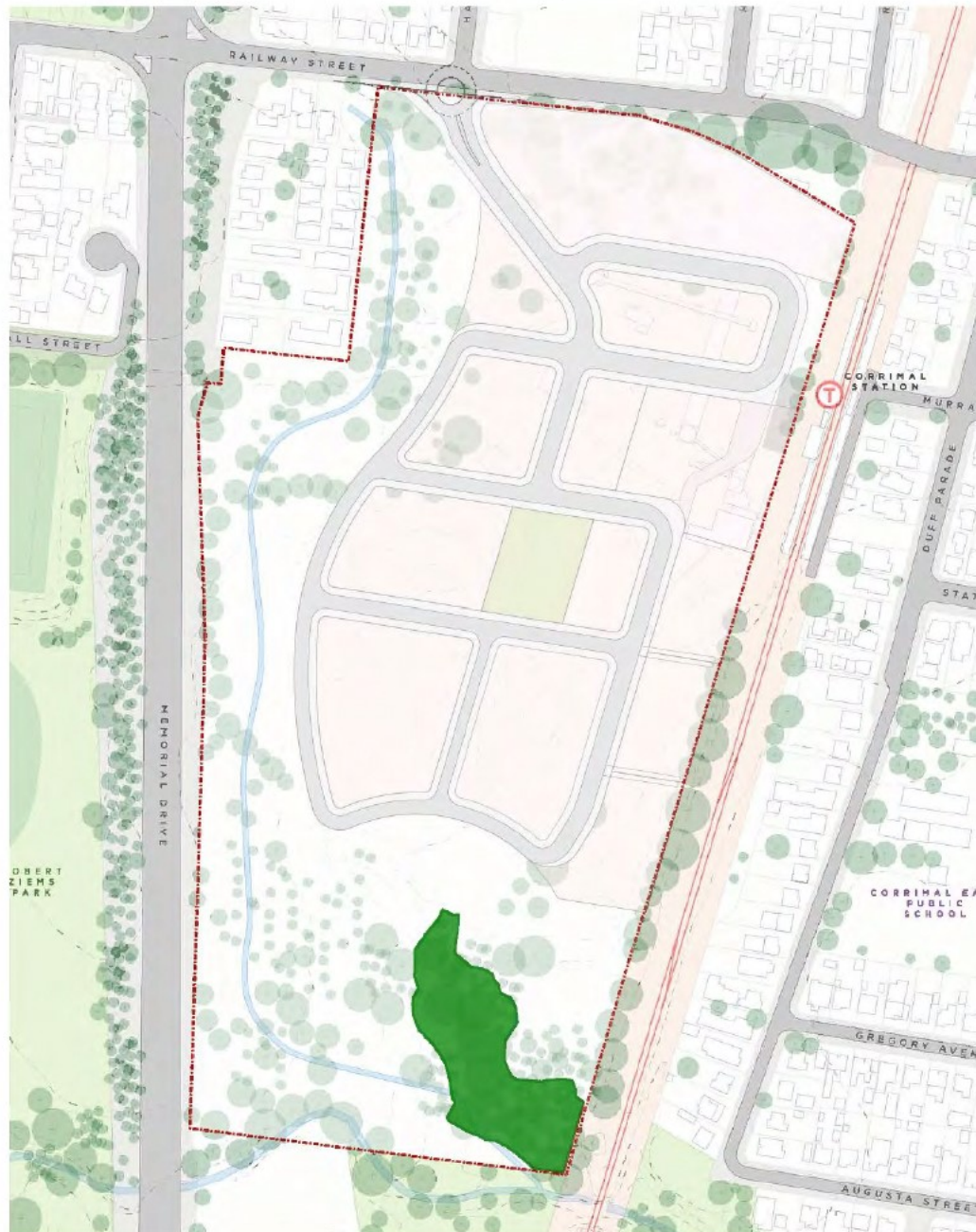
- Provide a continuous green corridor from Railway Street to Towradgi Creek that reflects the natural qualities of the area
- Appropriately manage ecological and flooding considerations on site, and not impact downstream properties
- Improve regional connectivity through the provision of a continuous shared path
- Provide appropriate passive recreational opportunities.
- The Ecological Area provides for the conservation and enhancement of the Illawarra Lowlands Grassy Woodland located in the southern part of the site.

Development Controls

1. North Corrimal Creek is to be realigned to provide a connected riparian parkland and environmental outcome from Railway Street down to Towradgi Creek.
2. Opportunities for passive recreation should be incorporated along the eastern bank of the riparian corridor, without comprising the flood management and stability requirements contained in Section 9 of this DCP.
3. A community event space may be included in the riparian corridor, and should include appropriate power and services to support community events.
4. The riparian corridor must be naturalistic in character and planting to support its ecological function, while still providing appropriate recreation opportunities and connectivity to deliver a regional asset.
5. Landscaping along the western embankment shall be designed to establish a natural bushland context to provide a visual buffer to Memorial Drive while also minimising maintenance requirements.
6. A shared pathway shall be provided along the riparian corridor from Railway Street and link with bridges (or as otherwise determined by Council) across the realigned North Corrimal Creek and Towradgi Creek, connecting to the Council owned reserve south of the site.
7. Prevent flooding impacts on site (and up and downstream) through the realignment of the riparian corridor, and creation of a flood free area suitable for residential development.
8. The design of the riparian corridor shall be intended to establish a stable riparian environment that minimises ongoing maintenance requirements. Access requirements for maintenance shall be incorporated as part of the design.
9. Retain and manage the Illawarra Lowlands Grassy Woodland EEC in the southern part of the site (see **Figure 16**) and other native plant communities according to the Vegetation Management Plan and GHFF Camp Management Plan.
10. A Vegetation Management Plan for the Ecological Area in the southern part of the site is to be submitted with the development application for the realignment of North Corrimal Creek.
11. Access to the Ecological Area is to be generally restricted except as required for maintenance.
12. The site stormwater management strategy should provide flows to the gully through the Ecological Area to maintain the existing hydrology of the area.

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Figure 16 Retained Illawarra Lowlands Grassy Woodland



ECOLOGICAL AREA

Legend



The Site



Ecological Area 7,450m²

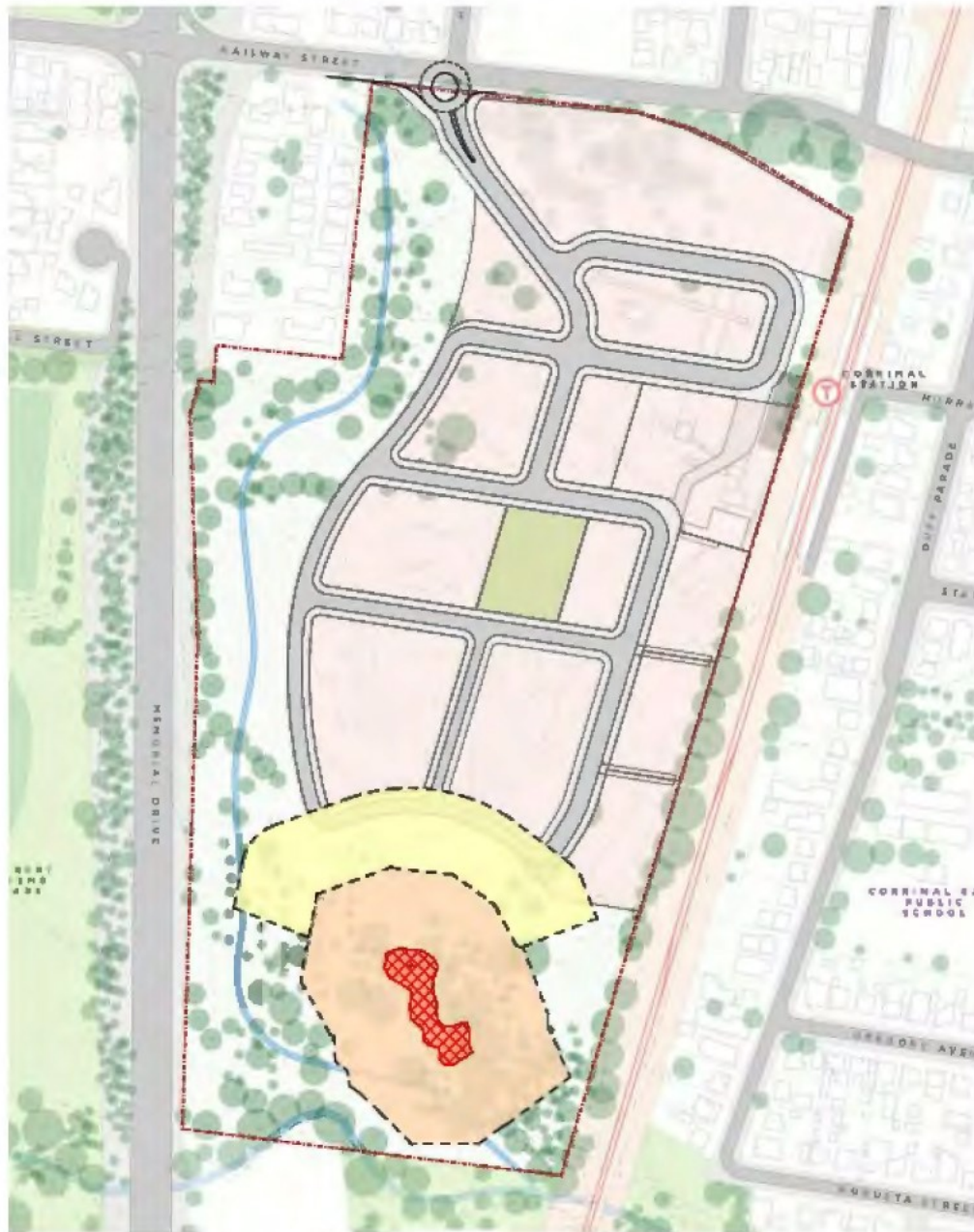
7.2 Flying Fox Management

Development Controls

1. A typical and averaged 100m buffer is to be provided between the core camp extent and future residential development, as identified in **Figure 17**. Flexibility in the buffer of +/- 10 metres is permissible.
2. A final Camp Management Plan is to be submitted at the development application stage and shall address ongoing management outcomes, including:
 - > Vegetation outcomes and management within the buffer area
 - > Community education
 - > Provision of aquatic habitat within the riparian corridor
 - > Any proposal for supplementary habitat to support expansion of the camp away from the residential development
 - > Construction management measures to minimise potential disturbance to flying foxes
3. The buffer shall be treated generally in accordance with the following approach:
 - > An inner buffer of approximately 50 metres. This area will be primarily ecological in function with limited public access, except as required for specific functions contemplated by this DCP.
 - > An outer buffer of a further 50 metres. This area should be used for recreational purposes to support community amenity.
4. Landscaping and vegetation within the buffer area will be designed to prevent the expansion of the camp further to the north.
5. Provide a shared path through the riparian corridor and within the flying fox buffer.
6. Future residential development that interfaces with the buffer area shall consider the need for any specific management or design measures as part of future development applications.
7. Encourage community interaction and facilitate unstructured recreation activities.
8. Retain and manage the Illawarra Lowlands Grassy Woodland EEC and other native plant communities according to the Vegetation Management Plan and GHFF Camp Management Plan.
9. The occasional flying fox camp is to be protected, retained and managed according to the GHFF Camp Management Plan. To prevent the flying foxes migrating into the newly planned built-up areas the following landscape principles should be considered in the landscape design:
 - > A treeless buffer between the existing occasional flying fox camp and the planned built infrastructure
 - > Additional open lawn recreational spaces to be provided directly north of the occasional flying fox camp
 - > Reducing permanent built structures inside the landscape in this area
 - > Recreational spaces will be surrounded by native grasses
 - > Designing of a "belly dip" area inside the realigned riparian corridor (in the south) relating specifically to the occasional flying fox camp.

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Figure 17 Flying Fox Management buffer



PARKING AND ACCESS
Legend

- The Site
- Core Camp Extent
- 50m Buffer
- 100m Buffer

7.3 Public Domain and Streets

7.3.1 Access to the site

Development Controls

1. Access to the site is to be provided from Railway Street with a new roundabout constructed at the intersection with Harbinger Street. The associated removal of vegetation is to be assessed through a Biodiversity Assessment Report (BDAR) as part of the development application

7.3.2 Streetscape character

Development Controls

1. The hierarchy of streets should be generally in accordance with **Figure 18**, and character as included in Table 6, and be integrated with the public transport railway station, kiss and ride and bus loop.
2. The Landscape Concept Plan for the streets and public domain should incorporate all positive streetscape elements including:
 - a) Street trees.
 - b) Remnant stands of trees.
 - c) Links and relationships to the heritage buildings and
 - d) On street car parking
 - e) Linkages with other open space areas in the locality
 - f) Street furniture, fences, and gates
3. The location and selection of street trees should have regard to requirements for utility services within the street verge.
4. Landscaping should reflect the character areas of the site and be used to soften the impact of buildings and to assist in providing visual relief to buildings.
5. Where the developer is responsible for the construction of footpath paving:
 - a) the type of paving is to be determined by Council according to the location
 - b) A nominal two percent (2%), minimum one percent (1%), maximum two and a half percent (2.5%) cross fall to be provided from property line to back of kerb.
 - c) The driveway entry threshold finish from the property boundary line to the face of the kerb must match the footpath and be designed to withstand predicted traffic loadings.
6. A change in driveway pavement is required at the entrance threshold within the property boundary to clearly show to motorists they are crossing a pedestrian area. Between the property boundary and the kerb, the developer must construct the driveway pavement in accordance with the conditions, technical specifications and levels to be obtained from the Council's Manager of Works. This requirement shall be reflected on the Construction Certificate plans and any supporting documentation.

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Figure 18 Road Hierarchy



**ROAD HIERARCHY
Legend**

- The Site
- Entry Boulevard - Entrance and Arrival
- Entry Boulevard - Beyond Bus Loop
- Bus Loop Road - Public Transport Access
- Main Loop Road - Circulation
- Riparian Edge Road - Activation of Green Space
- Local Street - Typical Street
- Shared Zone Road - Vehicle, Bicycle and Pedestrian

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7.3.3 Street Design and Road widths

Development controls

Streets within the precinct are to be designed in accordance with **Table 6** and Figures in this section of this DCP

Table 6 Street Design

Road Segment ID	Road Location	Street Type	Carriageway			Verge			
			Kerb Lane (m)	Centre Lane (m)	Total (m)	Verge (m)	Total Reserve (m)	Footpath (m)	Shared Path (m)
1	Entry Boulevard	Type 5 (modified) Parking on one side	2.6 & 1.5	3	10.1	9.75 (5.25m one side, 4.5m other side)	19.85	1.5	3
2	Entry Boulevard (beyond bus loop)	Type 5 (with parking)	2.1	2.8	9.8	9.0 (4.5m each side)	18.8	1.5	3
3	Bus access loop	Type 4 (with parking and with bus stops as needed)	2.6	3	11.2	9.75 (5.25m one side, 4.5m other side)	21.0	1.5	3
4	Main Loop road	Type 5 (with parking)	2.1	2.8	9.8	9m (4.5m each side)	18.8	3m (1.5m each side)	N/A
5	Riparian Edge Road	Type 6 (modified) parking one side	2.1	3	8.1	4.5m one side	12.6	1.5m each side	N/A
6	Local Street (Typical Street)	Type 6	2.3	3.5	8.1	9m (4.5m each side)	17.1	3m (1.5m each side)	N/A
7	Shared Zone Road	Type 7 (modified)	0	6	6	8 (4m each side)	14.0	N/A	N/A

An explanation and controls relating to each street are outlined below.

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Entry Boulevard

The Entry Boulevard reinforces the entrance and sense of arrival to the site.

Figure 19 Entry Boulevard Section



Character	Landscape	Parking
Grand entrance feel providing clear, direct access to the station and Civic Plaza for vehicles, pedestrians and cyclists	Mature trees in rows with understorey planting in verge.	No dedicated parking

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Bus Loop Road

Bus access road allows for the circulation of public transport route.

Figure 20 Bus Loop Road Section

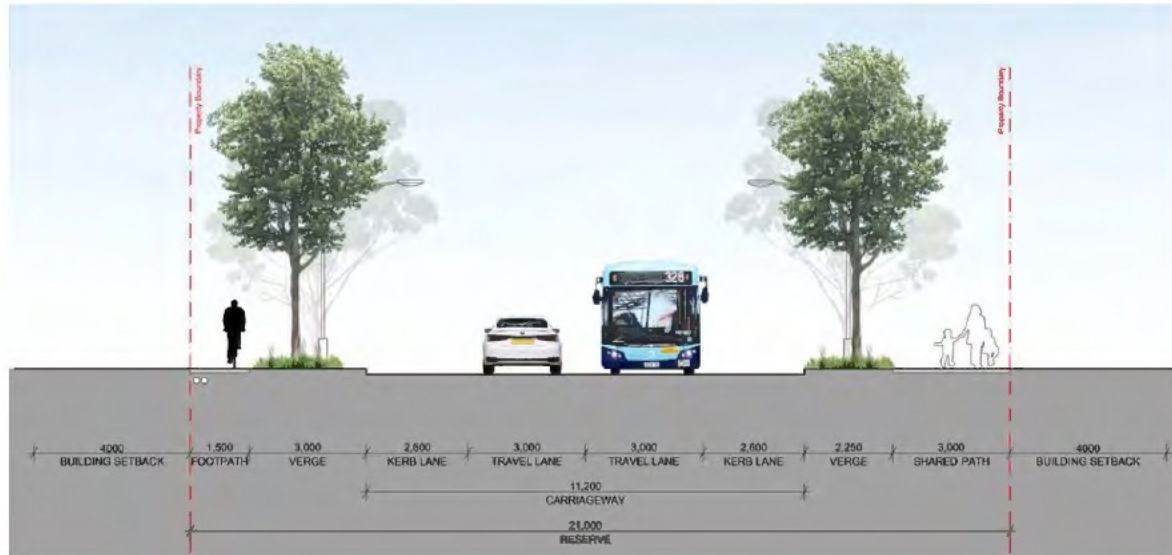
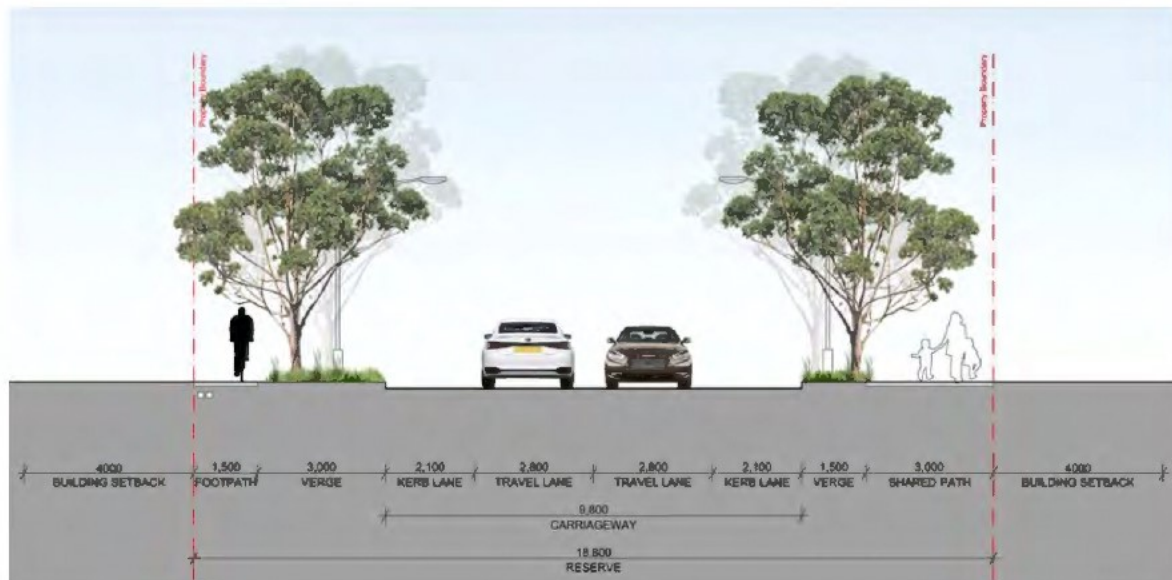


Figure 21 Boulevard Beyond Bus Loop



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Character	Landscape	Parking
A clearly defined route for quicker circulation in and out of the Precinct and reinforcing the link between the Civic Plaza and larger useable open space in the Riparian Corridor	Mature trees in rows with understorey planting in verge.	Dedicated parking on one side of the carriageway

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Main Loop Road

The main loop road clearly defines the main circulation loop around the precinct.

Figure 22 Main Loop Road Section



Character	Landscape	Parking
Clearly defined main circulation around the site for visitors and residents with flexibility to accommodate differing ground level interfaces	Mature trees in rows with understorey planting in verge.	Dedicated parking lanes on both sides of the carriageway

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Riparian Edge Road

Figure 23 Riparian Edge Medium Density



Character	Landscape	Parking
Provides interface to public open spaces.	Mature trees in rows with understorey planting in verge.	Single side of dedicated parking on the opposite side to the open space

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Local Street

Local Streets within the Precinct cater to everyday use and provide pedestrian amenity. Street verges are to include mature trees in rows with complimentary understorey planting to act as a buffer for residential buildings.

Figure 24 Local Street Section



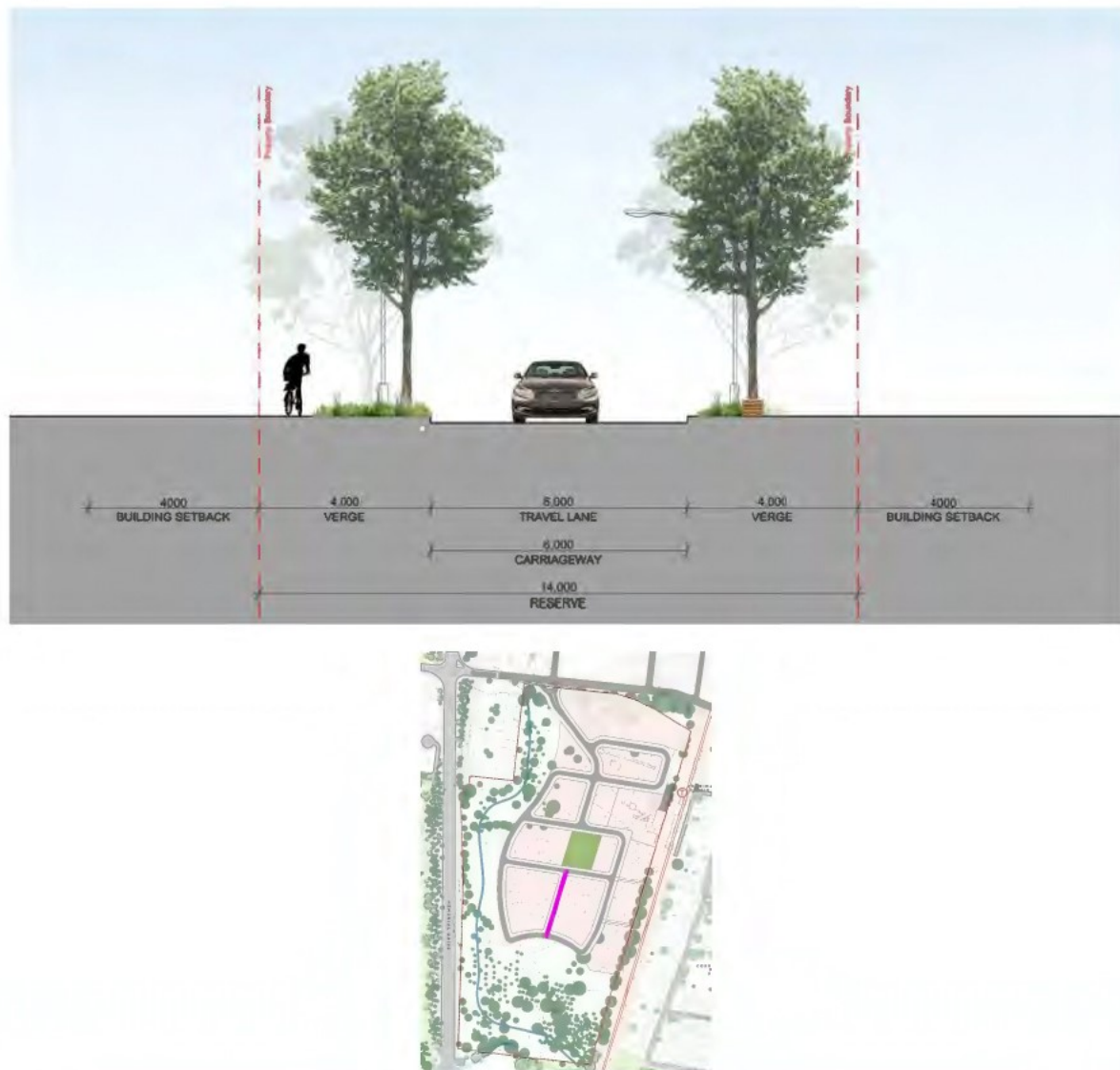
Character	Landscape	Parking
Provide local connections between more major streets and public open space within the Precinct	Mature trees in rows with understorey planting in verge.	Dedicated parking lanes on both sides of the carriageway.

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Shared Path

Shared roadway for cars and pedestrians to access residential lots, and to support pedestrian and cycle linkage to the southern recreation park.

Figure 25 Shared Path



Character	Landscape	Parking
Activated shareway for vehicles, bicycles and pedestrians. Provides for a pedestrian friendly green landscaped link from the central park to the southern recreational park	Planting bed with seating provided on one side of the shared pathway with space for small tree planting in between verge car parking on the other side	Verge parking is provided on one side of the shared vehicle, bicycle and pedestrian pathway

8 Other controls

Objectives

1. Manage the potential flood hazard and risk associated with the use and development of land within the floodplain
2. Provide for appropriate management of flooding to ensure that there is no increase in flood impacts around the site
3. Incorporate water sensitive urban design measures in the management of stormwater.
4. Establish a riparian corridor that supports a range of biodiversity, hydraulic and recreational outcomes.
5. Ensure that the realignment of North Corrimal Creek provides for the long-term stability and functioning of the creek.
6. Ensure that the land is suitable for its intended use
7. Ensure that earthworks are managed in an appropriate manner.

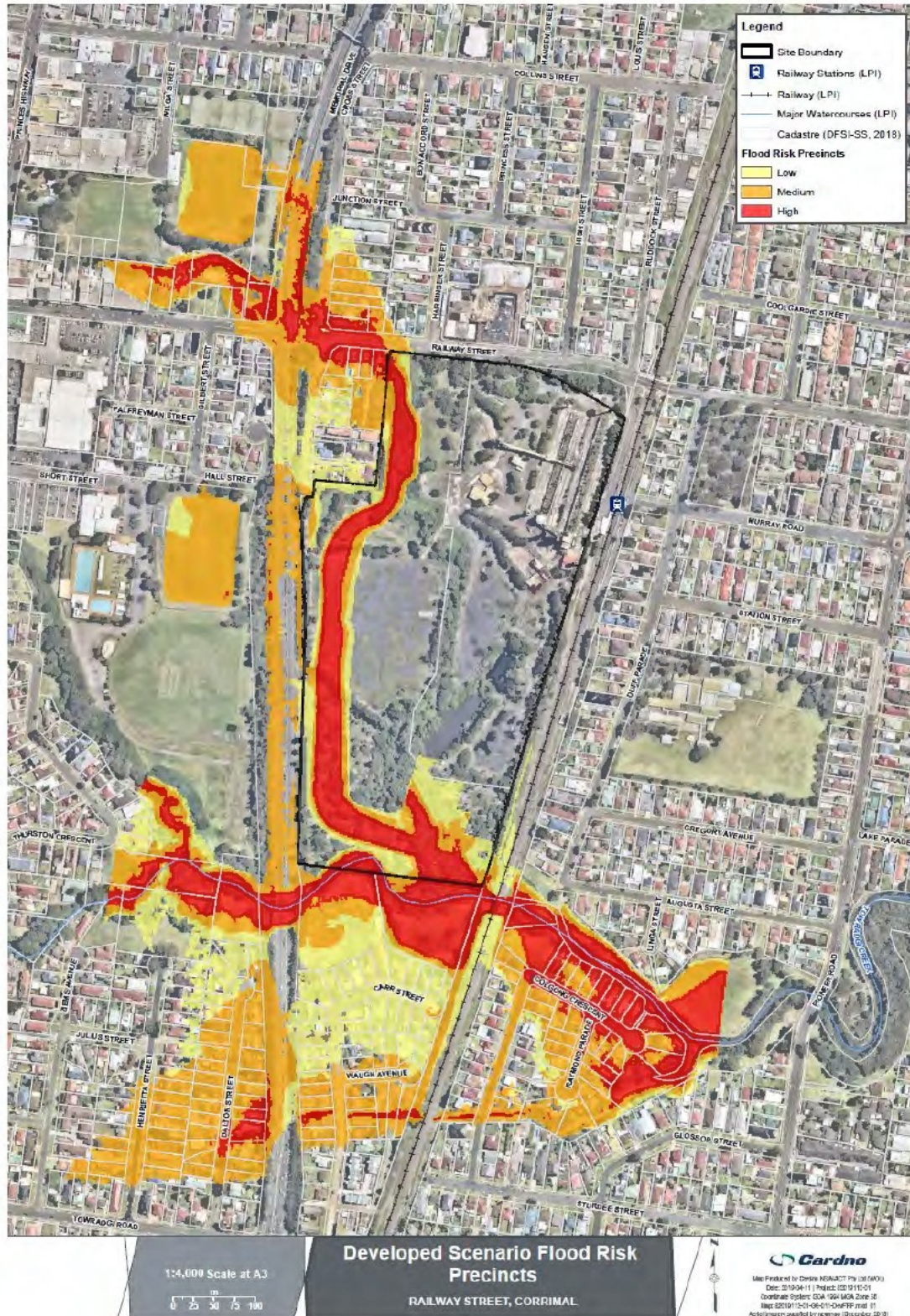
8.1 Flood management

Development Controls

1. An updated Flood Study is to be submitted with the development application for realignment of North Corrimal Creek to demonstrate that the design complies with the general parameters and flood outcomes from the Corrimal Coke Works Flood Study (Cardno, May 2019) as demonstrated in **Figure 26**.
2. Development should not detrimentally increase the potential flood affectation on other development or properties either individually or in combination with the cumulative impact of development
3. The development shall not result in any net loss of floodplain storage.
4. Provision of on-site stormwater detention (OSD) will be determined at development application stage and is to generally comply with Council's requirements
5. Vegetated Riparian Zones (VRZ) offset areas can be utilised for water quality treatment.
6. The 10m width requirement from top of bank, as outlined in Chapter E23 Clause 6.2 Table 2 and Clause 6.2.2 can be varied on the site, in cases where the realigned creek adjacent to Cross Street Residential dwellings will not result in any adverse impact upon the functions of the riparian corridor or any adverse flood hazard risk or other hazard risk, and in this situation Council may consider a variation to the minimum property offset.

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Figure 26 Flood Management



8.2 Stormwater management

Development Controls

1. A detailed stormwater drainage concept plan together with relevant calculations is required to be submitted with any Development Application for construction of roads and drainage.
2. The site stormwater management drainage will include directing appropriate flows to the gully through the ecological area in the south.
3. The proposed stormwater drainage system must incorporate water sensitive urban design techniques, wherever possible, in order to minimise runoff and restrict discharge from the site. This may be achieved by using grass swale drains, biofiltration, bio-retention basins, detention ponds, reuse systems and retention of natural watercourses including wetlands and pool and riffle zones. Other stormwater quality improvement measures such as artificial wetlands, sedimentation basins and gross pollutant traps or trash racks may also be provided to facilitate the removal of sediment and other pollutants.

8.3 Riparian corridor geomorphology and stability

Controls

1. An average longitudinal grade of 1% (generally ranging from 0.5-1.1% and allowing for features such as riffle zones and drops etc)
2. A micro channel ranging in width from 1-15m and wherever practical the low flows channel shall concentrate on either side of that channel, and be generally consistent with Figure 30. The micro channel should meander and include options to:
 - > Establish a pool – rock step morphology with rock steps constructed of large sandstone boulders. Where appropriate, the rock steps should be designed to provide opportunities for community access to the creek during low flows
 - > Provide alternating mini retaining rock structures or low rock benches along the low flow channel. Retaining rock structures should be approximately 500mm below the current design level of the macro-channel base and grade up gently in a downstream direction. These should be vegetated with dense plantings of native ground cover grasses/sedges and shrubs
 - > On outside bends, during establishment and stabilisation of all channel banks, vegetation will be used as part of the stabilising techniques.
 - > Where outside bends of the low flow channel are close to the bank of the macro-channel, there should be rock protection at the bend to prevent scour at the toe of the macro-channel bank.
3. Provide rock pitching or other hard scour protection measures where soft scour protection is inappropriate measures (e.g. stepping stones).
4. The riparian corridor shall generally have, and generally shown in **Figure 28-32** :
 - > A macro channel ranging in width from 20-80m wide
 - > An offset from top of bank (varying in width from 5-10m) along the western bank to Memorial Drive and private property
 - > The PMF shall be contained within the newly formed creek channel.
 - > Batter slopes no steeper than 1V:2H throughout, while allowing for landscape elements with an aesthetic or amenity value (eg sandstone blocks/seating walls) that are not structural or load bearing
5. All structural retaining walls to be located above PMF level. All retaining solutions would be designed by a structural engineer, comply with Council specifications and utilise natural materials (e.g. sandstone blocks or similar) to achieve the desired landscape character

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6. Western embankment to be planted with a range of native grasses, understorey shrubs and tree species with deep root structures and to be relatively low maintenance
7. For the Western Bank it is envisaged that the modified channel will have rock retaining walls and meander the centre line of the creek to create access on the western bank and cut in graded access to the channel. Retaining walls such as those shown in the below figures should be developed.

Figure 27 Design of retaining walls



8. Eastern embankment to combine areas of native planting with spaces and paths for creek-oriented recreational uses
9. Landscape plans to provide the specification for appropriate planting and establishment to support bank stability, riparian outcomes and compatible recreation uses.
10. All stormwater discharge from the site will be directed in a controlled manner to the creek system, limiting discharge over creek banks and directly into the micro channel.
11. The creek tie in and confluence point should avoid major disturbance and reinstate connectivity to Towradji Creek, as it creates a formed channel to an existing water course. Detailed design shall address how the tie in to the existing channel will minimise impact at the confluence and the adjacent Endangered Ecological Community (EEC).
12. The riparian land within a subdivision will be subject to a Vegetation Management Plan (VMP) to assist in establishing an ongoing management process. This VMP will include ownership, maintenance and management arrangements.

Figure 28 Riparian Corridor stabilisation design



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Figure 29 Creek section - Amphitheatre



Figure 30 Bridge Section

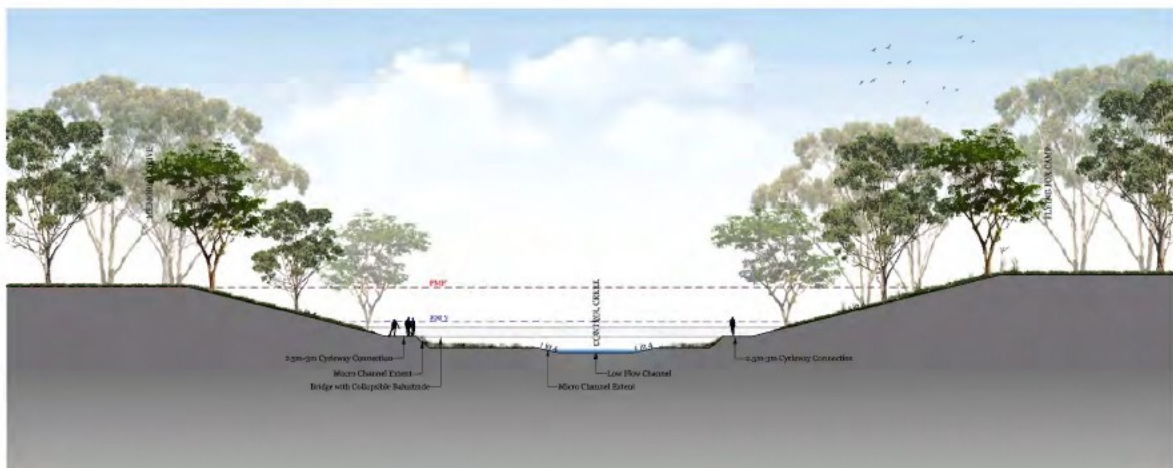
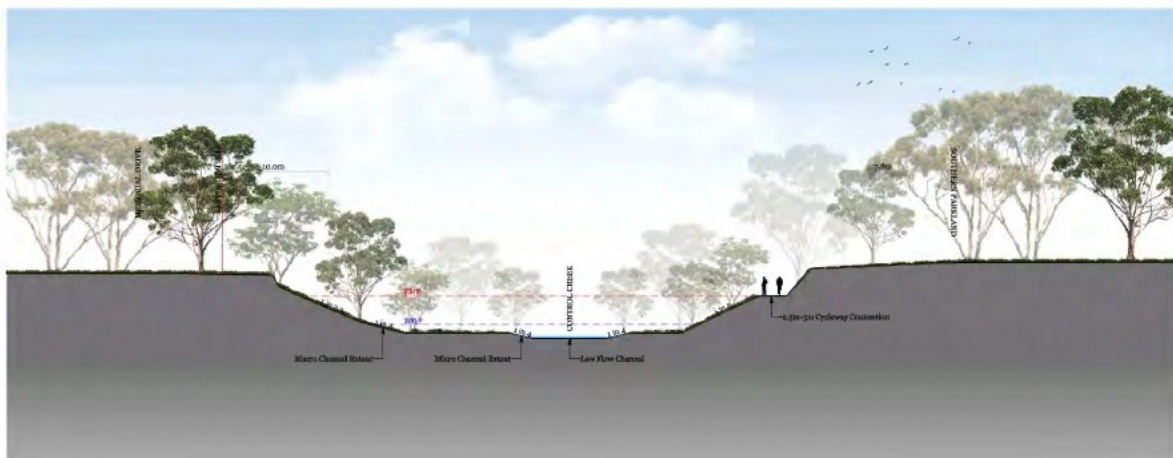
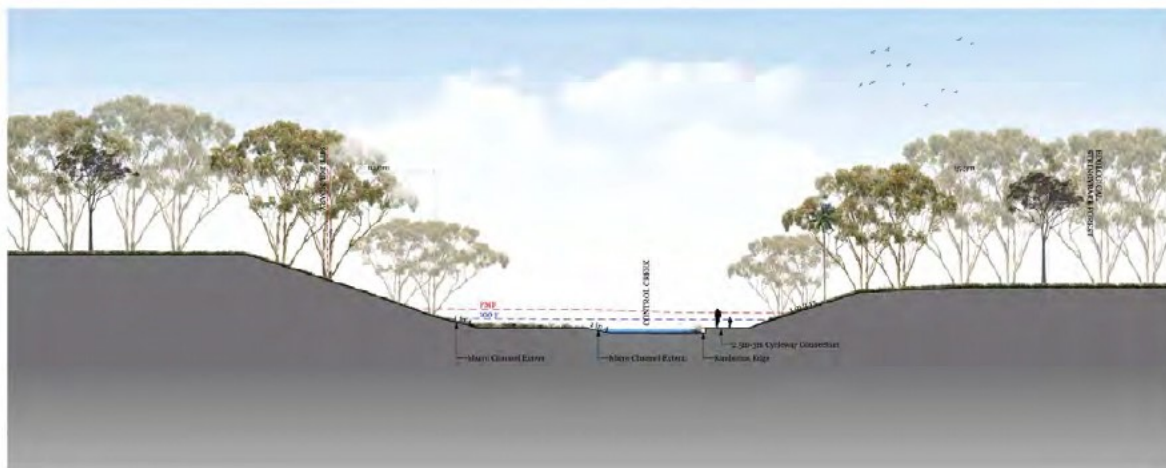


Figure 31 Southern Park Section





Controls

1. Remediation on the site should be undertaken generally in accordance with the Remediation Action Plan (Arcadis, May 2019) and under the oversight of an EPA accredited Site Auditor.
2. A Site Audit Statement is to be provided following the completion of remediation works and prior to the occupation of any residential dwelling.
3. Coal Washery Reject (CWR) can be considered as fill material at the site, and addressed as part of the remediation and proposed bulk earthworks plan.

Controls

1. The design of bulk earthworks should result in appropriate interfaces to existing site boundaries including Railway Street, the rail corridor and Memorial Drive. Earthworks should also have regard to generally maintaining existing ground levels in the vicinity of retained heritage items.
2. Bulk earthworks should be undertaken in a manner that seeks to achieve a balance of cut and fill as reasonably possible in order to reduce the extent of fill import or export.
3. The extent of cut and fill associated with bulk earthworks should be minimised, except where associated with:
 - > Realigning North Corrimal Creek, including the new creek corridor and filling the existing creek
 - > Filling existing dams
 - > Removing substantial earth berms/bunds across the site
 - > Remediation of contaminated areas.
4. All earthworks are to be undertaken under appropriate Level 1 geotechnical supervision.
5. A waste classification shall be undertaken for any fill material to be disposed off site.

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6. Imported fill must be free from any soil contamination and accompanied by an appropriate waste classification prepared in accordance with the requirements of the NSW EPA Waste Classification Guidelines (2014) and with consideration of the Protection of the Environment Operations (Waste) Regulations (2014). All imported material must be classified as virgin excavated natural material (VENM). In restricted circumstances where deemed appropriate Excavated Natural Material (ENM) may also be accepted as long as it is in keeping with the requirements of any regulatory bodies and the proposed site use.
7. Where earthworks involve land known or suspected to be contaminated, the provisions of the Contaminated Land Management Chapter in Part E of this DCP will also apply.

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9 References

Table 7 References

Report	Prepared by/author	Date
Aboriginal Cultural Heritage Assessment	Kelleher Nightingale Consulting Pty Ltd	17 May 2019
Conservation Management Strategy	Urbis	30 July 2019
Creek Realignment Stability Assessment	BG&E	19 March 2019
Supplementary information regarding creek realignment	BG&E	2 August 2019
Heritage Interpretation Strategy	Urbis	7 May 2019
Historical Heritage Assessment	Biosis	1 August 2017
Noise and Vibration Assessment	Renzo Tonin & Associates	20 May 2019
Structural Assessment of Existing Structures	BG&E	21 June 2019



12 October 2020

General Manager
Wollongong City Council
Locked Bag 8821
WOLLONGONG DC NSW 2500

Attention: Chris Stewart, Manager City Strategy

**LETTER OF OFFER TO ENTER INTO A PLANNING AGREEMENT WITH WOLLONGONG CITY COUNCIL –
CORRIMAL COKE WORKS**

LegPro 70 Pty Ltd as trustee for LegPro 70 Unit Trust (**Legacy**) offers to enter into a Voluntary Planning Agreement (**VPA**) with Wollongong City Council (**Council**) under section 7.4 of the *Environmental Planning and Assessment Act 1979* (**EP&A Act**) in accordance with the terms of this Letter of Offer (**Offer**).

1 Operation and Application

The VPA will be entered into in connection with a Planning Proposal for the rezoning of the Corrimal Coke Works, prepared by Elton Consulting entitled “Planning Proposal: Former Corrimal Coke Works” dated 6 August 2020 (**Planning Proposal**).

Legacy intends to lodge development applications for the residential and mixed-use redevelopment of the Land, subject to finalisation of the Planning Proposal (**Development**).

2 The Land

The VPA will apply to the land generally known as the Corrimal Coke Works comprising Lot 1 DP795791, Lot 5 DP749492, Lot 126 DP598190 and Lot 11 DP749492 (**Land**).

Illawarra Coke Company Pty Limited is the landowner and agrees to be a party to the VPA in its capacity as landowner. Legacy has rights in respect of the Land.

3 Application of sections 7.11, 7.12 and 7.24 of the EP&A Act

The VPA will exclude the application of sections 7.11 and 7.12 of the EP&A Act to the Development.

The VPA will not exclude the operation of section 7.24 of the EP&A Act to the Development.



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4 Development Contributions

Legacy agrees to provide the public benefits in accordance with the Schedules **attached** as Annexure A to this Letter of Offer, subject to the Planning Proposal being made in the form as submitted to Council.

Legacy reserves the right to amend this Offer should the Planning Proposal be amended prior to its finalisation.

5 Affordable Housing Contribution

The affordable housing contribution in this Offer will full satisfy the requirement to provide affordable housing as part of the Development.

The VPA will exclude the application of any future contribution or additional requirements in relation to affordable housing.

6 Enforcement and security

Security will be provided through:

- registration of the VPA on title to the Land;
- inclusion of a clause allowing Council to compulsorily acquire land to be dedicated for \$1;
- restriction on the issue of relevant certificates under Part 6 of the EP&A Act prior to the relevant obligation(s) being satisfied.

7 Registration

The VPA shall be registered against all the titles comprised in the Land. However, a mechanism will be required which allows for the VPA to be released from the Land in stages as relevant obligations under the VPA are satisfied.

8 Limitation of liability clause

The VPA will contain the limitation of liability clause in Annexure B to this letter of offer.

Yours sincerely,



Matthew Hyder
Chief Executive Officer



LEGACYPROPERTY

Annexure A

Schedule of public benefits

12 OCTOBER 2020

Corrimal Coke Works: Schedule 1 – Defined Public Benefits and Contribution Offsets

Item	Overview	Description	Total Public Benefit		Contribution Offset	Timing
			Land	Works		
Central Village Park	Provision of a neighbourhood scale park to provide amenity for future residents and the wider community.	<ul style="list-style-type: none"> » Dedication of a minimum 3,000sqm of land » Embellishment works » 36 month maintenance period from practical completion 	\$1,365,000	\$1,272,128	n/a	Embellishment and dedication prior to an occupation certificate for any residential flat building in Stage 2
Southern Recreation Park	Provision of a neighbourhood scale park to provide amenity for future residents and the wider community.	<ul style="list-style-type: none"> » Decision of a minimum of 5,000sqm of land » Embellishment works » 36 month maintenance period from practical completion 	\$2,275,000	\$1,848,200	n/a	Embellishment and dedication prior to an occupation certificate for any residential flat building in Stage 3
Riparian corridor	Establishment of green riparian corridor providing a range of passive recreation, active transport and ecological functions.	<ul style="list-style-type: none"> » Dedication of approximately 77,360sqm of land » Landscape embellishment works » Improving and enhancing retained ecological communities under Vegetation Management Plan » 5 year maintenance period from practical completion 	\$4,641,600	\$5,957,968	n/a	Embellishment and dedication prior to an occupation certificate for any residential flat building in Stage 3
Riparian Corridor shared pedestrian / cycle path	Provision of a regional pedestrian/cycling connection through the North Corrimal Creek riparian corridor	<ul style="list-style-type: none"> » 2.5m shared path from Railway Street to Council reserve south of Towradgi Creek (including bridge crossings) within riparian corridor 	n/a	\$1,609,575	\$1,609,575	Embellishment prior to an occupation certificate for any residential flat building in Stage 3
Railway Street shared pedestrian / cycle path	Provision of shared path on Railway Street for site frontage	<ul style="list-style-type: none"> » 2.5m shared path along the site frontage on Railway Street 	n/a	\$187,500	n/a	Embellishment prior to an occupation certificate for any residential flat building in Stage 1.

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Item	Overview	Description	Total Public Benefit		Contribution Offset	Timing
			Land	Works		
Road widening land dedication	This land is zoned SP2 Road Widening and is identified on Wollongong City Council's Land Reservation Acquisition Map.	» Dedication of approximately 2,467sqm of land (Lot 126 DP598190) for road widening (Railway Street) (facilitates future rail overpass bridge by Council)	\$1,122,485	n/a	\$1,122,485	Transfer prior to issue of an occupation certificate for any residential flat building in Stage 1
Railway Street/ Harbinger Street roundabout	Roundabout on Railway Street	» Construction of new 4 way roundabout at intersection of Railway Street and Harbinger Street (contribution offset amount represents the differential between constructing a channelised 'T' intersection and the proposed roundabout)	n/a	\$950,943	\$666,474	Prior to issue of an occupation certificate for any residential flat building in Stage 1
Station Plaza public access	Provision of permanent public access through the development to Corrimal station, delivering improved connectivity and promoting increased public transport use.	» Creation of public easement/right of way across Station Plaza providing public access to railway station » Embellishment works (excluding cost of major heritage restoration and interpretation elements) » Contribution offset represents 25% of the total embellishment cost reflecting broader public benefit » Any connection through rail corridor subject to approval of RailCorp	n/a	\$3,068,035	\$767,009	Prior to occupation certificate on adjoining development site to the south.
Affordable Housing	Facilitating the delivery of affordable housing within the site to meet Wollongong City Council's 5% target.	» Sale of land to a registered Community Housing Provider to allow delivery of a minimum of 35 affordable rental housing dwellings. » Restriction on title for affordable housing use in accordance with Affordable Rental Housing SEPP.	n/a	n/a	n/a	Prior to issue of an occupation certificate for any residential flat building in Stage 2
TOTAL			\$9,404,085	\$14,894,349	\$4,165,543	

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Notes:

- The Offer is conditional of finalisation of the Planning Proposal as submitted to Wollongong City Council. Legacy Property reserves the right to amend the Offer if the Planning Proposal is amended.
- The VPA will exclude the application of any further contributions under Section 7.11 and 7.12 for development of the site.
- The affordable housing contribution proposed in the VPA will fully satisfy any requirement for affordable housing in the project and will exclude the application of any future affordable housing contribution to the project (including but not limited to any contribution proposed under SEPP 70).
- The current scope and costs for proposed VPA items are based on concept plans at the rezoning stage. It may be necessary to review and amend the scope and associated costs as the design is developed through the development application process.
- The early handover and completion of maintenance periods may be considered in future, subject to negotiation of appropriate arrangements such as the provision of a monetary contribution in lieu of ongoing maintenance by the developer.
- Adopted land values, based on MMJ valuation (April 2019), as follows:
 - Road widening, central park, southern park and active/fully usable riparian corridor land - \$455/sqm
 - Ecological areas and passive riparian corridor land - \$60/sqm(updated valuation to be prepared prior to execution of VPA)

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Corrimal Coke Works: Schedule 2 – Scope of Embellishment Works

The scope of embellishment works will be generally in accordance with existing landscape concept plans (prepared by Clouston Associates) subject to future development applications, and as outlined below:

Item	Scope
1	<p>Central Village Park</p> <p><i>Site Preparation/Earthworks</i> – minor preparation for construction, trim and compact sub-grade of pathways</p> <p><i>Hardworks</i> – 1.2m concrete path, Paving, Soft fall</p> <p><i>Soft works</i> – turf zone, native grasses and shrubs (6/sqm), advanced trees 48 off</p> <p><i>Play equipment</i> – Suitable for 0-6 years</p> <p><i>Furniture & Fixtures</i> – Shade structure to play equipment, seating, bollards, tactile indicators</p> <p><i>Services</i> – Lighting as required to Australian Standard, 1 x waterpoint, signage</p> <p><i>Maintenance</i> – Planting establishment and maintenance 36 months</p>
2	<p>Southern Recreation Park</p> <p><i>Site Preparation/Earthworks</i> – minor preparation for construction, trim and compact sub-grade of pathways</p> <p><i>Hardworks</i> – 1.2m concrete path, Paving, Soft fall</p> <p><i>Soft works</i> – turf zone, native grasses and shrubs (6/sqm),</p> <p><i>Play equipment</i> – Suitable for 6+ years</p> <p><i>Furniture & Fixtures</i> – Shade structure to play equipment, seating, bollards, tactile indicators</p> <p><i>Services</i> – Lighting as required to Australian Standard, 2 x waterpoints, signage</p> <p><i>Community Gardens</i> – Area for planting, 1 x water point</p> <p><i>Maintenance</i> – Planting establishment and maintenance 36 months</p>
3	<p>Riparian Corridor</p> <p><i>Site Preparation/ Earthworks</i> – Trimming and grading, sediment and erosion control during preparation, topsoil blend assumed salvaged on site, topsoil for planting assumed salvaged, trim and compact subgrade of pathways and landscaped areas</p> <p><i>Hardworks</i> – Concrete footpaths varying widths 1.2m, Concrete stage area, sandstone logs as seating to amphitheatre, Amphitheatre stairs and handrail, stacked sandstone block to rip rap zone, drop structure</p> <p><i>Walls</i> – Creek edge wall, gabion retaining wall to Northwest corner (max 1.5m high), Retaining wall to footpath edge</p> <p><i>Softworks</i> – Native grasses, shrubs and small trees (9/sqm), Turf zones, Mass planting of shrubs (6/sqm), Wetlands planting, Trees planting 50 x 45ltr, Jute mesh to creek</p> <p><i>Furniture and Fixtures</i> – Seating, balustrades, bollards, tactile indicators</p> <p><i>Services</i> – Lighting as required to Australian Standard, Power to Stage area, water points x 4, signage</p> <p><i>Vegetation restoration and management</i> – in accordance with future Vegetation Management Plan for retained Endangered Ecological Community area</p> <p><i>Maintenance</i> – Planting establishment and Maintenance 5 years</p>
5	<p>Shared Pedestrian and Cycle Path</p> <p><i>Site Preparation/ Earthworks</i> – Trimming and grading, trim and compact subgrade of pathways</p> <p><i>Hardworks</i> – Concrete footpaths 2.5m</p> <p><i>Bridges</i> – 2 off 2.5m wide x 20m long timber and steel bridge over creek</p> <p><i>Services</i> – Lighting as required to Australian Standard</p> <p><i>Other</i> – Signage and line marking, note path terminates on the southern bank of Towradgi creek adjacent to bridge crossing.</p>

12 OCTOBER 2020

Corrimal Coke Works: Schedule 3 – Additional General Public Benefits

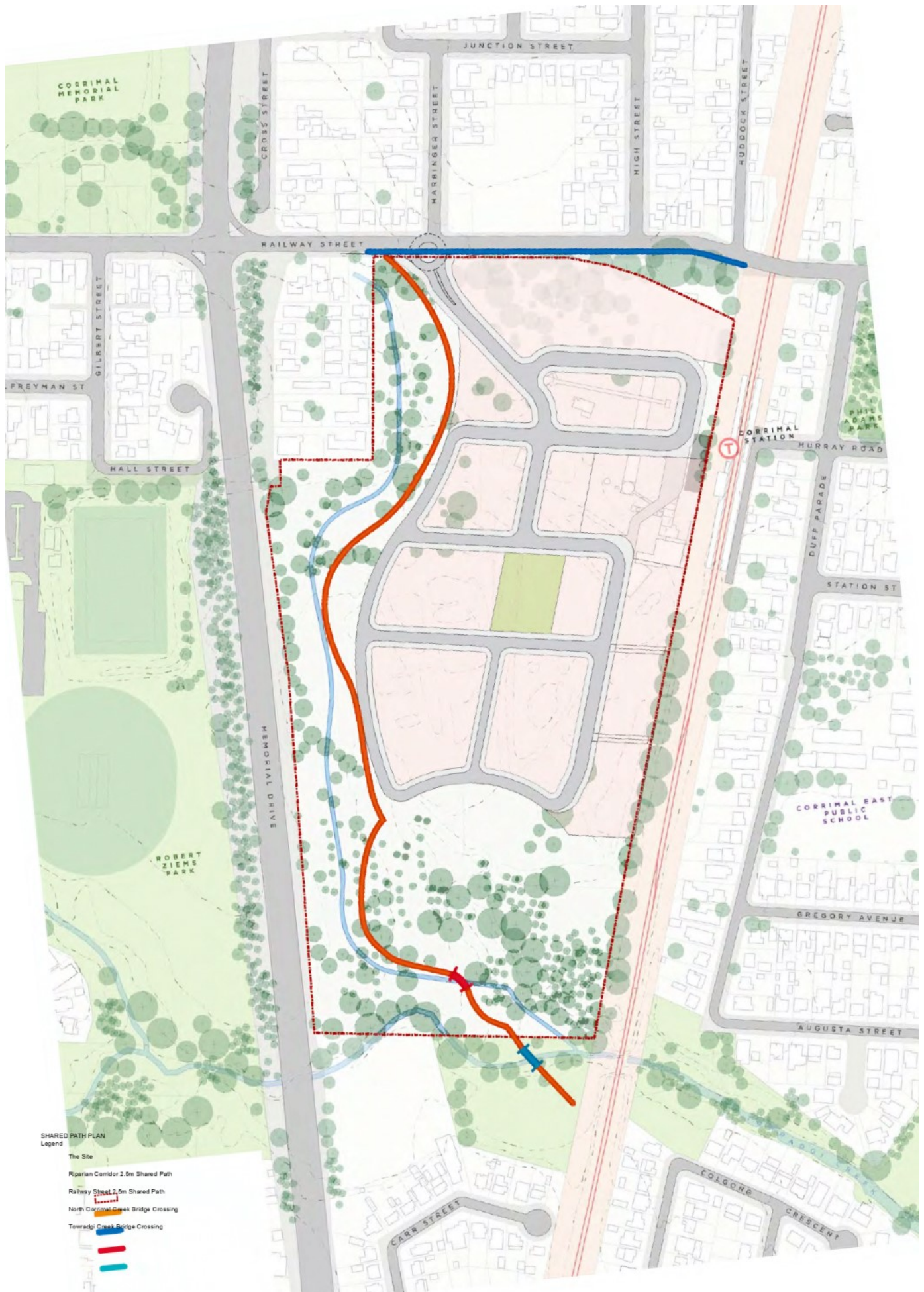
Revitalisation of the Corrimal Coke Works will deliver a broad range of additional public benefits that have not been quantified in monetary terms and/or do not represent offsets to s7.11 or s7.12 contributions.

These additional public benefits include:

- Preservation and restoration of various heritage structures
- Interpretation of existing heritage structures and general history of the site, including Aboriginal heritage
- Substantial retention of existing mature vegetation along Railway Street
- Reduction in flood levels in selected locations upstream and adjacent to the site
- Increased accessibility and permeability for the Corrimal community
- New housing opportunities
- Significant restoration of environmental and biodiversity values
- Provision of neighbourhood retail services at Corrimal station









Annexure B

(1) Definitions

In this clause:

- (a) Trust means the Legacy Property Unit Trust (ABN 75 600 144 578); and
- (b) Trust Deed means the trust deed establishing the Trust.

(2) Limitation of Liability

- (a) The Developer warrants that:
 - (i) it enters into this deed in its capacity as trustee of the Trust and in no other capacity; and
 - (ii) it is empowered by the terms of the Trust Deed or any other instrument constituting the Trust to enter into this deed in accordance with its provisions; and
 - (iii) it is entitled to be indemnified out of the assets of the Trust in respect of the obligations and liabilities assumed by it under the terms of this deed.
- (b) The Council acknowledges and agrees that, despite any other provision of this deed, any liability or obligation of the Developer arising under or in connection with this deed can only be enforced to the extent to which they are entitled to be, and are in fact, indemnified for that liability or obligation out of the assets of the Trust. This includes without limitation any representation, warranty or conduct by the Developer.
- (c) Clause 1(b) does not apply to any liability or obligation of the Developer to the extent there is a reduction in their ability to be indemnified for that liability or obligation out of the assets of the Trust as a result of the Developer's fraud, negligence or breach of trust.



7 June 2020

Ms L Davis
Director, Planning & Environment
Wollongong City Council
Sent via email

Dear Linda,

Re: Affordable Housing, Corrimal Coke Works site

I am writing at the request of Legacy Property who are the developer of this important site on the understanding that they have been progressing discussions with Council about a Voluntary Planning Agreement (VPA) that would include the delivery of Affordable rental housing.

Housing Trust has a Memorandum of Understanding with Legacy Property that provides an exclusive period for the parties' to progress commercial discussions about the delivery of Affordable rental housing. The basic intent is that Housing Trust would acquire a freehold parcel within the development and design, build and manage the Affordable rental housing proposed in the VPA.

We understand the VPA contemplates 35 Affordable rental dwellings, representing 5% of the total housing stock in the development. Although we would prefer the allocation was 10-15% we are grateful that Legacy Property recognise the need for developments of this scale to make a contribution that is consistent with Council's affordable policy and planning intentions. This must include a range of structural changes and incentives if the crisis identified in the Affordable Housing Options Paper, currently on exhibit, is going to be addressed.

Tenant eligibility for Affordable rental housing is defined by the NSW Ministerial Guidelines that include an income range of approximately \$52,000 - \$72,000 for households without children, and \$89,000 - \$124,000 for households with one or more children. This cohort are often the 'key workers' who are essential to the economic and social fabric of the community. Their incomes are above the eligibility threshold for Social housing which cannot provide without assistance from the NSW Government. No such assistance is envisaged in this project.

Planning for the Affordable housing units has not commenced however Council would be aware of the design and construction quality of recent Housing Trust buildings in Eager St. Corrimal, Robert St. Corrimal and Willinga Rd. Flinders. Affordable housing is indistinguishable from market housing. We hope to integrate some of the heritage features of the site in our design, perhaps by reusing bricks or steel in architectural features.

Subject to Council's planning requirements, we anticipate Housing Trust will construct a 3-4 storey building (including a basement carpark) with a minimum 35 units with one and two bedroom layouts. A 'dual key' floorplan could be considered to provide diversity and flexibility in the configuration. Our procurement policy favours local trades and suppliers in all design, supply, construction and maintenance programs.



The Constitution, regulatory and legislative frameworks within which Housing Trust operates can be relied on by Council to ensure a freehold asset at the Coke Works will be dedicated to a charitable purpose in perpetuity. Subject to a commercial agreement between Legacy Property and Housing Trust, an affordable housing restriction could be placed on the title for 10 years as anticipated in the SEPP provisions for affordable rental housing.

Please don't hesitate to contact me if further information is required.

Yours faithfully,

A handwritten signature in purple ink that reads "Michele Adair".

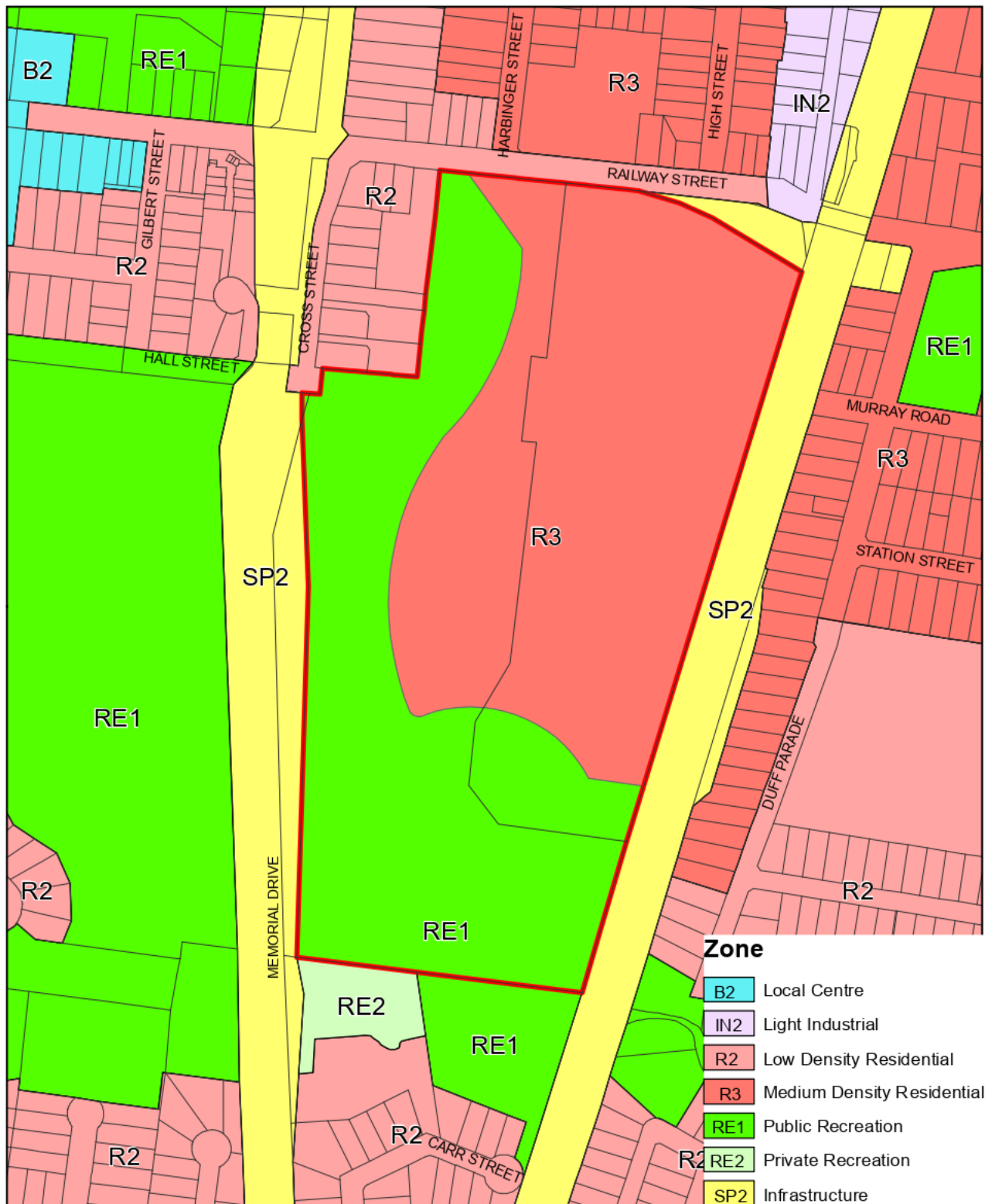
Michele Adair
CEO

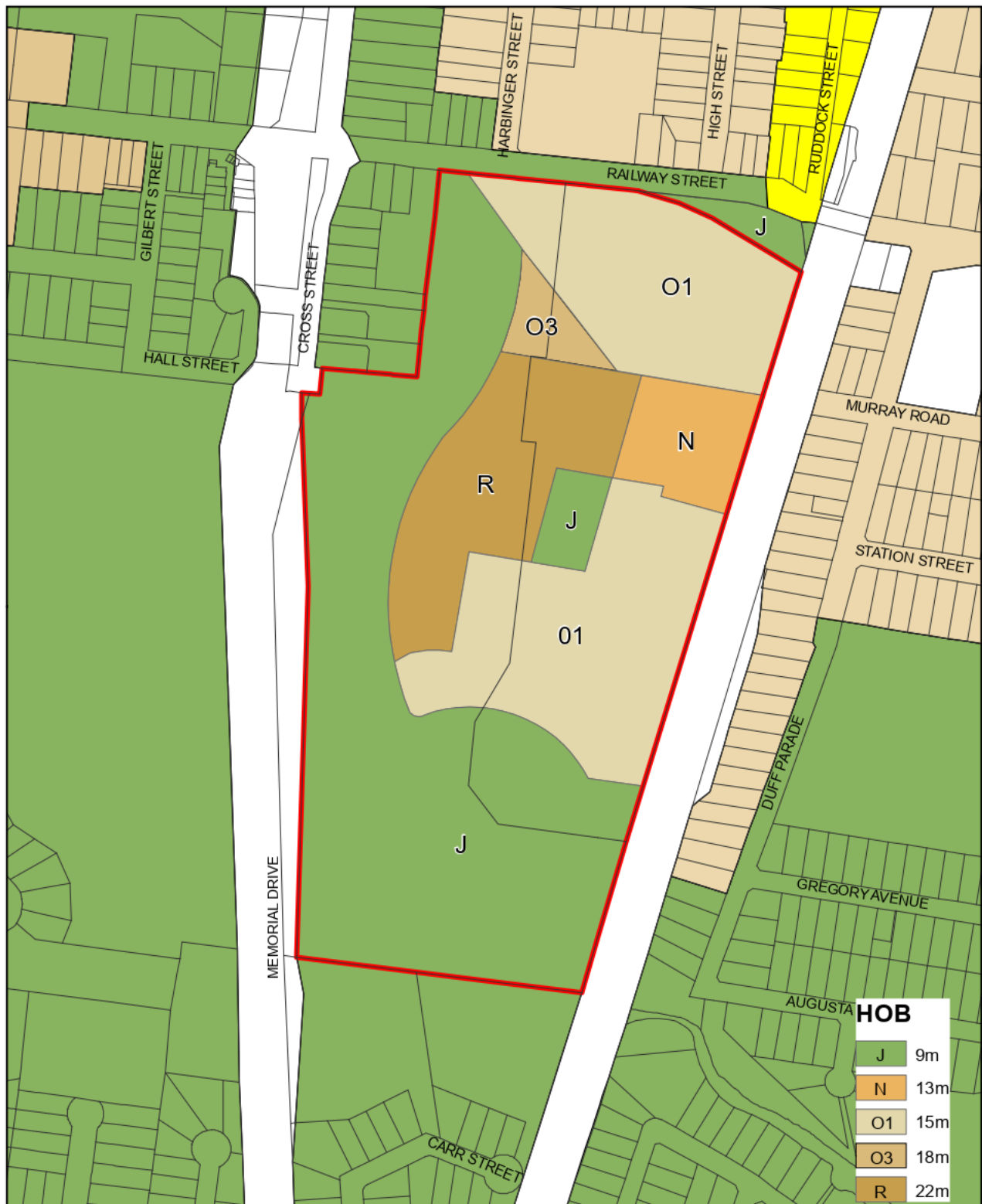
Cc Mike Williams, Legacy Property

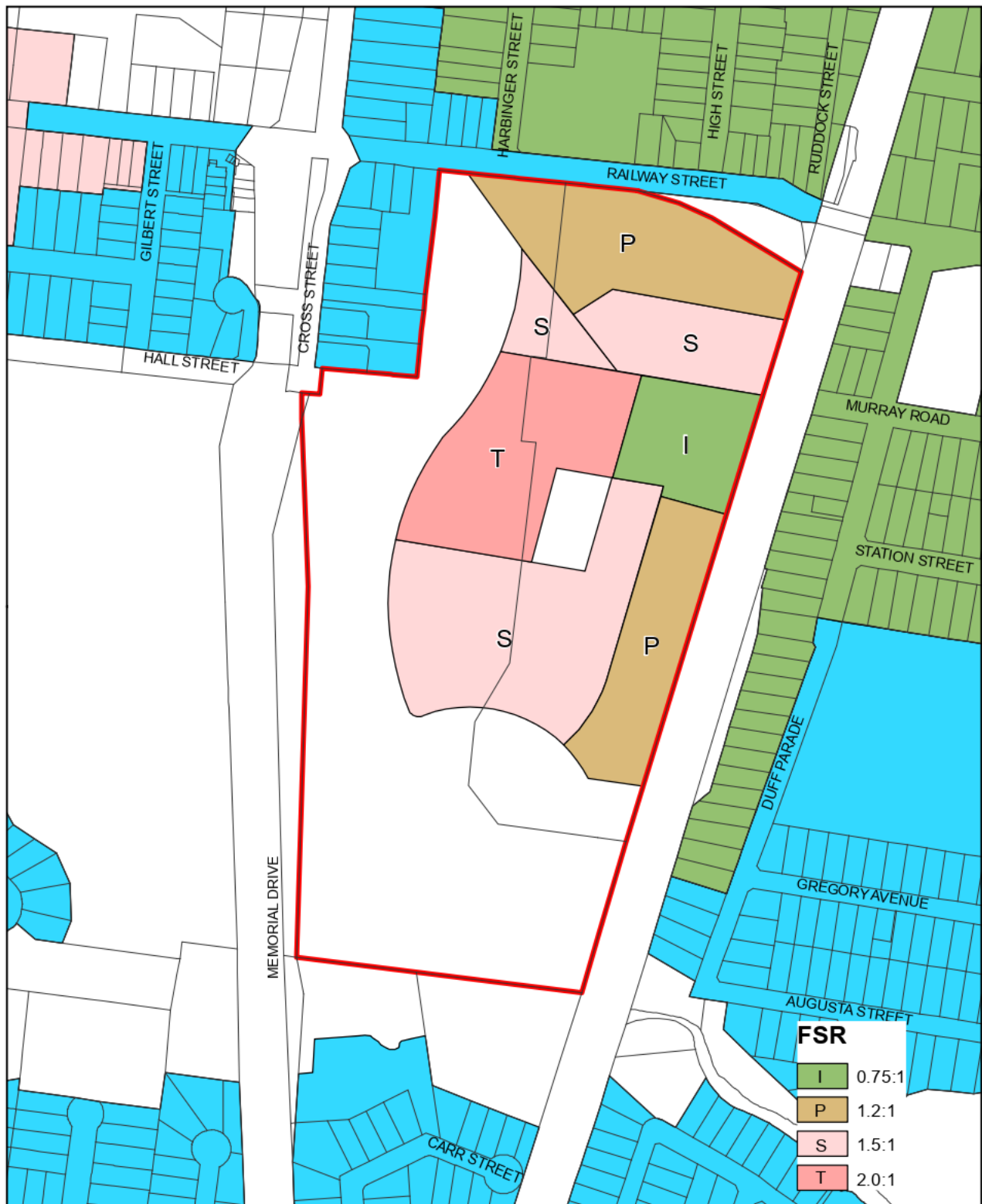
Housing Trust

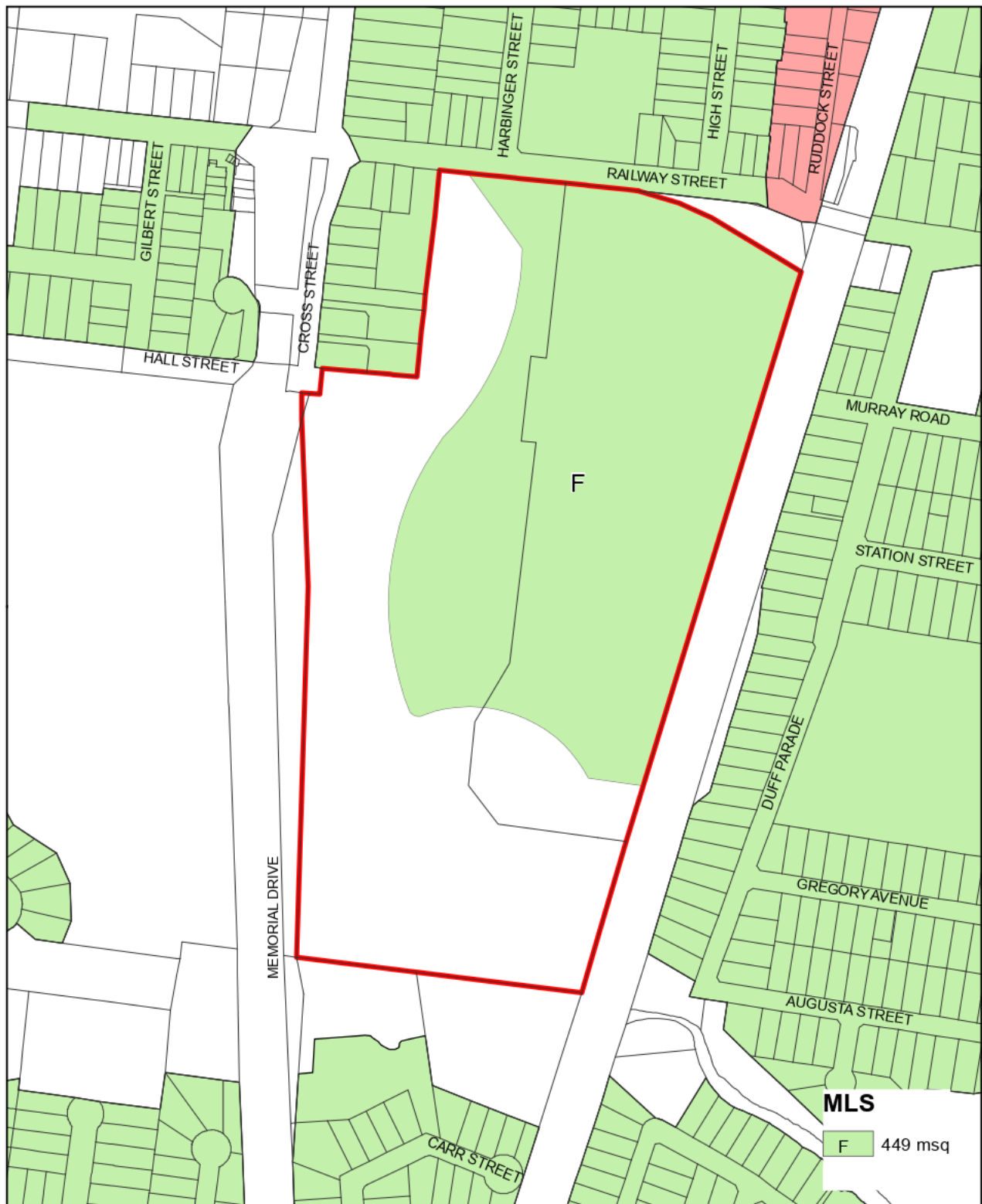
Level 7/5 Bridge Street, Coniston NSW 2500
PO Box 1, Coniston NSW 2500
P 02 4254 1166 F 02 4254 1122
E info@housingtrust.org.au
ABN 18 739 426 566

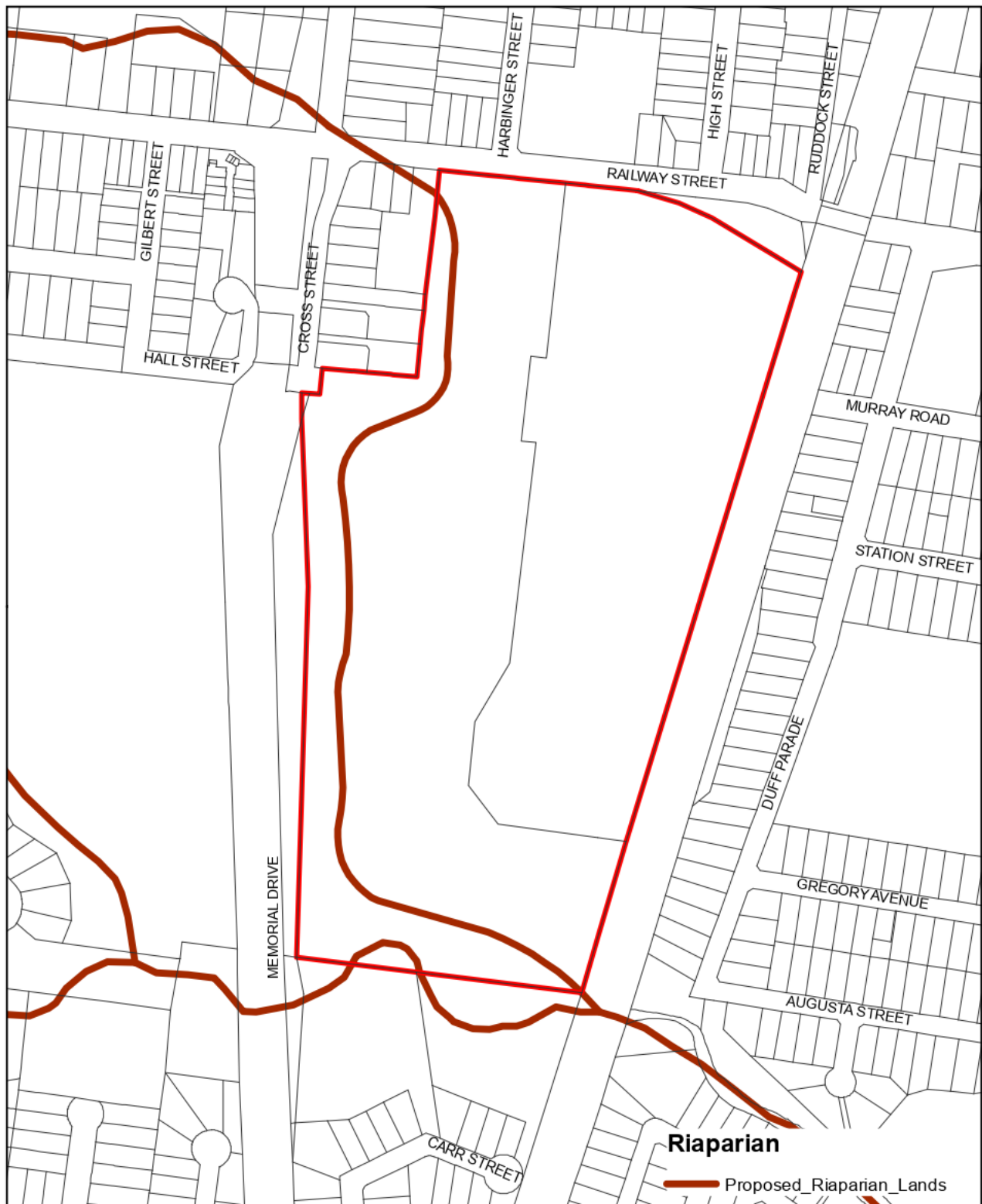
housingtrust.org.au

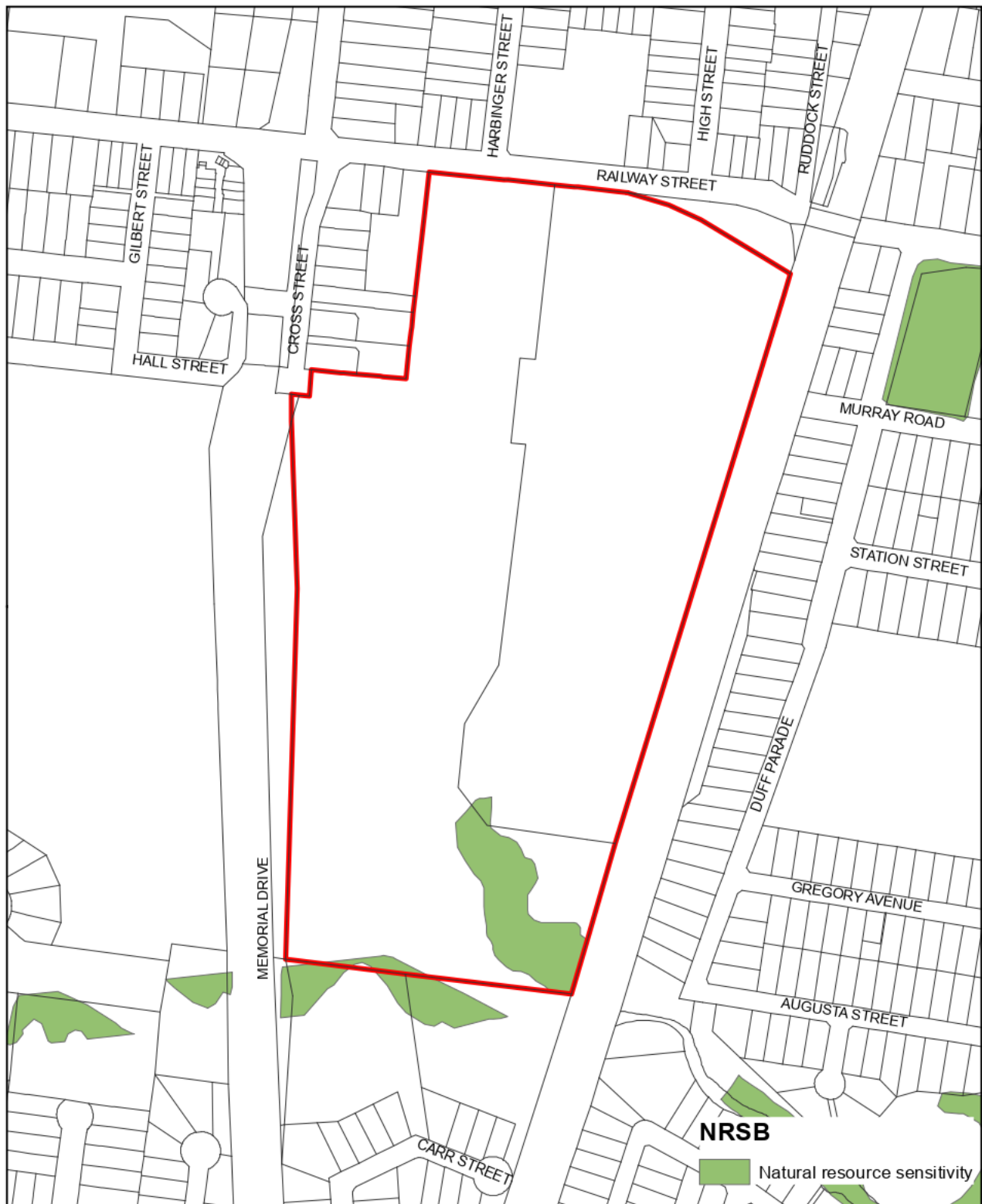












Wollongong Design Review Panel
Corrimal Coke Works, proposed Master plan
19th July 2019.

The panel has been engaged by Council to provide design commentary upon the proposed Master Plan and the accompanying draft Development Control Plan (henceforth draft DCP) forming part of the proposed Planning Proposal application for the site formally occupied by Corrimal Coke Works. The Panel understands that the proposed Master Plan has been provided by the Applicant as a vision for the site, which will ultimately inform the Planning Proposal to change the applicable LEP controls for the site and the creation of the site specific DCP. Therefore, the Panel understands the importance of adhering to best-practice and place-based outcomes for the site and the following comments are being issued in a cooperative spirit with the aim of achieving the expected levels of excellence for a site of this importance and significance.

In providing the below commentary, the Panel has received and reviewed Copies of the Planning Proposal, draft DCP and various supporting documents, provided to the panel prior to the meeting, visited the site (with the Applicant and Council Staff in attendance); held a briefing session with Council staff post site visit, and undertaken internal correspondence amongst panel members.

The briefing meeting with Council Staff took place at Council's offices after the site visit held on 16th July 2019; people in attendance at the meeting are listed below:

Panel

Karla Castellanos
Marc Deuschle
David Jarvis (Chair)

Council Officers:

Kathy Adams – Strategic Project Officer
Jon Bridge – Senior Strategic Project Officer
Joel Thompson – Coordinator Heritage
Suri Mora – Senior Environmental Project Officer
Brett Morrissey – Environmental Strategy Officer
Gaby Kirwood – Environmental Assessment Officer
Mathew Carden – Senior Stormwater Development Engineer
Andrew Heaven – Development Engineering Manager – Development Assessment & Certification
Felicity Skoberne – Design Officer - Project Delivery

Proponents:

Mike Williams – Legacy Property
Adrian Kilburn – Legacy Property
Jenny Rudolph – Elton Planning
David Randerson – DKO Architecture

Nicola Traise – DKO Architecture

Context

The 18.18-hectare site known as the Corrimal Coke Works site is located in close proximity to Corrimal centre, immediately adjoining Corrimal Railway station. Vehicular entry to the site is available only from the north edge of the site via a low-density residential street (Railway Street). The eastern edge of the site adjoins the rail corridor and the western edge of the site adjoins a busy road (Memorial Drive). An isolated pocket of low density residential adjoins the north western corner of the site. An ecologically sensitive area - a remnant community of Forest Red Gum Thin-leaved Stringybark Grassy Woodlands - that is currently home to a colony of flying foxes, is located on the southern edge of the site. A second ecologically sensitive area – a remnant community of Acacia Scrub – is located in the north-western corner of the site. The site has a high proportion of heritage-significant buildings and artefacts – remnant objects of the former use of the site, which are an intrinsic to the character of the site and history of the area.

Site planning

The fundamental planning of the site follows logical strategies for the majority of the site however some areas require further resolution and investigation or additional clarification. The following strategies are endorsed by the panel:

- A main entry road located to connect Railway Street (and Corrimal Centre beyond) to a public plaza created around the railway station.
- The inclusion of a modest area of retail around the railway forecourt.
- The creation of precincts focusing on common characteristics and fomenting diversified cluster identity and variety across the site, which can be reinforced by the different sections of the site specific DCP.

The areas that can potentially be endorsed pending further resolution, investigation or clarification include:

- Rational street network with lanes that potentially reduce vehicular access points on the primary streets. However, the proposed street hierarchy needs to comply with Council's road carriageways, especially those regarding on-street parking widths.
- The diversion of the creek contributing to the potential for a landscaped buffer around the eastern and southern perimeters of the site. The critical issue to be clarified is what areas truly contribute to the amenity of future residents without being segregated for the sole purpose of drainage and flooding mitigation, with particular reference to the steepness of slopes.
- The inclusion of a variety of housing typologies and building heights pending further clarification on exactly how many typologies are being proposed other than RFBs and terraces. The panel received conflicting messages that there are only two types rather than the variety and number of housing types shown in the leaflet titled Incorporating Housing Types dated 06/12/18.
- Incorporation of heritage elements within the site pending the resolution of the extent of the curtilage and the number of elements that are retained.

The relevant areas that cannot be endorsed due to a lack of information or clarity are:

- The Placement of buildings across the site in relation to the level of excavation against the existing topographical levels. Detailed documentation of the resulting levels after excavation across the site is missing.

- The strategy for remediation of the contamination across the site as this might impact on the publicly accessible areas of the open space versus being contained within the private domain.
- The resolution on the proximity, scale and extent of bulk placed around the Flying Fox colony currently present on site.
- The approach to water sensitive design across the site.
- The draft DCP chapters relating to lighting, awnings, public art and signage. Should these be included specifically for the site or as variations, omissions or exclusions from the comprehensive WDCP?

The Panel is of the opinion that some of the above mentioned areas need not a mere contextual justification, but material and palpable detail development, if the potential of this site is to be successfully realised. The following comments are issued in an attempt to assist the Applicant on resolving the most relevant issues:

Overall character

While the idea of the creation of distinct precincts is endorsed by the Panel, it is the Panel's general concern that this principle has not been used to promote significant differences in character between the the various precincts - setbacks, street widths, landscape character and even the typology of buildings appears to be very homogeneous across the site. The creation of character cannot simply be relegated to the appearance of the buildings, but must be carried throughout all sectors of the urban form and landscape.

Streets

The detail resolution of each street must be considered holistically, vehicular carriage widths must meet council's minimum requirements and adequate space must be allocated to accommodate significant landscaping within each street. For this to be successful it must be co-ordinated with vehicular access to buildings, on-street parking and street lighting requirements. This is a three-dimensional exercise which must be clearly demonstrated in a site plan as well as diagrammatic sections.

The road typology suggests a bus loop road that brings people to and from the station plaza only. If the bus route followed the 'main loop' instead, with an additional stop in the southern portion of the site, the site would be better serviced by public transport.

The road currently running EW as the 'bus loop' would be a great pedestrian link between the plaza and the riparian corridor. The focal connection between these two ends should be considered, celebrating the view to the riparian landscape in the same way the chimney focusses attention towards the urban landscape.

The single point of vehicle access may create safety issues on a development of this scale. If this entry point is blocked (road works, traffic accident, fire) there should be an alternative point of access and egress. It is essential that the vehicular access point connecting to the railway carpark becomes operational.

Street setbacks

Clause 5.2 table 4 of the draft DCP documents street setbacks and articulation zones. It was noted that the proposed setbacks (varying from 3.5m to 5m) are significantly less than would be permissible in surrounding areas. This was justified by the applicant, due to the typology

of building being different to that of the surrounding area. Surely some of the character areas: Tree Top Escarpment, Green Edge and perhaps Village Park warrant more significant setbacks to reflect the landscape character of these precincts. The Panel makes the following recommendations:

- Civic Hub
A setback of 4m is acceptable for residential buildings, pending an appropriate interface with the street to allow good amenity / privacy to residents whilst providing an appropriate contribution to the streetscape. This precinct will require wider streets and further development of built form to avoid the taller buildings with reduced setbacks from creating a canyon. Stepping of buildings to create a lower scale street wall contained within a generous tree lined boulevard is recommended. Detail sections and street level perspective are required to develop and demonstrate the street character. Basement areas should not be allowed to encroach into the 4m zone to ensure sufficient deep soil and space for tree root zones.
- Tree top escarpment, Creek Edge Road and Village Park.
The character of these precincts should be notably different to the Civic Hub and more akin to buildings in a landscape setting. A minimum landscape zone of 6m is recommended; this area must be dedicated to deep soil landscaping. Building articulation and significant areas of hard paving should not be located in this zone. If a building is a single dwelling with vehicular access from the street, the set back must not be dominated by driveways, carpark structures and it should provide increased opportunity for landscaping. A variety of housing types i.e. single stand alone, semi-detached and attached dwellings should form part of each precinct.

Articulation zones should be clearly defined within the draft DCP as the definition of the articulation zone will likely be very different for different typologies of building. An articulation zone for a six storey RFB should be aiming to achieve a very different out-come to that of a two – 3 storey dwelling.

Note: The setback table is confusing when the articulation zones can encroach up to 3.5 metres into a 4m setback. The articulation zones read as though the street setbacks is as small as 0.5m. The way in which this control is written can be interpreted as delivering an unwanted or unforeseen adverse outcome. Setbacks should be kept clear of articulation zones.

The draft DCP should include diagrams in plan form to evince the desired outcome of the front setbacks in relation to how the following elements are intended to relate to each other: the provision of driveways, pedestrian footpaths, fencing, mature planting, deep soil zones, landscape strips in front of fencing, footpaths, verges and street trees. The plan diagrams should be provided for all building typologies, but especially for areas where continuous front-loading terraces or stand-alone dwellings on narrow lots are being contemplated. The Panel will not endorse garage-dominated or driveway dominated front setbacks with a lack of mature planting in the front courtyards and the street verge due to inherent conflicts with multiple driveway crossings.

Creek

The site's eastern and southern interfaces provide the majority of common open space within the development. Yet, the majority of these spaces seem to be unusable due to their significant grades (1:2 - 1:4). The proposed topography of the site must be clearly documented. The panel is not yet convinced that adequate set back has been provided

around the creek to accommodate meaningful common open space and adequate creek edge batters allowing future access and maintenance.

The proposed batter on the eastern side of the creek is set at a grade of 1:2. Consideration must be given to how the bank is stabilised, how access is provided for users, its visual appearance, and the ease of access for maintenance. Further to this, it should be demonstrated that flood events common to the area will not erode and significantly affect the newly-aligned creek in the future.

The neighbours adjoining the north western corner of the site currently enjoy a densely landscaped interface with the site. This will change significantly with the realignment and development of the proposed creek and associated batters. The proximity of the creek to these neighbours should be adequate to allow a landscaped buffer at the perimeter of the site. This buffer should be suitable for maintenance access when required.

Height

Corrimal town centre currently allows a maximum building height of 15m. It must be noted that the proposal to provide significantly taller buildings (24m) on the outskirts of the centre is not consistent with a typical town centre hierarchy.

However, it is also acknowledged that the site's proximity to the railway station and its significant size create the potential for some taller building forms on this site. Justification must be provided for the proposed height in the form of a view analysis. This view analysis should be provided from distant vantage points around the site. It will assist in determining an appropriate height and the placement / extent of taller buildings on the site.

The heritage chimneys currently located on the site are prominent landmarks in this area and should remain as the dominant forms and silhouettes in the skyline visible in views across the site.

The relative level of each building should be established so the proposed relationship with the heritage chimneys is complementary and harmonious, with the relationship of these buildings within the broader context clearly represented.

Topography

The natural topography has been altered during the site's time as a Cokes Works. The site now consists of a series of artificial mounds and valleys that would significantly restrict the development potential of the site, if retained. The proponent's strategy to level a large portion of the site, to accommodate a rational street layout is understandable.

This is a major undertaking that will have a significant impact on the proposal. It must be more clearly documented, to evince the way buildings sit within the site and relate to the railway, to neighbouring buildings, and to Memorial Drive. A site plan should be provided showing road levels, major landscape levels, creek levels, and the extent of cut and fill proposed.

Part of this process should also be to develop a strategy to accommodate contaminated fill on the site. Proposed location / strategies for dealing with contaminated fill should be clearly

shown on the cut and fill diagram. Preference should be given to this fill not being in the creek corridor or under any public spaces.

Built form

Further detailed information is required to document the building forms shown in the master plan. Some basic dimensions and RLs, to show how buildings relate to each other, the streetscape, and the topography of the site are required.

It must also be demonstrated that building forms shown in the Master Plan are an accurate representation of the heights and FSRs that are proposed in the Planning Proposal. The Panel, currently has no way to determine if the proposed FSR can be achieved in the forms shown on the master plan, or if the forms outlined in the site-specific draft DCP are larger than necessary to accommodate the proposed FSR. It is assumed that the Applicant has undertaken this work as part of the design process. However, this information should be provided to Council and the Panel to assist with the review of the proposal.

One observation that can be made from the information provided is that the number of storeys shown in the master plan does not match the maximum heights that can be achieved in the planning proposal. Planning proposal heights vary from 13m up to 24m which will allow buildings between 4 and 7 storeys. Whereas the Master Plan information provided to the Panel shows buildings ranging from 2 to 6 storeys. Building heights should be refined to reflect the design intent.

The typology of each building shown on the Master should also be clearly stated.

Clause 7.13, Basement parking (RFB)

This clause as currently proposed allows basements (less than 1.2m above ground level) to extend to the property boundary. This should be avoided in the Tree Top Escarpment and Creek Edge precincts, where the aim should be to create more of a landscaped setting. It would be more appropriate to state that basements are not to extend past the footprint of the building above.

Minimum lot size

A minimum lot size of 135sqm is proposed; an example of this typology of housing has been provided (DKO Architects, page 65, Rear Loaded Terrace). The 4.5m width terrace is a reasonable building typology. However, consideration should be given to increasing the area to accommodate more generous setbacks to the street (as outlined above under Street Setbacks). It will be a better outcome to limit this lot size to buildings that are serviced by rear laneways to avoid garage-dominated streets.

Minimum private open space

The proposed minimum area of private open space (16sqm) is less than generous. It is noted that all the dwelling type examples provided by the applicant appear to have areas of private open space in excess of 16sqm. A minimum area of 25sqm, with a minimum width of 4m is recommended.

Building in south eastern corner

Entrances to the buildings proposed in the south eastern corner of the site do not appear to be serviced by a street. They also appear to be proposing an at-grade external carpark that abuts the railway.

Further detail information / development is required to demonstrate that these are appropriate building forms that will contribute to the quality of the development. A landscaped buffer should be provided between the buildings and the railway. A clear process for pedestrian entry and vehicular drop-off must also be demonstrated.

Building length

The master plan presents a number of indicative building forms that resemble RFBs located mainly on the 'Treetop Escarpment', 'Civic Hub' and the southern part of the 'Green Edge'. These forms are indicative of potential continuous footprints over 100m long footprints in some areas. A maximum building length control is strongly recommended per precinct to further reinforce the desired character of each precinct.

Setbacks above street wall height

The proposed upper level setback is 3m above 5 storeys for areas shown in Figure 6 (page 21). This applies to areas of the Entry Boulevard that are shown to be in the Road Hierarchy sections as being no more than 4 storeys tall. Furthermore, no street section in the draft DCP (pages 49-55) reflects heights greater than 4 storeys, which as discussed above, is confusing as the proposed heights can allow heights of 6-7 storeys.

Heritage

It is understood that the site is currently under-going assessment as a state significant item. This may potentially have a significant impact upon the placement of built form, retention of existing structures and curtilage around areas of significance.

Regardless of this assessment / determination, there are laudable principles and statements in the masterplan and associated documents which should be incorporated into the site-specific draft DCP.

A new section should be created in the DCP which outlines the significance of the industrial heritage of the site and explains how significant elements must be retained – directly or in some form of interpretation.

This section should specifically highlight/catalogue all heritage items on the site and indicate the level of retention/interpretation allowed and expected for each.

Public Art

A new section should be created in the draft DCP which specifically quantifies and locates public artworks within the development and outlines their objectives and controls. The re-use of suitable heritage elements and site materials should be dictated as required for all such artworks. Artworks should speak to the previous heritage and the future character of the site.

For example, the landscape masterplan notes that the riparian corridor will be provided with a distinctive character by the provision of artworks using salvaged site materials. In order for this to eventuate, it must become part of the controls of the site – how many artworks, their locations, their purpose, their materiality.

Further to this, all salvaged materials and items of heritage value should be retained and registered so that they can be successfully integrated into any such artworks and also into built form, architecture and landscape architectural elements such as furniture / structures / entry statements / signage / etc.

Interface with ecologically sensitive area to south

The ecologically sensitive area to the south of the site will be flanked by 4-5 storey buildings forms on three sides. This area currently accommodates a camp of flying foxes. It is important that existing wildlife habitats on the site are preserved and incorporated in such a way as to ensure they can co-exist in harmony with the proposed development. The Panel will defer to council's expert as to whether the proposal will maintain a suitable habitat for flying foxes.

However, the Panel members are concerned that the proximity to the flying fox camp will create a significant level of disturbance to people living in these buildings. The bats can be extremely noisy and will inevitably generate guano either on or in close proximity to these buildings. This must be a consideration in the detailed design of these buildings. It is likely to impact the orientation of dwellings, material selection and noise attenuation.

There is a question in the Panel's mind as to why the area around this colony is being proposed for heights of 4-5 storeys when the taller heights should be located around the Civic Hub and then gradually transition down as they move away from the main height concentration.

Landscape

It is appreciated that the landscape masterplan at this stage is somewhat indicative. The draft DCP should reflect the specific landscape controls and objectives specific to this site in greater detail.

With regards to the landscape masterplan, it is commended that the existing communities of vegetation on the site have been mostly retained and incorporated into the design. Further to these communities, it is clear from the site inspection that there are many mature, healthy trees scattered throughout the site. Admittedly some (such as those existing on artificial mounds and surrounding the dams) may not be possible to retain; however, it is expected that all trees, not subject to such conditions, should be retained to maintain as much tree cover as possible. Assessment of their removal should be determined as part of the proposed cut-n-fill exercise and subsequently upon the DA assessment of individual lots.

The riparian corridor forms the most significant open space within the development, but it is mostly inaccessible due to steep indicative levels (1:2-1:4). More detailed levels should be provided and consideration should be given to widening some parts of the creek / corridor to allow for usable open space within this zone.

Pedestrian connections to the riparian corridor appear to be minimal and limited to the northern two thirds of the site. Given the linear nature of the corridor and its potential of connecting spaces with the site, more consideration must be given to how the site, particularly the southern part of the site, can be connected.

The Central Park is a valuable addition to the masterplan. It appears the 'built / hardscaped' spaces within the park are placed on the southern edge, however, when considering

pedestrian movement through the park and from across the greater site this may be better suited on the eastern edge.

It is commended that street tree choices being considered at this stage. All trees proposed as street trees should have a form and growth pattern suitable to this use, and their proposed location – a high canopy and clear trunk and size relative to the hierarchy of the street. It appears some tree selections do not fit this criteria and should be removed as choices. A street tree masterplan should be considered to be included in the draft DCP outlining species choice, maximum spacing, planting size and location.

Water-sensitive Urban Design

Inclusion of WSUD has been presented as an overarching design principle and is discussed within several of the documents associated within the masterplan. It is commended that it is being included within the development, however, this does not form part of the chapters outlined in the draft DCP.

A new section should be created in the DCP which specifically outlines WSUD objectives and controls for the site. This section should indicate the broad and specific requirements for WSUD and should include locating, and indicating what type of WSUD device is to be used.

Draft DCP Comments

Given the reviewed draft DCP document is in draft form, it is expected that many of the general comments will be edited prior to the final review issue of the draft DCP.

Generally:

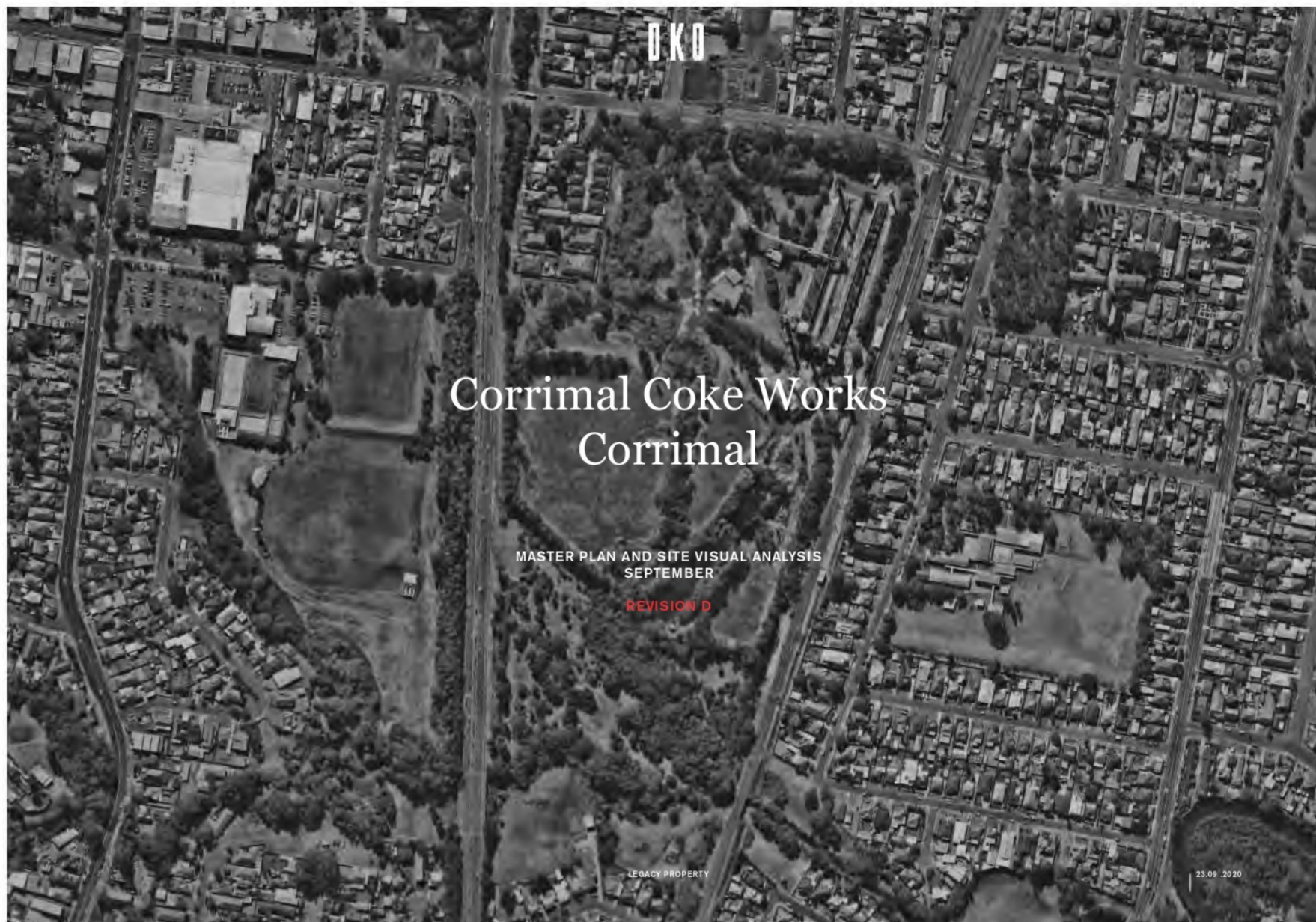
- Many sentences are overly long and difficult to comprehend. Further punctuation and breaking long sentences into several shorter sentences may help clarify the purpose and content.
- Several Principles or objectives do not direct the reader adequately leaving the interpretation of the principle open: e.g. p9 '*Dwelling Diversity and Innovation*'; could read instead '*Establish Dwelling Diversity and Innovation*' to clarify intent.
- Section 3 - Vision and Development Concept – may benefit from a clear set of diagrams outlining the major principles and major site moves as an introduction to the entire development.
- The existing character of the site should be further elaborated, p11.
- The character of the existing site, and the proposed precincts, should be strongly defined in the draft DCP – this is currently not the case with character statements not adequately describing the character.
- The Character Controls (4.2.1 – 4.2.5) need refinement to become actual controls. Remove 'Desired and Future' from title. The desired future character should talk more about actual character – several inclusions are principles or objectives – character is different. The precincts' character could be defined consistently by talking about the existing character to be retained, the built form character, and the landscape character phrased as controls.
- Instead of table 1 outlining superseded chapters, could this be covered specifically in each section of this DCP to be less confusing? For example, the first sentence in each specific section outlines whether the general DCP applies / does not apply / or is supplemented by the site-specific draft DCP.

- Plans of the streetscape character per building typology or streetscape should be included.
- Street sections need to reflect the intended number of storeys and upper level setbacks.

Summary

The Panel strongly recommends the draft DCP to be further resolved in order to provide Council and the surrounding community with a level of certainty that the proposed Master Plan can be delivered once a number of issues are resolved satisfactorily. It is the Panel's general opinion that given the site's proximity to Corrimal Railway Station, a varied and robust mix of dwelling types combined with a modest retail offering could potentially provide a positive contribution to this area. The draft DCP should aim to provide guidance and become a real tool for Council and the Panel in the assessment of future DAs on the site. However, for this aim to be realised, further detail information and development of the following issues is essential:

- Refine street interfaces
- Clear documentation of proposed building envelopes (relate proposed FSR to Master Plan forms)
- Building height to be changed to relate to the building height shown in the Master Plan.
- Further justification of proposed height in the form of a view analysis, then development of built form to respond to this analysis.
- Clear documentation of site levels (including but not limited to cut and fill diagram).
- Increase of minimum lot size and private open space.
- Clarification of the Heritage state significance of this site.
- Detail clarification of creek, embankment and associated areas of communal open space.
- More specific control and objectives generally within the draft DCP including additional sections as outlined above.



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ISSUE	DATE	PREPARED BY	REVIEWED BY	COMMENT
P1	14.08.19	NT	DR	DRAFT
A	15.08.19	NT	DR	FOR SUBMISSION
B	04.09.19	NT	DR	FOR SUBMISSION
C	10.02.20	PP	DR	FOR SUBMISSION
D	23.09.14	JY	PP	FOR SUBMISSION

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INTRODUCTION

This view analysis has been prepared on behalf of Legacy Property and Illawarra Coke Company to demonstrate the impacts of proposed built form on the Corrimal Coke Works site.

The report includes of site photographs to analyse of existing visibility of elements on the site, as well as photomontages to show the visibility and impact of the proposed development envelopes on these views.



INTRODUCTION
VIEW SELECTION & METHODOLOGY

A series of view locations were selected to illustrate potential view impacts for key areas surrounding the site, notably from existing lower-rise residential areas and towards Corrimal town centre. A key focus was to consider potential impacts on westerly views towards the escarpment from areas to the east of the site. All view locations were within the public domain.

From an inspection of surrounding areas it is evident that, while the features of the site notably the chimneys are prominent in selected locations, existing vegetation and other built elements create significant visual buffers that obscure many views to the site.

Photographs were taken from selected view locations at eye height using an appropriate lens to obtain images that reflect a human's perspective and view of the site.

The 3D model used to generate the photomontages represents building envelopes based on the proposed maximum LEP height limit and are therefore representative of the extent of view impacts. The presence of existing chimney structures on the site also provides a benchmark to ensure that the representation of building heights is accurate.

It is also important to note that the photomontages present solid building envelopes. In this regard, the visibility of proposed envelopes is more pronounced and does not reflect the benefit of a mixed palette of appropriate materials and colours, as well as build articulation, that will ultimately be delivered.



INTRODUCTION
SELECTED VIEWS



INTRODUCTION
SELECTED VIEWS



INTRODUCTION
SELECTED VIEWS



INTRODUCTION
SELECTED VIEWS



**VIEW 1
MURRAY ROAD**

View 1 is taken from the footpath on the southern side of Corrimal High School on Murray Road, approximately opposite 55 Murray Road.

The quenching towers and C2S stack (not proposed to be retained) are the most clearly identifiable, with the brick chimney slightly obscured by surrounding street trees.

The photomontage illustrates that the development generally sits within the context of existing vegetation and built elements, and the impact are considered negligible. Importantly, the view to the escarpment remains largely uninterrupted.



**VIEW 2
MURRAY ROAD**

View 2 is taken from the footpath on the southern side of Murray Road, approximately in front of 87 Murray Road.

This position has a clear view to Corrimal train Station, with the brick chimney stack (to be retained), quench towers and C2S stack (not proposed to be retained) also clearly visible.

The view impacts at this location are slightly increased, due to the closer proximity to the site, and there is some loss of the escarpment view. However, a significant portion of the proposed built form is obscured by existing mature trees and importantly, the escarpment and ridge line remain prominently visible so this important landscape connection is maintained.



EXISTING



PROPOSED

ALTERNATIVE VIEW 2
MURRAY ROAD, VIEW FROM
OPPOSITE SIDE OF THE STREET

An alternative View 2 is proposed from the footpath on the Northern side of Murray Road, approximately in front of 87 Murray Road.

This position has a clear view to Corrimal train Station, with the brick chimney stack (to be retained), quench towers and C2S stack (not proposed to be retained) also clearly visible.

This view highlights one of the big changes in the masterplan which is to align the masterplan with visual corridors. From Murray Road, there is a direct visual connection to the escarpment through the masterplan.



**VIEW 3
RAILWAY STREET EAST**

View 3 is taken from the footpath on the southern side of Railway Street, approximately in front of 19 Railway Street.

The northern chimney stacks are the most visible existing elements on the site, while existing mature vegetation obscures further views into the site. The closest northern stack is proposed to be retained.

The photomontage indicates that there are negligible view impacts, with only filtered views to the built form through existing vegetation, and the building height limit sits below the tree line. The closest stack, being the C1N stack, is proposed for retention and will remain the most prominent visual element from this location.



EXISTING



PROPOSED

**VIEW 4
HARBINGER STREET**

View 4 is taken from the road verge on the western side of Harbinger Street, approximately in front of 1 Harbinger Street and the intersection of Harbinger Street and Junction Street.

Views to the site are largely obscured from this position by the new residential flat building under construction on the former Corrimal Leagues Club site. The only clearly visible elements are the tops of the northern metal chimney stacks, as well as existing vegetation at the end of Harbinger Street.

There are no view impacts from the proposed development at this location.



EXISTING



PROPOSED

**VIEW 5
RAILWAY STREET WEST**

View 5 is taken from the footpath on the northern side of Railway Street, approximately outside 108 Railway Street. This view is representative of the site's visibility from the Corrimal town centre.

Despite being situated at a higher ground level than the site, the majority of the site is not visible from this position, being mostly obscured by the tree canopy and built form in the foreground. The most distinctly visible site elements are the top sections of existing chimney stacks.

The photomontage shows there would be minimal visual impact from this position, with the maximum building envelopes only just visible above the built form and tree canopy. The retained brick chimney and C1N stack will retain prominence as the visible site elements.



EXISTING



PROPOSED

**VIEW 6
RAILWAY STREET WEST**

View 6 is taken from the footpath on the northern side of Railway Street, approximately opposite 51 Railway Street.

As this position sits lower than View 5, the site and its existing elements are hardly visible as they are obscured by existing vegetation and tree canopy. Only a very small portion of the northern chimney stacks are visible.

There are no view impacts from the proposed development at this location.



EXISTING



PROPOSED

**VIEW 7
MEMORIAL DRIVE**

View 7 is taken from the eastern footpath on Memorial Drive, adjacent to approximately 2 Hall Street.

There is limited visibility of the site and existing elements, due to existing vegetation filtering the view. The solid fence sitting in front of Cross Street also limits visibility of the site.

The photomontage indicates that there are minimal visual impacts in this location, with a small portion of built form visible and significantly filtered by existing vegetation. The building heights sit well under the tree line and only just above existing built structures.



**VIEW 8
ROTHERY STREET RAIL BRIDGE**

View 8 is taken from the southern footpath in approximately the centre of the Rothery Street Rail Bridge.

With the elevated height of the bridge, the existing vertical built elements on the site are clearly visible above the horizon line, with the escarpment beginning to rise up West of the site. The brick chimney stack sits prominently adjacent to the rail and visual corridor looking South towards the site.

With the ground level of and subsequent perspective from this view, the proposed building envelope can be seen above the surrounding built fabric and vegetation. While the heights appear close to that of the brick stack, it remains clearly identifiable.



EXISTING



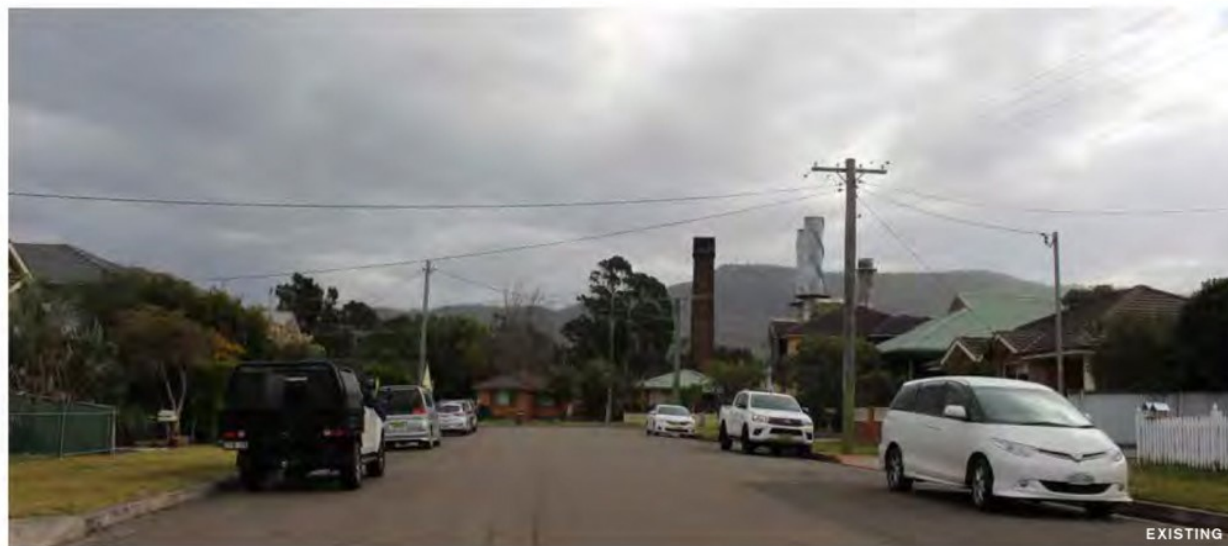
PROPOSED

**VIEW 9
8 STATION STREET**

View 9 is taken from the centre of Station street, in front of approximately number 8 Station Street.

From this position, the brick chimney stack is the most prominent element, sitting almost in line with the northern edge of the road and existing built form setbacks along it. The quench towers and C2S stack (not proposed to be retained) also are clearly visible. These vertical elements sit slightly higher than the escarpment which is slightly visually filtered by existing vegetation

As this view has the second closest proximity to the site and the higher portions of proposed built fabric, the view impacts from this location are slightly increased with some loss of the escarpment view and the proposed heights sitting above it. However, the upper ridge line of the escarpment and brick chimney stack remain prominently visible so these important visual connections are maintained.



EXISTING



PROPOSED

**VIEW 10
25 STATION STREET**

View 10 is taken from the centre of Station Street, approximately outside 25 Station Street.

The quenching towers (not proposed to be retained) and brick chimney are the most clearly identifiable elements on the site, sitting above the surrounding built fabric and vegetation. The escarpment is clearly visible also.

As illustrated by the photomontage, the proposed maximum building envelope sits above but generally in context of the surrounding built fabric. Importantly, it sits below the height of the brick chimney stack and the line of the escarpment, thus not impacting or interrupting their visual significance.



EXISTING



PROPOSED

VIEW 11
29 GREGORY AVE

View 11 is taken from the centre of Gregory Avenue, approximately outside 29 Gregory Avenue.

No existing built elements on the site are visible from this position and perspective. Views to the escarpment are significantly filtered by large existing trees along Duff Parade and within the lots along Duff Parade, as well as the higher existing built fabric in the foreground.

The photomontage shows that only glimpses of the maximum proposed building envelop are visible behind this significant vegetation. It can be seen that the proposed heights sit below and do not impact the visual connection to the escarpment line. Hence, it is considered there is only a minor visual impact.



VIEW 12
5 GREGORY AVENUE

View 12 is taken from the centre of the road outside approximately 5 Gregory Avenue.

Looking directly West at this position, the existing vertical built elements on the site are not visible. There is a clear view to the escarpment, and the significant vegetation along the site's perimeter screen views into the site.

As shown in the photomontage, very little of the proposed building envelope can be seen, with only a small portion visible above the tree canopy. Given this and there being no impact of the view to the escarpment, there is only minor visual impact from this position.



VIEW 13
32 AUGUSTA STREET

View 13 is taken from close to the middle of Augusta Street, approximately outside 32 Augusta Street.

No existing built elements on the site can be seen from this position. The escarpment is visible above the surrounding built form and vegetation in the foreground but is more filtered.

There is no impact on views to the escarpment.



EXISTING



PROPOSED

VIEW 14
3 AUGUSTA STREET

View 14 is taken from the southern road verge on Augusta Street, approximately outside 3 Augusta Street.

There is a clear view of the escarpment looking West from this position and no existing built elements on the site are visible. The vegetation around the site screens visually connectivity to and through it.

There is no visual impact with clear views to the escarpment maintained.

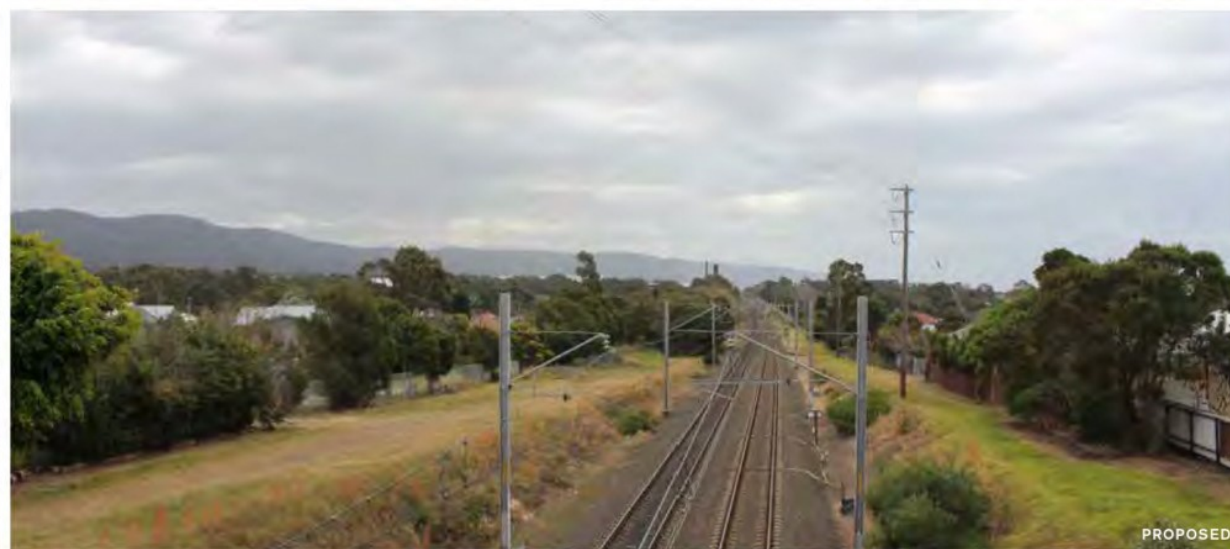
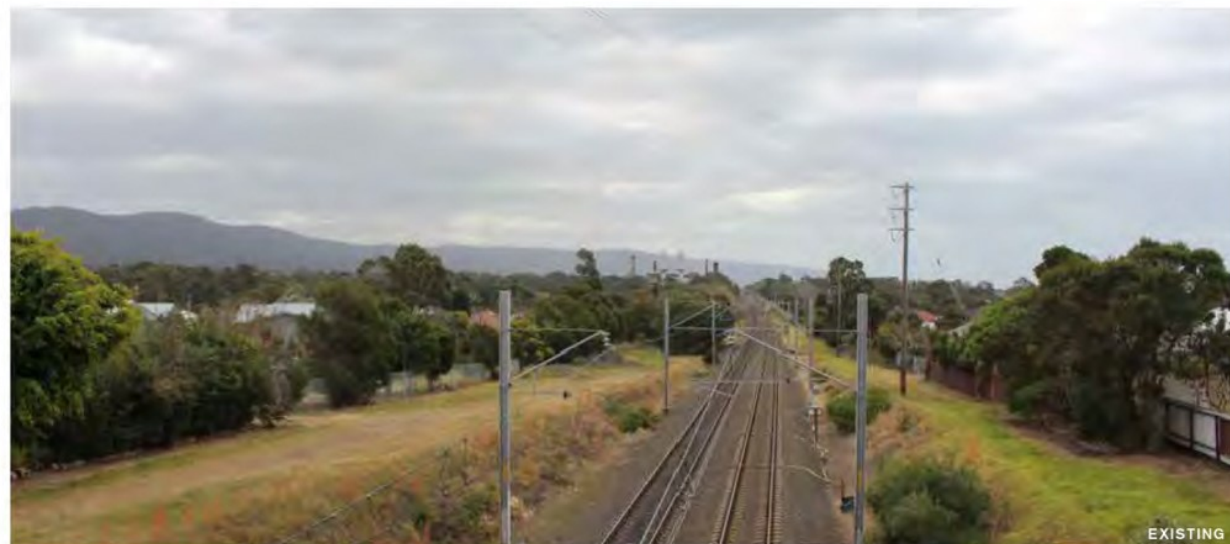


**VIEW 15
TOWRADGI ROAD RAIL BRIDGE**

View 15 is taken from the northern footpath in approximately the centre of the Towradgi Road Rail Bridge.

The existing vertical built elements are clearly visible sitting in line with the escarpment line, and above the substantial vegetation canopy. The brick chimney stack is visible but less prominent due to the vegetation in front of it.

From this position, a top portion of the proposed maximum building height envelop can be above the canopy line but sits below the escarpment line. It also sits below and does not impact the visibility of the brick stack hence it is considered to have a minimal visual impact.

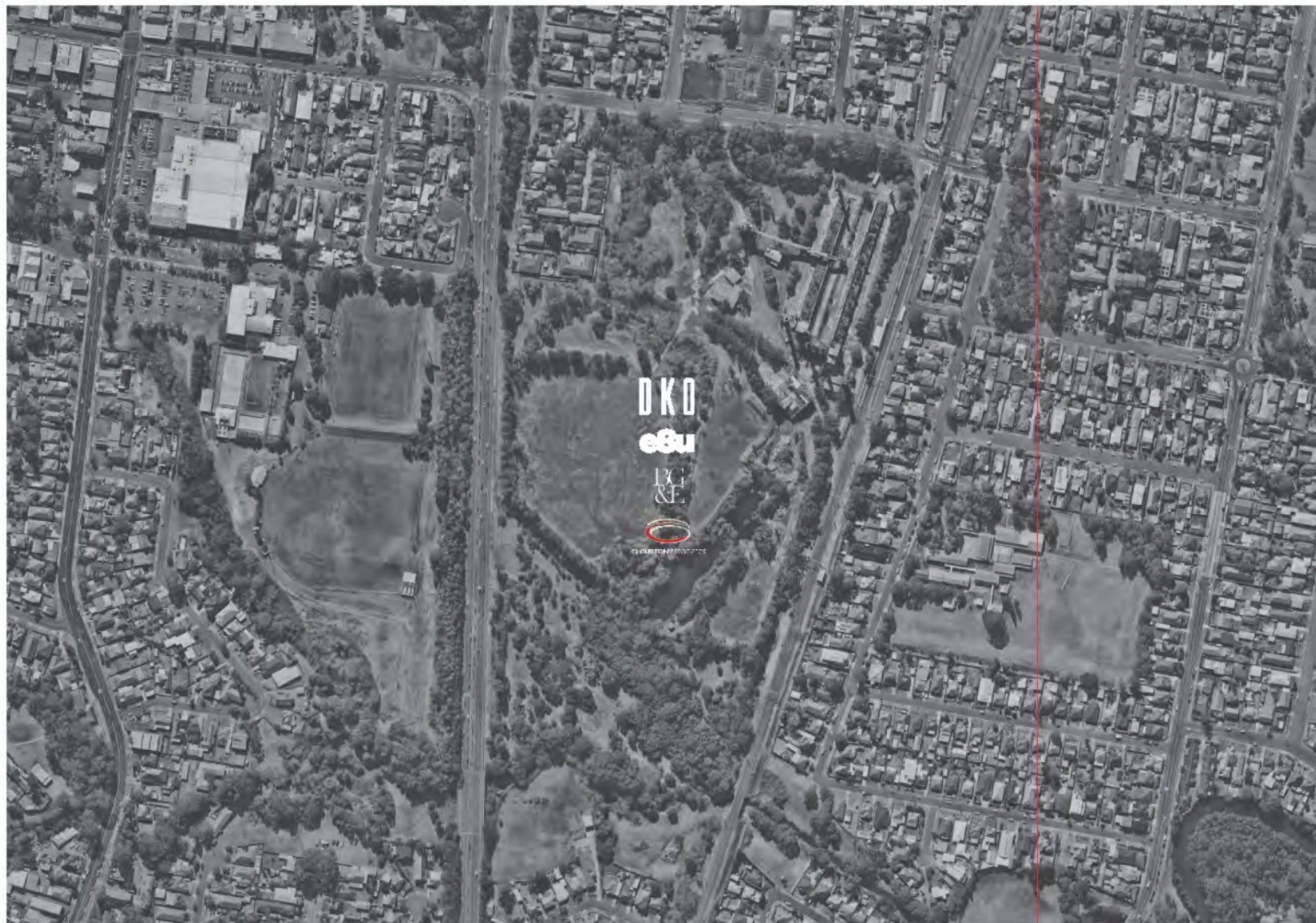


CONCLUSION

To conclude the findings of the visual analysis:

1. The proposed built form does not inhibit any significant or primary views, and in many cases, existing vegetation or built elements provide a visual buffer to the site and the future built form.
2. The proposed built form allows the brick chimney and C1N stack to remain visible from all locations
3. Key locations east of the site retain views of the escarpment.
4. View 2 and 9, representing the closest positions to the site where the proposed built form is more visible, demonstrates that key views to the escarpment and brick chimney are retained.
5. The photomontages demonstrate that the proposed built form has low to negligible impacts on existing views.





**VIEW 1
MURRAY ROAD**

View 1 is taken from the footpath on the southern side of Corrimal High School on Murray Road, approximately opposite 55 Murray Road.

The quenching towers and C2S stack (not proposed to be retained) are the most clearly identifiable, with the brick chimney slightly obscured by surrounding street trees.

The photomontage illustrates that the development generally sits within the context of existing vegetation and built elements, and the impact are considered negligible. Importantly, the view to the escarpment remains largely uninterrupted.



Overshadowing Study:

Between the hours of 9am - 3pm on June 21st, the Central and Southern Park shall receive more than 4 hours of solar access to at least 70% of its area

	9am	10am	11am	12pm	1pm	2pm	3pm
Central Park	✓	✓	✓	✓	✓	✓	✓
Southern Park	✓	✓	✓	✓	✓	✓	✓

Central Park 4hrs > 70%
Southern Park 6hrs > 70%



Overshadowing

	Park Area	Area Overshadowed	%
Central Park	3000m²	1253m²	41.8%
Southern Park	5177m²	1473m²	28.5%

June 21st

9am

Overshadowing Study:

Between the hours of 9am - 3pm on June 21st, the Central and Southern Park shall receive more than 4 hours of solar access to at least 70% of its area

	9am	10am	11am	12pm	1pm	2pm	3pm
Central Park	✓	✓	✓	✓	✓	✓	✓
Southern Park	✓	✓	✓	✓	✓	✓	✓

Central Park 4hrs>70%
Southern Park 6hrs>70%



Overshadowing

	Park Area	Area Overshadowed	%
Central Park	3000m ²	522m ²	17.4%
Southern Park	5177m ²	760m ²	14.7%

June 21st

10am

Overshadowing Study:

Between the hours of 9am - 3pm on June 21st, the Central and Southern Park shall receive more than 4 hours of solar access to at least 70% of its area

	9am	10am	11am	12pm	1pm	2pm	3pm
Central Park	✓	✓	✓	✓	✓	✓	✓
Southern Park	✓	✓	✓	✓	✓	✓	✓

Central Park 4hrs>70%
Southern Park 6hrs>70%



Overshadowing	Park Area	Area Overshadowed	%
Central Park	3000m²	331m²	11%
Southern Park	5177m²	397m²	7.7%

June 21st

11am

Overshadowing Study:

Between the hours of 9am - 3pm on June 21st, the Central and Southern Park shall receive more than 4 hours of solar access to at least 70% of its area

	9am	10am	11am	12pm	1pm	2pm	3pm
Central Park	✓	✓	✓	✓	✓	✓	✓
Southern Park	✓	✓	✓	✓	✓	✓	✓

Central Park 4hrs>70%
Southern Park 6hrs>70%



Overshadowing

	Park Area	Area Overshadowed	%
Central Park	3000m²	256m²	8.5%
Southern Park	5177m²	246m²	4.8%

June 21st

12pm

Overshadowing Study:

Between the hours of 9am - 3pm on June 21st, the Central and Southern Park shall receive more than 4 hours of solar access to at least 70% of its area

	9am	10am	11am	12pm	1pm	2pm	3pm
Central Park	✗	✓	✓	✓	✓	✓	✗
Southern Park	✓	✓	✓	✓	✓	✓	✓

Central Park 4hrs>70%
Southern Park 6hrs>70%



Overshadowing	Park Area	Area Overshadowed	%
Central Park	3000m ²	488m ²	16.3%
Southern Park	5177m ²	259m ²	5%

June 21st

1pm

Overshadowing Study:

Between the hours of 9am - 3pm on June 21st, the Central and Southern Park shall receive more than 4 hours of solar access to at least 70% of its area

	9am	10am	11am	12pm	1pm	2pm	3pm
Central Park	✗	✓	✓	✓	✓	✓	✗
Southern Park	✓	✓	✓	✓	✓	✓	✓

Central Park 4hrs>70%
Southern Park 6hrs>70%



Overshadowing	Park Area	Area Overshadowed	%
Central Park	3000m²	900m²	30%
Southern Park	5177m²	395m²	7.6%

June 21st

2pm

Overshadowing Study:

Between the hours of 9am - 3pm on June 21st, the Central and Southern Park shall receive more than 4 hours of solar access to at least 70% of its area

	9am	10am	11am	12pm	1pm	2pm	3pm
Central Park	✓	✓	✓	✓	✓	✓	✓
Southern Park	✓	✓	✓	✓	✓	✓	✓

Central Park 4hrs>70%
Southern Park 6hrs>70%



Overshadowing	Park Area	Area Overshadowed	%
Central Park	3000m²	2110m²	70.3%
Southern Park	5177m²	725m²	14%

June 21st

3pm



**Planning,
Industry &
Environment**

IRF20/3684

Mr Greg Doyle
General Manager
Wollongong City Council
Locked Bag 8821
WOLLONGONG DC 2500

Dear Greg

**Planning proposal PP_2018_WOLLG_005 – former Corrimal Coke Works site
Consistency with Section 9.1 Directions**

Thank you for contacting the Department to advise that additional studies relating to heritage and flooding have been completed for the above planning proposal and seeking confirmation that the proposal is consistent with relevant Section 9.1 Directions.

The Gateway determination, issued on 20 August 2018, identified the need for further studies to determine the consistency of the proposal with Section 9.1 Directions 2.3 Heritage Conservation and 4.3 Flood Prone Land.

I note that this proposal no longer includes specific heritage protection provisions and is therefore inconsistent with the terms of Direction 2.3. However, a proposal may be inconsistent with this Direction if heritage protection is provided through another means. Wollongong LEP 2009 (Amendment 46) is being finalised and will facilitate the heritage protection of the site. The inconsistency with Direction 2.3 is therefore considered to be justified and no further approval is required in relation to this Direction.

I have reviewed the information relating to flooding and consider the proposal is now consistent with Direction 4.3 Flood Prone Land following the completion of studies.

If you have any questions in relation to this matter, I have arranged for Louise Myler to assist you. Louise can be contacted on 4247 1822.

Yours sincerely



5 August 2020

Sarah Lees
Director, Southern Region
Local and Regional Planning

Overview of Challenges in the Management of Grey-headed Flying-fox Camps

Council's Environment and Land Use Planning Officers have undertaken due diligence extensive research of publications and consultation with State Agencies, Flying-fox specialists including OEH/ESS Conservation Planning and Threatened Species officers, other Councils and the OEH facilitated NSW Flying-fox Land Manager Network in an effort to understand the environmental and management implications of rezoning land for residential use in close proximity to a Flying-fox colony that to date has varied in numbers from the low hundreds to in excess of 6000 individuals. This has identified a range of challenges other Councils Australia wide currently face in the management of GHFF camps. As part of the consultation effort, Council officers have attended a number of site visits and used GPS technology to map distances from the edge of camps to monitor noise and smell impacts and have reviewed the DPIE "Working with communities living with Flying-foxes" web page and Flying-fox Management Plans commissioned by a number of Councils.

We note the following key information obtained about Flying-fox camps in proximity to residential development:

- Flying-fox migrate from one area to another depending on the amount of food and water available (which would account for fluctuations in numbers between surveys).
- Camps often develop over many years, the location and size depending on availability of resources within a 20 to 50 kilometre radius of camps – some camps are occupied permanently, some seasonally and others irregularly.
- Behaviours are complex and hard to predict and response to management intervention can be unpredictable (management actions that work on one occasion at one camp may not reliably work at another)
- Once they occupy a site and achieve a master threshold (e.g. a camp of 200) a "memory and loyalty to the camp" forms, the implication being that you have to plan that they will return – methods to attempt to relocate or nudge camps often fail
- DPIE advice is that "preventing Flying-foxes continuing to roost at an established camp is a difficult and fraught task" and "any relocation of Flying-foxes after disturbance is largely unknown and ultimately may not resolve the problem. The Flying-foxes are just as likely to ...return to the original site in future"
- Flying-fox agitate and make noise ("squabble") all day long, with most noise during the fly in and fly out, especially when mothers and babies are trying to locate each other (it is foreseeable that any camp can become a maternity camp)
- Flying-foxes in urban areas are commonly the source of conflict with the community
- Resident complaints centre on amenity impacts - smell, noise, droppings associated with the fly in and fly out (on washing, balconies, cars), loss of property values and community fear of disease.

It should be noted that the DPIE "Working with communities living with Flying-foxes" web page states that there are no health risks from Flying-fox camps unless you make direct contact with live animals, and Flying-fox urine or droppings pose no health risk to humans unless eaten".

We note the following camp management actions the OEH recommend and/or that Councils have had to undertake and allocate funding/staffing resources to in an attempt to address resident complaints about amenity and lifestyle impacts where Flying-fox camps have set up in close proximity to existing residential properties (noting that these actions are ongoing once the conflict is identified):

- Ongoing trimming or removing of whole canopy trees at the camp boundary to create and maintain a buffer between the Flying-fox camp and areas of human settlement, to prevent the Flying-foxes roosting within the buffer
- Installation of noise attenuation fencing between the Flying-fox camp and nearby residents – expensive to install
- Installation of sprinkler systems in trees to discourage roosting (trialled with limited success)
- Disturbance actions such as continuous loud noise at dawn and dusk to deter them from their daytime roost, and the use of light and smoke
- Obligation to monitor a 20 km radius of a dispersal permit if granted (full time job) and anything above a level 1 action in the vicinity of the camp requires a permit application
- Offer of financial benefits to residents seriously affected by the proximity of Flying-fox roosts, such as rate reductions, provision of cleaning services, construction of sound and odour barriers, provision of covers over outdoor living areas/cars, assistance in installation of air conditioners to help when strong odours prevent windows and doors from being left open (odours are strongest in the breeding season and following rain)
- Employment of full time Council officers to develop engagement strategies and handle complaints, and ongoing Council marketing and communication actions to address resident concerns and educate about the Flying-fox camps
- Employment of Council staff to work over time to clean the droppings from public areas to prevent people slipping (WHS directive).

The Eurobodalla Flying-fox Management Plan (2018) states that dispersal is a high risk and expensive management action and if successful it generally only provides temporary outcomes, with Flying-foxes regularly attempting to return to the original site. It recommends strategic actions, including land use planning and development controls. It is noted that the 2017-18 Annual Report for Eurobodalla Council referenced the expenditure of \$207,718 for "Flying-fox program", funded through the Council's Environment Levy and supported by grants, this figure increasing to \$622,644 in the 2018-19 Annual Report.

A Parliamentary Inquiry into Flying-foxes (2016) addressed a range of evidence about the negative impacts that increasing incidences of urban roosting of flying-foxes has had on individuals and businesses in affected communities, including social quality of life impacts, economic impacts to businesses and the cost to local governments responsible for managing camps. The inquiry cited 17 flying-fox dispersal attempts by Councils, with costs ranging from tens of thousands to millions of dollars, with a high degree of uncertainty regarding long term outcomes or success of dispersals. A submission received from Batemans Bay estimated costs of up to \$6.2 million.

A key recommendation of the Flora and Fauna Assessment (EcoLogical, May 2019) was that future development of the site will require careful planning and consideration of the interface between the Flying-fox and humans, and that consideration should be given to the Flying-Fox Camp Management Policy (OEH 2015). The OEH Flying-fox Camp Management Policy 2015 section 5 outlines planning options when considering the location of development near a Flying-fox camp. The Policy suggests the identification of Flying-fox camps as development constraints to prevent future conflicts arising over proposed development, and zone the camps "environmental protection" to ensure the long term security of Flying-fox camps, including those used irregularly (i.e. historically been known to be used). The Policy also identifies the need for appropriate zoning of land adjacent Flying-fox camps, particularly when this land is undeveloped or redevelopment is planned, in the context that Flying-fox camps expand or move locally. The Policy further states that certain land uses are inherently more compatible in the vicinity of a Flying-fox camp than others, for example light industrial or rural compared with high density urban residential.

The Policy identifies the need for a Development Control Plan (DCP) to be developed to define a Council's responsibilities for managing and conserving Flying-fox camps, ensure the camps are considered when planning and constructing developments in the area, and provide guidelines for assessment, particularly in terms of avoiding conflict between adjoining land uses.

Section 6 of the OEH Flying-fox Camp Management Policy 2015 recommends that land owners consider the location of historically and currently occupied camps or potential Flying-fox camps early in strategic planning processes, particularly when planning future residential areas, schools or other sensitive infrastructure. This is in recognition that most Flying-fox camps are not occupied continuously. In addition to recognising the presence of existing Flying-fox camps through appropriate land use zoning and development control plans, it is also stated that new development proposals give consideration to appropriate buffers such that any additional hazard reduction activities that become necessary will be able to occur without being unduly impacted on by the existing camp locations. Sites that have the potential to function as Flying-fox maternity camps should be a priority for conservation.

The OEH also provides advice on Level 2 actions – creation of buffers around camps to separate humans and Flying-foxes. The recommendation is “planning to prevent conflict”, using land use planning instruments to provide sufficient space between established camp sites and residential neighbours. It is stated that camp site boundaries and buffers should cater for large seasonal influxes of Flying-foxes, as these trigger greater community concern.

Guidance on appropriate buffers include:

- Draft National Recovery Plan (Dr Peggy Ebby 2008) recommends 300m buffer
- EES noted 300m buffer best practice, noting that the most appropriate buffer should be determined on a case by case basis
- A buffer of 300m is ideal (e.g. low trees <3m, cleared land, rivers, open space) – the usefulness of the buffer to reduce the impacts of smell and noise declines if less than 50m (SEQ Catchments 2012)
- Neighbouring Council recommendations: 100m plus
- Moreton Bay Planning Scheme Policy: 500 metres

Leading consultants (EcoSure) published (Eurobodalla Flying-fox Management Plan 2018) impact thresholds in relation to buffer distances between a GHFF camp and residential development:

<50m	Likely high impact
50-100m	Likely moderate impact
100-300m	Likely low impact
>300m	Unlikely to be impacted

The EES (formerly OEH) was consulted as to appropriate buffers between the proposed development and the camp, and in relation to whether an environmental zoning would be appropriate. The OEH response recommended the integration of buffers between the Flying-fox camp and residential development “*as wide as reasonably practicable with guidance of up to 300m in width, but determined on a case by case basis having regard to land use conflict and level of impact from Flying-foxes*”. The response acknowledged that a camp of 2000 individuals may significantly increase impacts where the camp is in such close proximity to houses – “*there exists the potential for significant issues with the co-location of the camp and proposed medium density residential use*”. The letter stated that the dispersal of Flying-foxes is not preferred and should be considered as a last resort only, as dispersal is likely to be costly, ongoing and unlikely to succeed. The submission additionally raised potential problems for Council ownership, given that the Planning Proposal includes the proposed dedication to

Council of the realigned riparian corridor, including the vegetation patch supporting the Flying-fox camp.

"Housing residents next to a known flying-fox camp will necessarily generate conflict, as experienced many times over with the increasing establishment of camps in urban areas. This, in turn, will lead to increased workload for Council staff in terms of community engagement/education and responding to community requests to remove flying-foxes. A buffer is considered particularly appropriate in planning for new urban areas as, unlike camps co-located with existing urban areas, there is the opportunity for strategic planning to incorporate suitable buffers."

It should also be noted that the original 2017 Planning Proposal proposed the removal of 0.06ha of the Flying-fox habitat, with additional habitat to be provided through the proposed revegetation of the realigned riparian corridor along the western extent of the boundary. The Forest Red Gum – Thin-leaved Stringybark Grassy Woodland which forms part Illawarra Lowlands Grassy Woodland is an endangered ecological community under the BC Act and as such Council Officers and the ESS (formerly OEH) have indicated they would not support any removal of this vegetation community.

The proponent has responded to Council officer concerns about the need to maintain a suitable buffer by undertaking fortnightly monitoring of camp use and targeted surveys and commissioning a number of additional reports to provide advice on a suitable buffer. The survey and monitoring report (EcoLogical March 2020) concluded that the camp size fluctuates over time as does the area of occupation and the camp is expected to hold between 100-7,300 bats depending on resource availability, weather and seasonability. The bats were observed using the dam for belly dipping immediately prior to flying out for their nightly forage. The report identifies a core camp estimated to be approximately 100-500 individuals and referenced a camp size fluctuation in January 2020 which may be the result of the unprecedented bushfires over the 2019/2020 summer.

The commissioned reports recommended buffers ranging from 20 metres to 50 metres to minimise the potential conflicts between future residents and the bats. However, while the latest EcoLogical report (March 2020) states that the implementation of an indicative 50m buffer would minimise the conflict, information contained in Section 3.3 of the Report appears to contradict that recommendation by acknowledging the potential for land use conflict. Section 3.3 discusses measures such as the use of acoustic treatments (e.g. double glazing) that might be used to address noise, or mechanical ventilation systems to address potential noise and odour, for residential development interfacing the camp, and raises the question of whether apartments or townhouses should be the preferred built form on the edge of the buffer – *"apartments would increase the number of dwellings but avoid some of the most common residential complaints (droppings on washing and cars), townhouses reduce the number of dwellings but still have uncovered private open space that could be affected by Grey-headed Flying-fox behaviour"*. The proposed camp management actions included in the Report are clearing of currently used vegetation to encourage the bats to move to the southern extent of the vegetation; incorporate a dam or deep ponds and additional plantings as supplementary habitat into the proposed creek alignment to nudge the camp south and mitigate the removal of the existing dam, along with suggested built form responses, acoustic treatment and an education campaign for future residents.

It should be noted that the abovementioned actions under the Flying-fox Camp Management Code of Practice relate to management actions Council may be permitted to undertake in response to a Flying-fox camp setting up in proximity to an existing residential area (as opposed to the planning of a new community adjacent an established Flying-fox camp as being considered through this Planning Proposal request). The Code recognises residential development as a sensitive land use requiring a buffer from flying fox camps.

Based on Council due diligence extensive research, the proponent was advised that a minimum 100 metre buffer would be required between the GHFF camp and any future

proposed residential development. The proponent responded that a 100 metre buffer has an unacceptable impact on the development footprint, however the 2020 Master Plan now incorporates the required buffer (noting the proposed built form in the south of the site has now also been changed in order to retain the 760 dwelling target).

The proponent intends to dedicate the land containing the GHFF camp into Council ownership. There are risks and financial and staff resourcing liabilities to consider in the decision to accept dedication of land into future Council ownership and therefore stewardship of a State and Nationally listed threatened fauna species and its habitat. This commitment will be realised regardless of the setbacks of future development to the non-static roost camp footprint, however the proximity of the development to the roost camp will directly correlate to the resources Council will have to commit and expend on the in-perpetuity management actions. Implementation of Flying-fox Management Plans require substantial ongoing funds. The Planning Proposal, while outlining the need for a Management Plan, remains silent on the issue of how the proposed ongoing management actions would be funded.

Many Councils rely on grants for their camp management actions – these grants are designed to assist Councils where an existing residential area is unexpectedly having to deal with conflict issues arising from a camp setting up in close proximity. Recent advice received is that these grants have now become very competitive, given the number of Councils seeking this assistance, and the grant amounts will be limited to \$30,000 per annum, assigned on a merit assessment and will require the Council to commit to matching the funds 1:1. It is a possibility that Councils creating this conflict situation through poor land use planning decisions (i.e. a decision to rezone for new residential use in close proximity to a known camp) may not qualify for such grant funding.

It is uncertain how the Flying-fox camp might change or develop over time and the community concerns that might arise as a result. In assessing the Planning Proposal there is a need to consider the potential impacts of the proposed development on the GHFF camp and the potential impacts of the camp on future occupiers of the development. In July 2019 Council's Environmental Officer estimated the extent of suitable roost habitat to be 1.2ha of remnant/regrowth native vegetation – in the draft Parramatta North Management Plan (EcoLogical 2020) an area the size of 2ha was noted as usually sustaining 5,000 to 10,000 individuals, and has been observed to have up to 20,000. An inadequate buffer is likely to result in ongoing/long term management issues for Council.

Recommendation:

Consistent with the recommendations of the EcoLogical report and the OEH Flying-Fox Camp Management Policy (OEH 2015), the following recommendations are made in line with a precautionary approach, and "planning to prevent conflict":

- future development of the site will require careful planning and consideration of the interface between the Flying-fox and humans – a buffer of a minimum 100 metres will be required at this Planning Proposal phase in allocating zoning boundaries between the existing Flying-fox camp and any proposed new residential development (recognising documented conflict between flying fox camps and residential uses), with future detailed assessments at the Development Application stage required to determine the extent of the area utilised by the Grey-headed Flying-fox and how the camp is used. A buffer of less than 100m is effectively planning for reactive, challenging and resource intensive management measures to safeguard the future community and GHFF camp.
- Consideration of a new Part 7 clause for the site, to prevent future residential development within 100 metres of the GHFF mapped core area.
- land currently occupied by the species should be zoned appropriately to ensure the retention of habitat and prevention of conflicting land uses – an E2 Environmental Conservation zoning should be considered to reflect the EEC and ensure retention of habitat and the conservation outcome for the Flying-fox camp.

- the areas mapped as low ecological constraint are preferable locations for development.
- should Council resolve to accept dedication of the EEC and Flying-fox camp, a funding mechanism will be required. A Biodiversity Stewardship Agreement/ BSS which would be established prior to finalising the Planning Proposal has been raised by Council Officers as a possibility for the proponent to explore further.
- information regarding proximity to the Flying-fox camp will be included in planning certificates to alert future residents (sec 10.7/sec 149 certificates).
- invite comment from the Commonwealth Department of Energy and Environment during the public exhibition.

Figure 1: GHFF Mapped Extent and Core Area



ITEM 3 HELENSBURGH TOWN CENTRE PLAN AND STREETSCAPE MASTERPLAN

The Helensburgh Town Centre Plan 2020–2045 (the Plan) identifies priorities for change and renewal to achieve the community's vision for the Helensburgh Town Centre. The Plan is accompanied by the Helensburgh Town Centre Implementation Plan 2020–2045 (TCIP) and guides the principles and design objectives of the Helensburgh Streetscape Masterplan. The Plan provides clear strategies and actions to meet the current and future needs of the local community. Supported by planning and urban design analysis, traffic studies and an accessibility audit, The Plan will guide change through actions related to infrastructure improvements, main street renewal and planning control amendments.

The Helensburgh Streetscape Masterplan (HSMP) is a long-term, comprehensive concept design that is informed by the Plan. The HSMP expresses the Plan's vision for the Helensburgh Town Centre, articulating infrastructure improvements and renewal. The HSMP demonstrates how Walker Street can be designed to support its role as a vibrant main street, including enhancements to adjacent streets (Parkes Street and Short Street) to make the Town Centre safe and accessible with improved amenity.

The draft Helensburgh Town Centre Plan was developed through comprehensive community consultation undertaken between March and June 2019. Staff worked with interested community members, representatives of the Northern Illawarra Chamber of Commerce, Neighbourhood Forum 1, and local school students to inform both the draft Plan and the development of the draft Streetscape Masterplan.

The Draft Helensburgh Town Centre Plan and draft Streetscape Masterplan were exhibited between 11 May 2020 and 22 June 2020 to enable the community to inform the finalisation of both of these projects. This report is to inform Council of engagement feedback, to report on how the feedback has informed refinement of the final Plan and Streetscape Masterplan and to seek adoption of the Plan and Streetscape Masterplan.

RECOMMENDATION

- 1 The Helensburgh Town Centre Plan 2020–2045 be adopted (Attachment 3).
- 2 The Helensburgh Streetscape Masterplan be adopted (Attachment 4).
- 3 The Helensburgh Town Centre Implementation Plan be noted (Attachment 5).

REPORT AUTHORISATIONS

Report of: Chris Stewart, Manager City Strategy

Authorised by: Linda Davis, Director Planning + Environment - Future City + Neighbourhoods

ATTACHMENTS

- 1 Town Centre Engagement Report
- 2 Streetscape Masterplan Engagement Report
- 3 Draft Helensburgh Town Centre Plan
- 4 Draft Helensburgh Streetscape Masterplan
- 5 Draft Implementation Plan

BACKGROUND

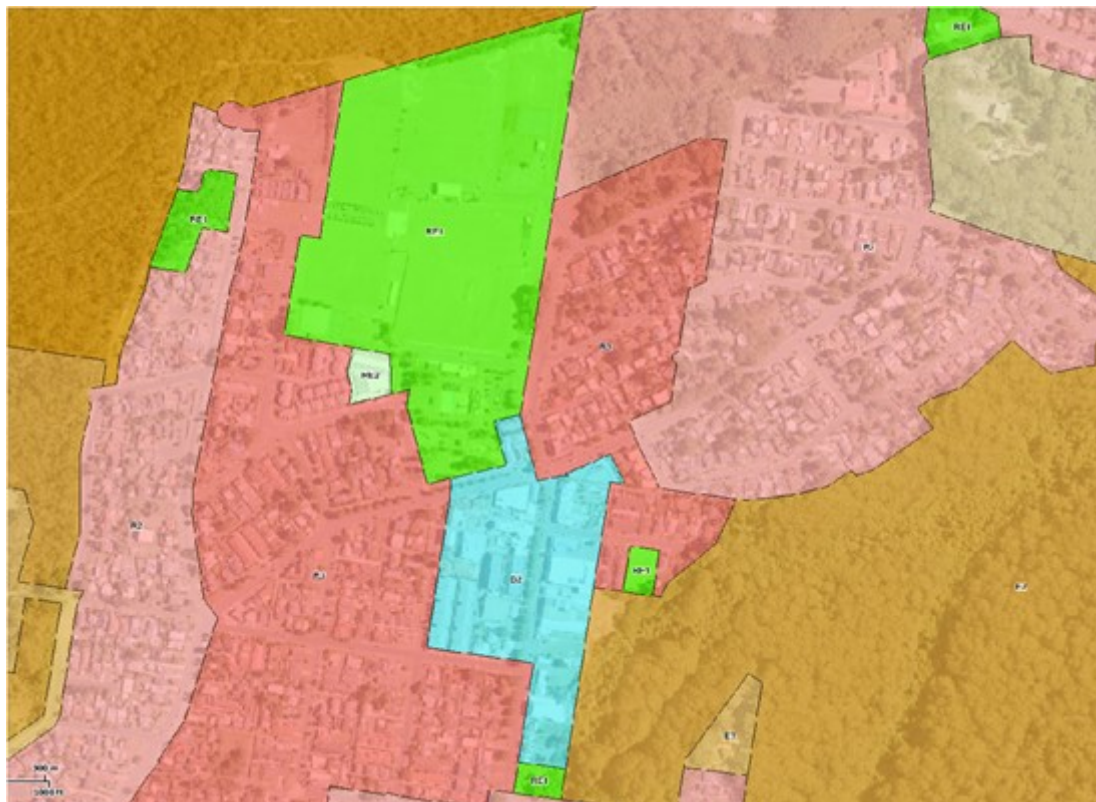
Location and Characteristics of Helensburgh Town Centre

The Helensburgh Town Centre is the northernmost commercial centre, located 34 km north of Wollongong and 45 km south of Sydney CBD, servicing Helensburgh and the surrounding suburbs of Coalcliff, Clifton, Stanwell Park Stanwell Tops, Otford and Lilyvale. Located in a bushland setting on the edge of the Royal National Park. Land to which the Helensburgh Town Centre relates is the land zoned B2 Local Centre.

The Town Centre is focussed around the intersection of Walker and Parkes Streets, extending between Whitty and Lilyvale Streets at the southern boundary to Charles Harper Park in the north. The western boundary is Stuart Street and the eastern boundary is Walker Lane.

Because of the remote location of Helensburgh, the Town Centre functions as an important hub for the local community, providing important community and recreational facilities, healthcare, childcare centre and a range of retail, professional and personal services.

Figure 1: Helensburgh Town Centre Planning Zones



Preparation of the draft Helensburgh Town Centre Plan 2020–2045

In 2017, footpath renewal was planned for the eastern side of Walker Street (between Whitty and Parkes Street) as identified within the Capital Works Program. Concept designs were developed and presented to the community in September 2017. The community feedback received indicated that footpath renewal did not respond holistically to the need for improved amenity, accessibility and safety for the Town Centre. The community requested a Town Centre study that could inform the streetscape design.

Aligned with the Town and Village Planning Delivery program, Land Use Planning commenced the Town Centre study in late 2018. A range of planning, urban design and traffic analysis was undertaken, and data collected about how people move through and use the Town Centre.

The objective of the Town Centre study is to -

- Work with Community to create a shared Vision for Helensburgh Town Centre.
- Undertake and document an urban design analysis of the Town Centre – quality and amenity.
- Bring together the range of Helensburgh Projects under one Implementation Plan.

Extensive community consultation for the Helensburgh Town Centre Plan occurred between March and June of 2019 to inform the content of the draft Plan and the Streetscape Masterplan. A community forum, consultation with local school children and meetings with Neighbourhood Forum 1 and the Business Chamber informed the analysis and recommendations of the draft Plan. A draft Vision was developed and workshopped with a smaller group of community volunteers.

The draft Plan and Streetscape Masterplan were then shared with the broader community to test alignment with community opinion and gain feedback to inform refinement.

Consultation was conducted between 11 May to 22 June 2020. Details relating to all engagement activities and reporting on feedback received is documented in the accompanying Engagement reports (Attachments 1 and 2). Further detail is also provided in summary in the Consultation and Communication Section below.

Resources for Regions Grant

In early 2019, Council was awarded \$1.6 million under the NSW Government's Resources for Regions program towards investment in the renewal of the Helensburgh Town Centre. This grant funding will provide a significant contribution towards the upgrade of the Helensburgh Town Centre streetscape, improved pedestrian crossing points, a new north-south footpath for Charles Harper Park and an upgrade to northern car park at Rex Jackson Oval (near the netball courts).

The endorsement of the Helensburgh Town Centre Plan and Streetscape Masterplan will mean that the Streetscape Masterplan can move to detailed design to allow construction to progress aligned with the deliverables of the Grant.

Recent Council investment in Helensburgh

Over the past 15 months there has been investment in a range of project within the suburb of Helensburgh, including -

- Maidstone/The Ridge Roundabout – construction of a new roundabout at the intersection of The Ridge, Maidstone Street, Junction 2020.
- and Fletcher Street.
- Walker Street Culvert Upgrade – repair and upgrade of the major culvert running under Walker Street located on an unnamed tributary of Gills Gully, which flows into the Hacking River.
- Rex Jackson Storage Shed – improved storage of gear for the range of sporting user groups at Rex Jackson Oval.
- Charles Harper Playground – replacement of the existing playground in Charles Harper Park, including multi-use equipment, new entry point and connecting footpath, new tree planting and the installation of shade sails.

PROPOSAL

Helensburgh Town Centre Plan

The *Helensburgh Town Centre Plan 2020–2045* provides Council staff, developers and the community strategic guidance for the future of Helensburgh Town Centre. The Plan seeks to guide and facilitate change through planning control amendments and infrastructure improvements. The Plan aims to improve the lived experience of residents and visitors by increasing the safety and comfort of the public domain, whilst encouraging investment and enhancement in the private realm, to activate and enliven the main street. The Plan emphasises council's investment based on the priorities expressed by the community's Vision and supporting principles for the Town Centre.

The Vision is -

"Helensburgh is an inclusive and welcoming village of diverse communities and rich heritage set within an extraordinary natural landscape. At its heart is a thriving main street which is connected to world-class local destinations, and alive with people."

The Plan Principles are -

- Unique, vibrant and community focussed.
- Thriving, sustainable and creative.

- Active, natural and healthy.
- Connected, accessible and pedestrian friendly.

The vision statements provide a framework to measure how development, infrastructure and community-led projects contribute to the achievement of the community's aspirations for Helensburgh Town Centre.

A series of Strategies have been defined which seek to deliver change in the Town Centre aligned with the Community's vision.

Table 1: Summary of Strategies and Actions with the Helensburgh Town Centre Plan

Strategy	Actions
<p>Updates to Planning and Policy</p> <p>A renewed focus on character and identity to reflect Helensburgh's unique attributes will be reflected in planning policy, setting the tone for future development.</p> <p>Revision of the Development Control Plan will ensure building controls promote interesting shopfronts and high-quality designs at the right scale for Helensburgh.</p> <p>The boundary of the Town Centre is redefined to focus on Walker and Parkes Streets.</p>	<ol style="list-style-type: none"> 1. Update Wollongong LEP 2009 to reflect changes to zoning boundaries. 2. Update Wollongong DCP 2009 Chapter B4 Development in Business Zones to set renewed planning controls for Helensburgh Town Centre, and progress the completion of the draft Helensburgh Character Statement for integration into the Wollongong DCP.
<p>Enhancing Connections</p> <p>Leverage and connect the Town Centre to surrounding destinations, making it accessible by foot, bicycle and car.</p> <p>Strengthen connection to the natural surroundings and natural areas.</p> <p>Wayfinding is clear and easy, and it is a comfortable place to walk around. Create an important sense of arrival into the Town Centre.</p> <p>Public space is celebrated and highly active.</p>	<ol style="list-style-type: none"> 3. Lodge a generic Events Development Application for Rex Jackson Park 4. Provide opportunities for Markets in Helensburgh Town Centre and surrounding open spaces. 5. Helensburgh Key Walking Connections Project – design concept for cycle/walking loop trail aligned with Urban Greening. 6. Develop and deliver a wayfinding Concept and Strategy including important links e.g. Temple to Station Trail Project. 7. Helensburgh Public Art Project
<p>Integrated Community Hub</p> <p>An integrated community centre and library facility will be at the heart of the community. This purpose-built facility will be a centrepiece of the town, delivering a high quality, welcoming place. Council is investigating several site options for the new community facility; however further investigations are required to ensure feasibility.</p> <p>Principles guiding the future of the Integrated Community Centre & Library include -</p> <ul style="list-style-type: none"> • Integrates into the landscape to provide opportunities for generations to come. • Well-connected and accessible, with 	<ol style="list-style-type: none"> 8. Site options investigation for an Integrated Community Facility (community centre/library) 9. Key site urban design masterplan

<p>integrated parking.</p> <ul style="list-style-type: none"> • Community heart - integrating community meeting place. • Exceptional design, unique to Helensburgh. • Programmed to bring life to the Town Centre - day and night. 	
Strategy	Actions
<p>Mainstreet Renewal</p> <p>Walker Street is Helensburgh's vibrant main street which is accessible and convenient for all. It invites people to visit and spend time. Street renewal will enhance the daily lived experience of residents, create a unique destination for visitors, and provide a catalyst for activation and investment.</p>	<ul style="list-style-type: none"> 10. Undertake main street renewal detailed design + construction as per the endorsed Streetscape Masterplan (Attachment 4). 11. Deliver a heritage interpretation project aligned with streetscape upgrades. 12. Undertake a laneway Activation project to enhance safety and usability of laneways 13. Enhance bus stops with a focus to improve active transport.

Helensburgh Streetscape Masterplan

Documented within the Mainstreet Renewal Strategy is a series of objectives, expressed in the Helensburgh Streetscape Masterplan -

- Enhance connection both physical and visual to the natural environment and the unique character of Helensburgh.
- Embrace and celebrate town setting by the introduction of human scale landscaping features to create a greater sense of place.
- Establish equal access pathways and a pedestrian friendly environment.
- Create a low vehicle speed environment to increase pedestrian and cycle safety.
- Increase shade along key pathways by planting additional canopy trees.
- Enhance pedestrian connections and experience through the development of the existing laneways.
- Express the indigenous, natural, service and mining heritage of Helensburgh throughout the landscape and by incorporating public art and interpretive devices.
- Acknowledge the Memorial planting of Brush Box Trees on Walker and Parkes Streets through interpretive signage.
- Provide improved infrastructure like recycling bins, seating, bike racks and bubblers.

These principles underpin the integrity of the Streetscape Masterplan and provide clarity in how the design will uphold the vision and expectations of the community.

Clear priorities of the Streetscape Masterplan

During the consultation, a staging diagram accompanied the plans to outline the extent of works that will be completed between now and 2022. See pages 25 and 26 of the Town Centre Plan (Attachment 3).

Staging priorities have been considered having regard to -

- 1 Alignment with the streetscape master plan objectives: pedestrian accessibility, comfort and safety.

- 2 Value for money: ensuring that infrastructure delivery is coordinated. What infrastructure works will bring the highest impact to how the main street functions.
- 3 Design functionality: ensuring that infrastructure elements are delivered in a coordinated way. Sometimes there are partnering elements.
- 4 Relationship with future investment programs: ensuring that works don't become redundant.

The following summary of works will be undertaken between 2020 and 2022 -

Parkes Street

There is an urgent priority to create a safe environment for pedestrians to connect between the main street and the services and amenities to the north.

Design is intended to slow and calm traffic, ensure pedestrian safety, improve accessibility, and to instil within this intersection a sense of arrival and entry to the Town Centre.

Walker Street

The priorities for Walker Street are focussed around delivering high levels of accessibility and quality place outcomes.

- New continuous accessible pathway along the Western edge at street level.
- Traffic calming at southern entry with blisters narrowing the road and creating a pedestrian crossing point.
- Improved connections to western shops from Walker Street (road level) with two new accessible ramps and one new set of feature stairs.
- Improve safety with upgraded crossing facilities in Walker Street and Short Street.
- Two newly defined public meeting places - outside Coles with a widened footpath area, and at the top of the new stairs linking with the western bank shopping area.
- Tree canopy lifted and native landscaping featured along the western bank. Enhanced character through interpretation project for memorial trees, native landscaping, and expressing the heritage sandstone curb where possible.
- Include lighting conduits, seating and human scale elements.

Helensburgh Town Centre Implementation Plan 2020-2045

The Helensburgh Town Centre Implementation Plan (Attachment 5) has been developed to complement the Helensburgh Town Centre Plan and to clearly communicate priorities and target delivery dates. The purpose of the Implementation Plan is to guide the delivery of actions in a collaborative and coherent way to promote the best possible outcomes. As such, the Plan needs to be flexible and ongoing refinement will occur over time. The Implementation Plan includes a commitment to the ongoing monitoring to clearly communicate project status and staging, as well as ensure future opportunities aligned with the Plan objectives can be included in Council's annual planning process and delivery program.

The Implementation Plan provides a greater level of detail for each action identified in the Helensburgh Town Centre Plan, identifying performance measures, a project leader, project partners, estimated costs, priorities, targets for delivery and strategic alignment for each action. It is important to recognise that some actions will require leadership and funding outside of Council control. The deliverables of the Implementation Plan have been produced in collaboration with the responsible Council Divisions. It is recommended that the Implementation Plan be noted, as it will be implemented as resources are available.

CONSULTATION AND COMMUNICATION

To help shape the draft Plan, Council undertook extensive community engagement from March 2019 to June 2019. Engagement included a series of community workshops, targeted stakeholder meetings,

workshops with school children and one-on-one conversations. A survey gathered data on what the community valued and their priorities for the Town Centre.

The themes emerging from these workshops, surveys, interviews and analysis formed a locally generated Vision and set of principles that developed into the Draft Helensburgh Town Centre Plan.

The Draft Helensburgh Town Centre Plan and draft Streetscape Masterplan were exhibited between 11 May 2020 and 22 June 2020. The objective was to check in with the community to ensure the Vision, Strategies and key priorities were aligned.

In response to the changing demands of face-to-face meetings brought about by the COVID19 pandemic, the communication strategy was adapted to ensure that the community could meaningfully connect and provide feedback. This was managed through the following approaches -

- All content was made available online.
- Feedback was encouraged via online surveys with the traditional means of making a submission in writing and email remaining available.
- An online Q&A was used to enable staff to answer questions on a weekly basis.
- Staff created videos to communicate the intent and detail of the projects.
- Copies of the plans were available at the local library for collection and were also individually distributed to all businesses in the Town Centre.
- Two double page advertisements were placed in the 2508 Magazine to reach the local residents.
- Outreach to contacts gathered over past consultation events included emails and phone calls to local community groups including the Historic Society, Neighbourhood Forum 1 and local schools.
- In place of face to face meetings, Skype and telephone discussions were made available. A number of these sessions were conducted where staff met with interested members of the community and local businesses including the Helensburgh Hotel, Northern Suburbs Chamber of Commerce/Post Office, Ray White Real Estate, Bensons Conveyancing Service, Premier Illawarra Bus Service and Metropolitan Mine.
- Community members were encouraged to contact staff directly with any questions, with staff contact details made available.

A total of 58 submissions for the Town Centre Plan were received, 38 via the online survey and 20 via email. Information was included in Neighbourhood Forum 1's newsletter, which resulted in some questions from Forum members. There were a high number of visits to the council engagement website.

The following table provides a summary of the online engagement through Council's website.

Table 2: Summary of participation through the engagement web site

Measure and Explanation	Usage
Unique Site Visits – Total number of visits to the project page	1,110
Aware – Total number of users who viewed the project page	927
Informed – Total number of users who clicked a hyperlink or opened a document	427
Engaged – Total number of users who actively contributed to the project via a tool, eg. submitted feedback via the survey or participated in the Q&A	41

Results of Exhibition and Common Themes

The community feedback received is summarised in Attachments 1 and 2.

The online survey asked respondents for their level of support for the plan's Vision and why. Results from this question found that the parameters of Strongly Support and Support were mostly chosen.

Also, the survey asked for their agreement with elements of the plan listed as Principles, Policy and Planning, Enhancing Connections, Integrated Community Hub and Main Street Renewal and why. Results from these questions found that the parameters of Strongly Agree and Agree were mostly chosen.

Most commonly, respondents in praise of the Vision said it captured the character of the town and that the Plan displayed great potential for improving the town. Many were in support of the Principles, Planning and Policy, and Enhancing Connections, for the direction and aspiration they offer. With this support were comments on parking and the number of empty shops.

Changes to Draft Plan following exhibition

A few minor changes were made to the draft Helensburgh Town Centre Plan (the exhibited version of the Plan) to produce the final version of the Plan. These changes were mainly aesthetic, to improve the flow and clarity of the messages within the draft Plan and no major changes to the intent or direction of the Plan have been made. The main changes to the Plan are tabulated below.

Table 4: Main Differences between the Draft Helensburgh Town Centre Plan 2020-2025 and the (final) Helensburgh Town Centre Plan 2020-2045

Pages	Change
3 & 4	The Section <i>Unique town with a point of difference</i> was expanded to integrate more detail around Aboriginal Heritage and recognise the role of First Nations People in the local area. Engagement summary from former consultation was removed, and reference made to the accompanying Engagement Report.
various	All maps have been updated to delete reference to a proposed lane linking Walker Street to Walker Lane (north of Short Street). This land is in private ownership and Council has resolved not to purchase this land.
15	Additional detail was added under the heading <i>Redefining the Town Centre boundary</i> to specify the building height and floor space ratio controls nominated for transition with boundary adjustments.
19 & 20	Adjustments made to the wording of the <i>Integrated Community Hub</i> Strategy to reflect current status/ consideration of site investigations on potential sites for an integrated community hub across the Town Centre.
22	Strategy <i>Main Street Renewal</i> has been updated to reflect the opportunity to integrate Aboriginal Heritage in the Main street design, and the importance of working in collaboration with the local Aboriginal community to understand connection with Country.
27	Conclusion added

It is recommended that the updated Helensburgh Town Plan 2020-2045 be adopted.

The Helensburgh Streetscape Masterplan has also been updated to remove the proposed lane linking Walker Street to Walker Lane. The Streetscape Masterplan provides a long-term, comprehensive concept design that expresses the vision for the Helensburgh Town Centre. The Streetscape Masterplan demonstrates how Walker Street can be designed to support its role as a vibrant main street, including enhancements to adjacent streets (Parkes Street and Short Street) to make the Town Centre safe and accessible with improved amenity. It is recommended that the updated Helensburgh Streetscape Masterplan be adopted.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 5, “We have a healthy community in a liveable city”. It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
5.1.4 Urban areas are created to provide a healthy and safe living environment for our community.	1.3.1.2 Develop planning controls and Town Centre and Neighbourhood Plans with regard to the economic, social and environmental impacts	1.3.1.2.1 Complete Helensburgh Town Centre Study

The Helensburgh Town Centre Plan (Attachment 3) and Implementation Plan 2020–2045 (Attachment 5) are supporting documents under Council’s Integrated Planning and Reporting framework. Supporting documents include Council endorsed strategies, plans, masterplans, town and village plans, precinct plan and action plans that include short, medium and long-term outcomes to support the development of Our Wollongong 2028: Community Strategic Plan, Revised Delivery Program 2018-2022 and Operational Plan 2020-2021. The annual delivery program planning process includes operational and capital budget allocations of people, finances and assets through a vigorous prioritisation and decision-making process.

The role of the Helensburgh Town Centre Plan and Implementation Plan is to inform this process and set priorities for planning and delivery of projects within the Helensburgh Town Centre. The vision and objectives of the Plan serve an important strategic function and should be used as a prompt for designers, architects, engineers, strategic and assessment planners, community groups and decision-makers in planning, assessing and delivering projects in the Helensburgh Town Centre to deliver outcomes aligned to community aspirations and priorities.

SUSTAINABILITY IMPLICATIONS

The *Helensburgh Town Centre Plan 2020–2045* recognises that the town centre has some capacity for growth. However, smart growth in the Helensburgh Town Centre area could support active and more environmentally-sensitive lifestyles and reduce private vehicle dependency given the proximity of shops, services, public transport and recreation areas. Within the Plan, the following objectives promote ecological sustainable practices -

- Leverage and connect the Town Centre to surrounding destinations, making it accessible by foot, bicycle and car.
- Strengthen connection to the natural surroundings and natural areas.
- Wayfinding is clear and easy, and it is a comfortable place to walk around. Create an important sense of arrival into the Town Centre.
- Public space is celebrated and highly active.

Embracing ecologically sustainable design and practices requires a collective effort from Council, business, community groups and residents.

FINANCIAL IMPLICATIONS

The Plan provides a vision for the future of the Helensburgh Town Centre and sets direction for future planning and infrastructure priorities. The Helensburgh Town Centre Implementation Plan 2020–2045 provides more detail on actions, including staging; targeted delivery dates and estimated capital costs (Attachment 5). Future design work and investigations will inform more detailed costings. The Implementation Plan will be used to inform annual planning priorities and budgeting processes. Currently many actions have already been funded through the annual planning process – the role of the

Implementation Plan is to align actions to strategic aspirations, clarify priorities and give weight to funding proposals, including Council's internal budgeting processes and external grant funding proposals.

CONCLUSION

The *Helensburgh Town Centre Plan 2020–2045* has been prepared to offer strategic guidance on the future growth of Helensburgh and is accompanied by an Implementation Plan to coordinate the delivery of actions outlined within the Plan. The Draft *Helensburgh Town Centre Plan 2020–2045* seeks to build on Helensburgh strengths and focuses on enhancing the centre's appearance and connectivity and encouraging social connections through activating and improving public space. A variety of community engagement activities has highlighted community support for the direction of the Plan and future investment in the centre. It is recommended that the *Helensburgh Town Centre Plan 2020–2045* and Helensburgh Streetscape Masterplan be adopted and the Implementation Plan be noted.

OUR WOLLONGONG

JOIN THE CONVERSATION



Draft Helensburgh Town Centre Plan

ENGAGEMENT REPORT

June 2020

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Executive Summary

Council presented the draft Helensburgh Town Centre Plan to the community for comment, after it was developed through extensive engagement conducted in 2019. It sets out a shared community Vision and Strategies which form an action plan for the Town Centre. The community and stakeholder views and ideas highlighted in this report will help Council understand and plan for the needs of the community, informing the new Town Centre Plan for Helensburgh.

With multiple projects being planned for Helensburgh, the community engagement for the Helensburgh Town Centre Plan, Streetscape Masterplan and the Community Centre Options were run at the same time, allowing for a streamlined engagement process. The engagement for all three projects ran from 11 May to 22 June 2020.

Due to the COVID-19 pandemic, the communication and engagement strategies were adapted to ensure the community could meaningfully connect and provide feedback, in the absence of face-to-face engagement.

A variety of communication and engagement methods were used to provide information and invite the community to comment. Information was made available on Council's engagement website with the opportunity to ask questions and provide comment and feedback. Respondents could also send in comments by mail, email or phone. Online videos replaced face-to-face presentations. The community also had access to technical staff via phone and email and could ask questions through an online Q&A tool.

The engagement campaign was promoted extensively through two advertisements in the Helensburgh 2508 Magazine, the Advertiser, social media posts, media releases, emails to key stakeholders and respondents from previous engagement on the Town Centre Plan. There were phone calls and follow-up emails to businesses in the Town Centre and conversations with Neighbourhood Forum 1.

There were 1,110 visits to the Helensburgh Town Centre Plan (Plan) webpage, resulting in 38 submissions through the online survey. In addition, 16 email submissions were received. Comments provided by email were mostly concerned with direct changes to the Town Centre and referred to the Streetscape Masterplan.

The online survey asked respondents for their level of support for the Plan's Vision and for comments on the Vision. Results from this question were that 'Strongly Support' and 'Support' were mostly chosen from the five options.

Also, the survey asked for level of agreement with elements of the Plan listed as Principles, Policy and Planning, Enhancing Connections, Integrated Community Hub and Main Street Renewal and why. Results from these questions showed that 'Strongly Agree' and 'Agree' were mostly chosen.

Most commonly, respondents in praise of the Vision said it captured the character of the town and the Plan displayed great potential for improving the town. Many were in support of the Principles, Planning and Policy, and Enhancing Connections, for the direction and aspiration they offer. Further comments on support were focussed on parking and addressing the number of empty shops.

Comments opposing elements of the Plan were mostly concerned with parking, the lack of public toilets and the need for a roundabout at Lilyvale Street. There were also comments on the number of empty shops, the poor quality and mismatched facades, and concern that businesses were failing.

Background

Extensive community engagement was undertaken from March 2019 to June 2019 to help shape the draft Helensburgh Town Centre Plan (Plan). Engagement included a series of community workshops, targeted stakeholder meetings, workshops with children and one-on-one conversations. The themes emerging from these workshops, surveys, interviews and analysis form a locally generated Vision and set of principles for the Helensburgh Town Centre.

2019 Engagement Summary

Helensburgh community members have been advocating for a Plan for many years. This advocacy resulted in Walker Street Footpath improvement project that was presented to the community in 2016 being placed on hold pending the creation of an agreed Plan.

Council staff met with community representatives in December 2018 to discuss methods of engagement to develop the Plan. A variety of methods were then used to encourage community members to join the conversation for the Plan.

The community engagement objectives for this project were: to communicate broadly to the community and key stakeholders to inform them about the development of the draft Town Centre Plan throughout the life of the project; to build a strong partnership with the stakeholders that will enable support and effective implementation; ensure the diversity of community voices are reflected in the engagement process, and that diverse opportunities are created for the community to be informed about and have input into the implementation of the project.

'Help Shape Helensburgh Town Centre Plan' postcards promoting the engagement were delivered to every letterbox in the 2508 area. Postcards were given to early morning commuters at Helensburgh train station and information packs were available at the Helensburgh Library.

Presentations about the Plan were made to Neighbourhood Forum 1 and the Northern Illawarra Chamber of Commerce. The 2508 magazine printed several articles promoting the engagement and emails were sent to an extensive stakeholder list that community representatives and council staff had created.





An online survey opened from 1 to 18 March 2019 and reopened 30 March to 17 April 2019 following community requests. 315 people completed the survey. Council staff conducted a workshop with 43 Year three and year six students at Holy Cross College, asking them several of the survey questions.

A Vision workshop was held on 30 March 2019 and 43 people attended. 30 from the Vision Workshop then signed up for continued involvement in the creation of the Plan.

The data from the surveys and the Vision Workshop was reviewed and the top 10 themes emerged. These themes included: improve the public spaces (342 responses), find parking solutions (130), create a community hub/facilities (122), improve the variety of local shops (120), install traffic calming (95), create pedestrian friendly areas (83), improve the physical connections, shop fronts and control development (51 each), encourage culture/art/music/heritage and public seats (45 each), encourage sustainability (37) and provide public gathering spaces (30).

The themes above informed the development of draft vision statements, principles and plans. These were then presented to the twelve people who attended the Refining Workshop on 1 June 2019 to provide further feedback. Council staff then developed the draft Plan and Implementation Plan informed from community feedback throughout the engagement period.

Details of the number of participants for each engagement activity from December 2018 through to September 2019 are presented in Table 2 below.

Table 2: Engagement Activities		Participation
Pre-exhibition – Co-Design Workshop 13 December 2018		8
Survey submissions received 1-18 March 2019 (287) Survey submissions received 1-18 April 2019 (28)		315
Holy Cross School visit 		43
Presentation to Neighbourhood Forum 1 meeting on 13 March 2019  <i>Wollongong City Council senior strategic project officer Bridget Jarvis details town centre plans to a March 13 meeting of the local Neighbourhood Forum.</i>		12
Vision Workshop 30 March 2019 		43
Attendance at the Northern Illawarra Chamber of Commerce 2 April 2019		12
Working Group 1- Refining Workshop June 2019 		12

Stakeholders

Stakeholders identified prior to the start of the engagement period included participants in the 2019 engagement, residents, community groups, businesses in the Helensburgh Town Centre and Neighbourhood Forum 1.

Methods

Due to the COVID-19 pandemic, the communication strategies were adapted to ensure that in the absence of face-to-face engagement, the community could meaningfully connect and provide feedback.

A variety of communication and engagement methods were used to invite the community to join the conversation:

- Two paid advertisements in the Helensburgh regional paper 2508 Magazine, one each in the May and June editions.
- Social media posts
- Media releases distributed to local media
- Phone calls and emails to all businesses in the Town Centre
- Emails to key stakeholders and participants in the 2019 engagement
- Notice listed in the Council column in The Advertiser on 20 May 2020
- Copies of the Plan were exhibited in the Helensburgh Library with hardcopies of the survey
- Council's engagement website, www.our.wollongong.nsw.gov.au

Due to COVID-19 restrictions, telephone calls and video conferences were conducted with interested parties as an alternative to face-to-face meetings. This allowed for detailed discussion to clarify any points raised and for Council staff to hear respondents' in-depth feedback.

Neighbourhood Forum 1 were contacted by phone and information was provided via email.

A landing page for Helensburgh projects was created on Council's engagement website directing the community to a project page for the Plan. This project page included a video that outlined the main features of the Plan and how it was developed. A Q&A tool was used for the community to ask questions which were then responded to by Council officers. An online survey was used for people to submit comments.

Results


A total of 58 submissions for the Plan were received, 38 via the online survey and 20 via email. Information was included in Neighbourhood Forum 1's newsletter, which resulted in some questions from Forum members.

An article published in the May edition of 2508 Magazine coincided with increased visitation to the project webpage.

Media stories were published in the Illawarra Mercury on 11 June 2020, the Helensburgh Blog on 31 May 2020 and the Bulli & Clifton Times on 29 May 2020.

Two social media posts, 14 May 2020 reaching 2,917 people with 132 engagements and 20 June 2020 reaching 3,989 people with 235 engagements

Shape the Future



Artist's Impression of a renewed Walker Street

Wollongong City Council is working on a series of strategic projects in your suburb as part of our future-focus for Helensburgh. Rather than looking at each of these projects independently, we're looking at them with the bigger picture in mind. That's why we're consulting these projects simultaneously. By taking this united approach we're able to get in place phased plans that can be ready to roll in early 2021. It also allows us to reflect upon what we've already achieved. We appreciate that Coronavirus, and measures to reduce the risk of its spread, might be considered a community engagement challenge. But we're responding and our website offers new ways to collate feedback and share information.

Thanks for working with us on Helensburgh's bright future.




4 / 2508 / MAY

of Helensburgh

The draft Helensburgh Town Centre Plan is based on community consultation carried out in early 2019. It focuses on building Helensburgh's individuality and sets a shared Community Vision and Strategies, which form the Town Centre action plan.

Some of the key directions include:

- A renewed main street which is safe, accessible, and honours the local identity. We've developed a draft Streetscape Masterplan that expresses what Walker Street will look like in the future and prioritises a pedestrian-friendly environment and we're asking for your comments on a draft streetscape design for Walker Street.
- A key Strategy is to deliver an Integrated Community Hub, bringing together important community facilities in a centralised location. We're exploring options for an integrated community centre and library facility, and we're seeking your opinion on how best to meet the needs of the community.

How to share your thoughts

Submissions close 8 June 2020

Online:
ourwollongong.nsw.gov.au
Complete the survey - Provide your opinion and feedback
Online Q & A - Where you can ask questions about the draft Plan. We will respond to questions once a week.

Write to us:
The General Manager
Wollongong City Council
Locked Bag 8821
Wollongong DC NSW 2500

Email: engagement@wollongong.nsw.gov.au
Phone: 4227 7111

MAY / 2508 / 5

Article published in May edition of 2508 Magazine

Information about the exhibition was included in the Neighbourhood Forum 1 newsletter, which generated questions and submissions from their members.

This section of the report provides details of the online participation (Table 1), responses to the online survey (Table 2), and a summary of comments received.

Table 1: Summary of participation through the engagement web site

Measure and Explanation	Usage
Unique Site Visits – Total number of visits to the project page	1,110
Aware – Total number of users who viewed the project page	927
Informed – Total number of users who clicked a hyperlink or opened a document	427
Engaged – Total number of users who actively contributed to the project via a tool, e.g. submitted feedback via the survey or participated in the Q&A	41

The online survey asked the community for their level of support and reasoning on the Vision Statement, the level of agreement with the Principles, Planning and Policy, Enhancing Connections, Integrated Community Hub and Main Street Renewal strategies. The survey also included a general comments question.

Survey Results

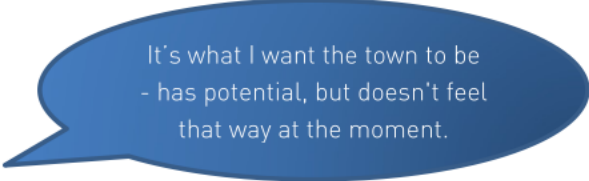
The online survey asked the community to indicate their level of support or agreement with each section of the Plan. Respondents were asked to provide comment for the level chosen. The responses are summarised as follows:

Vision Statement

Vision Online Survey Responses	
Strongly support	13
Support	15
Undecided	5
Somewhat oppose	4
Strongly oppose	1

Most were supportive of the Vision included in the Plan.

Many comments related to the character and village feel of Helensburgh. Some were supportive of the Vision and Plan's aspirations, its potential and that it honours the heritage and natural surroundings. There was some uncertainty as to how Council would progress the Plan. It was commented that the town was messy, but the Plan has potential to change this.

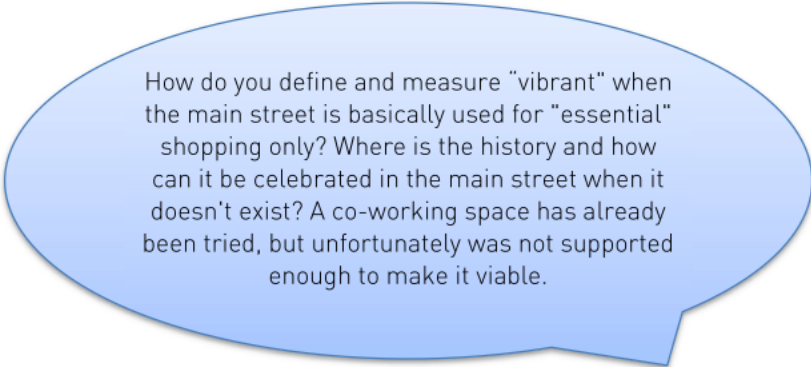


It's what I want the town to be
- has potential, but doesn't feel
that way at the moment.

There were comments about loving the town and residing in Helensburgh a long time. It was referred to as a beautiful village with character and heritage.

While there was much support for Helensburgh being a busy, thriving small town, there were also comments questioning the way it was described. Comments were made on the number of empty shops.

Some described Helensburgh as sleepy. It was referred to as the "sleeping town", as residents sleep in Helensburgh but work, shop, and play out of town. It was felt the main street businesses cannot survive because of this.



How do you define and measure "vibrant" when the main street is basically used for "essential" shopping only? Where is the history and how can it be celebrated in the main street when it doesn't exist? A co-working space has already been tried, but unfortunately was not supported enough to make it viable.

There were many comments that the destinations were world class, such as Darkes Forest, the temple and Sublime Point, but there were no connections from these to the Town Centre. Some said there was nothing unique or of interest in the Town Centre for tourists to consider it a "destination", resulting in it being bypassed. Others said using the term 'world class' was not meaningful and that 'rich heritage' was debatable. Some new residents objected to mentioning Helensburgh's mining heritage as they did not want to celebrate coal.

Comments were made that there needed to be residential development and limiting the plan to the main street was 'silly'. Better connection between suburban streets and the Town Centre was mentioned. This was highlighted in relation to active transport connections.

There was support for describing the Helensburgh community as welcoming and friendly. A small number mentioned racism and that Helensburgh does not host diverse communities.

Principles

Principles Online Survey Responses	
Strongly agree	15
Agree	19
Undecided	1
Disagree	1
Strongly disagree	2

Most survey respondents agreed or strongly agreed with the Principles in the Plan.

There was positive feedback saying the Principles were a true and good summary and the statements would help bring the Town Centre into line with what the community should be like. They were described as 'all-inclusive' and 'what I would like to see'. Sustainable and creative elements were supported.

Active transport was popular, with walking and cycling connections within the CBD mentioned, as well as connecting the CBD to attractions such as the Temple.

I am a supporter of active communities where people spend time outdoors together and ideally walk and cycle instead of taking the car

Making Helensburgh 'more pedestrian friendly' was popular, with the current struggle of parents with prams mentioned. Accessibility was raised, with a view that Helensburgh is not accessible, or is only somewhat accessible.

Including additional meeting areas was supported and there were some comments saying that cafes are the main meeting places in the town.

There was concern that businesses were failing, with reference to the number of empty shops. Some stated the town doesn't have the population needed to retain the number of shops.

Comments were made on the need for a roundabout at Lilyvale Street and parking. There were requests for more toilets, with mention that there was only one public toilet for the whole town.

The few that disagreed with the Principles in the Plan stated there were 'lovely sentiments, but not all are achievable.' Comments stated that apart from 'connected, accessible and pedestrian friendly' the principles required a major shift in the behaviour of residents to actively 'shop local' and not just for 'the essentials'.

Planning and Policy

Planning and Policy Online Survey Responses	
Strongly agree	11
Agree	14
Undecided	10
Disagree	0
Strongly disagree	3

Responses to the Planning and Policy statements were mixed. More people agreed or strongly agreed with them than disagreed or strongly disagreed, while some were undecided.

Respondents commented on the Town Centre boundary. Some asked why the Town Centre boundary needed to be defined and others commented that it needed to be expanded to include more of the town. There was a view that the Town Centre boundary should include the hotel, police station and courthouse. It was felt that the Plan ignores other parts of Helensburgh and that corner shops were needed in housing areas.

What would the plan be to transition away from the existing shops to create the diverse, open, sustainable and unique streetscape experience your vision implies?

While there was agreement about the need to create identity, there were some who asked Council not to promote the mining history. A resident asserted that the history outlined in the draft Helensburgh Town Centre Plan was incorrect. Their description was that Helensburgh (Camp Creek) started as a railway camp for the development of the rail line down the South Coast and that the town centre with post office and police station was opposite the mine entrance, which explains the location of the primary school. The resident inferred from this history that the draft Town Centre Plan is an attempt to make the current CBD 'modern faux heritage' and it would be better to see patches of grass, tree roots, undefined borders and dense bushes, which is more appropriate for a rural setting.

Many felt that the current mismatch of paths, shops and buildings would be more cohesive with the implementation of the Town Centre Plan. Others questioned how the Plan would help transition the town from the current uninspiring shop fronts.

A comment that planning had failed to connect Helensburgh to the wider environment was made. The lack of signage showing the connection between major arterial roads and the Town Centre was mentioned. Parking, a roundabout for Lilyvale, better roads in the area and parking at the train station were raised in response to the Planning and Policy section of the Plan.

Comments on planning controls to limit building height made. One submission stated that building heights should be limited to two storeys.

An exit road for disasters such as bushfires was also requested.

Enhancing Connections

Enhancing Connections Online Survey Responses	
Strongly agree	16
Agree	13
Undecided	7
Disagree	0
Strongly disagree	2

Most 'agreed' or 'strongly agreed' with the Enhancing Connections section of the Plan. There were some who were 'undecided' and only a small number who 'strongly disagreed'.

Some said more footpaths and connections to the natural surrounds and attractions are needed. There was mention of connecting the bike path to Wollongong.

Many who strongly agreed commented that the connections were not visible in the design, and the park and pool were not in the Plan's scope. There was confusion about how the trees on Walker St were to be treated.

While some requested more open space, others said there was already vast public space. Charles Harper Park was mentioned as needing better connection to the CBD.

Some submissions said the town's standing as a gateway to the Illawarra needs to be more visually recognised. More of a sense of arrival at the station and at entrances to Lawrence Hargrave Drive and Princes Highway were mentioned. A request was made to make it obvious where the CBD starts. Many said the arrival at the Parkes Street roundabout and Princes Highway is an eyesore.

There was comment on jargon in the Plan such as 'leverage' and that it didn't entail much change except some tinkering at the edges.

Some said the Plan was 'pointless' and doesn't consider the needs of the townsfolk that live and work in the area.

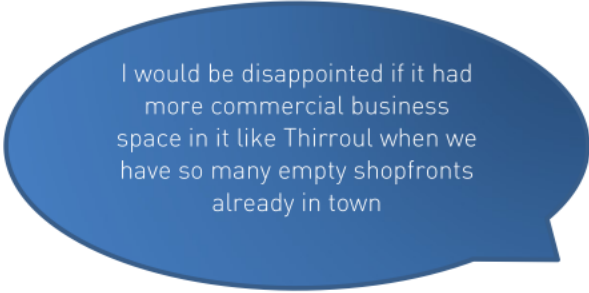
Integrated Community Hub

Integrated Community Hub Online Survey Responses	
Strongly agree	20
Agree	12
Undecided	2
Disagree	2
Strongly disagree	2

The concept of an integrated community hub was well supported, with most either 'agreeing' or 'strongly agreeing' with this section of the Plan. A small number of people said they were 'undecided', 'disagreed' or 'strongly disagreed' with this part of the Plan.

There were comments about a lack of services in the area, with more desperately needed. Some people felt the community works well without a Town centre, that the one that exists is not used or fit for purpose. Others stated that a multipurpose centre would be a better use of space. There were requests for 'a big library with a community hall and indoor sports.'

The idea of a community hub was questioned, with a view that it won't be a community hub as it won't be in the heart of the CBD.



I would be disappointed if it had more commercial business space in it like Thirroul when we have so many empty shopfronts already in town

Main Street Renewal

Main Street Renewal Online Survey Responses	
Strongly agree	15
Agree	17
Undecided	4
Disagree	0
Strongly disagree	2

There was much support for a renewal of the main street with many 'agreeing' or 'strongly agreeing' with this part of the Plan. Only a few were 'undecided' or 'strongly disagreed' with this section.

Some said that while it is a great vision for the street, it is also dependent on upgrading some of the existing shop fronts so that they are more attractive and become destinations for new business opportunities like cafes, bars and restaurants.

Parking was highlighted by many as an issue, with a view that parking needs to be considered as there is not enough. It was commented that there should be a maximum 15-minute parking on Walker St between Parkes Street and Lilyvale Street, both sides of road.

Requests included the consideration of lowering the speed limit to 10-30kph on Walker Street. The need for a roundabout at the Lilyvale/Walker Street intersection was highlighted.

Linking the different parts of the Town Centre to the main street and making it pedestrian and bike-friendly was seen as an important focus.

There was reference to the history of the area and how that relates to the renewal of the main street. It was commented that much of Walker Street's history has already been demolished and the existing historical buildings are the pub, post office and courthouse all on Parkes Street. The memorial trees and historical sandstone guttering are viewed as being important.

General comments about the Plan, including email submissions

A summary of general comments provided via email submissions and the online survey is included in this section of the report.

Submissions received via email did not address the various sections of the Plan and some broadly referred to both the Plan and the Streetscape Masterplan as the one plan. These submissions came from residents, businesses, Neighbourhood Forum 1 and the National Trust.

Praise for the Plan

The Plan received much praise, with comments saying it is a thoughtful and considered approach to creating a vision and that this program of rejuvenation is well overdue. Some appreciated the effort and purpose of the Plan, and mostly offered agreement. Comment regarding perceived issues concentrated on detail in the streetscape.

Character of Helensburgh

It was requested to review the Helensburgh Character Statement, locality-based precinct plan and to reduce the three-storey building height to two-stories in the controls to ensure the view of the National Park and State Conservation Area are maintained. It was suggested to add the memorial trees on the western side of Walker Street to the Heritage Schedule, with a succession plan created for their eventual replacement.

The town centre should not have buildings that block out the sun on the street or block out views of our natural surroundings.

Don't litter the street with old mining equipment and storyboards

Signage

For the arrival, there were requests that Council should install more signage at critical points to direct tourists to the Town Centre. It was requested to reinstate the large directional sign that said "Shopping Centre 1km" on the corner of Old Princes Highway and Parkes St.

This sign was ordered and paid for by my late father and installed by Council to encourage drivers to go into town. This sign was removed without reason or consultation when the roundabout was installed.

Town Centre Plan Boundary

Comments were made on the boundary and scope of the Plan. There was a view that the study area should include Charles Harper Park, Police station, Club and Hotel. Some expressed concern that 'it will do nothing for the northern end of Walker Street around the pool, Credit Union and Pub. The National Trust praised the boundary definition describing that the measures in the Plan will create a more compact and walkable Town centre.

How does Charles Harper Park integrate into the pool, club and ovals?

Modernising the Town Centre

Concerns were expressed about modernising Helensburgh, stating it is 'really important to not modernise it too much, as it will risk losing its charm and character'. In contrast, it was remarked that 'Helensburgh is more like an old village currently'. There was a desire for it to become 'a big town similar to Wollongong, with high-rise buildings/apartments/townhouses, more residential development and fast-food outlets like a proper town'.

Businesses and Shopfronts

Many described the need to activate and improve shopfronts, with requests for Council to work with shop owners. There was a view that the whole main street of shops needs a complete facelift with a common design throughout, both the structure of the façade and colour choice. It was felt that owners of businesses in the main street should be held more accountable for the presentation of their shopfronts.

It was suggested to create a "character" shopfront facade program, with requests to provide grants to assist in its implementation. It was also suggested to reduce Council rates within the B2 zone to assist property owners to invest in upgrading/changing the properties, and to permit a more diverse mix usage of existing properties.

A renewed focus on nightlife, with cocktail bars/lounges, indoor/outdoor eateries and beer gardens was suggested for the Town Centre. There was a view that the design of the Town Centre should express the unique character, rich heritage and natural environment of Helensburgh. There was a desire to steer clear of unappealing generic-style buildings and inject vibrancy with street art/murals.

Sustainability

Consideration of environmental sustainability and climate change were mentioned by some. It was suggested that all businesses be required to use renewable energy. There was a view that Council should ensure infrastructure for electric cars and lighting in the Town Centre were environmentally sustainable. When updating streetscape spaces and infrastructure, it was suggested that recycled materials, such as park benches using recycled plastics should be included.

Parking and Roads

Additional parking was often raised as needed or critical. It was questioned why parking was being taken away from the shops, as the number of people moving into the area is increasing. Requests were made for public toilets and bicycle parking to be included in the Plan. It was reported that there is currently nowhere safe to park bikes. The need for roundabouts at the intersection of Walker and Short Streets and Walker and Lilyvale Street were mentioned.

There was an objection to the shared pedestrian zone on Short Lane from a landowner who outlined that the proposal was on private property.

Accessibility and Safety

Accessibility was described as not ideal for strollers, people with disability and older people. It was felt that accessibility needs to be considered in any planning.

More street lighting was considered as paramount to keep the Town Centre area safe and usable, with examples that Walker Lane has no streetlights and is littered with rubbish.

Online Q&A

The online Q&A tool on the website received 11 questions. Questions were asked about the number of empty shops in the Town Centre, the intention to construct a roundabout at Lilyvale Street, plans for cycleway links and enquiries as to why the car park at Charles Harper Park is being changed.

There were also requests for more parking for people with disabilities, a review of timed parking, angle parking on Walker Street and for bicycle parking to be installed.

OUR WOLLONGONG

JOIN THE CONVERSATION



Helensburgh Streetscape Masterplan

ENGAGEMENT REPORT

July 2020

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Executive Summary

Council presented the draft Helensburgh Streetscape Masterplan (the Plan) to the community for comment from 11 May to 22 June 2020. The Plan was informed by extensive community engagement conducted as part of the development of the Helensburgh Town Centre Plan in 2019. The Streetscape Masterplan is a long-term, comprehensive design that articulates the community's vision for the Helensburgh Town Centre by guiding infrastructure improvements and renewal.

The Plan demonstrates how Walker Street can be designed to support its role as a vibrant main street, including enhancements to adjacent streets (Parkes Street and Short Street) to make the Town Centre accessible and convenient for all. It outlines nine objectives focussed on enhancing the physical and visual connectivity of the Town Centre by embracing its village-like setting; expressing its indigenous, mining and natural heritage through public art and interpretive signage; improving the pedestrian experience by activating laneways, improving the street canopy, traffic calming and providing public amenities. The Plan also sets out 25 'key site opportunities' with the themes of accessibility; connectivity and parking; character; and, meeting places

Community engagement for the Plan was run at the same time as engagement for both the Helensburgh Town Centre Plan and Helensburgh Community Centre Options. The community was directed to a central landing page for Helensburgh projects on Council's engagement website which directed them to the three project pages.

Due to the COVID-19 pandemic, the communication and engagement strategies were adapted to ensure that in the absence of face-to-face public meetings, the community could meaningfully connect and provide feedback. A variety of communication and engagement methods were used to invite the community to join the conversation.

The project page on Council's engagement website included a Consultation Plan, Frequently Asked Questions, an online survey, Q&A tool and a video that explained the plan in detail. Hardcopies were provided in the Helensburgh Library. These were also delivered when requested.

The engagement was promoted extensively through notices in the Helensburgh 2508 Magazine and the Advertiser, social media posts and media releases. Emails were sent to key stakeholders including Neighbourhood Forum 1 (NF1), local businesses and participants of the 2019 Helensburgh Town Centre Plan engagement. Phone calls were made to key stakeholders. Key staff were available to speak to via phone and email.

The community was invited to provide feedback via the online survey, email or phone. The online Q&A tool was also used to capture feedback and provided an opportunity for further information and clarification. Meetings were conducted via video conferencing for interested members of the community.

79 submissions were received, including 63 online surveys and 16 emails. Feedback was received by residents, business owners and NF1. Some emailed submissions were very detailed and included sketches and photographs as well as feedback relating to the Helensburgh Town Centre Plan engagement. The webpage received a significant amount of interest, with 1,040 unique site visits.

In the online survey, the community was asked to rate their overall support for the Plan, the nine objectives and 25 'key site opportunities' and to provide comment. Approximately two thirds of the 63 respondents selected either 'strongly supporting' or 'supporting' the Streetscape Masterplan. Slightly less than one third of respondents selected 'undecided' and a few selected 'against' or 'strongly against'. Thirty-five agreed with all the objectives, 19 were unsure about one, some or all, and nine did not agree with them. Thirty-three agreed with the key site opportunities, 27 were unsure about one, some or all, and three did not agree at all.

The Plan elicited a diversity of views about the impact it would have on the functioning of the Town Centre, its visual appeal, parking, traffic flow and access, the pedestrian experience and on local businesses. The Plan was both welcomed as an overdue upgrade and opposed for not addressing underlying issues with the Town Centre. Some fully supported the Plan's objectives, others gave them qualified support, and some objected to the Plan, recommending that it have a broader footprint or focus on different objectives.

Positive comments included that the Plan was needed, well thought out and holistic. The Plan received support for making the Town Centre pedestrian friendly and accessible and for recognising the town's history.

The impact and cost to businesses of the changes that may occur to the day-to-day operations in the Town Centre were also raised. The loss of parking was a common concern.

Opinions on the meeting places were mixed, with some supporting them, others suggesting alternative locations or upgrades to existing meeting places, and some stating that the cafés and Hotel already provide this function.

Suggestions included having additional roundabouts, pedestrian crossings, speed reduction, shared zones, improved footpaths and connectivity, improved parking and parking for people with disability. There were also requests for more active transport options, the use of sustainable technologies and support for new and varied businesses in the Town Centre.

Community support for the Plan included some requests by respondents for specific parts of the Plan to be revisited in detail during the design development phase, as part of a collaborative design approach.

Background

Extensive community engagement for the draft Helensburgh Town Centre Plan was undertaken from March to June 2019. This feedback shaped the Town Centre Plan and informed the development of the Helensburgh Streetscape Masterplan (the Plan). The Plan is a long-term, comprehensive design that articulates the community's vision for the Helensburgh Town Centre by guiding infrastructure improvements and renewal.

The priority of the Plan is to enhance the lived experience of everyone using the Town Centre, providing a great main street experience in a pedestrian friendly environment. It has nine objectives and 25 'key site opportunities' focused on accessibility, connections and parking, character and meeting places.

Key elements of the Plan include:

- Improving accessibility and safety, with a focus on delivering a pedestrian-friendly environment.
- Enhancing the quality and amenity of meeting places
- Expressing the local identity and heritage of Helensburgh

Engagement for the Plan ran at the same time as the Helensburgh Town Centre Plan and the Helensburgh Community Centre Options, from 11 May to 22 June 2020.

Stakeholders

Stakeholders identified prior to the engagement included participants in the 2019 engagement, residents, community groups, sporting clubs, schools, businesses in the Helensburgh Town Centre and Neighbourhood Forum 1 (NF1).

Methods

Due to the COVID 19 pandemic, the communication and engagement strategies were adapted to ensure that in the absence of face-to-face public meetings, the community could meaningfully connect and provide feedback. A variety of communication and engagement methods were used to invite the community to join the conversation:

- Two paid advertisements in the Helensburgh regional paper 2508 Magazine
- Social media posts
- Media releases to local media
- Phone calls and emails to all businesses in the Town Centre
- 171 emails to key stakeholders and respondents who provided comment during previous engagement.
- Notice listed in the Council column in The Advertiser
- Video conference or telephone calls with individuals on request
- For those who did not have computer access, hardcopies of the Town Centre Plan and the Plan were hand delivered as requested or made available at the local library for collection.

Due to COVID-19 restrictions, telephone calls and video conferences were conducted with interested parties as an alternative to face-to-face meetings. This allowed for detailed

discussion to clarify any points raised and for Council staff to hear respondents' in-depth feedback.

NF1 were contacted by phone and information was provided via email. This information was then included in the NF1 newsletter, which generated questions and submissions from their members.

A landing page for Helensburgh projects was created on Council's engagement website directing the community to a project page for the Plan. This project page included a video that outlined the main features of the Plan and how it was developed. A Q&A tool was used for the community to ask questions which were then responded to by Council officers.


Results

79 submissions, including 63 online surveys and 16 emails were received from residents, businesses and NF1.

Two social media posts, 14 May reaching 2,917 people with 132 engagements and 20 June reaching 3,989 people with 235 engagements

An article published in the May edition of 2508 Magazine (pictured) coincided with increased visitation to the project webpage.

Shape the Future



Artist's Impression of a renewed Walker Street

Wollongong City Council is working on a series of strategic projects in your suburb as part of our future-focus for Helensburgh. Rather than looking at each of these projects independently, we're looking at them with the bigger picture in mind. That's why we're consulting these projects simultaneously. By taking this united approach we're able to get in place phased plans that can be ready to roll in early 2021. It also allows us to reflect upon what we've already achieved. We appreciate that Coronavirus, and measures to reduce the risk of its spread, might be considered a community engagement challenge. But we're responding and our website offers new ways to collate feedback and share information.

Thanks for working with us on Helensburgh's bright future.




4 / 2508 / MAY

of Helensburgh

The draft Helensburgh Town Centre Plan is based on community consultation carried out in early 2019. It focuses on building Helensburgh's individuality and sets a shared community Vision and Strategies, which form the Town Centre action plan.

Some of the key directions include:

- A renewed main street which is safe, accessible, and honours the local identity. We've developed a draft Streetscape Masterplan that expresses what Walker Street will look like in the future and prioritises a pedestrian-friendly environment and we're asking for your comments on a draft streetscape design for Walker Street.
- A key Strategy is to deliver an Integrated Community Hub, bringing together important community facilities in a centralised location. We're exploring options for an integrated community centre and library facility, and we're seeking your opinion on how best to meet the needs of the community.

How to share your thoughts

Submissions close 8 June 2020

Online:
ourwollongong.nsw.gov.au
Complete the survey - Provide your opinion and feedback
Online Q & A - Where you can ask questions about the draft Plan. We will respond to questions once a week.

Write to us:
The General Manager
Wollongong City Council
Locked Bag 8821
Wollongong DC NSW 2500

Email: engagement@wollongong.nsw.gov.au
Phone: 4227 7111

MAY / 2508 / 5

Table 1 outlines visitor interaction on the engagement web site.

Table 1: Summary of online participation

Measure and Explanation	Number
Unique Site Visits – Total number of visits to the project page	1,040
Aware – Total number of users who viewed the project page	935
Informed – Total number of users who clicked a hyperlink or opened a document	402
Engaged – Total number of users who actively contributed to the project via a tool, e.g. submitted feedback via the survey or participated in the Q&A	65

Survey Results

The community was asked to rate their overall support for the Plan, their agreement with the objectives and key site opportunities and to provide comment. Table 2 provides a breakdown of responses.

Table 2: Online Survey Responses

Plan Support		Objectives		Key Site Opportunities	
Strongly support	18	Agree with all	35	Agree with all	33
Support	24	Unsure about one, some or all	19	Unsure about one, some or all	27
Undecided	17	No I don't agree with the objectives	9	No I don't agree with the objectives	3
Against	2				
Strongly Against	2				

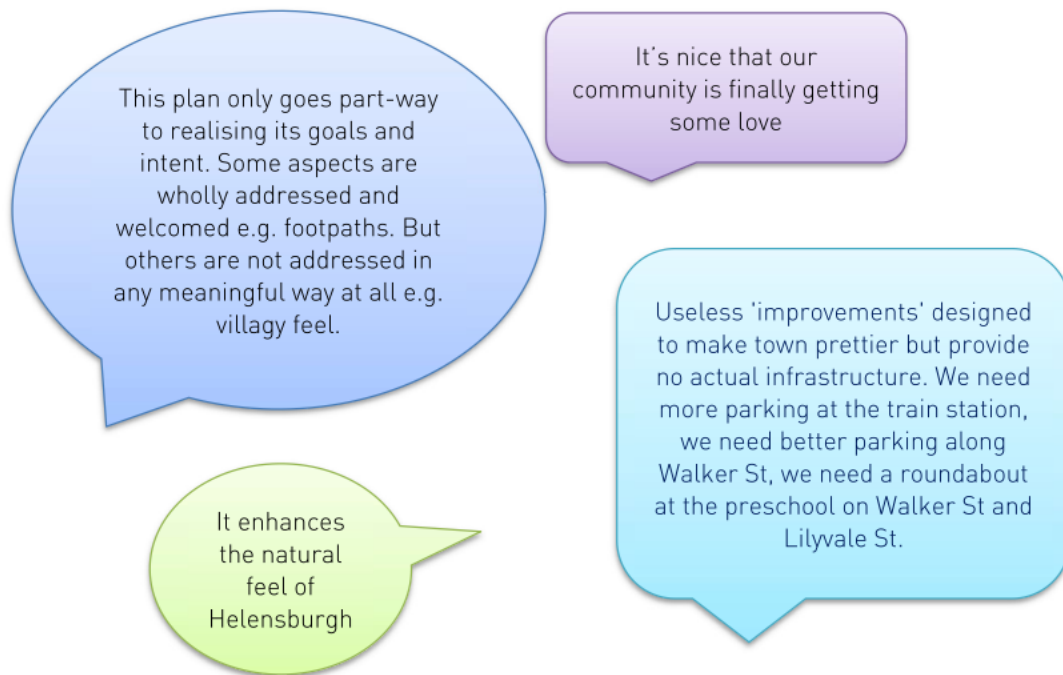
Comments

Comments were provided via the online survey and email from residents, businesses and NF1. Some emailed submissions were very detailed and included feedback on the Helensburgh Town Centre Plan, as well as sketches showing an alternative vision for the Town Centre. Common themes are reported below.

Level of Support for the Plan

Comments in support of the Plan included that it was: 'well thought out'; 'thorough'; 'good for the town'; 'enhanced the natural feel'; 'keeps the original look'; and 'badly needed to improve the mismatched environment'. There was also qualified support for the Plan, suggesting a holistic solution and that some elements required further refinement.

Metropolitan Coal confirmed their support for the Plan and stated agreement for the use of the funds contributed by the mine to achieve the Plan. Metropolitan had no concerns regarding the Plan given the road corridor design has been formulated with Metropolitan's trucks in mind. The mine is not trucking, or planning to truck, large quantities of material offsite, though there is always the possibility for a need to return to trucking, depending where the material needs to be delivered.



Business concerns

Perceived loss of business was raised as a concern by some business owners with comments that more understanding was needed about how the town currently functions. The concern was expressed that the Plan may negatively impact the town's day-to-day functioning. Feedback also questioned the cost-benefit-ratio of the upgrade.

Parking and traffic

Concerns about loss of parking was a common theme across those who 'supported', were 'undecided' or 'against' the Plan. Objections to proposed changes to the Charles Harper carpark on Parkes Street were also received.

Concerns were raised about the proposed removal of four 30-minute parking spaces for the creation of a meeting place, as some businesses said they relied on them.

There were suggestions for increased and improved parking on Walker Street including;

- Creating 12 to 14 perpendicular car parking spaces and angle parking on both sides of Walker Street.
- Angled parking on one -side only from Parkes Street to Lilyvale Street
- Rear to kerb parking
- 30-minute parking on one side and 15-minute parking and drop off on the other
- Increased accessible parking for people with disability on Walker Street opposite Short Street.

Where do people park their cars? Business is big in Helensburgh, coal trucks, Coles delivery trucks, general day to day operations for business, buses and everyone drives to the shops.....it's the shape of Helensburgh.

Removal of the four 30-minute car parking spaces currently outside the Helensburgh Post Office is totally unacceptable. Business relies heavily on those spaces to give our customers quick access. Delivery vans and couriers also use those spaces consistently to deliver to and pick up from businesses around the roundabout.

There were also comments about the difficulty of two-way traffic regarding the Charles Harper car park upgrade, vehicle turn around and impact on delivery vehicles.

Speed and Roundabouts

Slowing traffic and a pedestrian-friendly town was welcomed by many. Speed of vehicles and how this is managed was raised as a concern. There were comments about the road narrowing, speed bumps and that a roundabout at Lilyvale Street would slow traffic coming in through Walker Street. There was also suggestion that as roundabouts slow traffic, traffic calming was unnecessary. It was suggested to move the road narrowing blisters on Parkes Street further west to better define the Town Centre starting point and improve pedestrian safety.

The left turn from Walker Street to Parkes Street was described as a major issue as drivers were racing to beat traffic coming from the right. Drivers highlighted their concerns with looking right to find a break in the traffic, meaning they would not be watching for the pedestrian crossing on the left. Sightline concerns at the Short Street and Walker Street intersection were raised. It was suggested that a speed cushion be installed on Walker Street just before the roundabout.

Roundabouts at both the Short/Walker Streets intersection and the Lilyvale/Walker Streets intersection were requested. There was concern about the widening of the roundabout at the corner of Walker Street and Parkes Street as it will impact the turning of larger vehicles. It was suggested that slow traffic cushions be installed in Parkes Street near the intersection of Waratah Street.

Narrowing of the main roads of Parkes Street and Walker Street was raised as a concern in relation to bushfire and other emergencies.

Concerns were also raised about the closing of the exit of the Parkes Street car park.

Road narrowing on the two main roads would cause traffic chaos in trying to escape bushfire storms

Objectives of the Plan

Those that 'agreed' with the objectives of the Plan commented that they make sense and will help make Helensburgh a nicer place to visit. It was felt that people will want to spend time in a pleasant, leafy environment where they can move about easily and safely.

Those that were 'unsure' or 'disagreed' focussed on the need for a roundabout at Lilyvale Street; the impact on trucks and buses as a result of street narrowing; the lack of parking; the need for more public toilets; and that shared zones in laneways will cause anti-social behaviour.

Key Site Opportunities

Those that 'agreed' with the key site opportunities supported the slowing of traffic and making the Town Centre pedestrian friendly. Respondents in agreement also expressed concerns that parking is a priority and an issue on Walker Street; the shops and slope on the west side need upgrading; and there was need for a roundabout at Lilyvale Street.

Those that were unsure or disagreed with the key site opportunities commented on the selection of Short and Walker Lanes as locations for shared use; the lack of necessity for traffic slowing devices as more pedestrian crossings and a roundabout at Lilyvale Street will slow traffic; and, disagreement with the reduced parking.

Pedestrian Crossings and Footpaths

The position of the pedestrian crossing on Parkes Street was questioned with the comment that all four arms of the roundabout need safe crossing points. There were suggestions for additional crossings and informal crossing points on several locations along Walker Street with blisters and trees on the east side. The need for a safer crossing on Walker Street, north of the roundabout, due to parked cars was raised.

Changing the No Stopping zones next to the pedestrian crossing to landscaped areas will make it safer for pedestrians

The need for safe footpaths was highlighted due to existing uneven and hazardous paths. The footpath at street level on the west side of Walker Street was questioned as there is already a path in front of the shops. A footpath was requested to connect Short Street to the car park and to create more pathway links between the west side of Walker Street and the upper path. It was suggested to consider making the upper and lower paths on the west side on Walker Street shared paths.

There was a request to retain pedestrian access through the lane beside the community centre.

A parent whose child is a wheelchair user stated that the Plan shows steps and limited footpaths making it difficult for wheelchairs. Access to the upper footpath and concern with the stairs from the pedestrian crossing on Walker Street was also raised. It was suggested to connect the ramp directly to the crossing. Widening the Parkes Street footpath and installing fixed seating for outdoor dining was raised as having no benefit to the Hotel or the Town Centre.

There was a request not to plant more trees outside Coles or put more cement pots on the footpath as these were described as a hazard. It was suggested Walker Street become one way and for it to be covered with a street canopy.

Shared Zones

There was a range of feedback related to shared zones with support for a 10kph vehicle and pedestrian shared zone around Walker Street and Parkes Street roundabout and extended to the surrounding streets. It was suggested that Engadine and Sutherland have good examples. To maximise pedestrian safety, maintain a village feel and still allow vehicle access through the Town Centre, 10 kph vehicle and pedestrian shared zones were suggested for Parkes Street, from Stuart Street through to Hume Drive and Walker Street, from Lilyvale Street to Cowper Street.

There were also concerns raised that the proposed shared zone activating Walker Lane for pedestrians would not be used. It was requested to be retained as a service lane only. Zoning anomalies were highlighted.

Potential shared zone down Walker Lane will simply not work. The only traffic generally using the lane are the Coles semi-trailers and other trucks servicing businesses. There is basically no pedestrian access to businesses from Walker Lane so it simply won't be utilised by pedestrians. The lane isn't wide enough to safely cater for pedestrians, semi-trailers and large trucks.

Active Transport

There were suggestions for bike paths, including a shared bike path in front of the car parking area in Charles Harper Park, shared bike path from the Community centre to the Temple and from the Town Centre through residential links to residential areas. An extension of bike paths to Darkes Forest was also mentioned and the encouragement of non-vehicle transport across the village with active transport from the station to the top of Parkes Street.

Requests were made for U-rail bike racks outside the Post office and café on the south east side of the Parkes/Walker Streets roundabout, near the ramp at Coles and upper path on the west side of Walker St.

Meeting Places

The proposal for creating meeting places received varied responses with many welcoming the idea and others stating that they won't be used, as the cafes and the Hotel already provide this space.

There was a request to consider the position of all meeting places and to choose sites that were north facing, as these would be warm, sunny and well used compared to sites with southerly and easterly aspects which would be colder and less used. There was a suggestion to create seating areas on the upper path on the west side Walker Street and allow shops to use them, as they wouldn't affect people's paths of travel or impede safety.

It is unlikely that the Parkes St Footway seating will be able to be properly activated by the Hotel. If this does not happen then the footway seating would be largely unused and end up working against the objective of creating on street activity.

Other Suggestions

Other suggestions included improving the Town Centre entrances; allowing events at the plaza on the north west end of Walker Street, including markets and professional/community performances.

There were requests to encourage new and varied businesses to fill empty shops and have clear style guidelines for shopfronts. The use of sustainable technologies and recycled materials in infrastructure works was suggested. Requests were also made for large-scale artworks, more bins, toilets and bubblers.

Draft Helensburgh Town Centre Plan

2020 - 2045



Acknowledgements

Wollongong City Council would like to acknowledge and pay respect to the Traditional Custodians of the Land to which this Strategy applies. We pay respect to Elders past, present and those emerging and extend our acknowledgement and respect to all Aboriginal people who call our town home.

We recognise Aboriginal people as the first people to live in the area. We respect their living cultures and recognise the positive contribution their voices, traditions and histories make to our region.

In celebration of unity, culture, both traditional and contemporary, we acknowledge the rich history of our local Aboriginal heritage.

The draft Helensburgh Town Centre Plan was developed in partnership with interested community members, representatives of the Northern Illawarra Chamber of Commerce, Neighbourhood Forum 1, and local school children.

Wollongong City Council would like to thank everybody who contributed to shaping this draft Plan by providing feedback in engagement activities throughout 2019 - 2020.

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How to read this document

Community vision

This document reflects the community's Vision for the Helensburgh Town Centre. It identifies priorities for change and renewal to achieve this vision.

Its purpose is to outline clear strategies and actions to meet the current and future needs of the people who live, work, visit, play and belong here.

The draft Plan will guide change through actions related to infrastructure improvements, main street renewal and planning control amendments.

Future projects for the Helensburgh Town Centre, whether driven by Council, the State Government, businesses, or community groups, should align with the Vision and Strategies within this draft Plan.

Technical analysis

This draft Plan is supported by planning, urban design and traffic analysis.

To inform the analysis, a lot of data was collected about how people walk around and use the town centre. A quality assessment of streets and public places has also been carried out. An accessibility audit was carried out and safety observations made.

Community Feedback

Valuable input from the local community has shaped this document. Comments have been gathered from community workshops, student workshops, previous consultation processes and discussions with the Neighbourhood Forum and the local Business Chamber.

This draft Plan has been finalised following consultation with the community during May and June 2020. Refer to the accompanying Helensburgh Town Centre Plan Engagement Report and Streetscape Masterplan Engagement Report for detailed information regarding what we heard through this engagement process.

Understanding Helensburgh

This Plan has been prepared by bringing together technical analysis and community aspirations to create a Vision and Strategies for guiding the future of the Helensburgh Town Centre.

Helensburgh Town centre

Helensburgh is the northern-most settlement of the Wollongong local government area and a gateway to the region.

The Town Centre supports the northern suburbs, servicing the local population of more than 7,000 local people and has a 'trade area' defined by Otford, Lilyvale, Stanwell Tops, Stanwell Park, Coalcliff, Clifton and Helensburgh.

People come to the town centre to shop for food, visit the doctor and dentist, go to the butcher and the bottle shop. It's a place to meet friends, go to the library or the pool, have a coffee or enjoy a bite to eat in a local café or restaurant.

Town centres generally have a primary focus on the provision of weekly and daily convenience goods and services but also provide a limited range of non-retail professional and personal services.

Resilience

Helensburgh's seclusion within a bush setting means that the town centre performs many important functions for the community in a changing future climate.

Safety, the protection of land and water resources, and preparedness for emergency and natural disasters - particularly fires - are important to consider in parallel with this Plan.

Helensburgh is anchored by a Coles supermarket, and supports a successful hub of community services including the community centre, library, childcare and baby health care centre.

Helensburgh is a place that attracts many visitors, with regional destinations including Symbio Wildlife Park, the Sri Venkateswara Hindu Temple, Bald Hill, recreational and sporting facilities, and extensive nature adventure trails and coast on its doorstep.

Visitors come by car, train, foot and by bike.

Successful town centres put people, and public life, at the forefront of planning

Housing

Helensburgh's proximity to Sydney, lifestyle and relative affordability contributes to strong forecast median house price growth in the coming years.

There is a trend emerging of reduction in the number of single dwellings, and an increase in the number of flats and apartments within the town centre.

Future development driven by housing demand must meet the needs of a changing population whilst balancing protection of the area's ecology, character and unique setting.



Context map - Helensburgh within Wollongong LGA

Key:

- Wollongong LGA
- Helensburgh - Suburb Boundary
- Study Area Boundary - Town Centre
- Train Line
- Surrounding LGAs

The Study Area

This Plan focuses on the Helensburgh Town Centre as defined as the land zoned as Local Centre (B2 Zone).

The Town Centre is focussed around the intersection of Walker and Parkes Streets.

Extending from Whitty and Lilyvale Streets at the southern boundary, to include the Parkes Street intersection, extending north toward Charles Harper Park.

The western boundary is Stuart Street and the eastern boundary is Walker Lane.



- Key:
- - Study Area Boundary
 - Charles Harper Park
 - Building Footprints
 - Trees

Unique Village With a Point of Difference

Character is what makes a place special and unique. Every place has a physical and visual reflection of its history, natural environment and community.

Preserving and enhancing Helensburgh's character will bring richness and authenticity to the town centre for future generations. It will set it apart as a place and make it a desirable place to visit and spend time.

Celebrating a rich history

Helensburgh is a place of many diverse stories which reflect its history and communities. It is a place of significant natural resources sustained by a network of creeks through a diverse landscape of tall eucalypts, heaths, waterfalls and rockpools.

The local Aboriginal community left lasting physical imprints on the local landscape. Recording of these imprints are documented in a range of archaeological records and Management Plans for both the Metropolitan Coal Mine and Helensburgh Public School. Evidence of First Nations people in Helensburgh exists in the form of shell middens, stone tools, the residue from the manufacture of stone tools, scarred trees, decoratively carved trees (that define ceremonial sites and burial sites), painted art in rock

shelters, symbolic engravings on sandstone outcrops and grinding grooves on sandstone from sharpening stone tools and the hardwood tips, or stone tips, of spears.

There is an opportunity to connect with Country and tell the story of First Nations People in Helensburgh.

Coal was discovered in Helensburgh by Charles Harper in 1884 which triggered significant change. Helensburgh's bushland had been undisturbed by colonists but for occasional grazing and the passing of travelers and settlers on the tracks from the west and to Bulli on the coast. The Metropolitan Coal Co. opened a mine in 1888, bringing miners and their families to the temporary, frontier camp town above the mine site before the current town centre was developed on the plateau.

By the late 1880's, the Parkes and Walker Street junction comprised a shopping centre with a draper, butcher, baker and general store. As people moved to the land surrounding the town, the iconic double fronted timber Miners Cottages began to replace temporary tents and shacks.

Buildings from this period are important links to the past and some, like the Post Office and Police Station, are still in use today. As the settlement grew, so did its cultural life with the arrival of churches, the development of clubs and societies, bands, pubs, and one of Australia's first workman's clubs which remains one of the town's main institutions.



Dreaming Tracks at Bald Hill. Artist: Chris Edwards-Clarke

Today, the Metropolitan mine still produces coal, the forests are protected and regenerating, and the township is transforming.

Helensburgh is the village-like home of Sydney commuters, a focus of Hindu cultural celebrations, a great base for bushwalking and mountain biking, a place of seclusion to be restful, retire, a place to breath, raise a family.

Understanding local community

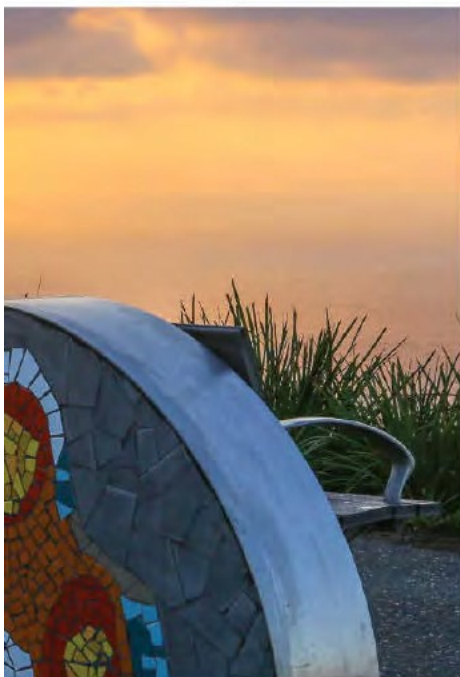
The demographics of Helensburgh help us to understand and plan for the current and future needs of the community by age, occupation, income and lifestyle factors.

Helensburgh, Otford and Stanwell Park has a combined population of 7072 people (Census ERP 2018)

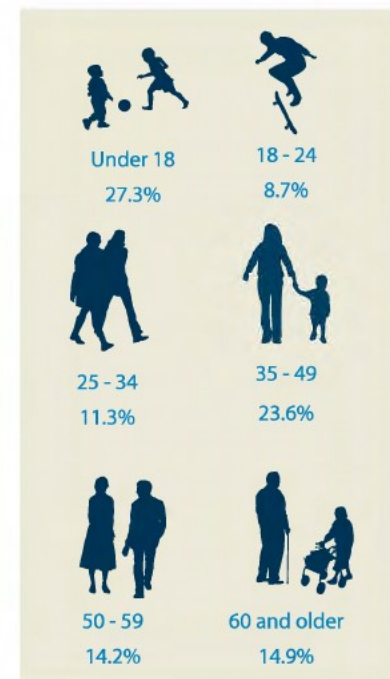
Helensburgh has a relatively high proportion of children (under 18) and a lower proportion of older people aged 60 or over than the rest of Wollongong.

The period 2011-16 has seen significant growth in the age groups of primary school aged children and older workers, retirees, and seniors. The majority of people currently live in a detached dwelling (separate house), with 15% of people living in

medium and high-density housing. There is a small trend away from the number of single dwellings, and an increase in the number of apartments. Population forecasts indicate that the population is anticipated to increase by 7.57% in the period 2020-36 (7,319), driven by the town's strong appeal to families and commuters.



Subdivision Plan 1969 showing Town Centre layout



Demographic profile of Helensburgh 2016 Census

Unique Village With a Point of Difference

We're a village

There is a strong view within the community that, regardless of its formal classification as a town centre, Helensburgh's character is that of a village. This is reflected in community feedback that they most value Helensburgh's sense of community.

It is important to ensure that the way buildings and streets are designed respect this value. A village is typically defined as being smaller than a town centre and located within a rural setting.

People are connected, the place is laid back and the character is rustic and draws from its surrounding countryside.

A sense of a village character is evident across the town centre through a number of building styles and materials and streetscape landscaping, however, there is significant scope to build on this desired future character for the town centre.



Charming frontages welcome people to stop and spend time



Helensburgh Centenary - the community has always been connected

Hidden heritage needs to be revealed

The community told us that 'our heritage and history are not celebrated enough'.

Lasting physical imprints of the Traditional Custodian's use of the land and culture survive in the landscape, yet this ancient heritage is not celebrated or reflected in the town centre.

European heritage is evident in a small cluster of heritage listed buildings at the intersection of Parkes and Walker including the Helensburgh Hotel, the Post office as well as landscape features in Charles Harper Park and the old Police Station.

As these are the few remaining historical buildings, they need to be celebrated and protected as they offer an opportunity to influence future character.



Interesting historical artefacts are scattered about the town



The Helensburgh Hotel - an important heritage building and landmark in the town centre

Connecting to the natural setting

Nestled in one of the most extraordinary natural landscapes, Helensburgh town centre sits on the cusp of the Garrawarra State Conservation Area and the Royal National Park. Connection to this amazing landscape setting is critical to defining the unique offer and character of place.

The physical connections linking people between the centre and nature are also very important. However, these are weak and not easy to navigate.

There is an opportunity to bring the local native landscape into the town centre, to bring character to the town centre itself. Interesting historic items are scattered in the main street, however, they lack context and not supported with interpretation.

Views across the town centre to natural areas are also important. These are not defined in local planning policy, and need to be identified to ensure their protection.



Iconic Angophoras, Gynea lilies and flannel flowers in surrounding bushland



View to the east to protect



A Memorial to fallen soldiers

The magnificent Brush Box trees lining the western side of Walker Street offer beauty, shade and character to the main street.

However, the importance of these trees extends beyond their beauty. The trees were planted as a memorial to those lost in WWI.

The story is told in the book 2508 Anzac, that on Arbor Day in 1934, 12 Sugar Gums were planted to honor fallen soldiers. Over time, the original trees that died were replaced with the Brush Box we enjoy today. This likely to have occurred during the 1940s/50s.

The intent to honour the lost friends of Helensburgh through these memorial plantings is a powerful story to tell. It is important that these memorial trees are protected and their story told to honour the past.



Helensburgh's memorial trees, Parkes St c.1950

Today the Brush Box trees make a statement in the mainstreet. Their story needs to be told.

Character to Inform Planning Controls

Guidelines for how buildings are designed are outlined in planning controls. Building heights, setbacks and design character are defined in the Wollongong Local Environmental Plan 2009 and Development Control Plan 2009. Helensburgh Town Centre has sound planning policy controls, however they lack clear guidance on the desired future character of the place.

Planning Controls need to better promote character

The town centre is zoned Local Centre (B2 Zone). Across the town centre, a mix of uses (retail, commercial, residential) are allowed. Residential development is only permitted above ground level.

More recent development across the town centre are predominantly residential, which is particularly evident along Stuart Street which has transitioned to residential.

Across the town centre, there is a generous supply of retail floor space available for development. This capacity (57,834m² SGS) will more than meet forecast demand (9,000m²).

Consideration should be given to how the vibrancy of the town centre can be better managed through defining a concentrated main street/ town centre area. This would be achieved through adjustments to the town centre boundary to better guide investment.

Design quality needs to be improved

Building character and quality in Helensburgh town centre is varied.

Buildings should be designed to respond to their context, whether that be the scale of the building, materials and colours used, the way the building interacts with the street or how it is designed to ensure views to the surrounds are captured.

Buildings are currently a comfortable scale as they present as one and two storey buildings along Walker and Parkes Streets.

Recent development throughout the town centre is observed to be fairly generic. This means that the design does not have a clear relationship to the identity of Helensburgh as a place, but are simply a building that could be anywhere.

WDCP Chapter D1 – Character Statements is intended to guide the desired future character for Helensburgh. However, the look and feel of the town centre is not defined by the character statement.

“a well maintained, historic streetscape”

“our heritage and history is not celebrated enough”

Quotes from community engagement



Building Heights defined in Wollongong LEP 2009

7



Floor Space Ratio defined in Wollongong LEP 2009



Heritage listings defined in Wollongong LEP 2009

Celebrating our point of difference

Helensburgh supports a wide diversity of businesses in and around the town centre.

There is a real opportunity to leverage from these commercial and natural offers to build the economic diversity of the town centre.



Old Railway Tunnel home to Glow Worms



Link to Nature



Helensburgh Pool



Rex Jackson and Helensburgh skate park



Sri Venkateswara Hindu Temple



Heritage Buildings such as the Post Office on Parkes Street



Connection to nature - rural/country charm



Connection to a coal history



Helensburgh Hotel



Symbio Wildlife Park

A Compromised Main Street Experience

The experience of a main street is critical to its success. A successful main street attracts people day and night with interesting things to do and see. It needs to offer more than a great retail mix to be successful. Mainstreets need to be a place people want to spend time, meet their friends and be part of the local community.

Public spaces are not attracting public life

The town centre has a great structure, with the main street (Walker Street) supported by a laneway network and a series of public spaces.

However, public spaces are poorly activated and are not designed with a clear purpose. Analysis identified that many public spaces are not 'comfortable'. A lack of sunlight, exposure to a windy southerly or poor seating means it is not a nice place to spend time.

Aligned with the town centre, Charles Harper Park offers an important play and recreation space for the community. However, this land is disconnected from the town centre by a car park area and Parkes Street itself.

Public spaces in and around the town centre brings an opportunity to support public life. Historically, markets have activated some of the town centre public spaces. Consideration needs to be given to how these types of activities can be encouraged and facilitated.

Business offerings are limited, but could be diversified

There is strong support throughout the community to welcome continued diversity in the retail and business offer in the Walker Street.

Significant proportions of the community commute to Sydney. There is scope to explore the opportunity of expanding the commercial offer within the town centre to support changing work patterns of the commuting population. Co-working spaces and live-work opportunities should be explored and promoted.

The town centre lacks energy day and night

Pedestrian audits undertaken in 2019 provided information about how the town centre was used. The data gathered tells us that few people are walking around the town centre, and there are only a few pockets along Walker Street that are 'active'.

The most successful places are where there is outdoor dining associated with cafes. Further consideration needs to be given to how retail uses can provide a high quality street experience, including an interesting and engaging shop front, and outdoor dining during the day and into the evening.

A large number of vacant or inactive shopfronts mean that the street lacks interest, and people were not recorded spending time in the town centre. Generally it was simple trips to undertake day to day shopping needs.

The town centre is much busier on weekends with people being home and families coming in to play sport.



Highly valued recreation areas, including the new playground, pool and sportsfields



Inactive and vacant shopfronts detract from the main street experience

54%
of shopfronts
surveyed are
inactive or dull



Memorial Brush Box trees line the western bank of Walker Street



Poor interface public space and adjacent shops



Buildings do not bring life to laneways

Quality Community Facilities, but they are disconnected

The town centre is home to a range of community facilities and services. The Community Centre, Library, a pre school and health clinic are clustered near the Lilyvale and Walker Street intersection.

Although sitting close to each other, these important community buildings feel quite disconnected.

The community centre and library are separated by Walker Street. A combination of steep topography, lack of a pedestrian crossing and no accessible path of travel mean it is hard to move between these two important facilities.

While community facilities are located on the main street, they are not designed to interact with the street or integrate into the public space. Additionally the 'aged' facade and design makes these facilities less visible and less inviting.

Council is committed to upgrading the Library and Community Centre.



Cluster of community facilities sit at the southern end of the town centre across State Government and Council land

Key:

- Charles Harper Park
- Council Land
- Operational Land
- Community Land
- Crown Land - Appointed

The community told us that they wanted a combined Community Centre and Library facility with adequate parking

Functionality of the Community Centre is currently compromised by the flow of water running beneath the community hall's subfloor, limiting usage of this area.



The existing Community Centre, whilst purpose built at the time, is difficult to access due to land topography and doesn't meet the current community needs



Helensburgh Library is a demountable building. While it is small and not purpose built, it is well utilised and brings many people to the town centre



A new library building presents a significant opportunity to create a community hub



The existing library has a lovely garden to the street



An addition at the front of the community centre does not address Walker Street

A Difficult Place to Walk Around

Helensburgh's success as a town centre is compromised by the dominance of traffic, and a lack of well-designed and accessible pedestrian connections. This makes it hard for people to get around, and discourages staying to explore and enliven the main street.

It's hard to be a pedestrian

Core services and amenities like the Post Office and Charles Harper Park are within easy walking distance, yet separated by Parkes Street.

An accessibility audit revealed that crossings and kerb ramps often fail to meet appropriate standards. Steep cross falls, stepped entrances and awkward paths of travel hinder access to many of the shops and services.

Observations of pedestrian activity revealed that there are more people criss-crossing Walker St to and from their cars than there are walking down it during the week. Around half of people crossing Walker Street are not accessing the pedestrian crossing.

This contributes to additional vehicle movements, as people try to park as close as possible to their destination to avoid the hazards and inconvenience of moving around town.

Great laneway structure that needs to be protected

Walker Street has 'great bones': wide proportions, a leafy aspect and north-south orientation. However at 500m, it is too long for a vibrant and active main street.

There is a great network of laneways, but they are not formed or activated. Large blocks limit pedestrian movement, particularly the east-west connections.

Parkes Street is acting as a pedestrian barrier, effectively bisecting the town centre. The current pedestrian crossing facilities fail to provide adequate safety and comfort for pedestrians from frequent traffic movements including heavy vehicles carrying coal from the Metropolitan mine.

Lots of cars, issues with parking

The town centre is filled with more cars than people. There are conflicts between the people who are walking, and the speed and number of vehicles.

The relative convenience of travelling by car between nearby destinations is contributing to congestion on the main street. This creates a perception that there is inadequate parking in the town centre, however monitoring has shown that there is ample capacity off street and in the Coles carpark.

Cycling infrastructure in the town centre is lacking, despite its popularity with local children, and Helensburgh's attraction as a road and mountain biking destination.



Access for people with limited mobility is poor.



Bush Pea Lane is a key pedestrian connection which is uninviting and underutilised



50% of people don't use the Walker St pedestrian crossing



Walker St pedestrian crossing lacks accessible connections on the west.



Pedestrian access between Club Lane and Walker St, adjacent to the community centre

350
car parking
spaces within
400m of the
town centre

It's not easy to connect to where you want to go

Helensburgh is surrounded by regional attractors and destinations but the town centre feels physically isolated and disconnected.

The sense of arrival is not defined and there is no cue to drivers that you have entered the town centre.

The area lacks signage connecting the town centre to key destinations including the train station.

The temple welcomes thousands of people every year. While some arrive by vehicle, many arrive by train and take the pilgrimage to the Temple via the town centre. This is a unique experience for Helensburgh. There is not sufficient signage to mark the way to the Temple.



Children on bikes crossing Parkes St



Complete footpath connections between the town centre and the Quarry Duck Ponds



Helensburgh supports many amazing attractions | Illustration by artist Lara McCabe www.thestudiocoledale.com.au | Extract from The South Coaster Magazine <http://thesouthcoaster.com.au>

"a shared zone or traffic calming between Lilyvale and Parkes Street"

Quotes from community engagement

"the pedestrian crossing is dangerous to use when cars, delivery vans are parked in the no stopping zone"

Town Centre Vision

Helensburgh is an inclusive and welcoming village of diverse communities and rich heritage set within an extraordinary natural landscape.

At its heart is a thriving main street which is connected to world-class local destinations, and alive with people and possibilities.

Unique, vibrant and community focussed

The town centre is safe and welcoming, embracing people from all walks of life. There is a strong sense of community with connections to a range of amazing natural assets and local attractions.

Our Indigenous, natural, service and mining heritage is celebrated. This heritage is interpreted and expressed in buildings, materials, landscaping, events and activities.

The historic village streetscape has a unique character which is preserved, and important views to surrounding areas are enhanced.

Thriving, sustainable and creative

The town centre is alive with people, offering a diversity of options for work, play, recreation, shopping, education, entertainment and community life.

Leveraging off the surrounding attractions and destinations, a diverse offer provides services for visitors and opportunities for growth.

A community hub supports the daily needs of the wider community, as it grows and changes. Our town celebrates its diversity and embraces the opportunities of a village-like setting.

An emerging gig and entrepreneurial economy is thriving. Sensitivity to the unique natural environment is reflected in sustainable technology and design features.

Active, natural and healthy

There are quality, active connections between the welcoming, pedestrian-friendly town centre and the parks, natural areas, sports fields, playgrounds, schools, transport nodes and other community places.

Recreation, sporting, natural and play spaces support a healthy active community. There is pride in being able to lose yourself in nature, and in sharing these experiences with visitors.

Appropriate infrastructure such as water bubblers, canopy trees for shade along key pathways, and bike racks support a healthy lifestyle.

Connected, accessible and pedestrian-friendly

People of all ages and abilities can move freely and easily around Helensburgh, accessing everything that this unique area has to offer.

Walking and cycling between places is easy. There are well developed pedestrian, cycling and public transport connections.

Traffic in the town centre is calm, and it is easy to park and walk where you need to go via a defined network of accessible paths of travel.

Strategies to deliver the Vision

A series of Strategies have been defined which seek to deliver change in the Town Centre aligned with the Community's vision. These strategies, as listed below are outlined in the next section.

- Updates to Planning and Policy
- Enhancing Connections
- Integrated Community Hub
- Mainstreet Renewal

Updates to Planning and Policy

A renewed focus on character and identity to reflect Helensburgh's unique attributes will be reflected in planning policy, setting the tone for future development. Through the Development Control Plan, ensure building controls promote interesting shopfronts and high quality designs at the right scale for

A redefined the town centre boundary

The boundary of the town centre is redefined to focus on Walker and Parkes Streets. Stuart Street is transitioned to residential development in recognition of its established land use character.

The southern end of the Town Centre is contracted, transitioned to medium density residential. This will reduce the extent of the mainstreet to a more walkable catchment.

Building height and floor space controls transition to that of the surrounding R3 lands (0.5:1 and 11m height). To the north, the boundary is extended to include the Helensburgh Hotel (previously Centennial Hotel). The existing height and floor space should remain unchanged.

A renewed focus on character

Planning controls promote high quality design outcomes that contribute to what makes Helensburgh unique.

A revised Character Statement outlines how unique elements of Helensburgh's character should be retained including key views, how the natural and cultural heritage will be celebrated and the Architectural styles, materials and colours that reflect local place.

Heritage buildings and places are clearly identified and documented to inform the look and feel of Helensburgh town centre.

Improved building interface with the street

All buildings along Walker Street and Parkes Street within the Town Centre boundary are required to have well maintained and attractive shopfronts that connect with, and activate the street.

The lands owned by State and local governments are identified as an opportunity precinct. These lands require planning controls to be reviewed as part of built form review.

Promote diversity in land uses with the introduction of co-working spaces to engage the local workforce within the town centre.



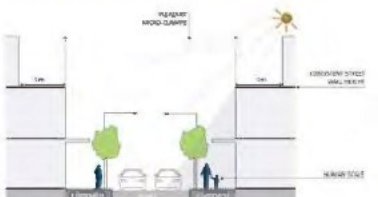
Well designed and proportioned medium density residential supports town centre



Views along Short Street to nature are preserved



Interesting facades open up and activate Walker Street



Shop top housing is well proportioned and set back to let sun into the street



Outdoor dining and an evening economy is encouraged



Buildings in the mainstreet reflect Helensburgh's preferred scale and character



A redefined town centre and opportunity site

Key:

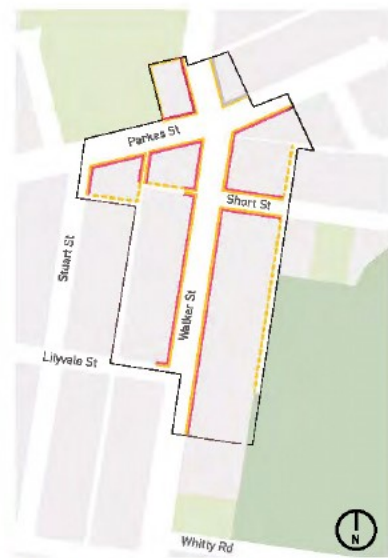
- Transition to Residential (R3 Medium Density Residential)
- Key opportunity site
- Include Helensburgh Pub in town centre (B2 Local Centre)



Key views protected

Key:

- A View east along Parkes Street to bushland
- B View east down Lane 10 and Short Street to surrounding bushland
- C View east through pedestrian link to surrounding bushland
- D View north to historic Helensburgh Hotel



Active Frontages and defined street wall

Key:

- Active frontage - 2 storey street wall with 4m setback above
- Active laneways

Recommendations

- Update Wollongong LEP 2009 to reflect changes to zoning boundaries
- Update Wollongong DCP 2009 Chapter B4 Development in Business Zones to set renewed planning controls for Helensburgh Town Centre, and progress the completion of the draft Helensburgh Character Statement for integration into the Wollongong DCP

Enhancing Connections

Helensburgh town centre is connected to its natural surroundings, linking to natural areas, surrounding destinations and is accessible by foot, bicycle and car. Wayfinding is clear and easy and it is a comfortable place to walk around.

Leverage and connect to surrounding destinations

The town centre remains at the heart of local destinations, connected via walk/ cycle loop trail.

The pilgrimage trail from the train station to the Temple is defined and celebrated as a point of cultural difference.

The pedestrian environment between key destinations is upgraded with renewed lighting, greening, wayfinding and seating to invite people to experience all of what Helensburgh has to offer as a whole destination.

Major roads, including Parkes Street, integrate purpose designed pedestrian crossings to offer safe pedestrian connections.



Wayfinding signage helps people get around and link the town centre with key destinations



A series of open spaces link people through the town centre

Creating an important sense of arrival

Vehicle entry points are defined on Parkes Street and Walker Street. These entry points are narrowed to slow vehicle and provide a sense of arrival.

Native vegetation is characteristic of the town centre and denotes entry points.

Accessible paths of travel link Charles Harper Park and town centre parking areas to Walker and Parkes Streets, making it easier to move around and connect within the town centre.

An active laneway network connect people with safe pedestrian links through the town centre to surrounding residential streets. Long term, Walker Lane and Gibbons Lane are constructed as shared zones and building facades are activated.



Landscaped blisters signal town centre entry points



Renewed lighting invites evening activity

Public space is celebrated and highly active

Artistic, playful and enterprising strategies program the use of public spaces, including the formalisation of a generic Development Application for markets and events in Charles Harper Park.

The town centre will connect via linked public spaces to regional sporting and recreational facilities in Rex Jackson Park. Connected visually with improved sight lines from the town centre, and an enhanced interface of Charles Harper Park to Parkes Street.

Public art is visible in the Town Centre. Existing public art is appreciated and new art introduced to represent the local community and history of the area.



Charles Harper Park continues to support community events and markets



Laneways are active and are pleasant places to walk

Acknowledgement of Country

Helensburgh and its surrounding lands are the northern gateway of the Illawarra. This presents an opportunity work in partnership with First Nations people to understand how they would like to represent an Acknowledgement of Country.

The role of the project would be to acknowledge, and pay respect, to the Traditional Owners of the land, and the ongoing custodians of the land - the Aboriginal and Torres Strait Islander people.



Laneways are active and new pedestrian links make it easier to connect within the Town Centre, to Station and to the Temple.

Key:

- Potential shared zones
- Retain existing pedestrian connections
- Proposed pedestrian connections
- Connection from Station to Temple



Open spaces connect through the town centre with pockets of space to stop and spend time

Key:

- Trees
- Green Spaces

Recommendations

- Lodge a generic Events Development Application for Rex Jackson Park
- Provide opportunities for Markets in Helensburgh Town Centre and surrounding open spaces.
- Helensburgh Key Walking Connections Project - Design concept for cycle/walking loop trail aligned with Urban Greening
- Develop and deliver a wayfinding Concept and Strategy including important links e.g. Temple to Station Trail Project
- Helensburgh Public Art Project

Integrated Community Hub

An integrated community centre and library facility will be at the heart of the community. This purpose built facility will be a centrepiece of the town, delivering a high quality, welcoming place. Council is investigating several site options for the new community facility; however further investigations are required to ensure feasibility.

A centrepiece of design

The integrated community centre and library will contribute to the quality and character of the town centre by showcasing design excellence.

The building will activate the town with a strong interface and interesting facade. The design will respond to the natural setting, connect to views and will integrate landscaping.

With a range of formal and informal meeting spaces to cater for a range of services and activities and a state of the art library, the new facility will be the heart of the community where people will come to meet, create, learn and spend time.

The facility will be multi-purpose and accessible, showcasing sustainable technologies.

Programming is community-focused, and engaging for all.



Example of an historic shopfront presented to the street: Junee Library

An integrated approach

A major investment in the town, the design of the integrated facility will extend to the surrounding pedestrian and road network.

A focal point for enhancing connections. Inclusive, inviting and accessible to provide a diverse range of experiences for all of the community.

Integrated into the landscape, strategically planned to enhance the functional relationships and character of the site's surrounding features.



Opportunity for community space extending from the library/community centre

Key site opportunity

Once retired, the current community centre will be decommissioned.

A master plan will be undertaken to test appropriate development opportunities for the site and adjacent lands owned by State and local government.

The master plan will test development capacity and public domain needs of the lands to provide direction for appropriate planning controls, and public space outcomes.

This work would be subject to ongoing consultation with the community.



Community facilities connecting with nature



Example of a library in a native setting: Fitzgibbon Library, QLD

Guiding Principles for a new facility

Artist sketch above shows an integrated Community Centre & Library.

An integrated Community Centre & Library will provide a hub which attracts and connects people to this place and each other. Reflecting the community and environment providing opportunities for socialising, learning, exploring and relaxing.

Principles guiding the future of the Integrated Community Centre & Library include:

1. Integrates into the landscape to provide opportunities for generations to come
2. Well-connected and accessible, with integrated parking
3. Community heart - integrating community meeting place
4. Exceptional design, unique to Helensburgh
5. Programmed to bring life to the town centre - day and night



Sketch : An integrated community facility to service diverse community needs

Recommendations

- Site options investigation for an Integrated Community Facility (community centre/ library)
- Key site urban design masterplan

Main Street Renewal

Walker Street is Helensburgh's vibrant main street which is accessible and convenient for all. It invites people to visit and spend time. Street renewal will enhance the daily lived experience of residents, create a unique destination for visitors, and provide a catalyst for activation and investment.

A vibrant main street experience

Helensburgh delivers a welcoming, vibrant town centre experience.

Encourage land owners to activate vacant tenancies and invest in the upgrade of shopfronts to reflect the character of the local area. Shops are filled with interesting businesses, shops and community uses.

Outdoor dining is encouraged, and interesting ways to open up facades to better interact with the street is evident.

Evening ambiance is enhanced with curated lighting of the town centre. Deliver a lighting strategy that considers street lighting (including lanes), lighting of historic trees.

Connection with Charles Harper Park

Charles Harper Park continues to be an important place of recreation and leisure for the community, with the playground and the community pool continuing to be important meeting places.

Charles Harper Park supports community events and markets which brings diversity in what happens locally.

All edges of the park will be enhanced to improve visual and physical connections. Buildings facing onto the Park add visual interest and interface with the Park.

The interface of Charles Harper Park to Parkes Street is improved with the car parking edge transitioning to a place for people. In the meantime, car parking will be retained, realigned to improve pedestrian access and views into the Park itself.

Enhanced meeting places

Dispersed along the main street are a series of formal and informal spaces which invite people to relax, take in the ambiance, or meet friends.

Walker Street (west-bank) transforms to a lively, natural place where landscaping responds to the natural setting in species and materials palette. An interactive landscape to sit and eat lunch, or to capture the imagination of young children as they walk north to the playground in the Park.

Lighting is used to create atmosphere and increase visual prominence of public spaces and heritage buildings like the Helensburgh Pub.



Sketch : Artists impression of a renewed Walker Street

Accessible and pedestrian friendly main street

Accessible paths of travel link the east and west side of Walker Street, and connect people to important community services.

A lower footpath is introduced along the western side of Walker Street to provide an accessible and safe walking path.

Parkes Street has new pedestrian crossing points to allow people to comfortably cross the road.

Walker and Parkes Streets are narrowed, and traffic calming elements introduced will create a low vehicle speed to enhance pedestrian and cycle safety.

History is celebrated in the mainstreet

The design of the mainstreet renewal is an opportunity to connect with Country, and to develop ways to share knowledge about local Aboriginal culture. Working in partnership with the Traditional Custodians of the Land, determine how Aboriginal Heritage is identified and represented.

The story of the Memorial Trees along Walker Street are celebrated through heritage interpretive art and signage.

The street is designed and renewed around the opportunity and character the trees bring to place.

Conserve and celebrate the historic main street character, with Helensburgh Hotel and the post office remaining significant historic buildings.



A place for co-working space

Helensburgh is a place of innovation and business.

Co-working spaces are evident along the main street, bringing more energy and innovation to Helensburgh.

This new innovation in the main street provides an opportunity for people to stay local, and spend time immersed in their community rather than commuting to Sydney.

Recommendations

- Undertake main street renewal detailed design + construction as per the endorsed Streetscape Masterplan
- Deliver a heritage interpretation project aligned with streetscape upgrades
- Undertake a laneway activation project to enhance safety and usability of laneways
- Enhance bus stops with a focus to improve active transport

Streetscape Masterplan

The Streetscape Masterplan is a more detailed design response to demonstrate how Walker Street can be designed to support its role as a vibrant main street which is accessible and convenient for all.

Objectives of the Masterplan

- Enhance connection both physical and visual to the natural environment and the unique character of Helensburgh
- Embrace and celebrate village-like setting by the introduction of human scale landscaping features to create a greater sense of place
- Establish equal access pathways and a pedestrian friendly environment
- Create a low vehicle speed environment to increase pedestrian and cycle safety
- Increase shade along key pathways by planting additional canopy trees
- Enhance pedestrian connections and experience through the development of the existing laneways
- Express the indigenous, natural, service and mining heritage of Helensburgh throughout the landscape and by incorporating public art and interpretive devices
- Acknowledge the Memorial planting of Brush Box on Walker and Parkes Streets through interpretive signage
- Provide improved infrastructure like recycling bins, seating, bike racks and bubblers



Street furniture to reflect village charm



Native planting brings in surrounding nature and connects with Country



Improved public transport facilities (bus)



Walker and Gibbons Lanes formalised, with active edges and shared



Wayfinding connects us to place and Country



Heritage interpretation - Aboriginal and European history is celebrated



Create a series of town squares; places to spend time and enjoy the Town Centre



Wide footpaths have high quality paving and allow space for outdoor dining



Lighting is a feature and creates ambience

Draft Streetscape Masterplan

Parkes / Walker intersection is upgraded to allow safe pedestrian crossing points

Upgraded pedestrian crossing point

Improved interface with Charles Harper Park

Parkes Street narrows to signal entry to town centre

Accessible ramp connects street level footpath to shop level

Pedestrian crossing retained with new access stairs and seating area

Lanes upgraded as shared zones

Trees retained - canopy lifted and native planting introduced

Parking lanes retained

New footpath at street level

Road narrows to signal entry to town centre and upgraded pedestrian crossing point



LEGEND

	UPGRADED PEDESTRIAN PATHWAYS		PROPOSED ACCENT AND SHADE TREE PLANTING
	OUTDOOR COMMUNITY SPACES		UNDERSTOREY OF FLOWERING SHRUBS
	EXISTING TREES TO BE RETAINED		POTENTIAL SHARED ZONE

Priority Staging

Delivering the streetscape masterplan will be staged. There is an initial budget set aside (\$1.8M), through Council budget and the State Government's Resources for Regions Grant.

A staging plan has been developed, based on clear objectives to ensure best outcomes can be delivered aligned with the community's Vision. Priorities have been identified and mapped for transparency. Over time, and as funding is made available, ongoing investment can be managed in a cohesive way, in accordance with the streetscape design.

Staging objectives

Staging priorities have been considered having regard to:

1. Alignment with the streetscape master plan objectives: pedestrian accessibility, comfort and safety
2. Value for money: ensuring that infrastructure delivery is coordinated. What infrastructure works will bring the highest impact to how the main street functions.
3. Design functionality: ensuring that infrastructure elements are delivered in a coordinated way. Sometimes there are partnering elements.
4. Relationship with future investment programs: ensuring that works don't become redundant.

Parkes Street



Stage 1 construction priorities for Parkes Street

Prioritise safe crossing opportunities on Parkes Street

The urgent priority here is to create a safe environment for pedestrians to connect between the main street and the services and amenities to the north.

Design is intended to slow and calm traffic, ensure pedestrian safety, improve accessibility, and to instil within this intersection a sense of arrival and entry to the town centre.

Key elements:

- Upgraded accessible crossings at all four edges of the roundabout with kerb ramps, wider refuge islands and footpaths at each corner
- Pedestrian accessibility and safety achieved through upgraded pedestrian crossing on Parkes Street to link to Charles Harper Park and shops to the north
- Road narrowing to create a slower speed environment and improved safety with blisters introduced at the western entry/exit
- Improved bus stop safety and upgraded shelter

Walker Street



Stage 2 construction priorities for Walker Street

Prioritise accessibility and main street experience along Walker Street

The overall intention for stage one works is to achieve maximum advantage from the beautifully proportioned main street by strategically investing in high impact changes.

New pathways and crossings will focus on improving the overall structure and functionality of the streetscape, with a focus on accessibility and safety. Existing footpaths will be maintained and repaired as required.

Upgrading the Lilyvale and Walker Street intersection and connections to pathways will be considered as part of the integrated design investigation for the new library and community centre.

Key elements:

- New continuous accessible pathway along the Western edge at street level
- Traffic calming at southern entry with blisters narrowing the road, and creating a pedestrian crossing point
- Improved connections to western shops from Walker Street (road level) with two new accessible ramps and one new set of feature stairs
- Improve safety with upgraded crossing facilities in Walker Street and Short Street
- Two newly defined public meeting places - outside Coles with a widened footpath area, and at the top of the new stairs linking with the western bank shopping area
- Tree canopy lifted and native landscaping featured along the western bank. Enhanced character through an interpretation project for the memorial trees, native landscaping, and expressing the heritage sandstone curb where possible
- Include lighting conduits, seating and human scale elements

Conclusion

Helensburgh Town Centre is a beautiful place, set in a tranquil setting, and supported by a committed and passionate community.

This Plan sets the Community's Vision for Helensburgh Town Centre, defining a series of Strategies and Actions to deliver the Vision.

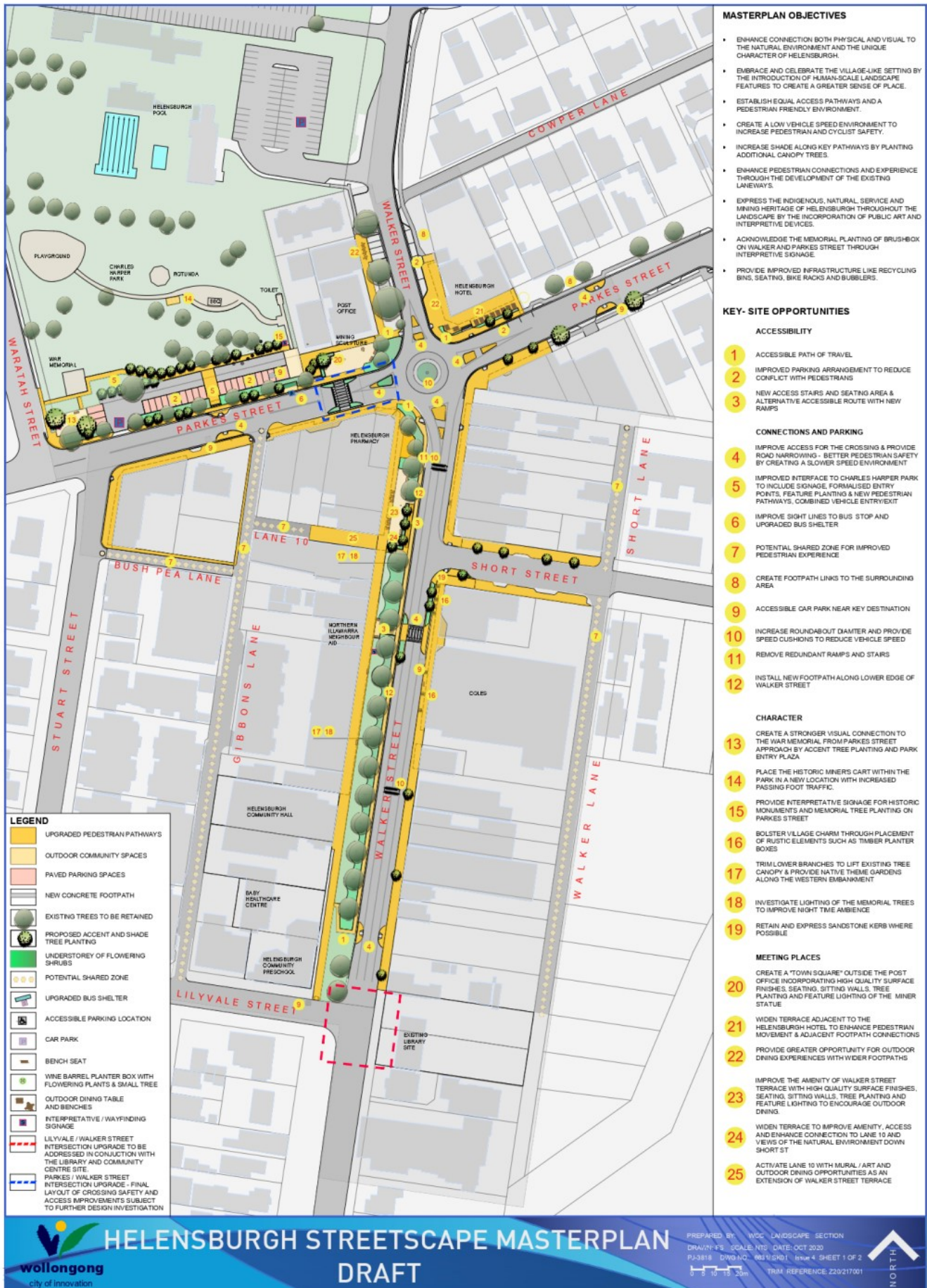
Turning ideas into action requires collaboration and partnership between Council, the community, local business and Government Agencies. The accompanying Implementation Plan sets out the key actions for delivery against timeframes and budget needs.

Council would like to thank everyone who has contributed to informing this Plan. From early analysis through to the wording of the Vision and setting of priority actions. Only together can we successfully deliver change.











RAIN GARDENS AND FEATURE TREES



NEUTRAL TONE SEGMENTAL PAVING



PUBLIC ART FOR HERITAGE INTERPRETATION



TIMBER TREE PLANTERS



PUBLIC ART, SEATING AND
FLOWERING PLANTS



COUNTRY CHARM



SANDSTONE SITTING WALLS - EXAMPLE FOR OUTDOOR
COMMUNITY SPACES



NATIVE GARDENS



GYMEA LILLIES AS ACCENT PLANTING



NATIVE FLOWERS



UPGRADED BUS SHELTER



WAYFINDING SIGNAGE

DRAFT - HELENSBURGH TOWN CENTRE IMPLEMENTATION PLAN, 13 October 2020

[illegible]

3.1		Site options investigation for an Integrated Community Facility (community centre/library)	A large number of sites across the Halesburg Town Centre has been investigated for suitability against social infrastructure planning criteria and local planning requirements, as well as in relationship to the strategic outcomes of the Halesburg Town Centre Plan.	Site investigation works will continue to be undertaken to determine an appropriate site for this project.	H	UG	BL & CF	Libraries and Community Facilities	Wallingang City Council		\$55	\$18M	Funded (Partially)	M	X	X	X	X	✓	X								
3.2		Key site urban design masterplan	The lands owned by State and local governments are identified as an opportunity project. These lands require planning controls to be reviewed as part of built form review. The consideration of these extends to Ulysses Street intersection.	Following determination of a preferred site for the new Integrated Community Facility (centre/library), undertake planning and design study to develop clear options for decommissioned community facility sites.	L	GT	LUP	URD	Wallingang City Council		\$5	\$3000	Unfunded	L	X	X	X	X	X	✓								
4		Main Street Renewal																										
4.1		Mainstreet Renewal Detailed Design + Construction, Walker Street	Walker Lane needs to be integrated into the detailed design, even though it might not be funded to deliver. We need to have the design placed ready in case we get a new C/D developer etc.	Aligned with the principles of the Town Centre Plan, and in consultation with the community, progress with a detailed design for Walker Street and Perles Street, as the main streets of Halesburg.	H	GP	GL Asset Management	Design and Technical Services + City Works	Wallingang City Council	Community LUP	\$5	\$1.1M	Funded with support from Grant	S	✓	✓	✓	X	X									
4.1a		Walker and Park Streets intersection upgrade		Investigate the redesign of Walker and Park Street intersection to delivery high amenity pedestrian crossing.	M	GP	Transport Services	Road Safety, Traffic and	Wallingang City Council		\$	\$680,000	Funded with support from Grant	M	X	X	✓	✓	✓									
4.1b		Walker Ulysses intersection upgrade	Upgrades to Walker Lane and the Ulysses-Walker Street intersection to remain aligned with the design concepts for the Mainstreet Renewal and the proposed southern Community Centre/ Library facility (if it is identified on the current library site) and the Streetscape Masterplan design, as it relates to Walker Street.	Upgrades to Ulysses-Walker Street intersection to improve pedestrian and traffic safety and create a southern gateway to the town centre. Detailed design to be completed following resolution of the site selection for the Integrated Community Hub (community centre/library).	M	GP	Transport Services	Design and Technical Services + City Works	Wallingang City Council		\$5	\$265,000	Funded with support from Grant	M	X	X	✓	X	X									
4.2		Heritage Integration Project	General consideration of an Indigenous heritage element if possible. Recognise Memorial trees.	Aligned with the main street renewal, deliver a heritage interpretation project which celebrates important local heritages such as the memorial planting of Brush Box along Walker Street.	M	GT + GP WH	PD + GT	Design and Technical Services	Wallingang City Council		\$	\$50,000	Unfunded	M	X	X	✓	X	X									
4.3		Halesburg Town Centre Laneway activation projects	Lane 30 Gibbons Lane Walker Lane Buch-Pan Lane	Continue to deliver attractive and activated laneways. An innovative project to deliver visible change, to activate the laneways through the introduction of art, colour and light.	H	CCCD	City culture and activation	Community Development	Wallingang City Council		\$	\$15,000	Unfunded	S	✓	X	X	X	X	X								
4.4		Halesburg Active Transport + Public Transport	Would need to check the city wide bus stop list to see if these are already on a list.	Enhance bus stops at key locations along Perles and Walker Street with a focus to improve active transport.	M	GP	Transport Services	Road Safety, Traffic and	Wallingang City Council	RMS, Sydney Trains, SPT	\$5	750	Unfunded	M + L	X	X	X	X	X	✓								
5		Citywide																										
5.1		Urban Greening	Don't need to add to IPR. Need to find the ref link to existing action on IPR, ongoing so per UGS	Plan how to deliver the target for increasing tree canopy cover on priority streets to 20% by 2030. Ensure street upgrades, masterplans etc. deliver on the principles and objectives of the Urban Greening Strategy. Where possible, Urban Greening will be integrated into detailed design and master planning of key areas.	M	CGS	Parks and Open Space	Parks and Open Space	Wallingang City Council	RMS, Land Services	\$5	750	Separately Funded WCC	M	✓	✓	✓	✓	✓	✓								
5.2		Deliver against the Disability Inclusion and Access Plan	Don't need to add to IPR. Need to find the ref link to existing action on IPR.	Deliver accessible footpaths to key community infrastructure, including playgrounds and amenities in alignment with the DAP.	H	GP	Transport Services	Footpaths, Cycle Ways and Transport Nodes	Wallingang City Council	Community	\$		Funded within current program	S	X	✓	✓	X	X	X								
5.3		Halesburg Sustainability	Don't need to add to IPR. Need to find the ref link to existing action on IPR.	Continue to implement a range of environment and sustainability actions for Halesburg guided by LGA wide Environmental strategies including: Environmental Sustainability Strategy 2022 (update pending November 2025) Resource Biodiversity Strategy Resource Equipment Management Plan Climate Change Mitigation Plan	H	GT	Env	Environmental Planning and Strategy	Wallingang City Council	Council, Community, Business, ENR, Key Agencies	\$	750	Part Funded Part Unfunded	Ongoing	✓	✓	✓	✓	✓	✓								

ITEM 4 PUBLIC EXHIBITION - WEST DAPTO SUBURBS REVIEW PROJECT

The aim of the West Dapto Suburbs Review Project is to ensure that the future suburbs in the West Dapto Urban Release Area (WDURA) meet the current and future needs of the community, and The Geographical Name Board's (GNB) criteria. Three suburbs in the WDURA: Kembla Grange, Huntley and Avondale, are very large, reflecting their rural character. We propose to divide them into six (6) new suburbs, shown in Figure 1, to reflect their future urban characters.

In March, the community was invited to suggest names for the six (6) future suburbs, and to provide comment on the relevance of the existing names: Kembla Grange, Huntley and Avondale. The Community Working Groups, established to research and review the nominations with Council's Project Team, created a shortlist of suitable names. The shortlist, and the proposed new suburb's boundaries, have been preliminarily assessed by the GNB.

RECOMMENDATION

Council endorse the proposed shortlist of names, excluding Knapp, as detailed in Figure 2, for public exhibition. The exhibition period will be for six (6) weeks.

REPORT AUTHORISATIONS

Report of: Chris Stewart, Manager City Strategy

Authorised by: Linda Davis, Director Planning + Environment - Future City + Neighbourhoods

ATTACHMENTS

- 1 Longlist of names as suggested by the community
- 2 West Dapto Suburb Naming Project Draft Shortlist - Research
- 3 Communication and Engagement Strategy West Dapto Suburb Naming
- 4 Proposed New Suburbs Map showing WLEP Heritage Items
- 5 Geographical Names Board of NSW Policy - Place Naming

BACKGROUND

Since 2010, the WDURA's predominantly rural character began evolving into an urban area that will contain three (3) town centres and five (5) villages. Considering the land use changes occurring, and the existing large suburb sizes, the suburb boundaries need revising so that the future suburb densities align with the GNB guidelines. The GNB is the state body that oversees Place and Road Naming in NSW. Their criteria/guideline for the maximum size of a suburb suggests 500ha. Kembla Grange, Huntley and Avondale are two (2) to three (3) times this size.

The WDURA was always going to need new suburbs to be created. The GNB publication "Process PA3: Address Locality Boundaries - Consideration for Creating a New Address Locality", recommends creating the new, smaller, suburbs before residential densities increase significantly. This is to minimise disruption to residents because new populations, understandably, become attached to the names of their suburbs. The proposed new suburb boundaries align more closely with the GNB's advice. New boundary lines follow existing creek lines that topographically bisect two (2) of the existing suburbs: Dapto Creek in Kembla Grange and Robins Creek in Huntley. A proposed future road, that will be a main north south connection through the release area (and is included in the release area's structure planning), traverses Avondale, and was selected as the most appropriate way to segment the suburb.



We understand the historical significance of Avondale, Kembla Grange and Huntley, and we needed to know if these names remain relevant to the community. The consultation period was extended from four (4) to six (6) weeks, due to the Covid-19 Pandemic. This helped ensure that community had enough time to put forward their suggestions. In the first phase of the project, we –

- Two (2) Community Working Groups, who worked alongside the project team, were created to research, review and provide comment on the naming suggestions (Attachment 1), and to create a shortlist from those names. Working Group Members were sourced from our Aboriginal, Heritage and Dapto communities. All submitted names were researched and assessed for applicability and suitability. Additionally, all Aboriginal naming suggestions have been reviewed by members of our Aboriginal Working Group – as aligns with Council's Draft Aboriginal Engagement Framework. High value was placed on proposed names which represent the local Aboriginal communities. While the names are on exhibition, we will engage an Aboriginal cultural linguist specialist to advise on the correct pronunciation of the Aboriginal names.

The longlist of names (Attachment 1) was sent to the GNB for informal review, along with details relating to the public consultation process on 21 July 2020. The West Dapto Review Committee also reviewed the list on 12 August 2020 (while it was also under consideration by the GNB) and Council's Executive Management Committee on 15 September 2020. The GNB also received a copy of the Engagement Report – Phase 1 (Attachment 3) summarising the public's submissions and shortlisting process, and two (2) maps: the first map showed the affected suburbs – Kembla Grange, Huntley and Avondale, the second map demarcated the new suburbs and their corresponding names.

GNB's Comments

The GNB's preliminary comments on the shortlist provided clarity on the likelihood that some names would be rejected or accepted. The proposed names must meet the GNB's Place Naming Policy criteria (in Attachment 5) to be considered as potential new names. The criteria include considerations of language, form, character and similarity to any existing suburb name nationally. Duplication is not permitted. Duplication with a suffix, such as Huntley North, will not be supported either. GNB officers advised that the first seven (7) letters of a name are critical to the efficiency, for instance, of call centre operators. This is to ensure that Emergency Services Operators can distinguish between suburbs quickly and easily. For the same reason, names that sound similar to other names: even if the spelling is different (homophones), are unlikely to be supported.

The existing names, Kembla Grange, Huntley and Avondale were not on the shortlist as the GNB has previously advised that they are acceptable.

Table 1: Comments from the Geographical Names Board

SHORTLIST		
Proposed Name	Similar Sounding Locations	Comments
Knapp		After a preliminary check no issues identified
Murrindarry (<i>Mount Marshall, The Mountain*</i>)		After a preliminary check no issues identified
Woolangurry (<i>track over the mountain</i>)		After a preliminary check no issues identified
Lindsay	Mount Lindsey in NSW (Wingecarribee LGA) Mount Lindesay in QLD	Further advice from Board would be needed
Logbridge	Lockridge in WA	Further advice from Board would be needed
McPhail	McKail in WA	Further advice from Board would be needed
Edenfield	Eaglefield in QLD Edenville in NSW	Potential to be rejected by the Board based on duplication.
Guwara (<i>High Winds</i>)	Goowarra in QLD	Potential to be rejected by the Board based on duplication
Murringah (<i>coal</i>)	Murrigal in QLD Murringa in NSW (Young LGA)	Potential to be rejected by the Board based on duplication
Newton Park	Newton Boyd in NSW (Clarence Valley LGA)	Potential to be rejected by the Board based on duplication. Note: Policy states that for new suburbs it is not permissible to add a word to an existing NSW address locality. Eg, Hotham Heights would not be acceptable if there is an existing locality of Hotham. (Section 6.8.3 - NSW Address Policy and User Manual)

Osborne	Osborne in QLD (Burdekin Shire) Osborne in NSW (Lockhart LGA)	Most likely rejected based on direct duplication in QLD
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*Italicised words reflect the definition

Research, by Council staff and the Working Groups into all names, has been thorough and objective. Using the information provided by the GNB in Table 1, and follow up discussions based on our research with the GNB, we have concluded the following –

- Of the 11 shortlisted names, three (3) have no preliminary issues; Knapp, Murrindarry and Woolangurry. Knapp, however, is not supported as a suburb name. Knapp was originally nominated by a community member as Knapp was one (1) of the original surveyors of land in West Dapto. The GNB suggested that the potential mis-spelling of Knapp, due to the silent 'k', is a pertinent concern. Also, based on Council staff and Community Working Group member research, there are names on the list, that are considered to have a stronger linkage to the West Dapto community and are considered more worthy of veneration. Council is therefore asked to consider endorsing the removal of Knapp from Figure 2 prior to community exhibition.
- Osborne is not recommended for further consideration. It is likely to be rejected as suburbs with the same name exist in South Australia, Queensland and New South Wales. Edenfield, Logbridge, and Newton Park have also not been progressed as they sound very similar to existing localities – see Table 1.
- Guwara (high winds) and Murringah (coal) are supported for progression by the Aboriginal community representatives. Lindsay and McPhail are meaningful to West Dapto but require further consideration and advice from the GNB due to there being similar sounding localities. The Lindsay's are long term residents of West Dapto, with strong family associations in farming and retail. The McPhail family received an original land grant in Kembla Grange, erecting Stan Dyke Homestead and donating land for West Dapto School – current Wollongong Local Environmental Plan Heritage Items. McPhail descendants remain in the area.

PROPOSAL

The proposed names, that are both worthy of recognition and more likely to be supported by the GNB's, are incorporated into Figure 2 below. We have also recommended which of the new suburbs the proposed names are most applicable to. Kembla Grange, Huntley and Avondale are recommended to be retained.

It is recommended that Council endorse the proposed names, excluding Knapp, as detailed in Figure 2, for public exhibition. The exhibition period will be for six (6) weeks.



Figure 2: Potential names for new suburbs

CONSULTATION AND COMMUNICATION

Consultation for Phase 1 of the project commenced in March 2020 and ended on 20 April 2020. The Engagement Report is at Attachment 3. The purpose of Phase 1 was to –

- Inform the community of the suburb boundary reviews.
- Invite the community to suggest names for the new suburbs.
- Establish two (2) Community Working Groups to review and assess the suggested names against the GNB criteria and any significance to West Dapto.
- Use the information collected by the Community Working Groups and Project Team to develop a draft shortlist of names for each of the new suburbs.
 - Draft shortlist to go to GNB for review

Phase 2

- Final shortlist presented to Council for endorsement.
- If supported by Council, the following engagement steps will be undertaken –
 - Public Exhibition of the final short list for six (6) weeks.
 - Ward 3 Review Committee to make final recommendation about proposed suburb names.
 - Proposed final names and boundaries submitted to GNB for review.
 - GNB exhibit new suburb names.
 - GNB to inform Council of new suburb names.
 - Council endorse new suburb names.

We expect the new names to be effective by mid-2021.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 Goal “We have an innovative and sustainable economy” “We are connected and engaged community”. It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
2.1.5 West Dapto urban growth is effectively managed to balance employment and population growth.	2.1.5.1 In collaboration with key agencies, facilitate the West Dapto Taskforce to deliver the first stages of the West Dapto Urban Release Area	In collaboration with key agencies, facilitate the West Dapto Review Committee to deliver the first stages of the West Dapto Urban Release Area

SUSTAINABILITY IMPLICATIONS

The review of suburbs at West Dapto does not have any specific environmental sustainability implications. However, the existing size and scale of the Kembla Grange, Huntley and Avondale suburbs is not sustainable in an ultimate urban development context. In addition, once the three (3) existing suburbs reach their ultimate urban residential density capacity it would not be sustainable for emergency services coordination. Therefore, the proposed six (6) suburbs will provide a more sustainable outcome for the community and public resources.

RISK MANAGEMENT

As mentioned previously in this report the GNB guidelines recommend creating new, smaller, suburbs before residential densities increase significantly in an urban context. One of the main risks with reviewing suburb boundaries later in a new release area development life is the increased disruption and inconvenience to residents with address change. Council staff are managing this risk by undertaking this suburbs review project relatively early in the life of the urban release area at West Dapto.

FINANCIAL IMPLICATIONS

There are minimal financial implications of the proposed public exhibition West Dapto Suburbs review project. Staff resources in the City Strategy Division and Engagement team have been utilised to undertake the project.

CONCLUSION

The naming of the new suburbs is an important milestone in the West Dapto Urban Release Area's development. The suburbs; Kembla Grange, Huntley and Avondale are larger than the GNB's guidelines recommend. Their guidelines suggest creating new, smaller, suburbs before the residential densities increase significantly, to minimise disruption to residents in the future. This report outlines the community engagement processes undertaken in the naming of the new suburbs. Council staff worked alongside the two Community Working Groups, to collate and research all suggested names against the GNB's naming criteria, as well as their applicability and suitability for the West Dapto area. An Aboriginal cultural linguist will be engaged to advise the correct pronunciation of the Aboriginal names in the final shortlist.

Figure 2 illustrates the relevant naming options that are supported for each new suburb. It is recommended that the final shortlist of names (Figure 2) be endorsed for exhibition, excluding Knapp, to allow community consultation on the proposed names for each new suburb.

West Dapto Suburb Naming Project

The names in the table below were suggested by community members through our community engagement from March to April 2020. Our two community working groups have reviewed and assessed each name with summary comments below.

Name	Shortlist Y/N	Comments
Alderaan	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Aquila	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Arkell	No	Not significant to West Dapto. Sensitive nature of name. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Avondale	Yes	Historical significance and already being used.
Bamal (Earth)	No	No specific relevance to West Dapto. More significant names to use.
Beachrow	No	No significance to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Beachwood	No	Not significant to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Bentley	No	Duplicate. Suburb in WA postcode 6102. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Bespin	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Bong Bong	No	Duplicate. The name 'Bong Bong' is already extensively identified in the Wingecarribee LGA
Burelli (Mountain)	No	Already used as a street in Wollongong
Camayragal	No	Not Dharawal language
Coleridge	No	Not significant to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Cowley	No	Duplicate name suburb in QLD postcode 4871 Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Dhargarigal	No	Not Dharawal language
Dombarton	No	Duplicate. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3
Dreamtime	No	While this word is significant to Aboriginal culture, it is not specific to West Dapto.
Dunlop Vale	No	Continuation of 'Kembla Grange' more significant.
Edenfield	Yes	Historic property in West Dapto

Name	Shortlist Y/N	Comments
Farmborough Valley	No	Duplicate under GNB guidelines
Farmbrowarra	No	Duplicate under GNB guidelines
Fibin	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Flame Tree	No	Community working group decision to not use flora or fauna.
Flametree	No	Community working group decision to not use flora or fauna.
Frisia Hill	No	No great significance to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Gardner	No	Living person Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point c
Gerrigulli	No	Already commemorated in a park
Gerringulli	No	Already commemorated in a park.
Glengarry	No	Duplicate. Suburb in Victoria postcode 3854. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Glenlee	No	Duplicate. Rural locality in QLD postcode 4711. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Goon	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point f
Gooragle (Possum)	No	Other significant names to use.
Gunston	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Guwara (High wind)	Yes	Relevant to suburbs along the escarpment near Wongawilli
Gweagal	No	Not Dharawal language
Gweagal	No	Not Dharawal language
Haynesridge	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point c
Hazelton	No	Hazelton family has greater connections with Albion Park/Shellharbour LGA.
Hillsborough	No	Duplicate. Suburb in NSW postcode 2290 Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Hoth	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Huntley	Yes	Relates to Huntley mine. Already being used.

Name	Shortlist Y/N	Comments
Jenkins	No	Not relevant to West Dapto or Aboriginal culture. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Jenkins	No	No significance to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Johnson Park	No	Significant person in history. Feedback from Aboriginal community preferred to not use names of people.
Johnston	No	Other more prominent historical figures
Jumbullah	No	Requested not to use
Jum-Bullah	No	Requested to not use
Kembla Grange	Yes	Historical significance and already being used.
Kembla Heights	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Kembla Heights	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Kembla Plains	No	Duplicate. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Kembla Plains	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Kembla Vale	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Kembla Valley	No	Duplicate. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Kembla View	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Kemblavale	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Knapp	Yes	
Konmaru-Nula	No	Not relevant to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Koradji	No	Not significant to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Lambert	No	Living person. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point c
Lang	No	Other more prominent historical figures

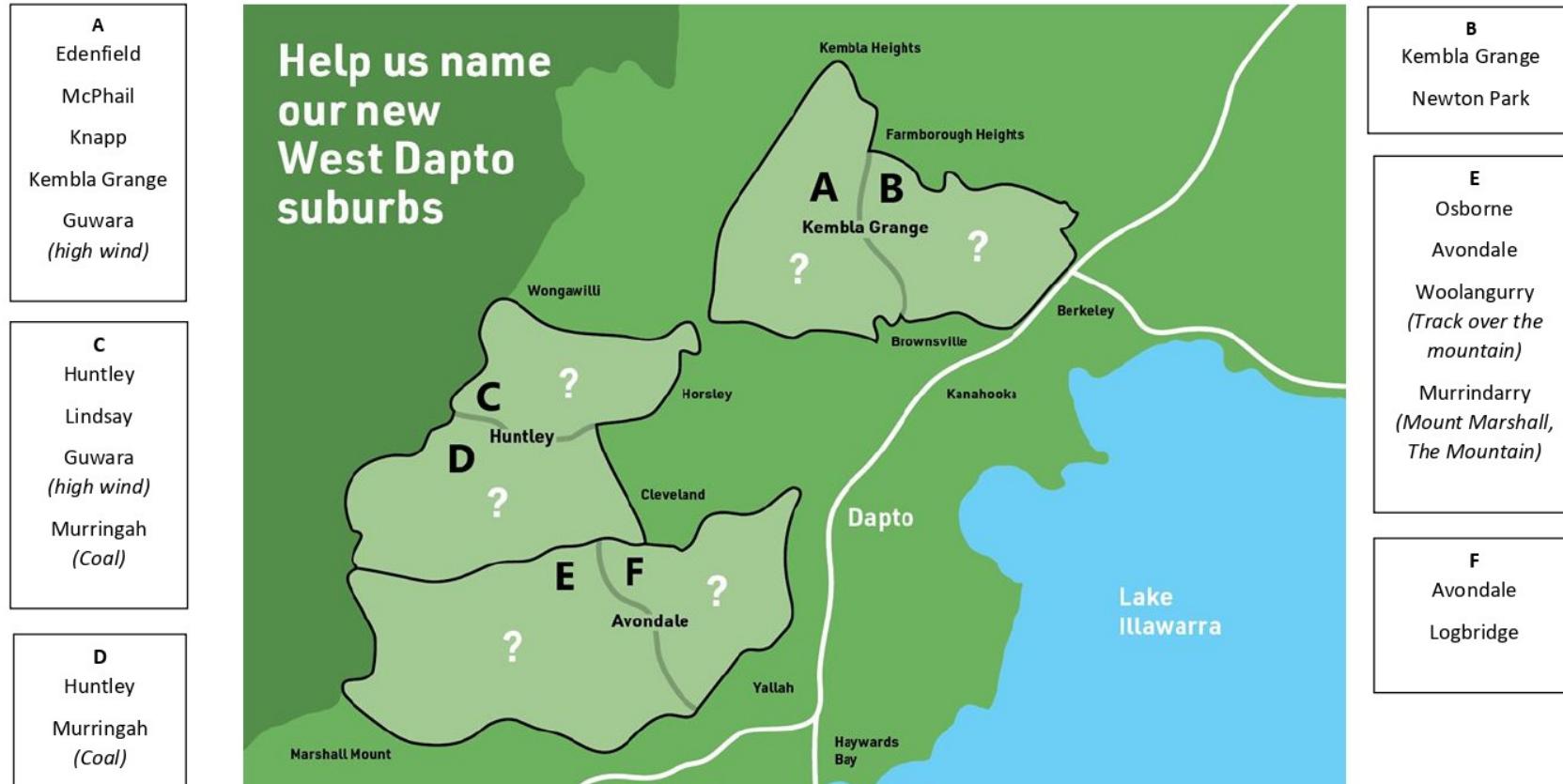
Name	Shortlist Y/N	Comments
Lilly Pilly	No	Community working group decision to not use flora or fauna.
Lindsay	No	Not relevant to West Dapto or Aboriginal culture. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Lindsay	Yes	The Lindsay's are long term residents, landowners and business owners in the West Dapto area.
LLamuna	No	No significance to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Logbridge	Yes	Historical property in West Dapto
Lower Dombarton	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point g
Malvale	No	Community working group decision to not use flora or fauna.
Marshall	No	Name already commemorated
McPhail	Yes	Family with long connections with Dapto/West Dapto
Melrose Park	No	Duplicate. Suburb in Sydney postcode 2114. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Minersville	No	More historically important themes to commemorate.
Mirrigan	No	Means 'dog' - offensive
Mountain View Estate	No	Duplicate. Mountain View – location in NSW 2460. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Mountain View Hills	No	Duplicate. Reserved named Mountain View Park in Albion Park Rail. There is also a locality called Mountain View in the Clarence Valley LGA. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Murrindarry (Mount Marshall-means the mountain)	Yes	Relevant to West Dapto area of Mount Marshall
Murringah (Coal)	Yes	Relevant to West Dapto area of Huntley
Newton Gardens	No	Name not historically correct
Newton Park	Yes	Name of a historic property which has architectural significance
Oaree	No	Not Dharawal language
Osborne	Yes	Significant land owner, long term association with the West Dapto / Avondale / Marshall Mount Area.

Name	Shortlist Y/N	Comments
Quoll	No	Community working group decision to not use flora or fauna.
Rainforest	No	Community working group decision to not use flora or fauna.
Red Cedar	No	Community working group decision to not use flora or fauna.
Ribbonwood	No	Already in use for community centre
Rixon	No	No significance to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Robins Creek	No	No significant connection to Aboriginal culture. Name already commemorated in street name.
Saunders	No	Living person. Living person Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point c
Shaina place	No	Not significant to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Soldier's Rest	No	Names with greater significance to be used.
Sunnyside	No	Duplicate. Suburb in QLD postcode 4737. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
Sunnyvale	No	Not relevant to West Dapto or Aboriginal culture. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Surge	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Tagarigal	No	Not Dharawal language
Tatooine	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Temple District	No	Not significant to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Tharawal	No	Not Dharawal language or significant to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Throsby	No	No great significance to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
West Horsley	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point f

Name	Shortlist Y/N	Comments
Whyllies Flat	No	Duplicate (with different spelling) suburb in NSW 2330. Doesn't meet GNB guidelines . Page 103 Section 6.8.2
William Beach Park	No	No significance to West Dapto. Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e
Wollongolow (Name of the Chief of the Clan groups of the Illawarra)	No	Significant person in history. Feedback from Aboriginal community preferred to not use names of people.
Woolangurry (Avondale-means track over the mountain)	Yes	Relevant to West Dapto area of Avondale
Yavin	No	Doesn't meet GNB guidelines . Page 103-104 Section 6.8.3 point e

West Dapto Suburb Naming Project Draft Shortlist - Research

(proposed for Geographical Names Board consideration)



West Dapto Suburb Naming Project Draft Short List

Research Summaries

Guwara means high wind.

Murrindarry refers to Mount Marshall, The Mountain.

Murringah means coal.

Woolangurry means track over the mountain.

Edenfield

Edenfield is a long-established property on Darkes Rd, Kembla Grange, part of an original land grant from the 1840's to Augustus Hayward. The name stopped being used when street numbering came in. It is located adjacent to the Motoring Museum on Darkes Rd.

Knapp

Edward J H Knapp's only association with West Dapto was as surveyor, tasked with survey of the new 'Veteran's Grants' concept on Dapto Creek in Kembla Grange, in 1829. Research indicates that he was a surveyor only for a few years.

Lindsay

The Lindsay's are long term residents of West Dapto, with strong family associations in farming and retail. The Lindsay's were Innovative leaders in dairying and made a significant contribution to the farming, dairy, meat and retail sectors.

John Lindsay is associated with Horsley Homestead and the evolution of agriculture, dairying, meat and retail sectors in Illawarra and Dapto specifically. The son George developed an award winning Ayreshire herd and opened a small store in Unanderra (then called Charcoal). The other son, John, operated an abattoir, dairy and piggery on West Dapto Road north of Robbins Creek.

Existing street names or other Wollongong sites/places linked to the name:

- 1 Lindsay St in Unanderra (unsure if linked – could be after Lindsay Mayne, or George Lindsay [see above]).

Logbridge

Logbridge is named after Log Bridge Farm. William Thomas (a carpenter), arrived in Australia in 1838 and set up the farm at 'Log Bridge' on Marshall Mount Road after first working on the construction of Marshall Mount House, for the Osborne's. William's son Henry lived on the farm until 1920. It was originally called Elm Bank. It was listed a local significant heritage item in WLEP 1990. It has been demolished and is not in WLEP 2009.

McPhail

The McPhail family has a long history with West Dapto. George McPhail received an original land grant in Kembla Grange. Their son George erected Stan Dyke Homestead and donated land for West Dapto School – current WLEP Heritage Items. Family members remain in the area. In the 1950's the CSIRO's Dapto Solar Radio Field Station operated on the McPhail's property.

Existing street names or other Wollongong sites/places linked to the name:

- 1 McPhail Reserve – Corner Paynes & Sheaffes Roads (next to the old school).
McPhail St in Unanderra (unsure if linked)
- 2 Stan Dyke Road in Kembla Grange.
- 3 Stand Dyke Park in Kembla Grange.

Newton Park

Newton Park is the name of an historic property built in 1845, adjacent to 'Kembla Grange Racecourse, for David Williamson Irving who named it after his uncle, Lord Newton from Scotland. The house is listed on the State Inventory Statement of Significance as: "Substantially intact Victorian colonial homestead. Rare example of an asymmetrical house of this type".

Existing street names or other Wollongong sites/places linked to the name:

- 1 Newton Park and Gardens is listed on the WLEP 2009 – Item 5949.

Osborne

Henry Osborne is a significant historical figure-farmer and politician, and the family have long term association with the West Dapto / Avondale / Marshall Mount Area.

He was granted land in Marshall Mount, and Kangaroo Valley. The Osborne's home was 'Marshall Mount House'. He was a pioneer in the dairy industry and the "Osborne Memorial Church at Brownsville, Dapto, was built and endowed by his family," (Lindsay. B., and Organ. M. 1994. Early Land Settlement in Illawarra. UOW).

Existing street names or other Wollongong sites/places linked to the name:

- 1 Osborne Street, Dapto.
- 2 Osborne Park and Street – Cliff Rd Wollongong (possibly named after Henry's brother, John).

OUR WOLLONGONG

JOIN THE CONVERSATION



ENGAGEMENT REPORT

West Dapto Suburb Review & Naming

July 2020

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Executive Summary

In March 2020, Council informed the community of suburb boundary reviews in West Dapto and invited the community to suggest names for the new suburbs. Due to urban release in West Dapto and the size of suburbs becoming too large, the existing suburbs of Avondale, Huntley and Kembla Grange need to be split in half. This project is being run in two phases:

Phase 1

- Inform the community of the suburb boundary reviews.
- Invite the community to suggest names for the new suburbs.
- Establish two (2) Community Working Groups to review and assess the suggested names against the GNB criteria and any significance to West Dapto.
- Use the information collected by the Community Working Groups and Project Team to develop a draft shortlist of names for each of the new suburbs.
 - Draft shortlist to go to GNB for review

Phase 2

- Final shortlist presented to Council for endorsement.
- If supported by Council, the following engagement steps will be undertaken –
 - Public Exhibition of the final short list for six (6) weeks.
 - Ward 3 Review Committee to make final recommendation about proposed suburb names.
 - Proposed final names and boundaries submitted to GNB for review.
 - GNB exhibit new suburb names.
 - GNB to inform Council of new suburb names.
 - Council endorse new suburb names.

This engagement report is a summary of the engagement process of phase 1 of this project.

Engagement started with a survey asking the community for suggestions for names for the new suburbs and checking the relevance of the existing names of Avondale, Huntley and Kembla Grange. The survey was available online through Engagement HQ, as well as hardcopy surveys at Customer Service and Dapto and Wollongong Libraries. The engagement period was initially from the 9 March to 6 April 2020, however extended to 20 April due to the Covid-19 pandemic. During the engagement a number of methods were used to communicate with the community including; an engagement website, two information stands in Dapto, presentations to the Wollongong Aboriginal Reference Group and Neighbourhood Forum 8, postcards distributed to Dapto and West Dapto businesses, a media release, social media posts and posters in local Dapto primary and high schools. Two Frequently Asked Questions documents were developed. The first

provided information about the suburb boundary review, while the second provided information about the suburb naming process and the current community engagement.

The Engagement HQ website had 747 visits, with 60 online submissions. Seven submissions were received via email and letter and 13 through social media.

Once the broad community engagement period ended, two community working groups were set up to review and assess all names against the Geographical Naming Board criteria and significance to West Dapto. Due to safety restrictions with Covid-19, the working groups were run online, through email and phone communication. The shortlist can be viewed in Appendix 1. Several Aboriginal and non-Aboriginal names were suggested for the six suburbs as well as the existing names of Avondale, Huntley and Kembla Grange. The names will now be initially reviewed by the GNB, with the final proposal to be made by the Ward 3 Review Committee.

Background

West Dapto Urban Release Area is located in the Illawarra region, nestled between the Illawarra escarpment to the west and existing settlements of Dapto, Kanahooka, Koonawarra and Lake Illawarra to the east. Wollongong City Council estimates that the West Dapto Urban Release Area will provide around 19,500 dwellings once fully developed, which would occur over a 40-year timeframe with an additional population of around 56,500 people.

With urban growth in this area, suburb sizes are now larger than the 500ha that the Geographical Names Board of NSW (GNB) recommends. This project involved splitting the suburbs of Kembla Grange, Huntley and Avondale each into two suburbs and asking the community to suggest names for the new West Dapto suburbs. Council took the opportunity to ask the community about the existing suburb names of Kembla Grange, Huntley and Avondale. The intention was to find out if the community felt these names are relevant and meaningful to West Dapto and need to continue to be used.

Naming Criteria

GNB's *NSW Address Policy and User Manual* Section 6.8.2 states the following criteria to suburb naming;

- The name should not already exist in Australia
- Local Aboriginal significance
- Gender – consider female names
- Characteristics of the suburb
- Commemorate a person
- Be appropriate to relevant local cultural or historical influences in the region
- Be appropriate to relevant local natural influences in the region
- Be easy to read, spell and pronounce
- Not be offensive or likely to cause offense
- Not be a company or business name

If naming after a person, they need to have been deceased for a period of at least two years. Consent of the family members of the person being commemorated must also be obtained.

Suburb Naming Process

The GNB is the State body that oversees naming in NSW. The project team followed the GNBs '*Address Policy and User Manual*' to guide our suburb naming project. The West Dapto Review Committee accepted the role of supervising and providing guidance for the project.

The project team felt that the community should be involved in the suburb naming process. During the first phase of engagement, we asked the community to suggest suburb names and checked the relevance of the existing suburb names of Huntley, Kembla Grange and Avondale. When the broad community engagement period ended, a number of representatives from heritage groups, Dapto and Aboriginal communities came together as two working groups to review and assess each of the suggested names against the GNB criteria as well as researching the significance of the names to West Dapto. One working group included Aboriginal community members who reviewed and assessed all suggested Aboriginal names. Three Traditional Custodian Groups and the Illawarra Local Aboriginal Land Council were invited to be part of this group. The other working group was made up of members of heritage groups and the Dapto community who reviewed and assessed all non-Aboriginal names. The two working groups worked closely with the project team through this process to develop a shortlist of names that, through research, meet the GNB criteria and are significant to West Dapto.

Unfortunately, the Covid-19 pandemic began towards the end of the broad community engagement period. The project team feels that our two face-to-face community information stands were affected by the pandemic, and certainly the pandemic affected the working groups. Initially the two working groups were going to be run as face-to-face workshops. With safety restrictions in place, Council's Engagement HQ platform was developed as an interactive space for the working group participants, with resources, space for back-and-forth conversation between participants and the project team, space to upload research documents for participants and live discussion times with the project team. Conducting the workshops online presented challenges for both the project team and working group participants, however the process was a success and received positive verbal feedback from both the project team and working group participants.

Engagement with Aboriginal community

The approach to engagement in this project was informed by draft guidance documents for how we engage with the Aboriginal community. Using these draft documents as a guide, the project team was able to build positive and trusting relationships with a number of Aboriginal community members and key Aboriginal organisations very early in the project, before broad community engagement had commenced. The project team consulted with an Engagement Officer and Council's Aboriginal Community Development Officer to plan a meaningful and culturally sensitive engagement. The guidelines place high importance on face-to-face meetings with community members on a consistent basis (i.e. not just for the life of the project). However, with the Covid-19 pandemic and restrictions on face-to-face meetings in place, we were no longer able to meet with anyone from the community. The team found that those community members who we had met face-to-face or had multiple phone calls with before the safety restrictions came into place, continued

to stay in contact. Those community members who we were unable to meet or talk with before the pandemic remained uncontactable. The life of this project saw approximately 78 phone calls, 86 emails, 3 letters and two face-to-face meetings with Aboriginal community members. The project team organised face-to-face meetings in local cafes and people's homes where Aboriginal community members felt safe and comfortable. By following the draft guidance documents, this project saw positive and trusting relationships being built between the project team and Aboriginal community members. It is important for the project team to continue to stay in contact with those people to maintain a positive working relationship.

Early in the project, the Aboriginal Community Development Officer and Engagement Officer provided advice to the project team about Aboriginal community members and organisations they should be engaging with. It was also advised that three Traditional Custodians Groups and the Illawarra Local Aboriginal Land Council would be invited to participate in the working group. The way in which these groups were engaged remained flexible throughout the project. While the Engagement HQ working group website was built with the Aboriginal working group in mind, the Aboriginal working group had minimal involvement with it. Communication with the Aboriginal working group was through phone calls, emails, video calls and posted letters.

Many other Aboriginal community members were engaged with during this project. A number of Aboriginal community members were notified of the project before it began and then when the project commenced they were invited to suggest a suburb name. Once the engagement period had closed many Aboriginal community members were emailed the suggested names and asked to provide feedback.

Stakeholders

Stakeholders included:

- Residents of Avondale, Kembla Grange, Huntley and the wider West Dapto and Dapto communities
- Neighbourhood Forum 8
- Wollongong Heritage Reference Group
- Illawarra Shoalhaven National Trust
- Illawarra Historical Society
- Community groups such as CareWays, Rotary and Quota
- Local primary and high schools
- Migrant Heritage Project
- Traditional Custodian Groups; The Wodi Wodi Elders Corporation, Wadi Wadi Coomaditchie Aboriginal Corporation and Korewal Elouera Jerrungurah Tribal Elders Council.
- Illawarra Local Aboriginal Land Council
- Aboriginal Reference Group
- Aboriginal organisations and individual knowledge and language holders

Methodology

Methods	Details of methods
Communication	methods
Advertiser	Wednesday 11 March 2020
Presentations	Neighbourhood Forum 8 Wednesday 11 March Aboriginal Reference Group Tuesday 17 March Heritage Reference Group – cancelled due to Covid. Information passed through Council Heritage Officers.
Councillor briefing	24 February 2020
Information packs	Containing poster, map of proposed suburbs, FAQ & feedback forms placed at Customer Service and Wollongong and Dapto Libraries.
Postcards	Distributed to local businesses and organisations in Dapto and West Dapto
Email	Email to key stakeholders announcing the engagement and link to website
Engagement HQ	Live Monday 9 March 2020, closed 20 April 2020 (extended for 2 weeks due to Covid)
Frequently Asked Questions (FAQ)	Available online, Customer Service, Wollongong and Dapto libraries. <ul style="list-style-type: none"> One FAQ about suburb boundary review One FAQ about suburb naming process
School newsletters	Poster emailed to primary and high schools in the Dapto area
Engagement	methods
Information stands	Saturday 14 March 2020 in Dapto Mall 12-2pm Tuesday 17 March 2020 at Dapto Ribbonwood Centre 1-3pm
Engagement HQ – survey tool	Survey live from 9 March to 20 April 2020 (extended due to Covid-19)
Hardcopy feedback form	Placed at Customer Service, Wollongong and Dapto Libraries
Presentations	Neighbourhood Forum 8 Wednesday 11 March Aboriginal Reference Group Tuesday 17 March Heritage Reference Group – cancelled due to Covid-19
Aboriginal working group	Three Traditional Custodian groups and Illawarra Local Aboriginal Land Council invited to participate. Our Traditional Custodian groups provided feedback and information through emails, phone calls and posted letters.
Non-Aboriginal working group	Members of heritage groups and the Dapto community were invited to review and assess all non-Aboriginal names. Four community members took part via Council's engagement website, emails and phone calls.
Phone calls	Aboriginal community members Non-Aboriginal working group members
Emails	Emails with many Aboriginal community members Email invite to the two working groups
Facetime video calls	Aboriginal community members
Post letters	Aboriginal community and Traditional Custodian group members
Engagement HQ working group website	Working group website for the two working groups.
Engagement HQ working group website – Discussion forum	Discussion forum used for deliberative conversations. The project team took turns at being available on Engagement HQ for live discussions in the discussion forum. Seventeen live sessions occurred over a 4-week period. The discussion forum was an effect tool with over 140 messages posted.
Engagement HQ working group	Used to update working group participants on the next steps they need to take or any changes

website – Newsfeed tool	
Engagement HQ working group website – survey tool	The survey tool was used to allow working group participants to upload their 'name research templates'. A document library was created to store all the research on the website, so all working group participants could access them.
Engagement HQ working group website – quick poll	Used to ask working group participants how they would like to divide the research workload for the project.

Results

City of Wollongong Facebook post saw 44,255 people reached and 14,528 engagements. An engagement is interaction with a post beyond viewing it, for instance likes, comments, shares, image views and link clicks.

The Illawarra Mercury published a media release on 10 March 2020.

Engagement Participation Results

Table 1: Engagement participation results

Engagement activity	Participation
Information stand attendance numbers	16
Submissions received during the engagement period	60 Online 7 hardcopy/letters 13 social media

Online Engagement Results

A total of 60 submissions were received online. Table 2 presents the usage statistics for the project page on Council's website.

Table 2: Summary of online participation

Measure and Explanation	Usage
Unique Site Visits – Total number of visits to the project page	747
Aware – Total number of users who viewed the project page	628
Informed – Total number of users who opened a hyperlink or read a document	276
Engaged – Total number of users who have actively contributed to the project via the project page	60

Submission results

Community members were asked to:

- Suggest a name for the new suburbs of West Dapto
- Assign the name to a specific suburb

- Select the GNB criteria that the name meets
- Tell us their thoughts about the existing suburb names of Huntley, Kembla Grange and Avondale and if these names are still relevant.

Results

Questions 1, 2, 7 & 8

These questions asked community for their name suggestion, where the name should be located, the GNB criteria the name meets and the significance of the name to West Dapto. A total of 80 names were suggested. The community working groups reviewed and assessed each name against the GNB criteria and significance to West Dapto. Appendix 1 shows the shortlist of names and which suburb each name is proposed for.

Table 3: Results Question 3-5

Questions 3-5 asked the community if the existing suburb names of Kembla Grange, Huntley and Avondale are still significant to West Dapto and which new suburb should retain the existing name. A total of 65 people told us if we should keep the existing names. A total of 31 people told us which suburb should retain Avondale with 27 people telling us which suburb should be called Huntley and 41 people telling us where they believe the name Kembla Grange is most relevant.

Question	Y/N	Location
Should we keep Avondale?	Yes=31/65 No=34/65	East side keeps name=19/31 West side keeps name=10/31 (2 didn't say)
Should we keep Huntley?	Yes=29/65 No=36/65	North side keeps name=11 South side keeps name=16 (2 didn't say)
Should we keep Kembla Grange?	Yes=42/65 No=23/65	East side keeps name=31 West side keeps name=10 (1 didn't say)

Appendix 1: West Dapto Suburb Naming Project shortlist on map.

Appendix 1: West Dapto Suburb Naming Project

SHORTLIST

(proposed for GNB consideration)

A

Edenfield

McPhail

Knapp

Kembla Grange

Guwara
(high wind)

**Help us name
our new
West Dapto
suburbs**

B

Kembla Grange

Newton Park

C

Huntley

Lindsay

Guwara
(high wind)

Murringah
(Coal)

E

Osborne

Avondale

Woolangurru
(Track over the
mountain)

Murrindarry
(Mount
Marshall, The
Mountain)

F

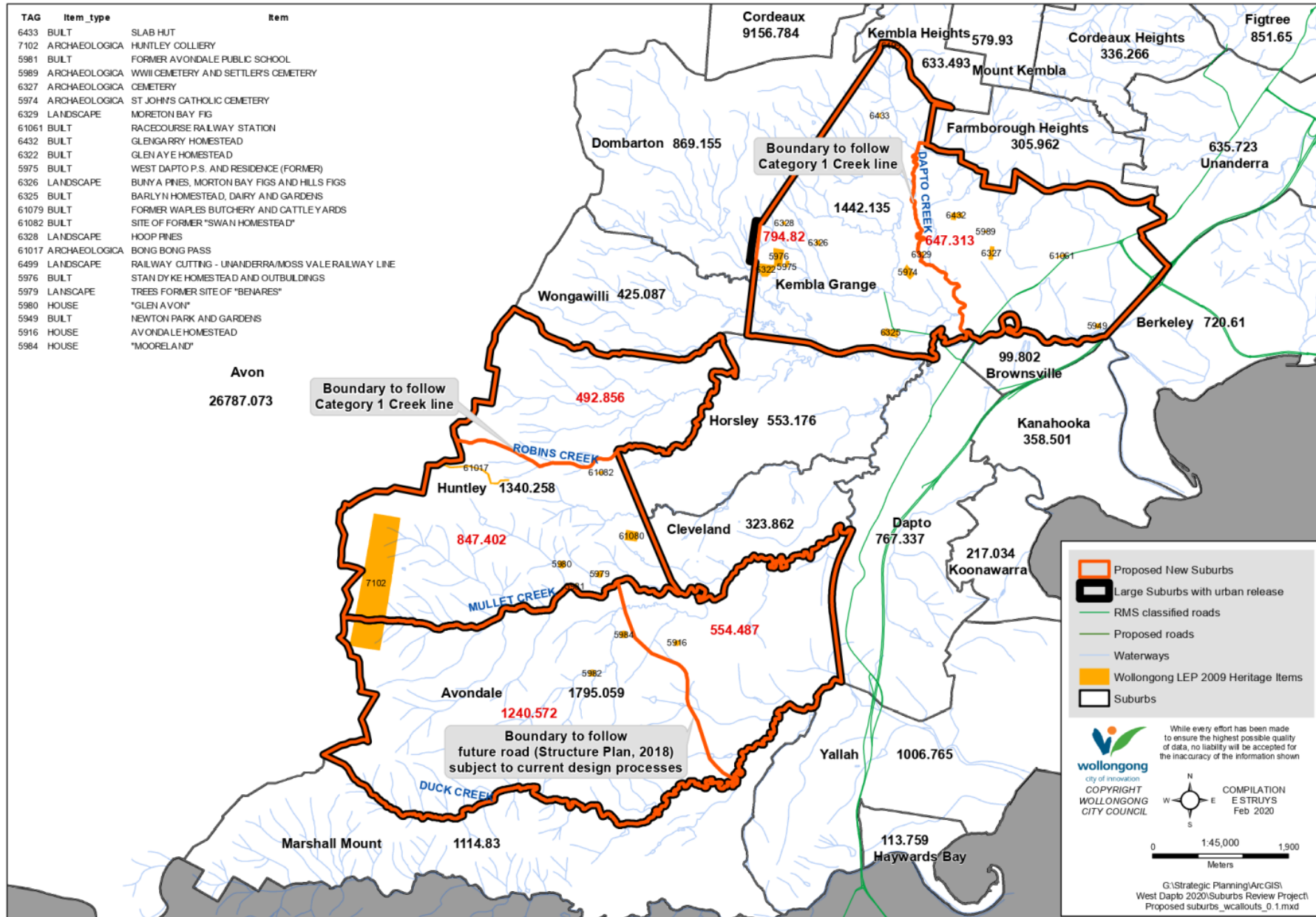
Avondale

Logbridge

D

Huntley

Murringah
(Coal)





**Geographical
Names Board**

Geographical Names Board of NSW Policy

Place Naming

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This information is correct at the date of publication; changes after the time of publication may impact upon the accuracy of the material.

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Document Control

Version and amendment table

Date	Version	Amendments	Authorised
23 April 15	1.0	First draft	B Hirst / B Goodchild
12 Nov 15	2.0	First circulation to members	B Hirst / B Goodchild
20 Dec 15	3.0	Amended based on GNB feedback	B Hirst / B Goodchild
8 March 16	3.1	Aboriginal policy updated based on feedback received.	B Hirst / B Goodchild
10 March 16	4.0	Formatted for document consistency	B Hirst / B Goodchild
18 March 16	5.0	Amended based on March GNB meeting	B Hirst / B Goodchild
19 April 16	5.1	Minor amendments and changes accepted	B Hirst / B Goodchild
2 May 16	6.0	Amendments based on Secretariat input	B Hirst / B Goodchild
6 May 16	6.1	New section on changing names	B Hirst / B Goodchild
May 17	6.2	Update to section 6.1 and 11.1	N Underwood
Sep 18	6.3	Update branding	N Underwood
4 Jul 19	6.4	Update to section 10.2	N Underwood

Repeals and Review

The policies identified in this document are consistent with national and international best practice. They repeal all existing Geographical Names Board of NSW (GNB) policies and guidelines in respect to geographical naming in NSW, apart from those included in the NSW Addressing User Manual, which are complementary to these policies. They do not apply retrospectively, and any arrangements that predate these rules are not necessarily subject to its terms.

Recognition of any existing NSW geographical name that does not conform to these policies does not establish a precedent for any future naming proposal.

This document is to be revised annually or as required. Where minor changes are required, the GNB must ensure the version number is updated. However, where changes in legislation or operating environment result in substantive rewriting of the document, the sponsor must create a new document and ensure it is entered into TRIM (electronic file management system). This will ensure the integrity of the original document.

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1 Scope

This policy applies to the operations of the GNB and its Secretariat.

2 Responsibilities

Geographical Names Board

The GNB is responsible for the governance of this policy.

Department of Customer Service (DCS)

DCS is responsible for the administrative management, technical support and promotion of the policy under the auspices of the GNB.

3 Glossary

Act	<i>Geographical Names Act 1966</i> No 13
CGNA / PCPN	Committee of Geographic Names of Australasia – now renamed the Permanent Committee on Place Names. Part of ICSM
GNB / Board	Geographical Names Board of NSW as constituted under the Act
DCS	Department of Customer Service
Gazetteer	List of geographical names. In NSW the Geographical names gazetteer is stored in the Geographical Names Register database.
Geographical name	The name of a place as determined by the provisions of the Act and been notified in the Gazette as a geographical name, but does not include a name which has ceased to be a geographical name under the Act.
GNB (the GNB)	Geographical Names Board as constituted under the Act
ICSM	Intergovernmental Committee on Surveying and Mapping
Place	Defined by the Act as <i>'any geographical or topographical feature or any area, district, division, locality, region, city, town, village, settlement or railway station or any other place within the territories and waters of the State of New South Wales but does not include any road, any area (within the meaning of the Local Government Act 1993) or area of operations of a county council (within the meaning of that Act), any electoral district under the Parliamentary Electorates and Elections Act 1912, any school or any place or place within a class of places to which the provisions of this Act do not apply by virtue of the regulations.'</i>
Recorded name	Defined by the Act as <i>'the name of a place as it appears on a Lands Department map or, where the name of a place appears differently on two or more such maps, the name of that place as it appears on whichever of those maps was published later than the other or others.'</i>
Road / Road Naming Authority	Road and Road Naming Authority are defined under the <i>Roads Act 1993</i> and summarised in section 5.3 of the <i>NSW Address policy</i>
Secretariat	GNB secretariat appointed in accordance with section 4 of the Act
Spatial Services	A division of the Department of Customer Service (DCS). Spatial Services is the government body responsible for the implementation of the Geographical Names Act.

4 Introduction

Consistent use of accurate place names is an essential element of effective communication worldwide, and supports socio-economic development, conservation and national infrastructure. (Permanent Committee on Place Names)

Department of Customer Service, Spatial Services and the GNB have statutory responsibility to establish, update, preserve and publicise place naming in New South Wales.

The *Geographical Names Act 1966* recognises the importance of unambiguous and official place names. Place names reflect the relationship between people and place and create a link that forms the basis for communication, location and addressing. Clear and unambiguous place names are essential for emergency services, postal and service delivery as well as professional and personal navigation.

The GNB is committed to recognising our Aboriginal cultural heritage by registering place names given by Aboriginal people so that they can be assigned as geographical names alone or used alongside existing non-Aboriginal names.

The GNB is committed to open and transparent practices and procedures in the selection of place names. This document brings together the policies adopted by the GNB and enables all interested parties to understand why specific names are selected.

The policies detailed in this document are consistent with national and international policies, guidelines and practices.

5 Legislation and Authority

As set out in the *Geographical Names Act 1966* (Section 5), the GNB has the following powers and functions:

- assign names to places
- approve that a recorded name of a place shall be its geographical name
- alter a recorded name or a geographical name
- determine whether the use of a recorded name or a geographical name shall be discontinued
- adopt rules of orthography, nomenclature and pronunciation with respect to geographical names
- investigate and determine the form, spelling, meaning, pronunciation, origin and history of any geographical name
- the application of any geographical name with regard to position, extent or otherwise
- compile and maintain a vocabulary of Aboriginal words used or suitable for use in geographical names and to record their meaning and origin
- compile and maintain a dictionary of geographical names with a record of their form, spelling, meaning, pronunciation, origin and history
- publish a gazetteer of geographical names
- inquire into and make recommendations on any matters relating to the names of places referred to it by the Minister

The GNB may compile, maintain and publish a list of road names.

6 Policy – Universal Naming Principles

The following principles shall apply for all new geographical names in New South Wales.

6.1 Language

- 1 Geographical names shall be written in standard Australian English or a recognised format of an Australian Aboriginal language local to the area of the geographical name.
- 2 Diacritical marks (symbols such as ´ in é, ¸ in ç or : in ö) are not used in Australian English names, and shall be omitted from names drawn from languages that use such marks.
- 3 Geographical names shall be easy to pronounce, spell and write, and preferably not exceed three words (including any designated term) or 25 characters. An exception to this is in the use of Aboriginal names when it is accepted that a traditional name may at first appear to be complex but will, over time, become more familiar and accepted by the community.
- 4 The following types of punctuation as used in Australian English shall not be included as part of a geographical name: period (.), comma (,), colon (:), semi-colon (;), quotation marks (""), exclamation mark (!), question mark (?), ellipsis (...), hyphen (-), solidus (/) and parenthesis (()). For surnames or other names that include a hyphen, the hyphen shall be omitted when used for a geographical name.
- 5 An apostrophe mark shall not be included in geographical names written with a final 's', and the possessive 's shall not be included e.g. Georges River not George's River. Apostrophes forming part of an eponymous name shall be included (e.g. O'Connell Plains).
- 6 A geographical name shall not include a preposition e.g. Avenue of the Allies.
- 7 Geographical names shall not include the definite article (the) as the sole name element of a place name e.g. The Reserve is not acceptable.
- 8 A geographical name shall not be abbreviated or contain an abbreviation, initial or acronym e.g. Point, not Pt except that St shall be used for Saint. An exception may be where an abbreviation may have become widely accepted by the community. Eg CWA for Country Women's Association.
- 9 For the purposes of consistency, names starting with Mc or Mac shall not have a space included between the Mc or Mac and the rest of the name.
- 10 A geographical name shall not include Arabic numerals e.g. 3 or 4th or Roman numerals e.g. IV or X. Where numbers are included in a geographical name they shall be written in full e.g. Fourth Top Ridge, Eleven Mile Creek.
- 11 A geographical name shall not include initials e.g. A F Wyatt Reserve.
- 12 The spelling of geographical names derived from the same source shall be uniform in spelling. e.g. Mount Kosciuszko is now spelt with z to be consistent with original spelling.
- 13 Postnominals and titles shall not be included in geographical names eg John Smith not John Smith AO. An exception is the use of 'VC'.

Justification

The principles identified above ensure consistency thereby reducing the potential for confusion. These principles are consistently applied throughout Australia and commonly adopted internationally. They also facilitate reliable electronic searching essential for navigation systems, service delivery and public safety.

6.2 Form and Character of Names

Place names shall be recognisable words or acceptable combinations of words and shall be appropriate to community sensitivities.

Discriminatory or derogatory names are not acceptable. Such names are those perceived, at a given point in time, to be offensive, demeaning, or harmful to the reputation of individuals, or to social, ethnic, religious or other groups. It is recognised that the perception of 'discriminatory' or 'derogatory' may vary through time and from place to place. In response to requests from the public, the GNB will investigate the appropriate status of any names deemed to be discriminatory or derogatory.

Commercial and business names shall not be used for geographical names, particularly where the name can be construed to be promoting a business. However, business names no longer in use which promote the heritage of an area are acceptable.

Use of club, society, association or special interest group names is discouraged. Such association may change their focus or for some reason lose community support. Community based associations, particularly those philanthropic associations, may be acceptable (eg Rotary, Lions, Apex).

7 Policy - Commemorative Names

Commemorative names are those that commemorate a person, event or place. Acts of bravery, community service and exceptional accomplishments are typical grounds for this recognition. The name of persons who gave their lives in service for their country are often used as commemorative names.

The person commemorated should have contributed significantly to the area around the geographic feature or locality.

When such a name is applied, it shall be given posthumously, at least one year after the decease of the person. Names of living persons are by their nature subject to partisan perception and changes in community judgement and acceptance.

Commemorative names shall not be used to commemorate victims of, or mark the location of, accidents or tragedies. Ownership of land is not in itself grounds for the application of an owner's name. Names of persons holding public office shall not be used.

Personal names, including those of persons still living, may be used for built features e.g. pavilions and grandstands etc., however these features are not formally assigned by the GNB and are not covered by the Act.

7.1 Personal Names

The names of deceased persons are suitable for the naming of reserves. Such persons shall have had a long term association with the area, or have made a significant contribution to the area of the proposed park or reserve. To assist local governments in determining the suitability of a name the GNB offers the following guidelines regarding association or contribution:

- Two or more terms of office on the governing local government council.
- Twenty or more years association with a local community group or service club.
- Twenty or more years of association or service with a local or state government or organisation.

- Action by an individual to protect, restore, enhance or maintain an area that produces substantial long term improvements for the community.
- The death of a person within a place is not solely to be considered sufficient justification for commemoration.
- Local residents of note.

Justification

Using the name of a living person is unacceptable (nationally and internationally) as it may lead to favouritism and/or inappropriate naming. There are examples where people commemorated have later proven to be of poor character or otherwise thought to be unworthy.

8 Policy - Duplication of names

8.1 Duplication of place names

In accordance with the NSW Addressing User Manual (6.8.1 Uniqueness, Duplication), no new locality name shall be duplicated within NSW or any other state or territory in Australia.

Duplication includes identical or similar spelling and/or pronunciation.

8.2 Place names other than localities

Uniqueness is the most essential quality to be sought in proposing a new place name. Duplication should be avoided wherever possible, but new place names may be duplicated provided there is no duplication of the name within the local government or adjoining local government.

Place names with a different designation value are not considered to be duplications. For example, Jenolan River and Jenolan Caves are acceptable.

The GNB encourages efforts by local governments to change or modify duplicate names wherever ambiguity or confusion is likely to occur. Such name changes should be coordinated with the GNB.

Justification

The purpose of place names is primarily to provide unambiguous direction and reference to identify geographical entities. Duplication of locality names is to be avoided because of the confusion this will cause, particularly in the dispatch of emergency services, which is now often coordinated from call centres. Duplication of locality names used for addressing purposes can result in delays in arrival of essential services.

Duplication can also cause personal difficulties such as failed parcel and service delivery and difficulty for tourists and visitors.

Where duplication occurs inter State or Territory, the respective authorities should liaise in order to attempt to arrive at an acceptable solution.

9 Policy – Place Naming Process

9.1 General

This policy is for the application of place names within the territories and waters of New South Wales including reserves under the management of local government.

The Department of National Parks and Wildlife Services has a separate policy for the naming of national parks under its management.

- All proposals for place naming shall conform to the GNB's Naming Principles.
- All place name proposals shall include a map or diagram clearly defining the extent of the feature proposed to be named.
- Proposals for place naming shall be submitted to the GNB for consideration and formalisation process.
- Private ownership of the land on which a geographical or physical feature is located does not confer any naming right to the land owner or manager. This is also true in respect to land under the various forms of public management, including national parks and reserves.
- Place names (excluding localities) shall not be duplicated within the same LGA or locality or in an adjoining LGA or locality.

9.2 Selection of names

- Aboriginal names are encouraged as the name to be used for any feature that currently does not have a name recognised by the GNB.
- Names acknowledging the multicultural nature of NSW are encouraged.
- Names associated with the heritage of an area are encouraged, especially the names of early explorers, settlers, naturalists, events.
- A name suggested for any place that owes its origin to the peculiarity of the topographic feature designated such as shape, vegetation, animal life etc. may be accepted.
- Gender diversity in names is encouraged.
- The multiplication of names for different parts of the same topographical feature such as a stream or mountain range shall be avoided, and the one name applied to a stream or mountain range throughout its entire length. However, an Aboriginal name may apply to a limited section of a feature.
- The naming of forks, arms and branches of a river as North Branch and South Branch is not supported. Unique names shall be assigned to river branches.
- When a choice is offered between two or more names for the same place, locality or feature, all supported by local usage, the GNB may adopt one of such names as is considered appropriate in accordance with its principles and policies.
- The use of cardinal points of the compass as a prefix or suffix to an existing name shall not be used.
- The changing of long established place names is to be avoided except where necessary to avoid ambiguity or duplication.
- The GNB may approve a first or given name as part of a geographical name only where it is necessary to appropriately honour the person referred to or where it is necessary to avoid ambiguity.

9.3 Changing names

Where names have been changed or corrupted by long established local usage, it is not usually advisable to attempt to restore the original. Changes are discouraged unless the change has been deemed to be in the public interest or for safety reasons. Changing well established names can lead to address or location confusion, especially to electronic navigation services.

9.4 Correct designation values

The GNB has compiled a Glossary of Designation Values in the Geographical Names Register in order to assist in determining the correct designator to be applied to place names at the time of naming.

New names proposed for place names shall include the designation value appropriate to the nature of the feature.

The Glossary of Designation Values is attached as Appendix A.

9.5 Council resolutions to identify community support

When Council submit a naming proposal, it should be supported by a Council resolution. Council should also supply evidence that they have sought community feedback on the proposal. This could include advertising and inviting comment using:

- Local newspapers.
- Relevant web site.
- Local council facilities (eg offices, libraries etc).
- Notices to residents in the area surrounding the feature of the proposed name.
- Notices to local progress associations.

9.6 Naming of Cross Border Features

The name for any feature that crosses the State boundary shall be the same on both sides of that boundary. The basis for the selection of a name for such a feature should be the consensus between relevant authorities. Primary responsibility for obtaining consensus should rest with the authority within which the majority of the feature is located. Any matters regarding naming or renaming of features that cross the state border shall be referred to the Cross Border Commission.

9.7 Referring names to Local Government

Where a submission proposing a name is received by the Secretariat, that submission must be referred to the relevant Local Government and the submitter notified of this action.

9.8 Generic reserve names

The GNB has classified a number of reserve names as "generic", and when one of these names is proposed it shall also include the name of the locality within which the reserve is located. For example, Volunteer Park is a generic name, and if this name is proposed in Muswellbrook, it shall be proposed as Muswellbrook Volunteer Park.

The following park names have been classified as “generic” by the GNB

ACACIA PARK	ANZAC PARK
APEX PARK	BI-CENTENARY PARK
BI-CENTENNIAL PARK	CENTENARY PARK
CENTENNIAL PARK	CENTRAL PARK
CIVIC PARK	GALLIPOLI PARK
HERITAGE PARK	JUBILEE PARK
KINGS PARK	LIONESS PARK
LIONS PARK	MEMORIAL PARK
OLYMPIC PARK	PEACE PARK
PIONEER PARK	PRESIDENT PARK
PRINCE PARK	PRINCESS PARK
QUEENS PARK	QUOTA PARK
REMEMBRANCE PARK	RIVERSIDE PARK
ROTARY PARK	SESQUI CENTENARY PARK
SESQUI CENTENNIAL PARK	VOLUNTEER PARK
WAR MEMORIAL PARK	

9.9 Use of the name Anzac

The use of the name Anzac is protected by Commonwealth regulations, and may only be used for the naming of a road or park in which, there is situated a public memorial relating to the war which commenced on the fourth day of August 1914, or the war which commenced on the third day of September 1939. (Commonwealth ‘Protection of the word ‘Anzac’ regulation.’)

9.10 Naming of facilities within reserves

Facilities within an officially assigned reserve, such as a pavilion, grandstand, garden, buildings etc. may also be named according to this policy, but do not require the formal approval of the GNB. However, the GNB shall be notified of such names to ensure the name, position and origin is recorded in the Spatial Services’ Digital Topographic Database and the name shown on maps, where relevant.

9.11 Renaming of reserves

Names chosen for reserves are expected to be enduring, and the renaming of these features is confusing and disruptive and is discouraged. If the renaming of a reserve is proposed, evidence of community support for the name change must be provided. The GNB will then evaluate the merits of the proposal before making a decision.

9.12 Naming of properties and homesteads

The GNB does not officially assign names of properties or homesteads. However names of properties and homesteads may be recorded in the NSW Digital Topographical Database (DTDB).

10 Policy – Recognition and use of Aboriginal names

10.1 Background

The names we give to places convey their significance through a sense of history, identity and connection between people and a place. The land is seamless with spirituality and identity for Aboriginal people. A key manifestation of this connection are the names given to features on the land that relate to the ancestors, histories, law and lore of its people.

For Aboriginal people connection with 'Country' is intrinsically connected to identity. Country is the area where an Aboriginal community is connected by language, cultural practices and long held relationships between people and the land. Countries are said to own people whereas for non-Aboriginal people land is owned through a range of legal titles. Country and people are inseparable for Aboriginal communities.

This policy is designed to encourage and promote recognition of Aboriginal place names and to foster the more frequent and official use of these names, particularly for places where the names have not been assigned as geographical names. The policy also provides, where it is possible, for the reinstatement of an Aboriginal place name through the dual naming process.

The NSW Government is committed to continuing the recognition of our Aboriginal cultural heritage by registering the original place names used by Aboriginal people to identify geographical features. Where a feature is identified by a non-Aboriginal name and that name is well established, an Aboriginal name put forward for the feature can be assigned as a dual name and sit alongside the existing non-Aboriginal name.

The GNB prefers the use of Aboriginal names for geographical features. Where a feature currently has a non-Aboriginal name, it may be considered for a dual name provided that documentary or oral evidence of the Aboriginal name is provided.

10.2 Recognition and use of Aboriginal names

- Aboriginal place names are preferred for the name of any place that does not have an assigned geographical name.
- Prior to submitting an Aboriginal name for consideration by the GNB, the proponent should consult the Local Aboriginal Land Council and Aboriginal communities on all matters concerning Aboriginal place names occurring in their area of current occupation and traditional association, in line with self-determination policies. This includes any proposals to assign new names, alter spellings of existing names or assign dual names.
- A name nominated by a Local Government Council will not be accepted by the GNB unless the Local Aboriginal Land Council and relevant Aboriginal communities have been consulted. The GNB Secretariat can provide guidance.
- Aboriginal place names which have been assigned as geographical names shall not be amended in form, spelling, extent or position without the consent of the relevant Local Aboriginal Land Council or Community.
- A dual naming system may be used for the naming of a physical and environmental place[s] of significance to the local Aboriginal Land Council or Community when a non-Aboriginal assigned geographical name already exists. Dual naming shall not apply to localities, towns or roads.

- A dual name can only be assigned where there is plausible historical evidence in the form of documentary or oral sources, that the feature has an existing Aboriginal name and that some authority or authenticity can be attributed to the source or sources for the form, origin, spelling, history and meaning of the name. The name cannot be a new name assigned for the purpose of a tribute etc.
- Signs or notices explaining the origins of Aboriginal place names should also identify the name of the language group from which the name originated. For example, the name '...''... means '.....' from the '...' Aboriginal language group.
- The GNB endorses and supports the Permanent Committee on Place Names "Guidelines for the Use of Aboriginal and Torres Strait Islander Names" which is included in the PCPN's "Guidelines for the Consistent Use of Place Names".
- The GNB does not have a role in the determining naming, spelling or determination of boundaries of Aboriginal Countries or Nations.

11 Policy - Infrastructure

11.1 Railway Stations

Railway stations shall be named after its locality or area of interest unless to do so would lead to a duplicated station name. In these cases a name that identifies the area or location of the station should be used. For example Town Hall Station and Martin Place Station are situated in Sydney where multiple stations are in one locality.

Justification

Railway station names assist in location and navigation and are particularly important for visitors and other travellers.

11.2 Other infrastructure

The GNB does not have specific statutory responsibility for formally naming infrastructure (other than Railway Stations and Post Offices), schools, private estate names or building names.

Nevertheless, naming any prominent feature should follow the accepted practice for naming as detailed in this policy.

Justification

Infrastructure such as buildings and sporting facilities are generally not used for addressing purposes however there is a high likelihood that they may be used for location including emergency services. Unique names for all infrastructure will assist in ensuring their location for emergency services.

11.3 Bridge Naming

The GNB does not name bridges, however, it encourages all bridge naming to follow the guidelines as set out in this policy.

The naming of bridges and other structures on roads does not have a formal legislative basis. However, the same procedures for road naming applies to bridges and other road infrastructure:

- RMS is responsible for the naming of bridges and other structures on freeways.
- Local councils initiate the naming of bridges on local, regional and state roads (other than freeways). RMS to approve these proposals.

RMS will approve a naming proposal for a bridge or structure provided that:

- The name has wide community support.
- An Aboriginal name has the support of local Aboriginal groups.
- Consideration has been given to National and State commemorative initiatives involving the naming of new of key road infrastructure.
- The name is consistent with GNB place name criteria.
- The design of the name plaque accords with RMS requirements.

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Appendix A



**Geographical
Names Board**

Glossary of designation values in the Geographical Names Register

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ABORIGINAL RESERVE

Crown land set aside for Aborigines, where they may continue their traditional lifestyle away from the influence of white Australians and access to which is controlled by federal or state authorities or by Aboriginal Land Councils.

AERODROME

All licensed aerodromes and government aerodromes maintained by the Federal Airports Corporation, other than those designated 'airports'.

AIRFIELD

A landing or taking-off area for aircraft.

AIRPORT

An aerodrome that handles regular schedules of passengers and freight.

AMPHITHEATRE

Basin shaped hollow, particularly one having steep sides. Considerable variation in size.

ANABRANCH

A tributary of an anastomosing river which links up with other distributaries and sometimes with the parent stream.

ARM

A comparatively long, narrow and natural waterway extending from a larger body of water.

ARTESIAN BORE

A hole bored perpendicularly into strata, producing a constant supply of water at the surface without pumping.

BACKWATER

A body of stagnant water connected to a river.

BASIN

1. The tract of country drained by a river and its tributaries, or which drains into a particular lake or area.
2. A circumscribed formation in which the strata dip inward from all sides to the centre; the stratified deposit, especially of coal, lying in such a depression.
3. An area of water limited in extent and nearly enclosed by structures alongside which vessels can lie. A non-tidal basin is one closed by caisson of gates to shut off from open water, so that a constant level of water can be maintained in it. Also called a 'wet dock'. A tidal basin is one without gates in which the level of the water rises and falls with the tide. Sometimes called an 'Open Basin'.

BAY

A well-marked indentation made by the sea or a lake into a coastline, whose penetration is in such proportion to the width of its mouth as to contain land locked waters and constitutes more than a mere curvature of the coast.

BEACH

The sloping shore along a body of water that is periodically washed by waves or tides and is usually covered with sand or gravel.

BIGHT

A crescent-shaped indentation in the coastline usually of large extent and not more than a 90 degree sector of a circle. See 'Bay' and 'Gulf'.

BILLABONG

An efflux from a stream, usually an old bend in the stream, which has been cut off by erosion and deposition. When the fall of a stream is only a few centimetres per kilometre channel is usually incapable of clearing flood waters, which overflow into this efflux. As the water recede the efflux or billabong becomes a pool or a series of pools, which in dry periods may completely dry up.

BLUFF

A spur or ridge terminating in a steep, rocky face.

BORE

A deep vertical hole of a small diameter drilled to obtain water. Designation includes 'Artesian Bore'.

BOUNDARY

That which serves to indicate the limits of a particular area. Various types of boundaries which may be encountered are:

1. UNCLASSIFIED BOUNDARIES; those drawn by the compiler prior to classification to delineate a change in surface characteristics.
2. INTERNATIONAL BOUNDARIES; those defining the territorial sovereignty of a country.
3. STATE OR TERRITORY BOUNDARIES; those defining the major administrative or political divisions within a country.
4. ADMINISTRATIVE AREA BOUNDARIES; those defining areas of common local or regional administration.
5. PROHIBITED AREA BOUNDARIES; those defining the limits of an area into which entry is prohibited, without prior permission from a controlling authority, for security or safety reasons.

BREAKWATER

A natural or artificial structure along a coast capable of checking the force of the waves, thereby reducing beach erosion. The designation includes 'groyne', 'training wall' and 'levee'. The latter two are to restrict rivers to a defined course.

BROOK

A small stream or rivulet.

BUTTE

A small residual of a mesa. The level top being the upper surface of the hard stratum but little lowered by erosion. The slopes on all sides are escarpments and its maximum horizontal dimension in any one direction is about 400 metres.

BUTTRESS

A very steep spur projecting from a hill, mountain, plateau, range etc., having the appearance of supporting it.

CAMP

A place where tents, cabins, etc. are erected for the use of military troops, etc.

CANAL

A large artificial watercourse used for irrigation or navigation.

CANYON

A gorge, relatively narrow but of considerable size, bounded by steep slopes. It has often been formed by a river cutting through the soft rocks of an arid region; the scantiness of the rainfall prevents denudation of the canyon walls, and so maintains their steepness. The walls of a large canyon, however, rarely approach the vertical, and their irregularity of slope is due to inequalities in the hardness of the rock.

CAPE

A piece of land jutting into the sea; a projecting headland or promontory.

CATCHMENT AREA

The region which drains all the rain water that falls on it, apart from that removed by evaporation, into a river or stream, which then carries the water into the sea or a lake; it may thus coincide with the 'River Basin'. Its boundary is defined by the ridge beyond which water flows in the opposite direction - away from the basin.

CAUSEWAY

A raised roadway of solid structure built across low or wet ground or across a stretch of water.

CAVE

A hollowed-out chamber in the earth, especially a natural cavity with an opening to the surface.

CEMETERY

A place or area for burying the dead.

CHANNEL

1. An artificial watercourse used for drainage or irrigation purposes.
2. A comparatively deep and narrow waterway affording a passage for vessels. The waterway may be natural or dredged and can occur in a river, harbour or sea.

CHASM

A particularly narrow portion of a gorge or ravine where the width is notably exceeded by the depth and the sides are vertical or nearly so.

CITY

A centre of population, commerce and culture with all essential services; a town of significant size and importance, generally accorded the legal right to call itself a city under, either, the Local Government Act, the Crown Lands Act or other instruments put in place by government.

CIVIC PLACE

A pedestrian area or open space, especially a square or plaza, within an urban environment which is frequented by citizens for a variety of purposes including public activities. It may be a place of commemoration. It does not include areas specifically created for commercial or business purposes. It is not to be used in an official address.

CLEARING

An area of ground within a forest, where less than 15% of the ground is covered by trees or scrub. Clearings within areas of dense vegetation may be manmade or naturally occurring.

CLIFF

A perpendicular or steep face of rock considerable in height, either inland or along the coast.

COLLEGE

An establishment for technical or vocational education usually post secondary.

COMMON

A tract of land which belongs to the local community as a whole, and is open to common use.

COUNTY

Territorial division of the state for administrative purposes.

COVE

A small indentation in a coast, usually sheltered.

COWAL

A small lake or dam.

CRATER

A bowl shaped cavity, in particular, at the summit or on the side of a volcano. And from which smoke and steam may emanate if the volcano is active. Craters of extinct volcanoes may contain crater lakes. The word crater is applied to other depressions especially those caused by the fall of large meteorites onto the earth's surface. Volcanic craters are sometimes called calders.

CREEK

A natural watercourse that is usually a tributary of a river or another creek. It may be perennial or non-perennial and in some areas its course may become indefinite or even peter out.

CROSSING

A place where a street, railway, stream, etc., may be crossed.

CUTTING

An open excavation through high ground, generally for a transportation system.

DAM

1. A barrier built across a stream to impound its water for any purpose.
2. An earthen structure built to contain water for stock purposes.

DEPRESSION

A depressed or sunken place.

DESERT

An almost barren tract of land in which precipitation is so scanty or spasmodic that it will not adequately support vegetation.

DIP

A place for controlling ticks on cattle.

DISTRICT

1. Territory marked off for special administrative purposes.
2. A tract of country, up to about 1600 sq. kms in area, distinguished by certain common characteristics, natural or cultural.

DOCK

An artificial structure in which ships are built or repaired.

DRAIN

A channel, man made or natural, by which liquid is drained or gradually carried away.

DUNES

Mounds or ridges of sand formed, either in a desert or along the sea coast, through transportation by the wind.

ESCARPMENT

A more or less continuous line of cliffs or steep slopes terminating any generally level upland surface, and is due to erosion or faulting.

ESTUARY

The tidal mouth of a river, where the tide meets the current of fresh water; more commonly, an arm of the sea at the lower end of a river.

FALLS

A sudden, more or less perpendicular, descent of water over a natural step in the bed of a river or stream.

FAULT

A fracture in the earth's crust along which movement has taken place, and where the rock strata on the two sides therefore do not match.

FLAT

A relatively level piece of ground within an area of greater relief; a tract of country without hills and smaller than a plain. In river valleys they may be Valley or River Flats, along the foreshores and subject to tidal action they are Tidal Flats and according to the nature of the surface they may be Mud, Stony or Sandy Flats.

FLORA RESERVE

Crown land set aside for the protection of flora, and access to which is controlled by federal or state authorities.

FORD

The shallow part of a stream or other body of water, where it may be crossed by vehicle or by wading. The crossing may be natural or improved, but not by bridging.

FOREST

An area of land proclaimed to be a forest under a Forest Act.

GAOL

A place for the confinement of persons convicted and sentenced to imprisonment or of persons awaiting trial.

GAP

A low point or opening between hills or mountains or in a ridge or mountain range.

GLEN

A narrow, wooded valley with a stream flowing at its bottom. Its sides being generally steep.

GOLF COURSE

An area of ground laid out for the playing of golf.

GORGE

A valley deep in proportion to its width, usually with precipitous or very steep sides. Generally a feature of some magnitude, relative to the surrounding base.

GRADIENT

A noteworthy gradient inclination or slope of the surface of the ground on the side or end of an elevated relief feature.

GRAVEYARD

A place for graves; a burial ground, esp. a small one or one in a churchyard.

GULF

Large valleys in mountain ranges OR an area of sea partly enclosed by land; usually of larger extent, and greater relative penetration than a bay, that is, Gulf of Carpentaria.

GULLY

A natural watercourse formed in the earth's surface, especially a hillside, by the action of water. It only carries water after rain and its sides are generally steep. Usually one of the smallest branches of a drainage system, and often associated with erosive action.

HARBOUR

A natural or artificially improved stretch of water where vessels can anchor or secure to buoys or alongside wharves etc and obtain protection from sea and swell. The protection may be afforded by natural features or by artificial works. The place may be provided with terminal and transfer facilities for loading and discharging cargo or passengers.

HEAD

A comparatively high promontory of land projecting into the sea with a steep face. An un-named head is usually described as a 'Headland' when a specific name is assigned, it becomes a 'Head'.

HEADLAND

A narrow area of land jutting out into a sea, lake, etc.

HILL

A small portion of the earth's surface elevated above its surroundings, of lower altitude than a mountain. Generally its altitude is less than 300 metres above the surrounding country but this can change in areas of low relief.

HILLOCK

A small hill or mound.

HISTORIC AREA

An area or precinct containing no or minimum present activity, but which at one time was an area of recognised name and purpose.

HISTORIC SITE

A specific place or site which has at one time been the site of an event or purpose.

HISTORICAL LOCALITY

An area or precinct containing no or minimum present activity, but which at one time was an area of recognised name and purpose.

HISTORICAL RECONSTRUCTION

An area or precinct which at one time was an area of recognised name and purpose and has now been redeveloped to recognise its past.

HOLE

An area hollowed out in or an opening in the ground.

INLET

A narrow indentation in the coastline or in the lake or river by which the water penetrates into the land.

ISLAND

A piece of land usually completely surrounded by water.

ISLET

A comparatively small insular landmass. Smaller than an Island but larger than a Cay.

KNOB

Rounded projection from a surface.

KNOLL

A small rounded Hill.

LAGOON

An enclosed area of water separated from the open sea or from a stream by some more or less effective, but not complete, obstacle such as low sandbanks.

LAKE

An extensive sheet of fresh or saltwater, natural or artificial, enclosed or nearly enclosed by land. It may or may not have in and out-flowing water, and in dry areas may even dry up at times.

LAKE BED

The area of a lake which is under water or once was under water.

LANDING PLACE

The act of coming to land. A place of disembarkation.

LANDMARK

A prominent or well known object in or feature of a particular landscape. A boundary marker. A large continuous area of land, as opposed to seas or islands.

LANDSCAPE FEATURE

This designation is used for a feature of the landscape, whether natural or cultural, which does not fit comfortably in any other designation and the number (actual and expected) of such places in NSW does not warrant a specific separate designation.

LIGHTHOUSE

A distinctive structure on or off the Coast, exhibiting a major light designed to serve as an aid to navigation.

LOCALITY

A bounded area within the landscape that has a 'Rural' Character.

LOCK

A section of a canal or river that may be closed off by gates to control the water level and the raising and lowering of vessels that pass through it.

LOOKOUT

A natural scenic viewpoint on elevated ground. Works or structures within the immediate vicinity of the view point improving the safety, amenities or view may be evident.

LOOP

A railway branch line which leaves the main line and rejoins it after a short distance.

MARINA

A docking facility for yachts and other pleasure boats accessible for private patrons only.

MARSHES

Low poorly drained land that is sometimes flooded and often lies at the edge of lakes etc.

MESA

A flat table-like upland, which falls away steeply on all sides (escarpments). It is larger in area than a 'butte' but smaller than a 'plateau'.

MONOLITHS

Large block of stone or anything that resembles one in appearance, intractability, etc. A statue, obelisk, column, etc, cut from one block of stone. A large hollow foundation piece sunk as a caisson and filled with concrete.

MOOR

A tract of unenclosed ground, usually covered with heather, coarse grass, bracken, and moss.

MOUNT

A natural elevation of the earth's surface rising more or less abruptly from the surrounding level, and attaining an altitude which, relative to adjacent elevations, is impressive or notable. In general the elevation of a mountain is more than 300 metres from foot to summit, but this distinction is arbitrary. For reasons of euphony and local usage 'Mount' is usually used when the generic term precedes the specific term and 'Mountain' when it succeeds it.

MOUNTAIN

A large natural elevation of the earth's surface.

MOUNTAIN LAKES

A lake created by an extinct volcanic crater.

MOUNTAIN PEAK

A prominent point of a hill or mountain. The separately named summits on a range of hills or mountains.

MOUNTAIN RANGE

A series or line of mountain or hill ridges with or without peaks, in which the crests are relatively narrow. Its minimum length is about 16 kilometres.

NAVAL ESTABLISHMENT

An institution, pier or building specially designed and equipped for use by the Navy.

NECK

A narrow strip of land; peninsula or isthmus.

NEIGHBOURHOOD

The immediate environment; surroundings. A district where people live. The people in a particular area. Living or situated in and serving the needs of a local area.

OBSERVATORY

An institution or building specially designed and equipped for observing meteorological and astronomical phenomena. Any building or structure providing an extensive view of its surroundings.

OCEAN

A very large stretch of sea. The vast body of water on the surface of the globe that surrounds the land.

PARISH

Territorial division of the state for administrative purposes.

PASS

A depression or gap in a range of mountains or hills permitting easier passage from one side to the other.

PASSAGE

A comparatively deep and narrow waterway affording a passage for a vessel.

PEAK

A prominent point of a hill or mountain. The separately named summits on a range of hills or mountains.

PENINSULA

A piece of land almost surrounded by water, especially one connected with the mainland by only a narrow neck of land or isthmus.

PICNIC AREA

A location to which people bring food to be eaten in the open air.

PINNACLE

The highest point. A towering peak, as of a mountain.

PIT

A large usually deep opening in the ground.

PLAIN

A tract of country the general surface of which is comparatively flat or slightly undulating. In extent generally not less than 2,500 hectares and sparsely, if at all timbered.

PLATEAU

An elevated tract of comparatively flat or level land, having a large part of its total surface at or near the summit level. Its local relief may be very great in cases where it is cut by gorges, or it may have a small local relief like a plain in cases where erosion has not been severe. Its minimum horizontal dimension in any direction generally exceeds 1.6km.

POINT

A location, spot, or position. Point of land. A small promontory.

POND

A pool of still water, often artificially created.

POOL

A small body of still water, usually fresh. A deep part of a stream or river where the water runs very slowly.

PORT

A town or place alongside navigable water with facilities for the loading and unloading of ships.

POST OFFICE

A local office for receiving, distributing and transmitting mail, providing telecommunication services etc.

POWER STATION

An electrical generating station.

PRISON

A place to which persons are legally committed, either while awaiting trial or for punishment.

PUBLIC WATERING PLACE

An artificial waterhole.

RACECOURSE

A place which has been licensed by government for the holding of horse races.

RAILWAY

A permanent track composed of a line of parallel metal rails fixed to sleepers for transport of passengers and goods in trains.

RAILWAY CUTTING

An excavation in a piece of high land for a railway.

RAILWAY LOOP

A short branch off a railway track, often connected at both ends to the main track where trains can pass on a single line stretch of railway. In some cases freight may be handled at these sidings. This designation includes 'Railway Siding'.

RAILWAY SIDING

A short branch off a railway track, often connected at both ends to the main track where trains can pass on a single line stretch of railway. In some cases freight may be handled at these sidings. This designation includes 'Railway Loop'.

RAILWAY STATION

A structure beside a railway line with facilities for passengers and freight.

RAMP

An area set aside for the launching of small water craft, usually paved.

RANGE

A series or line of mountain or hill ridges with or without peaks, in which the crests are relatively narrow. Its minimum length is about 16 kilometres.

RAPIDS

Portions of a stream with accelerated current where it descends rapidly without a break in the slope of the bed sufficient to form a waterfall.

RAVINE

A deep narrow steep sided valley.

REACH

A comparatively straight part of a river or channel between two bends.

REEF

A ridge of rocks or coral lying near the surface of the sea, which may be visible at low tide, but is usually covered by water.

REGION

A region is a relatively large tract of land distinguished by certain common characteristics, natural or cultural. Natural unifying features could include same drainage basin, similar landforms, or climatic conditions, a special flora or fauna, or the like. Cultural determining features could include boundaries proclaimed for administrative purposes, common land use patterns etc.

REGULATOR

Any of various mechanisms or devices such as a governor valve, for controlling fluid flow, pressure, temperature, etc.

RESEARCH STATION

An institution, farm or building specially designed and equipped for carrying out agricultural research.

RESERVE

An area proclaimed to be a public reserve by government legislation.

RESERVOIR

An artificial lake or structure storing water for domestic or other uses.

RIDGE

A long and narrow stretch of elevated ground. It generally has a length less than 16 kilometres.

RIFLE RANGE

An area used for target practice with rifles.

RIVER

A major natural stream in a large catchment basin, carrying water to another river, a lake or the sea. Usually perennial, but not necessarily so in arid areas.

RIVER BEND

A curve in the course of a stream. This designation includes 'meander'.

RIVER CROSSING

A place where a river may be crossed.

RIVER FLAT

A relatively level piece of ground within an area of greater relief; a tract of country without hills and smaller than a plain, caused by the laying down of sediment by a river.

RIVER MOUTH

The area at which a river makes contact with the sea.

RIVULET

A small stream.

ROAD BEND

A bend in a road.

ROADS

An open way, usually surfaced with tarmac or concrete, providing passage from one place to another.

ROADSTEAD

An open anchorage for ships, which may be sufficiently sheltered to give protection from seas, usually by reefs, sandbanks, or islands.

ROCK

A prominent or isolated out crop of rock, or even a single large stone. This designation includes 'boulder' 'crag' 'needle' 'pillar' and 'tor'.

ROCK FACE

An area of exposed rock, generally in a vertical position.

RURAL PLACE

A place, site or precinct in a rural landscape, generally of small extent, the name of which is in current use.

SADDLE

A col or pass or any land form recalling in shape a saddle.

SANDBANK

A bank of sand in a sea or river that may be exposed at low tide.

SANDBAR

A ridge of sand in a river or sea, built up by the action of tides, currents, etc, and often exposed at low tide.

SANDHILL

A mound, ridge or hill of drifted sand either in a desert or along a sea coast, formed by the action of wind.

SANDRIDGE

Sand drifts in long ridges tending parallel to and elongating in the direction of the prevailing winds.

SCHOOL

An establishment for primary or secondary education created by the Education Act.

SCRUB

A vegetation consisting of stunted trees, bushes, and other plants growing in an arid area. An area of arid land covered with such vegetation.

SEA

One of the divisions of the oceans, especially if partly enclosed by land.

SHOAL

A ridge of sand or of rocks just below the surface of the sea or of a river and therefore dangerous to navigation.

SPORTSGROUND

A reserve used for sporting fixtures.

SPRING

A flow of water issuing naturally out of the ground, either continuously or intermittently.

SPUR

A minor linear projection off a range, ridge, mountain, tableland, hill or plateau being generally not more than 2 kilometres in length and decreasing in altitude from the parent feature.

STATE

A major administrative or political division within a country.

STATION

A structure beside a railway line with facilities for passengers and freight.

STEEPS

The very steep and deep sides of a mountain or high plateau.

STRAIT

A comparatively narrow passage connecting two seas or two large bodies of water.

STREAM

Small river, brook. Any steady flow of water or other fluid.

SUBURB

A bounded area within the landscape that has an 'Urban' Character.

SURF BREAK

A permanent obstruction such as a reef, bombora, rock or sandbar which causes waves to break thus making conditions conducive to surfing.

SWAMP

A tract of land normally saturated with water, having little or no drainage and characterised by a growth of grass or reeds. This designation includes 'marsh'.

SYPHON

A tube/pipe placed with one end at a certain level in a body of water and the other in a body of water below this level.

TABLELAND

An elevated tract of land with a generally level surface of considerable extent, generally with a minimum area of 2,500 hectares.

TANK

An artificial waterhole forming a reservoir for rainwater and adjacent run-off.

TERMINAL

A reception or departure building at the terminus of a bus, sea or air transport route.

TERRACE

A level or nearly level strip of land, usually narrow and bordering the sea, a lake or river, lying between a slope upwards to hills on one side and a slope, often abrupt, downwards on the other.

TOPS

The top of a hill

TOWER

A tall usually square or circular structure, sometimes part of a larger building and usually built for a specific purpose.

TOWN

A commercial nucleus offering a wide range of services and a large number of shops, often several of the same type. Depending on size, the residential area can be relatively compact or (in addition) dispersed in clusters on the periphery.

TRACK

A formed and/or marked track that is used by people either walking, cycling or riding a horse. This designation includes 'trails'.

TRAINING WALL

See 'Breakwater'.

TRIG. STATION

A point on the ground, the geographic position of which has been determined by geodetic survey.

TUNNEL

An underground passageway, esp. one for trains or cars. Any passage through or under something.

UNIVERSITY

An institution of higher education having authority to award bachelor and higher degrees, usually having research facilities.

URBAN LOCALITY

Not now recommended, see 'Urban Place'.

URBAN PLACE

A place, site or precinct in an urban landscape, the name of which is in current use, but the limits of which have not been defined under the address locality program.

URBAN VILLAGE

A cohesive populated place in an urban landscape, which may provide a limited range of services to the local area.

VALLEY

Long depression in the land surface, usually containing a river, formed by erosion or by movements in the earth's crust. Any elongated depression resembling a valley.

VILLAGE

A cohesive populated place in a rural landscape, which may provide a limited range of services to the local area. Residential subdivisions are in urban lot sizes.

WATER AERODROME

All licenced aerodromes and government aerodromes maintained by the Federal Airports Corporation, other than those designated 'airports' which have landing facilities on water for sea planes etc.

WATER FEATURE

A feature within water.

WATER SYPHON

See 'Syphon'

WATERFALL

A sudden descent of water over a step in the bed of a stream, the fall being much steeper than in the designation 'rapids'. In place names frequently shortened to 'Fall' or 'Falls'. This designation includes 'cascade' and 'cataract'.

WATERHOLE

A natural hole or hollow containing water, often in the dry bed of an intermittent river.

WEIR

A barrier, erected across a stream to impound and raise the water level for the purpose of maintaining it at the level required for irrigation or navigation purposes.

WELL

A hole or pit dug in the ground to obtain water.

WHARF

A platform alongside of which ships may be secured for loading or unloading cargo or passengers. This designation includes 'pier', 'quay', 'jetty', and 'marina' for those marinas that only have public access.

ITEM 5

LAKE ILLAWARRA ESTUARY MANAGEMENT COMMITTEE – COMMUNITY AND SCIENTIFIC ADVISOR APPOINTMENTS

The draft Lake Illawarra Coastal Management Program (CMP) has been submitted to the NSW Minister for Local Government for certification. The Lake Illawarra Estuary Management Committee (LIEMC) was established to assist in the development of the CMP and is comprised of councillors, community members, Aboriginal community representatives and independent scientific advisors from both Shellharbour and Wollongong City Councils, as well as representatives from key government agencies.

The community representatives and scientific advisor appointments to the LIEMC lapse on 31 October 2020.

The current representatives have expressed interest to continue in their current roles to progress finalisation of the CMP. It is recommended that the current independent scientific advisors and community members for Wollongong City Council, continue with their appointment until 31 August 2021, to align with local government elections.

RECOMMENDATION

- 1 Annie Marlow and Wayne Cook continue to serve as community members on behalf of the Wollongong LGA, on the LIEMC, until 31 August 2021.
- 2 Dr Kerrylee Rogers and Dr Brian Jones continue to serve as independent scientific advisors on the LIEMC until 31 August 2021.
- 3 Council adopt the amended Terms of Reference for the LIEMC in terms of the membership timeframe.

REPORT AUTHORISATIONS

Report of: Chris Stewart, Manager City Strategy

Authorised by: Linda Davis, Director Planning + Environment - Future City + Neighbourhoods

ATTACHMENTS

- 1 Draft Amended Terms of Reference for the Lake Illawarra Estuary Management Committee (LIEMC)

BACKGROUND

The Lake Illawarra EMC is a joint committee of both Wollongong and Shellharbour City Councils. The Lake Illawarra EMC was established to provide advice and support to Wollongong City and Shellharbour City Councils on the preparation of the Lake Illawarra CMP. The EMC operates under a Terms of Reference and comprises people interested in the sustainable management of the health of Lake Illawarra. Membership of the EMC is as follows -

- Three councillors from each of the two Councils
- Two community members from each LGA
- Two Aboriginal community representatives. One nominated by the Shellharbour City Council Aboriginal Advisory Committee and one nominated by the Wollongong City Council Aboriginal Reference Group
- Two independent scientific advisors
- Representatives from each of the following State Government agencies – Office of Environment and Heritage, Department of Industry (Crown Lands), Department of Primary Industries (Fisheries), and Department of Planning

- One representative from the Roads and Maritime Services
- One representative of the South East Local Land Services
- One representative from Sydney Water
- One representative from the Illawarra Local Aboriginal Lands Council.

At its meeting on 29 October 2018, Wollongong City Council resolved to appoint Annie Marlow and Wayne Cook as community representatives as well as Dr Kerrylee Rogers and Dr Brian Jones as the independent scientific advisors, to the Lake Illawarra EMC until 31 October 2019, following a public selection process.

At its meeting on 30 October 2018, Shellharbour City Council similarly resolved to appoint community representatives for the Shellharbour LGA and support the appointment of the aforementioned independent scientific advisors.

On 9 December 2019 and 4 February 2020 both Wollongong and Shellharbour councils endorsed the extension of terms of appointment for the respective community representatives and independent scientific advisors, to the Lake Illawarra EMC, until 31 October 2020.

Following Council endorsement of requested amendments from the NSW Department of Planning, Industry and Environment, it is understood that certification of the Lake Illawarra CMP is imminent, however a precise date is not known. Given the Local Government elections are less than 12 months away, it is proposed to extend the tenure of the current LIEMC members to align with a review of the structure and membership of all Council committees.

PROPOSAL

It is considered appropriate to continue the function of the Lake Illawarra EMC, whilst the draft Lake Illawarra CMP awaits certification. The current members of the Committee have contributed considerable personal knowledge and skills to the development of the CMP and if approved to remain in their positions until the CMP is certified it would allow them see the project through to finalisation and celebrate the milestone.

The current representatives have confirmed their interest in continuing their roles on the Lake Illawarra EMC, via communications with the Committee Secretariat. At the Lake Illawarra EMC meeting on 19 August 2020 the EMC recommended to Wollongong City Council and Shellharbour City Council that the existing community representatives and scientific advisors on the LIEMC until 31 August 2021.

It is therefore recommended that the resolution of Lake Illawarra EMC be supported and the Terms of Reference for the Committee be adopted accordingly, to reflect the new term for the current appointments. Draft revised terms of reference for the Lake Illawarra EMC are attached to this report.

A similar report has been presented to Shellharbour City Council on 29 September 2020.

Upon review of the LIEMC, the stakeholder engagement and consultation arrangements associated with Lake Illawarra CMP implementation will be determined in accordance with proposed action PM2 – Provide ongoing coordinated management of the Lake.

CONSULTATION AND COMMUNICATION

The subject members of the Lake Illawarra EMC have confirmed their interest in continuing their roles on the Lake Illawarra EMC, via communications with the Committee Secretariat. At the Lake Illawarra EMC meeting on 19 August 2020 the EMC recommended to Wollongong City Council and Shellharbour City Council that the existing community representatives and scientific advisors on the LIEMC until 31 August 2021. Wollongong Councillors Ann Martin, Dom Figliomeni and Cath Blakey attended this meeting.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal “Our natural Environment and Waterways are protected and enhanced”. It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
1.1.2 Manage and effectively improve the cleanliness, health and biodiversity of creeks, lakes, waterways and oceans	1.1.2.1 Protect and conserve the health and biodiversity of our waterways and coast	Coordinate the LIEMC

RISK MANAGEMENT

If the proposal is not supported, then the gazettal of the Lake Illawarra Coastal Management Program may be delayed as new committee members are appointed and skills and knowledge acquired.

FINANCIAL IMPLICATIONS

In accordance with Item 12 of the Terms of Reference of the LIEMC, there is no remuneration for members. There are no additional financial implications for Council.

CONCLUSION

The Lake Illawarra EMC has continued to provide advice and support to Wollongong City and Shellharbour City Councils on the preparation of the Lake Illawarra CMP. Under the Terms of Reference the current term for community representatives and independent scientific advisors is due to expire on 31 October 2020.

The draft Lake Illawarra CMP has been submitted to the NSW Minister for Local Government for certification. Whilst certification of the draft Lake Illawarra CMP is awaited it is recommended that Annie Marlow and Wayne Cook continue to serve as community members on behalf of the Wollongong LGA and that Dr Kerry Lee Rogers and Dr Brian Jones continue to serve as independent scientific advisors on the LIEMC, until 31 August 2021.

It is further recommended that the Terms of Reference for the Lake Illawarra EMC be updated to reflect the extension of terms for these representatives, as attached to this report.

TERMS OF REFERENCE

LAKE ILLAWARRA

ESTUARY MANAGEMENT COMMITTEE



1 INTRODUCTION

Lake Illawarra lies in the Local Government Areas of Wollongong City and Shellharbour City Councils. The Lake Illawarra Estuary Management Committee has been established to provide advice and support to Wollongong City and Shellharbour City Councils on the preparation and implementation of a strategic Coastal Management Program for Lake Illawarra. The Committee comprises people interested in the sustainable management of the health of Lake Illawarra.

2 AUTHORITY

The Lake Illawarra Estuary Management Committee will provide advice, feedback and support to Wollongong City and Shellharbour City Councils in developing, implementing and monitoring a Coastal Management Program and projects for Lake Illawarra, that are primarily focused on protecting estuary health.

The Committee does not have decision making authority, the power to bind the two Councils or the power to incur expenditure.

3 RESPONSIBILITIES AND FUNCTIONS

The responsibilities and functions of the Lake Illawarra Estuary Management Committee are to:

- assist Wollongong City and Shellharbour City Councils to develop a Coastal Management Program for Lake Illawarra in accordance with the NSW Government guidelines;
- develop a better understanding of estuary health and identify issues which need to be addressed;
- assist in developing suitable strategies to address estuary and coastal zone management issues;
- monitor and evaluate the implementation of the Coastal Management Program; and
- provide advice on planning proposals and major capital works proposed to take place in the Lake Illawarra catchment.

Routine operational and maintenance matters relating to Lake Illawarra will not be the business of the Lake Illawarra Estuary Management Committee. These matters are to be followed up using the Customer Service functions operating at Wollongong City and Shellharbour City Councils.

4 PRIORITIES

The immediate priority of the Lake Illawarra Estuary Management Committee is to oversee the completion of the Lake Illawarra Coastal Management Program in accordance with NSW Government guidelines. Upon completion, the Lake Illawarra Coastal Management Program shall be submitted to both Councils for adoption, and implementation in line with their business priorities.

5 COMPOSITION OF THE LAKE ILLAWARRA ESTUARY MANAGEMENT COMMITTEE

The Lake Illawarra Estuary Management Committee is proposed to be made up of:

- THREE (3) councillors from each of the two Councils. One of the three Wollongong councillors is expected to be a member of its Estuary and Coastal Zone Management Committee;
- TWO (2) community members from each LGA;
- TWO (2) Aboriginal community representatives. One nominated by the Shellharbour City Council Aboriginal Advisory Committee and one nominated by the Wollongong City Council Aboriginal Reference Group;
- TWO (2) independent scientific advisors;
- Representatives from each of the following State Government agencies – Office of Environment and Heritage, Department of Industry (Crown Lands), Department of Primary Industries (Fisheries), and Department of Planning;
- one representative from the Roads and Maritime Services;

TERMS OF REFERENCE

LAKE ILLAWARRA

ESTUARY MANAGEMENT COMMITTEE



- one representative of the South East Local Land Services;
- one representative from Sydney Water; and
- one representative from the Illawarra Local Aboriginal Lands Council.

Hosting of the Committee will alternate between the two Councils annually, and the Chairperson will be appointed by the host Council from its councillor representatives.

Vacancies that occur on the Committee will be filled by nomination.

Staff from the two councils will attend meetings as observers, to provide information to the Committee or to fulfil an administrative function (eg taking minutes and/or distributing minutes within the two Councils and to Committee members). These individuals will act as ex-officio members. Administrative support for the Committee will be provided by the host Council staff.

Term of appointment for the Committee is until 31 August 2021.

6 APPOINTMENT OF MEMBERS

Councillors

Three councillors each are to be appointed by the Wollongong City and Shellharbour City Councils.

Community Members

The community member positions available for each LGA will be advertised by the respective Council at the start of the Committee's term. The applications will be assessed by the respective Council staff and recommendations made for appointment by their corresponding Council. The selection criteria for community members will be:

- Demonstrated interest in and knowledge of estuary management issues.
- Demonstrated ability to dedicate time to attend meetings and perform tasks related to committee business.
- Demonstrated ability to contribute positively and constructively within an agreed management framework.
- Demonstrated contact with a cross-section of the local community for the purpose of passing on information and receiving feedback.
- Resident/rate payer in the LGA for which they are applying.

The community members may be eligible for re-appointment to the Committee, following the expiration of their term, by registering their interest for re-appointment with the Committee chairperson and receiving approval of appointment from the respective Council.

A community member may resign from the Committee at any time by advising in writing to the Committee chairperson. Council may directly appoint a community member to fill the vacancy for the remainder of the term.

Aboriginal Community Representatives

The Aboriginal community representatives will be appointed after one nomination is received from the Shellharbour City Council Aboriginal Advisory Committee and one nomination is received from the Wollongong City Council Aboriginal Reference Group.

TERMS OF REFERENCE

LAKE ILLAWARRA

ESTUARY MANAGEMENT COMMITTEE



Independent Scientific Advisors

The Independent Scientific Advisor positions will be advertised by the Council hosting the Committee at the start of its term. The applications received will be assessed jointly by staff from the two Councils and a recommendation made for endorsement by both Councils. The selection criteria for the Independent Scientific Advisors will be:

- High-level experience in research or management of estuaries.
- Demonstrated ability to dedicate time to attend meetings and perform tasks related to committee business
- Demonstrated ability to provide sound scientific advice and a high level of personal commitment while engaged in an honorary position.
- Independence from Council, government, developers or any other group with a vested interest in the management of estuaries.

The Independent Scientific Advisors may be eligible for re-appointment to the Committee, following the expiration of their term, by registering their interest for re-appointment with the Committee chairperson and receiving approval of appointment from the respective Council.

State Agency Representatives

The host Council at the start of the Committee's term will invite the State Agencies listed to nominate a representative to the Committee.

7 OBLIGATIONS OF MEMBERS

Members of the Lake Illawarra Estuary Management Committee, in performing their duties, shall:

- Act honestly and in good faith;
- Participate in the work of the Committee;
- Perform their duties in a manner that ensures public trust in the integrity, objectivity, and impartiality of the Committee;
- Exercise the care, diligence and skill that would be expected of a reasonable person;
- Comply with the Committee's Terms of Reference; and
- Comply with the Model Code of Conduct for Local Councils in NSW – Code of Conduct for Council Committee Members, Delegates of Council and Council Advisors 2018.

8 MEETINGS AND MINUTES

The Committee shall meet at least four times a year to progress the work involved in the Lake Illawarra Estuary Management Process.

A quorum will consist of half plus one of the Committee members, including at least one elected representative (councillor) from each Council.

Meetings will be chaired by the Council appointed chairperson. If the chairperson is absent from a meeting, the first business of every such meeting is to elect a chairperson from the members present to preside over such meeting.

The Lake Illawarra Estuary Management Committee has an advisory role to the two Councils and will make recommendations by consensus. In the absence of consensus, advice from the Lake Illawarra Estuary Management Committee may be presented with supporting and dissenting views of members.

Meeting agendas will be distributed at least one week prior to the meeting.

TERMS OF REFERENCE

LAKE ILLAWARRA

ESTUARY MANAGEMENT COMMITTEE



9 REPORTS

The minutes of the Lake Illawarra Estuary Management Committee meetings will be provided to all Councillors and executive management of both Councils for information. Minutes will also be distributed to all Lake Illawarra Estuary Management Committee members.

Advice and recommendations of the Lake Illawarra Estuary Management Committee relating to specific Council projects will be reported to the two Councils as part of their project reporting process.

Any matters arising that require a separate decision of one or both Councils may be reported to the respective Council(s) by managerial staff at their discretion.

10 EVALUATION AND REVIEW

A review of the Lake Illawarra Estuary Management Committee will be undertaken every 12 months to ensure the purpose, membership and operation of the Committee is effective and to make appropriate changes.

11 COMMITTEE RESOURCING

Resources required to progress the work of the Committee will be considered and made available by one or both Councils, in line with their budgetary constraints and other business priorities. Where both Councils agree to contribute to a program or works, the proportion of funding will be negotiated at 2:1 Wollongong to Shellharbour. External grant funding may be sought to supplement council contributions.

12 REMUNERATION AND EXPENSES

There is no remuneration for members.

Reasonable expenses incurred by the Lake Illawarra Estuary Management Committee members in relation to their responsibilities as members of the Committee will be met by prior approval. These expenses should relate directly to tasks completed for the Lake Illawarra Estuary Management Committee business and will be reimbursed at the discretion of one or both Councils.

ITEM 6

APPOINTMENT OF INDEPENDENT MEMBER TO THE AUDIT, RISK AND IMPROVEMENT COMMITTEE

This report recommends the appointment of an independent member to the Audit, Risk and Improvement Committee (ARIC) to fill a vacancy arising from the conclusion of the current appointment of Mr Terry Clout.

RECOMMENDATION

- 1 Approve the appointment of Ms Donna Rygate to the Audit, Risk and Improvement Committee for an initial term of three years, expiring on 31 October 2023.
- 2 Thank Mr Terry Clout for his service and advice to Council as an independent member and Chairperson of the Committee.

REPORT AUTHORISATIONS

Report of: Todd Hopwood, Manager Governance and Customer Service
Authorised by: Renee Campbell, Director Corporate Services - Connected + Engaged City

ATTACHMENTS

There are no attachments for this report.

BACKGROUND

Following the conclusion of the term of appointment of Mr Terry Clout, Council invited Expressions of Interest from suitably experienced and qualified persons to fill the independent member vacancy on the Committee.

A total of 28 submissions were received from interested persons.

PROPOSAL

A selection panel was appointed to consider the applications received. The selection panel comprised Cathy Hudson (ARIC independent member), Cr Mithra Cox (ARIC Councillor member), Renee Campbell (Director Corporate Services), and Todd Hopwood (Manager Governance and Customer Service).

The selection panel invited a shortlist of five applicants to interview, and following the interview process the panel unanimously endorsed that Donna Rygate be recommended for appointment as an independent member of the Audit, Risk and Improvement Committee for an initial term of three years.

Ms Rygate has a strong background in public sector management including previously holding the Chief Executive Officer roles with Local Government NSW and with the NSW Office of Communities, as well as senior executive roles with a number of NSW Government agencies. Ms Rygate is an experienced member of Audit Risk and Improvement Committees and is currently a member of the Audit and Risk Committees of NSW Local Land Services, City of Parramatta, Mid Coast Council, Blayney Council, Cabonne Council, Central Tablelands Water, and Shoalhaven City Council.

As Mr Clout also held the role of ARIC Chairperson, the Chairperson role will also need to be appointed once the existing vacancy is filled. The ARIC Charter states that the voting members of the Committee will endorse one of the independent members as chairperson for an initial term of at least three years, with a maximum period of five years.

Once the replacement member for the current vacancy has been confirmed, the voting members of the ARIC will meet to endorse an independent member as Chairperson. A report will be provided to Council to confirm the appointment by resolution.

CONSULTATION AND COMMUNICATION

Appropriate reference checks were undertaken by the selection panel and no issues or areas of concern were raised, and those parties contacted confirmed Ms Rygate's professionalism and strong knowledge of the functions of Audit Risk and Improvement Committees.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 Goal 4.3 "Our council is accountable, financially sustainable and has the trust of the community".

It specifically delivers on core business activities as detailed in the Governance and Customer Service, Service Plan 2020-21.

CONCLUSION

The Audit Risk and Improvement Committee is a key component of Council's governance framework. The objective of the ARIC is to provide independent assurance and advice to the General Manager and Council by overseeing and monitoring Council's governance, risk and control frameworks and its external accountability requirements. Filling the current vacancy on the Committee with a new independent external member will ensure that these important oversight functions continue to occur.

ITEM 7

TENDER T20/21 - ADMINISTRATION BUILDING - INSTALLATION OF SOLAR PANELS ON COUNCIL CAR PARK

This report recommends acceptance of a tender for the Design, Supply, Construction, Commissioning and Scheduled Maintenance of a new solar panel installation to be built on the upper levels of the existing Wollongong City Council multi-storey car park situated at the rear of 41 Burelli Street, Wollongong in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

The purpose of the works is to contribute towards the aspirational greenhouse gas emissions reduction target of net zero emissions by 2030 for Council's organisational operations. The project has also been evaluated to provide an economic return over its life.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Autonomous Energy Pty Ltd for the Administration Building Solar Car Park, in the sum of \$924,677.70, excluding GST noting a proposed variation of \$7,599.00 excluding GST post-award, in accordance with clause 176 of the Regulations.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan

BACKGROUND

This project has been developed as a component of Council's aspirational greenhouse gas emissions reduction target of net zero emissions by 2030 for Council's organisational operations. The project scope has been developed from feasibility analysis that indicates that there will be an economic return for the proposed new asset over its life.

The project involves the design, construction and first year's maintenance of steel support structure, solar array capacity of approximately 220kWp, car park lighting, electrical distribution boards, cabling and connections into Council's Administration Building.

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on 1 September 2020.

Seven (7) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures and comprising representatives of the Project Delivery, Infrastructure, Strategy and Planning, City Works and Governance and Customer Service Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope
- 2 Financial assessment acceptable to Council which demonstrates the tenderer's financial capacity to undertake the works
- 3 Clean Energy Council Accreditation

Assessable Criteria

- 1 Cost to Council – 35%
- 2 Appreciation of scope of works and concept design – 20%
- 3 Demonstrated prior experience and satisfactory performance in undertaking project of similar size, scope and risk profile – 10%
- 4 Staff qualification and experience – 5%
- 5 Proposed Subcontractors – 5%
- 6 Project Schedule – 10%
- 7 Workplace Health and Safety Management System and Environmental Management Policies and Procedures – 5%
- 8 Demonstrated strengthening of local economic capacity – 10%

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Autonomous Energy Pty Ltd	1
Smart Commercial Solar Pty Ltd	2
Ecosave Pty Ltd	3
Harvey Norman Commercial Division Solar ®	Non-Conforming
Inspire Energy	Non-Conforming
Solgen Energy	Non-Conforming
Level 2 West Electrical	Non-Conforming

PROPOSAL

Council should authorise the engagement of Autonomous Energy Pty Ltd to carry out the Design, Supply, Construction, Commissioning and Scheduled Maintenance of a new solar panel installation to be built on the upper levels of the existing Wollongong City Council multi-storey car park situated at the rear of 41 Burelli Street, Wollongong in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

An acceptable financial capability assessment has been received in relation to the recommended tenderer.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 1 “We value and protect our environment”. It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
1.5.1 Participate in the Global Covenant of Mayors and set emissions reduction targets for the LGA	1.5.1.1 Set an emission reduction target and carry out actions to reduce greenhouse gas emissions through the Global Covenant of Mayors	Deliver commitments made under the Global Covenant of Mayors and support Council's climate emergency declaration

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered medium on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works is considered low based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

SUSTAINABILITY IMPLICATIONS

This project directly contributes to Council's aspirational greenhouse gas emissions reduction target of net zero emissions by 2030 by reducing energy consumption of Council's Administration Building located at 41 Burelli Street, Wollongong.

Feasibility analysis has been undertaken which indicates an economic return on the project.

The procurement process has incorporated and assessed concept designs from industry leaders in solar shade structures to support innovation in delivering a value for money solution.

FINANCIAL IMPLICATIONS

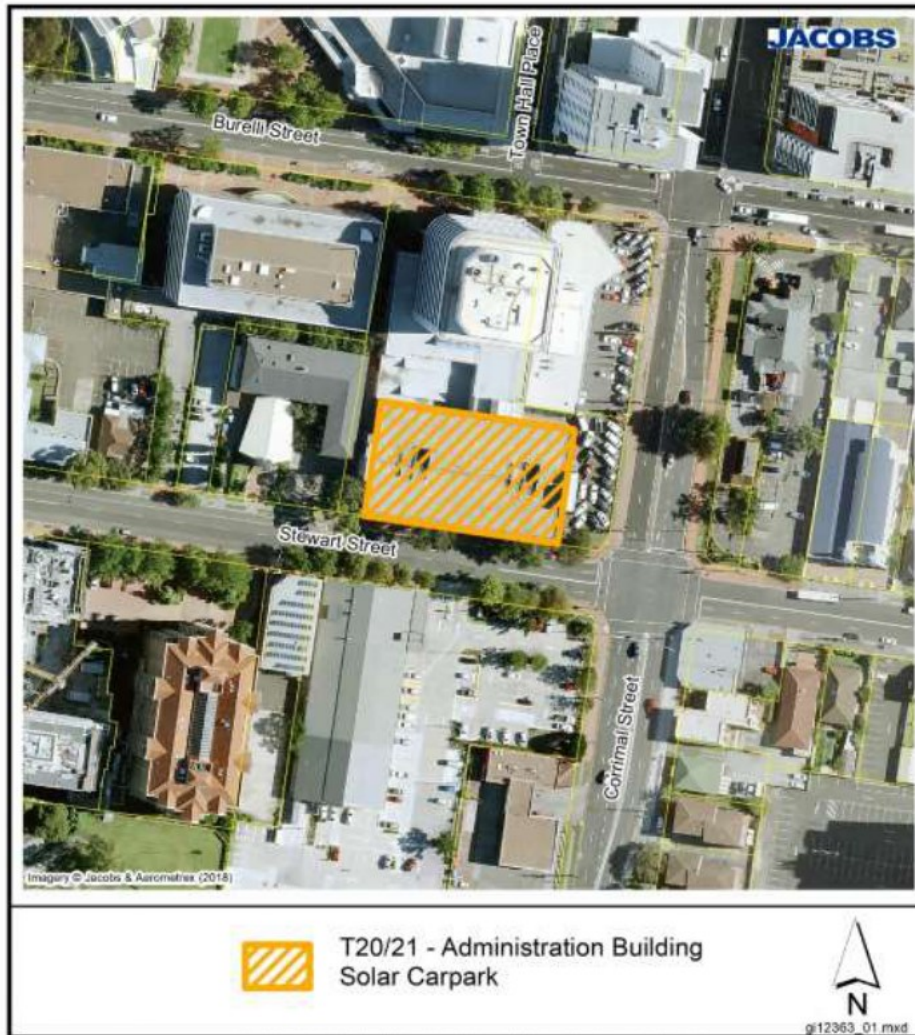
It is proposed that the total project be funded from the following source/s as identified in the Annual Plan –

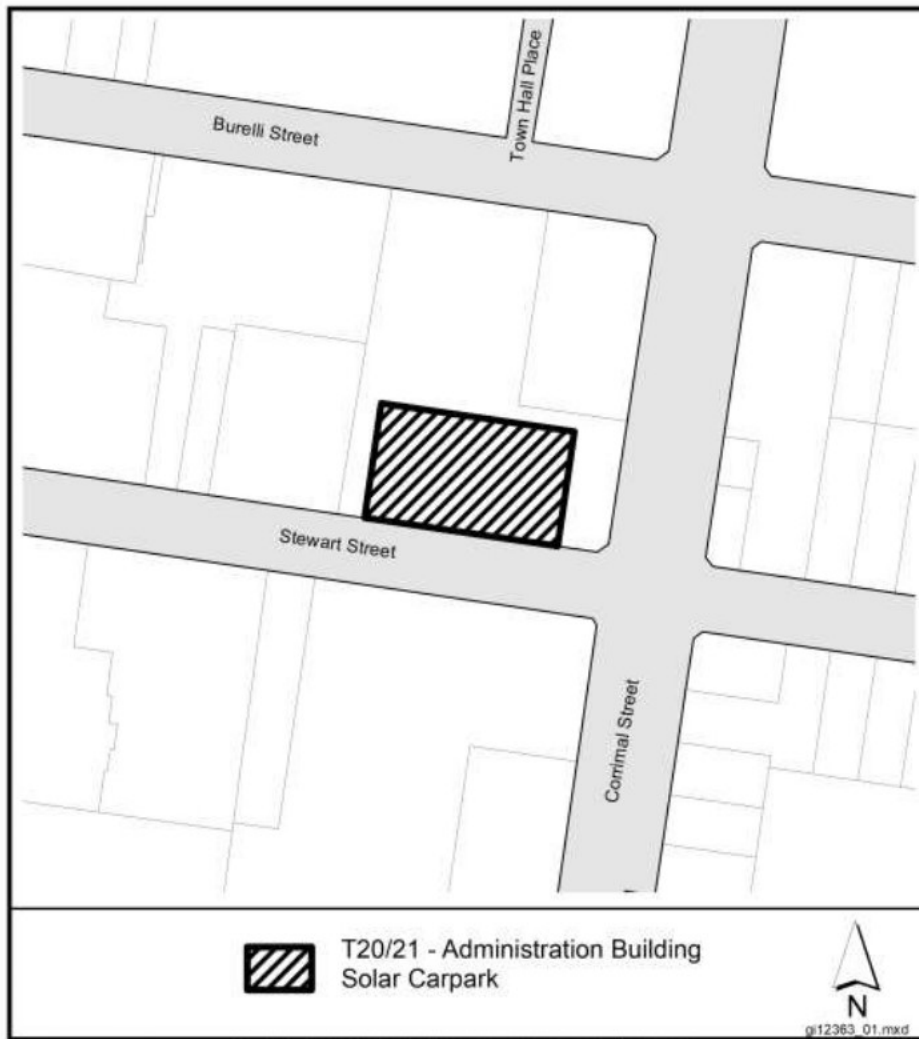
Capital Budget 2020/21

It is recommended Council accept the tender of Autonomous Energy Pty Ltd, acknowledging a variation to the tendered price will be required post-award in the amount of \$7,599.00 excluding GST to allow for adjustment to an anomaly in the tender as permitted under Clause 176 of the *Local Government (General) Regulations 2005*.

CONCLUSION

Autonomous Energy Pty Ltd has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





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ITEM 8

TENDER T20/23 - BULLI COMMUNITY CENTRE ACCESSIBLE TOILET, ACCESS RAMP AND INTERNAL AND EXTERNAL PAINTING WORKS

This report recommends acceptance of a tender for Bulli Community Centre upgrades including a new accessible toilet, access ramp, lighting, internal and external painting works in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

Modifications to the building are required to facilitate access to Bulli Community Centre in accordance with Council's commitment to create an accessible city through the Disability Inclusion Action Plan and to comply with relevant Australian Standards for access and mobility and the Disability Discrimination Act 1992.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Batmac Constructions Pty Ltd for Bulli Community Centre upgrades to accessible toilet, access ramp and internal/external painting works, in the sum of \$337,766.71 excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan

BACKGROUND

Modifications to the Bulli Community Centre are required to facilitate access to the building in accordance with Council's commitment to create an accessible city through the Disability Inclusion Action Plan and to comply with relevant Australian Standards for access and mobility and the Disability Discrimination Act 1992. Works include the delivery of a new accessible toilet (including removal of asbestos), installation of a compliant access ramp at the front entrance, lighting, removal of redundant laybacks on the Highway to improve gradings of the footpath and painting (both internally and externally).

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on 1 September 2020.

Twelve (12) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures and comprising representatives of the Project Delivery, Library and Community Services and Governance and Customer Service Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope

- 2 Financial assessment which demonstrates the tenderer's financial capacity to undertake the works if required
- 3 Satisfactory Work Health and Safety Management Systems
- 4 Site Inspection

Assessable Criteria

- 1 Cost to Council – 40%
- 2 Appreciation of scope of works and construction methodology – 15%
- 3 Experience and satisfactory performance in undertaking projects of similar size, scope and risk profile, including staff qualification and experience – 15%
- 4 Proposed sub-contractors – 5%
- 5 Project Schedule – 5%
- 6 Work Health and Safety and Environmental Management Systems – 10%
- 7 Demonstrated strengthening of local economic capacity – 10%

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Batmac Constructions Pty Ltd	1
Direct Projects Pty Ltd	2
Sullivans Constructions	3
Malsave Pty Ltd	4
Davone Constructions Pty Ltd	5
Assett Group Services	6
Advanced Constructions Pty Ltd	7
Specbuilt Constructions Pty Ltd	8
Maestro Group Building and Construction	9
AWS Services Sydney Pty Ltd	NON-CONFORMING
Programmed Building Projects	NON-CONFORMING
VBuilt Construction Pty Ltd	NON-CONFORMING

PROPOSAL

Council should authorise the engagement of Batmac Constructions Pty Ltd to carry out the Bulli Community Centre upgrade works including accessible toilet, access ramp and internal and external painting works in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

During preparation of these design plans, Council consulted with the manager and end users of this facility regarding the scope and staging of these works. Council's Heritage Officers were also involved to ensure the works do not impact on significant items of this Local Heritage Listed building.

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 5 "We have a healthy community in a liveable city". It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
5.5.1 Public facilities in key locations and transport routes are maintained and clean, accessible and inviting to our community and visitors	5.5.1.3 Coordinate an access improvement program through pre-planning and renewal activities	Provide advice on access related matters that reflect the priorities in Council's Disability Inclusion Action Plan 2020-25

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works is considered low based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

SUSTAINABILITY IMPLICATIONS

The following sustainability implications have been considered:

- Specification of paint systems that contain low Volatile Organic Compounds (VOC)
- Specification of fittings and fixtures that have minimum water efficiency and labelling standards (WELS) 'star' ratings
- Specification of sourcing structural and reinforcing steel from a responsible steel maker
- Specification of a maximum amount of demolition and construction waste being sent to landfill based on building floor area
- Specification of the use of PVC meeting best practice guidelines or the use of products that do not contain PVC
- Sourcing of materials, plants and soils within a maximum distance of the site

FINANCIAL IMPLICATIONS

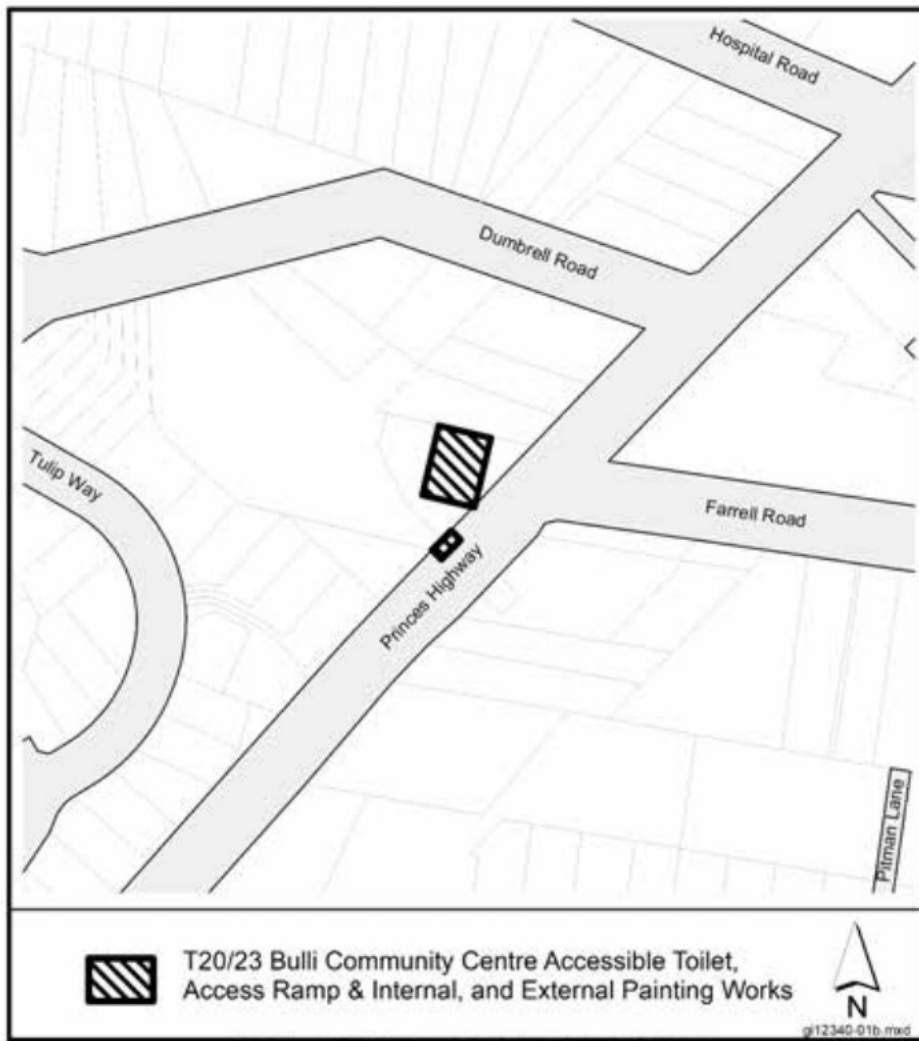
It is proposed that the total project be funded from the following source/s as identified in the Operational Plan –

2020/21 Capital Budget
2020/21 Maintenance Budget

CONCLUSION

Batmac Constructions Pty Ltd has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





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ITEM 9 TENDER T20/25 - PUCKEYS ESTATE BOARDWALKS AND BRIDGE REPAIRS

This report recommends acceptance of a tender for Puckeys Estate Boardwalks and Bridge Repairs in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

Tenders were sought from experienced contractors for the replacement of the bridge over the Towradgi Arm of Puckeys Lagoon, the repair of the boardwalk between the bridge and Squires Way and the repair of the smaller boardwalk on the lower walking track at the northern end of Puckeys Estate, close to Elliotts Road and Fairy Meadow Beach Reserve. These works will ensure the community continues to enjoy this walking track through the popular coastal nature reserve.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Cadifern Civil for Puckeys Estate Boardwalks and Bridge Repairs, in the sum of \$718,899.00, excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan

BACKGROUND

Puckeys Estate is a green space that spans the area between Fairy Meadow Beach to the east, Fairy Creek and Puckeys Lagoon in the south, Squires Way to the west and Elliotts Road, Fairy Meadow Beach Reserve and Thomas Dalton Park in the north.

The Estate has a highly utilised walking track that runs through it from the north to the south. This track includes two (2) timber boardwalks and a timber bridge. These boardwalks and the bridge have deteriorated over time and require repairs/replacement to maintain and improve the functionality and safety to users.

The scope of works associated with this project include:

- The removal and replacement of the bridge (and piling) from the east to the west over the Towradgi Arm waterway in the south. The installation of replacement piling will be from equipment loaded on a barge launched from the southern side of the lagoon near the car park.
- The repair of the boardwalk from the bridge to Squires Way
- The repair of the boardwalk in the north of the reserve near Fairy Meadow Beach Reserve
- The existing timber deck and handrails will be replaced with more durable fibre reinforced polymer (FRP) decking and stainless-steel handrails.

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on Tuesday, 18 August 2020.

Ten (10) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures and comprising representatives of the Project Delivery, Infrastructure, Strategy and Planning and Governance and Customer Service Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope
- 2 Financial assessment acceptable to Council which demonstrates the tenderer's financial capacity to undertake the works

Assessable Criteria

- 1 Cost to Council – 40%
- 2 Appreciation of scope of works and construction methodology – 12.5%
- 3 Experience and satisfactory performance in undertaking projects of similar size, scope and risk profile – 10%
- 4 Staff qualifications and experience – 5%
- 5 Proposed sub-contractors – 5%
- 6 Project Schedule – 7.5%
- 7 capacity Environmental Management Policies and Procedures – 5%
- 8 Workplace Health and Safety Management System – 5%
- 9 Demonstrated strengthening of local economic – 10%

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Cadifern Civil	1
Select Civil Pty Ltd	2
Zauner Construction	3
Davone Constructions Pty Ltd	4
Joss Facility Management	5
Diverse Civil Contracting Pty Ltd	6
Dynamic Civil Pty Ltd	7
PETTRO Engineering Pty Ltd	8
Fulton Hogan Ltd	9
Kenpass Pty Ltd	10

PROPOSAL

Council should authorise the engagement of Cadifern Civil to carry out the Puckeys Estate Boardwalks and Bridge Repairs in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

An acceptable financial capability assessment has been received in relation to the recommended tenderer.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

Identified and known users of the Puckeys Estate were notified of the upcoming works which requires closure of the walking track during construction. During construction users will be diverted along the Squires Way cycleway. Appropriate signage and media will be used to inform the general public of the closure when construction is about to commence.

The following groups were consulted during the tender process:

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 5 "We have a healthy community in a liveable city".

It specifically delivers on core business activities as detailed in the Infrastructure Planning and Support Service Plan 2020-21.

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works or services is considered medium based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

There are safety, financial and reputational risks that should be considered if Council does not repair the boardwalks and bridge. Increased deterioration and possible failure are risks which would remain if repair works are not completed. Consequently, the safety of the public utilising the boardwalks and bridge would be at risk. Possible environmental harm caused by such a failure is included in that risk.

SUSTAINABILITY IMPLICATIONS

Economic sustainability implications are considered relevant to the repair of the boardwalks and bridge within Puckeys Estate to reduce ongoing maintenance costs of the boardwalks and bridge and to improve the amenity of the area. This would lead to increased use by both local and out of area users, thereby increasing the health and wellbeing of our community.

The proposal submitted by the tenderer includes provisions for recycling and reuse of the existing timbers that are to be removed and replaced. It is proposed that the replaced timber will be docked and reused as recycled timber in a wide range of applications.

The design for the repair works incorporates Fibre Reinforced Polymer (FRP) members for the new boardwalks and bridge decking. These will provide a long-term, low maintenance finished product that also enhances the aesthetic appearance of the facility.

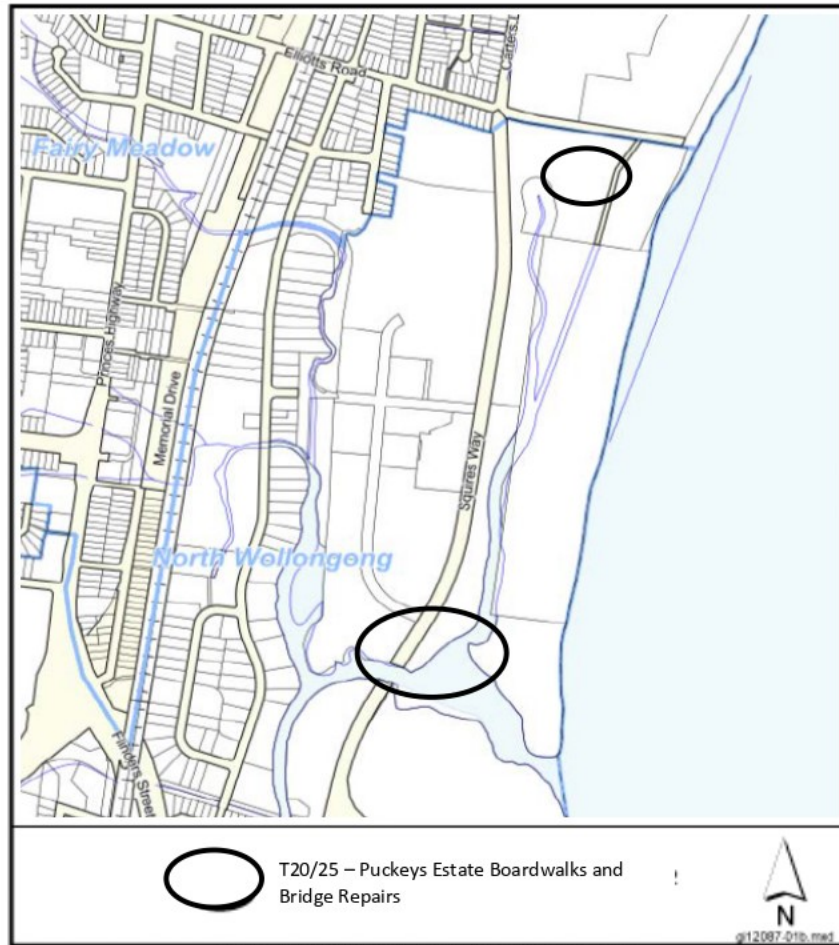
FINANCIAL IMPLICATIONS

It is proposed that the total project be funded from the following source/s as identified in the Operational Plan –

2020/21 Capital Budget

CONCLUSION

Cadifern Civil has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





ITEM 10

TENDER T20/28 - ROOF REPLACEMENT - ELIZABETH PARK, BELLAMBI SOCCER CLUBHOUSE AND OCEAN PARK, WOONONA SOCCER CLUBHOUSE

This report recommends acceptance of a tender for replacement of the roof sheeting and associated works at both Elizabeth Park, Bellambi Soccer Clubhouse/Amenities and Ocean Park, Woonona Soccer Clubhouse in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

Both Bellambi and Woonona Soccer Clubhouse existing roofs have reached the end of their serviceable lives and require urgent replacement to ensure these assets continue to reliably service their user groups into the future.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Batmac Constructions Pty Ltd for Roof Replacements for both Elizabeth Park, Bellambi Soccer Clubhouse and Ocean Park, Woonona Soccer Clubhouse, in the sum of \$444,513.94, excluding GST.
- 2 Council delegates to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan - Bellambi Soccer Clubhouse
- 2 Location Plan - Ocean Park Clubhouse

BACKGROUND

During an asset condition inspection of the roofs at the Bellambi Soccer Clubhouse (located in Elizabeth Park off Cawley Street, Bellambi) and Ocean Park Soccer Clubhouse (located off Carrington Street, Woonona), it was determined that the existing roofs had reached the end of their serviceable lives. These inspections noted evidence of corrosion of sheeting, bracing straps, failure of fixing screws, damage to fittings and fixtures and evidence of leaking during rainfall events at one or both sites. It was considered a priority to replace the roofing on both structures to ensure Council maintains the clubhouses at a serviceable standard to support the local sporting groups who use these facilities.

The scope of works for both clubhouses are similar which includes replacement of all roof sheeting, asbestos removal (if required), roof mesh, insulation, flashings, capping, roof screws and associated fittings. The works include the installation of new gutters, downpipes, rain diverters, new fascia's and over flashing of all roof penetrations. The expected outcomes for both structures will be kept in a serviceable condition for another 20-years, which in turn will provide support to the leaseholders and the broader local community.

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on Wednesday, 16 September 2020.

Nine (9) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies

and Procedures and comprising representatives of the Project Delivery and Governance and Customer Service Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope.
- 2 Financial assessment acceptable to Council which demonstrates the tenderer's financial capacity to undertake the works.
- 3 Site Inspections for the sole purpose of allowing tenderers to view the sites of the proposed works or services at both Bellambi Soccer Clubhouse and Ocean Park Soccer Clubhouse, Woonona.
- 4 WHS Management Systems – with a minimum Safety Management Policy and WHS Management System Manual or Plan.

Assessable Criteria

- 1 Cost to Council – 45%
- 2 Appreciation of scope of works and construction methodology – 15%
- 3 Experience and satisfactory performance in undertaking projects of similar size, scope and risk profile including staff qualifications and experience – 15%
- 4 Proposed sub-contractors – 5%
- 5 Project Schedule – 5%
- 6 Demonstrated strengthening of local economic capacity – 10%
- 7 Workplace Health and Safety and Environment Management Systems – 5%.

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Batmac Constructions Pty Ltd	1
Project Coordination (Australia) Pty Ltd	2
Malsave Pty Ltd	3
RJ McMillan Plumbing Services Pty Ltd	4
Royal Contractors	5
Momentum Built Pty Ltd	6
Colin Joss & Co Pty Ltd	7
RMA Contracting Pty Ltd	8
TL Building Pty Ltd	9

PROPOSAL

Council should authorise the engagement of Batmac Constructions Pty Ltd to carry out the roof replacements at both Bellambi Soccer Clubhouse and Ocean Park Soccer Clubhouse, Woonona in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

An acceptable financial capability assessment has been received in relation to the recommended tenderer.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 5 "We have a healthy community in a liveable city". It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
5.1.4 Urban areas are created to provide a healthy and safe living environment for our community	5.1.4.1 Provide an appropriate and sustainable range of quality passive and active open spaces and facilities	Pursue key actions outlined in 2017-2021 Sports Ground and Sporting Facilities Strategy.

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works is considered low based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

SUSTAINABILITY IMPLICATIONS

The following sustainability implications have been considered:

- Elements of this refurbishment have been designed in accordance with the principles set out in the Sustainable Building Strategy.
- Sustainable procurement by providing open tender allowing local companies the opportunity to tender for the work.
- Tender assessment incorporated weightings to support the ongoing economic development of the region including the source and supply of local labour and materials.

FINANCIAL IMPLICATIONS

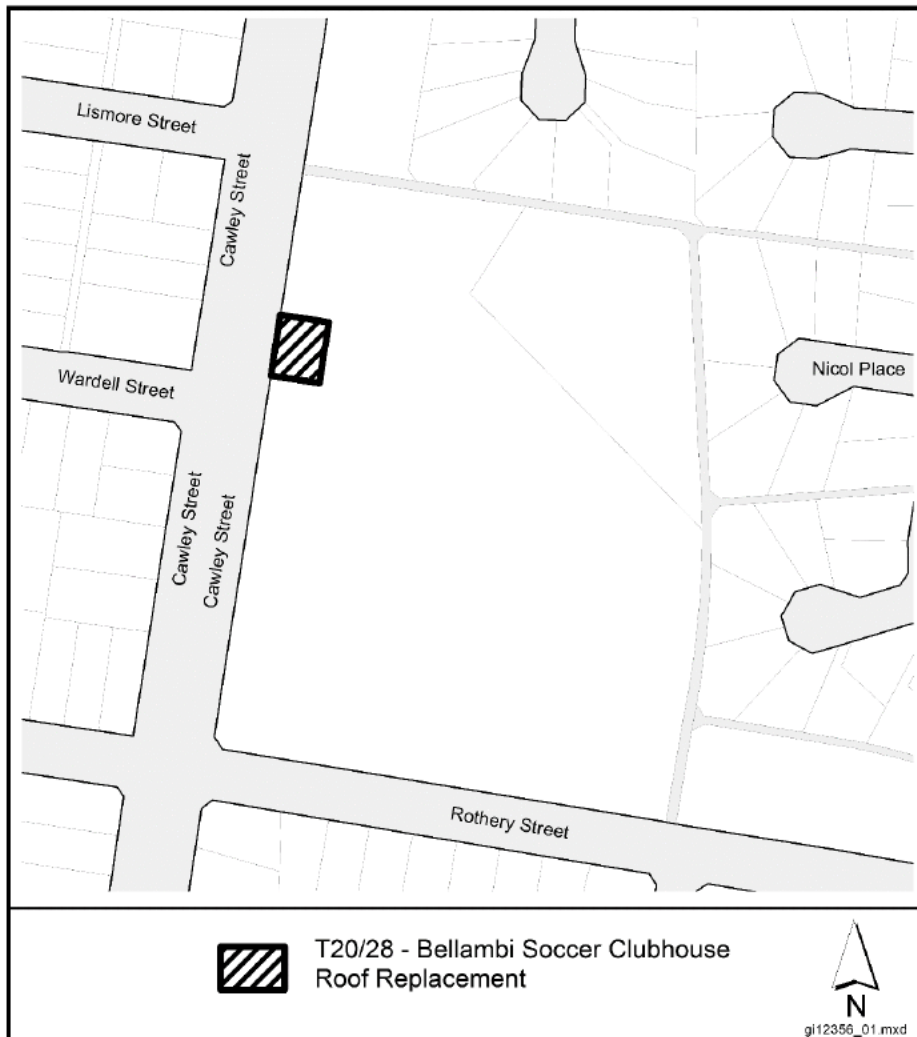
It is proposed that the total project be funded from the following source/s as identified in the Annual Plan -

2020/21 Capital Budget

CONCLUSION

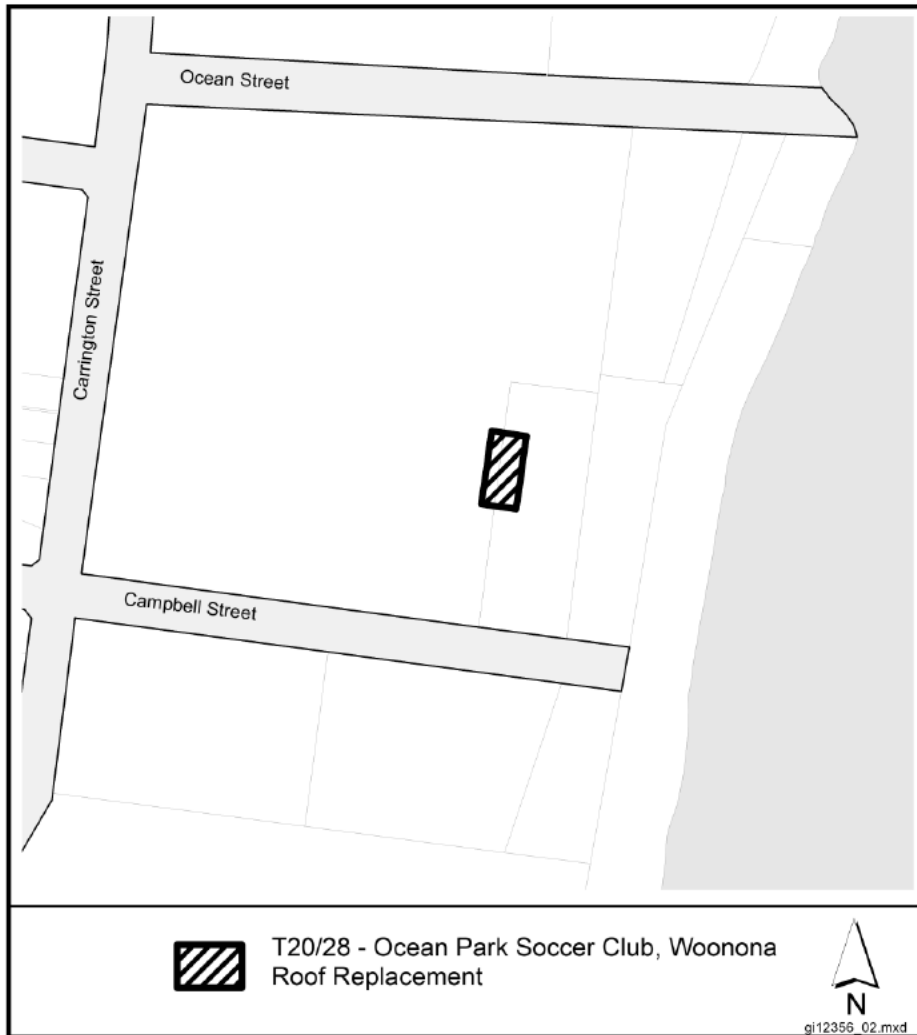
Batmac Constructions Pty Ltd has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





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ITEM 11

TENDER T20/31 - WOLLONGONG TOWN HALL - ROOF TRUSS STRENGTHENING AND RIGGING UPGRADES

This report recommends acceptance of a tender for Wollongong Town Hall Roof Truss Strengthening and Rigging Upgrades in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

The purpose of these works is to undertake strengthening works to the existing timber roof trusses, provide supports and bracing to openings within a brick load bearing wall, install dedicated rigging points including installation of metallic safe working load signage to nominated lifting points on rigging beams for (speakers/lighting etc). This will facilitate the ongoing operational use of the Wollongong Town Hall.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Colin Joss & Co Pty Ltd trading as Joss Facility Management for Wollongong Town Hall Roof Truss Strengthening and Rigging Upgrades, in the sum of \$342,961.11, excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan

BACKGROUND

Council engineers were requested to assess the structural capacity of the existing roof trusses to carry rigging loads for suspended lighting and sound systems within the Wollongong Town Hall. During site inspections and subsequent initial analysis, it was noted that the existing trusses had suffered some damage to various truss elements resulting in splitting of sections, opening of connections and damage to some members. This damage appeared to have occurred over a period of time due to a number of causes, including modifications of the original bracing during replacement of the original ceiling, installation of various rigging points, modifications made during installation of air conditioning ductwork and subsequent additional loading due to installation of recent smoke extraction units.

As this raised some issues with relation to the roof structure, Council immediately removed all existing rigging and placed operational limits on future installations to address this risk. Specialist consulting engineers were then engaged to inspect the trusses, grade the existing timber used, undertake a structural review and prepare construction documentation to restore the structural capacity of the existing roof structure. The structural modifications were also designed to provide for the future installation of solar panels on nominated areas of the Town hall roof.

The works under this proposed contract will include:

- Provision of temporary construction access into the roof space through the ceiling and supply and installation of temporary construction access flooring within the roof space
- Strengthening works to the existing timber roof trusses, (i.e. use of additional steel plates, angles, hanging rods and bracing members)

- Supports and bracing to openings within a brick load bearing wall which had been penetrated by a large air conditioning duct
- Supply and installation of metallic signage to nominated lifting points (stating safe working loads)
- Remedial works to air-conditioning ductwork
- Removal of temporary construction access into the roof space and access flooring and reinstatement of the existing ceiling on completion of the works

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on 17 September 2020.

Six (6) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures and comprising representatives of the Project Delivery, City Works and Governance and Customer Service Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope
- 2 Financial assessment acceptable to Council which demonstrates the tenderer's financial capacity to undertake the works
- 3 As a minimum, WHS Management Systems that comply with AS-NZS 4801:2001 or ISO 45001:2018 and is certified to either of these standards by a JAZ-ANZ accredited company OR WHS management system accredited by an NSW Government Agency
- 4 Site Inspection – Tenderers to attend a site inspection of the roof cavity.

Assessable Criteria

- 1 Cost to Council – 40%
- 2 Appreciation of scope of works and construction methodology – 20%
- 3 Experience and satisfactory performance in undertaking projects of similar size, scope and risk profile including staff qualifications and experience – 15%
- 4 Proposed sub-contractors – 5%
- 5 Project Schedule – 5%
- 6 Demonstrated strengthening of local economic capacity – 10%
- 7 Workplace Health and Safety and Environmental Management Systems – 5%

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Colin Joss & Co Pty Ltd trading as Joss Facility Management	1
Advanced Constructions Pty Ltd	2
Project Coordination (Australia) Pty Ltd	3
Murphy's Remedial Builders Pty Ltd	4
Momentum Built Pty Ltd	5
Dynamic Civil Pty Ltd	6

PROPOSAL

Council should authorise the engagement of Colin Joss & Co Pty Ltd trading as Joss Facility Management to carry out the upgrades in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

An acceptable financial capability assessment has been received in relation to the recommended tenderer.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 3 "Wollongong is a creative, vibrant city".

It specifically delivers on core business activities as detailed in the Community Facilities Service Plan 2020-21.

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works is considered medium based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

SUSTAINABILITY IMPLICATIONS

The following sustainability implications have been considered:

- Durable products incorporated in the design to ensure long-term integrity of the existing structures (i.e. galvanised steelwork)
- Sustainable procurement by providing open tender to give local companies the opportunity to tender for the work

- Weighting in tender assessment provided for using local services, labour and materials.

FINANCIAL IMPLICATIONS

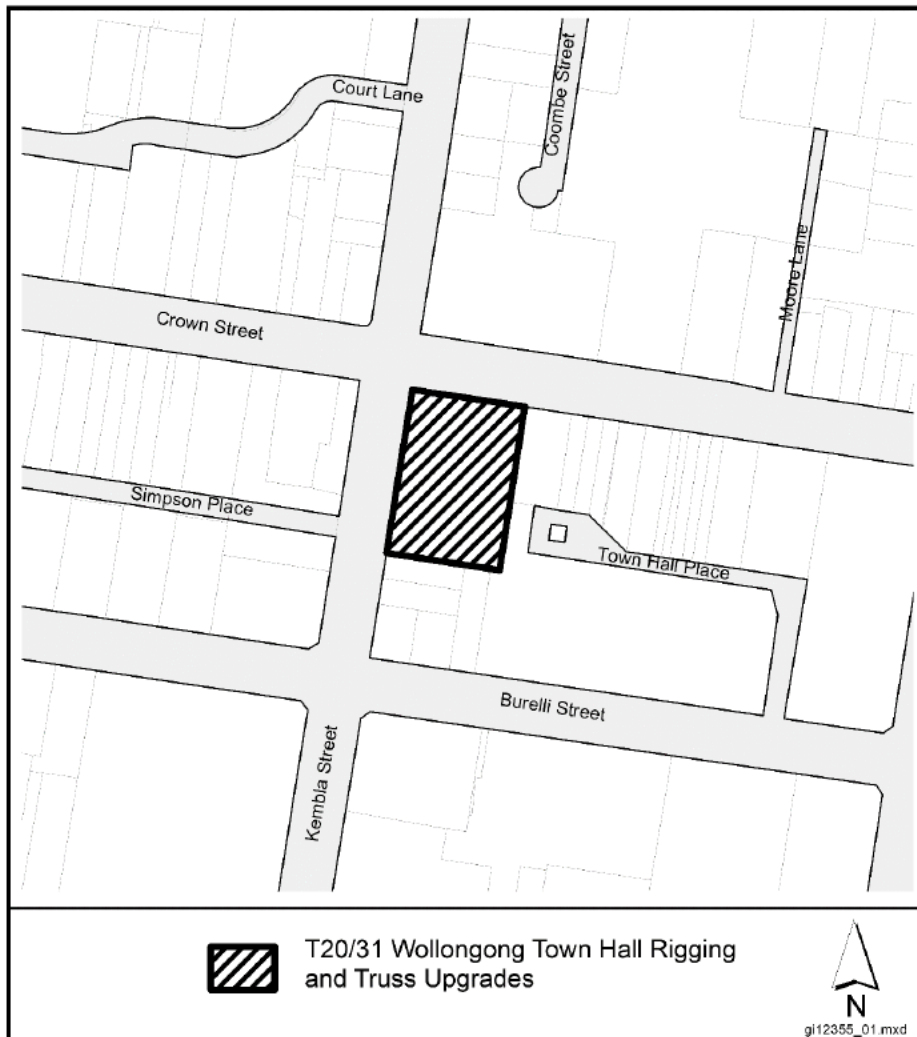
It is proposed that the total project be funded from the following source/s as identified in the Operational Plan –

2020/21 – Capital Budget

CONCLUSION

Colin Joss & Co Pty Ltd trading as Joss Facility Management has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





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ITEM 12

TENDER T20/32 - SANDON POINT BEACH ACCESS STAIRS (MULTIPLE) AND EROSION CONTROL

This report recommends acceptance of a tender for replacement of the Sandon Point Beach Access Stairs and installation of erosion control measures in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

The intended outcome of this project is to provide safe access to Sandon Point Beach for the large number of people who use this area. This is intended to be achieved through a combination of replacement of existing formalised access stairs, formalisation of existing informal access and considerations for addressing dune/beach erosion and scour across four discrete locations.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Dynamic Civil Pty Ltd for replacement of Beach Access Stairs and installation of erosion control measures at Sandon Point, in the sum of \$362,294.00, excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan

BACKGROUND

Sandon Point and Sandon Point Beach have several access points that join the green space between Blackall Street, Bulli and Sandon Point Beach. Four (4) of these have been identified as requiring attention to provide safe access to the beach. Generally, the works consist of the installation of timber posts, stairs, fibre reinforced plastic (FRP) landings and handrails typically used along our coastline, however several sites also incorporate large rock armour (buried below normal sand levels) at the base of the stairs to minimise scour during large sea events. Refer to Location Plan for site locations.

1 Access Point 9

Formalisation of the existing beach access point located adjacent to the Sandon Point Surf Life Saving Club by constructing new access stairs and installation of erosion control measures at the lower level.

2 Access Point 10

Complete replacement of the existing beach access stairs and installation of erosion control measures at the lower level.

3 Access Point 11

Complete replacement of the existing beach access stair.

4 Access Point 13

Complete replacement of the existing beach access stair.

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on 10 September 2020.

Ten (10) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures and comprising representatives of the Project Delivery, Infrastructure, Strategy and Planning and Governance and Customer Service Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope
- 2 Financial assessment acceptable to Council which demonstrates the tenderer's financial capacity to undertake the works
- 3 Work Health and Safety Management System

Assessable Criteria

- 1 Cost to Council – 40%
- 2 Appreciation of scope of works and construction methodology – 15%
- 3 Demonstrated prior experience and satisfactory performance in undertaking projects of similar size, scope and risk profile including staff qualifications and experience – 10%
- 4 Proposed sub-contractors – 5%
- 5 Project Schedule – 5%
- 6 Workplace Health and Safety and Environmental Management systems – 15%
- 7 Demonstrated strengthening of local economic capacity – 10%

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Dynamic Civil Pty Ltd	1
Cadifern Civil	2
Davone Constructions Pty Ltd	3
Cooper Commercial Constructions Pty Ltd	4
GC Civil Contracting Pty Ltd	5
Enter Building Group Pty Ltd	6
Select Civil Pty Ltd	7
Avant Constructions Pty Ltd	8
Affective Services	9
Joss Facility Management	10

PROPOSAL

Council should authorise the engagement of Dynamic Civil Pty Ltd to carry out the Sandon Point Beach Access Stairs and Erosion Control project in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 5 "We have a healthy community in a liveable city".

It specifically delivers on core business activities as detailed in the Infrastructure Planning and Support Service Plan 2020-21.

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works or services is considered medium based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

There are safety, financial and reputational risks that should be considered if Council does not repair, replace or install the beach access points. Increased deterioration and possible failure are risks which would remain if replacement works are not completed. Consequently, the safety of the public utilising the beach access points would be at risk. Possible environmental harm caused by such a failure is included in that risk.

SUSTAINABILITY IMPLICATIONS

Economic sustainability implications are considered relevant to the repair of the beach access points within the Sandon Point precinct to reduce ongoing maintenance costs and to improve the amenity of the area. This would lead to increased use by both local and out of area users, thereby increasing the health and wellbeing of our community.

The design for the repair works incorporates Fibre Reinforced Plastic (FRP) decking members for the new access points. These will provide a long-term, low maintenance finished product that also enhances the aesthetic appearance of the facility.

FINANCIAL IMPLICATIONS

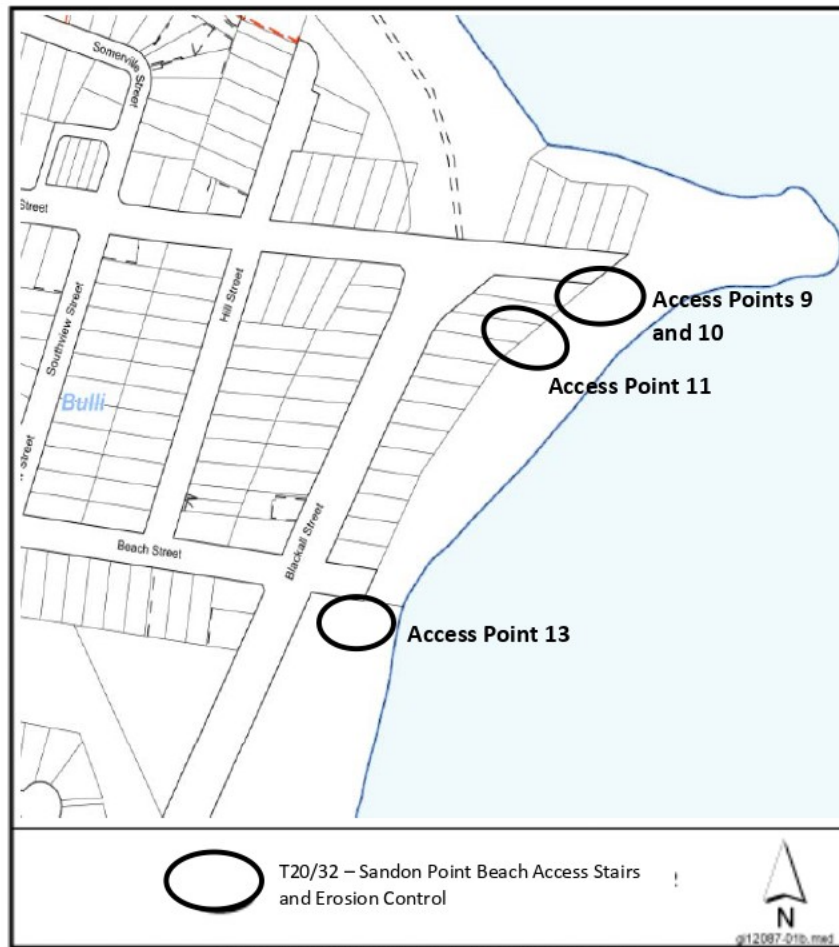
It is proposed that the total project be funded from the following source/s as identified in the Operational Plan –

2020/21 Capital Budget

CONCLUSION

Dynamic Civil Pty Ltd has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





○ T20/32 – Sandon Point Beach Access Stairs and Erosion Control



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ITEM 13 TENDER T20/33 - EMPLOYEE ASSISTANCE PROGRAM

This report recommends acceptance of a tender for an Employee Assistance Program in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

This service provides assistance to employees who experience personal and work-related problems that affect their performance at work. It is recognised that the operation of an effective employee assistance program provides direct benefits to Council employees and consequently to the productivity of Council.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Mentor Services for the provision of an employee assistance program in the sum of \$174,000 excluding GST for a term of 3 years with 2 x 1 year extension options.
- 2 Council delegates to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Shaun Martin, Work Health + Safety Manager

Authorised by: Renee Campbell, Director Corporate Services - Connected + Engaged City

ATTACHMENTS

There are no attachments for this report.

BACKGROUND

Council has offered an Employee Assistance Program (EAP) to its employees since 1984. This program provides assistance to employees who have experienced personal and work-related problems affecting their performance at work. It is recognised that the operation of an effective EAP program provides direct benefits to Council employees and consequently to the productivity of Council.

Tenders were invited for this program by the open tender method with a close of tenders of 10.00 am on 22 September 2002.

Nine (9) tenders were received by the close of tenders. Tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures that comprised of representatives from the Governance and Customer Service, Regulation and Enforcement and Safety and Workplace Services Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees
- 2 Location in Wollongong City

Assessable Criteria

- 1 Cost to Council (30%)
- 2 Methodology for service delivery (20%)
- 3 Prior Experience (20%)

- 4 Staff quals and experience (10%)
- 5 WHS & Risk (10%)
- 6 Demonstrated Strengthening of Local Economic Capacity (10%)

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Mentor Services	1
Access EAP	2
SMG Health Pty Ltd	3
Benestar	4
Converge International	5
Acacia EAP	6
Prima Career Pty Ltd	7
MYEAP Pty Ltd	8
Pinnacle Health group	9

PROPOSAL

Council should authorise the engagement of Mentor Services to carry out the EAP Services in accordance with the scope of works.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal "We are a connected and engaged community". It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
4.3.1 Positive leadership and governance, values and culture are built upon	4.3.1.2 Build a workplace culture that is safe, engaged, responsive and professional	Develop the WHS behaviour strategy and program to enhance our WHS culture through the application of identified constructive behaviour

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project work or services is considered low based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

SUSTAINABILITY IMPLICATIONS

This program supports the provision of a positive, productive, caring and innovative work environment that maintains and enhances our employees' health and well-being. This demonstrates Council's commitment and investment to its most valuable assets, our employees.

FINANCIAL IMPLICATIONS

It is proposed that the total project be funded from the following source/s as identified in the Human Resources Service Plan – Employee Assistance Counselling.

CONCLUSION

An employee assistance program is an important component of the support offered to employees.

ITEM 14

TENDER T20/34 - HEININGER HALL (DAPTO RIBBONWOOD CENTRE) - AMENITIES REFURBISHMENT

This report recommends acceptance of a tender for the refurbishment of the amenities at Heining Hall located in the Dapto Ribbonwood Centre, Princes Highway, Dapto in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

The purpose of this work is to refurbish the existing amenities originally constructed in 1985 to meet current service standards, address sustainability expectations and comply with relevant Australian Standards for access and mobility.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Trinity Quality Interiors Pty Ltd for the refurbishment of the amenities at Heining Hall, in the sum of \$188,650.75, excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan

BACKGROUND

The current amenities in Heining Hall are in the original condition when the hall was constructed in 1985. The current amenities, whilst still remaining functional are failing to meet the service standards that are associated with a district level community facility. The outcome of this project is to achieve modern durable amenities that complement Heining Hall and not only meet the service standard but also lower the expected maintenance costs and are environmentally more sustainable than the current amenities. The scope of works includes replacement of all fixtures with energy or water saving devices, provision of airlocks and installation of an improved ventilation system.

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on 10 September 2020.

Eight (8) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures and comprising representatives of the Project Delivery, Library and Community Services and Governance and Customer Service Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope
- 2 Financial assessment acceptable to Council which demonstrates the tenderer's financial capacity to undertake the works – not required in accordance with Council's risk assessment

3 Safety Management Policy and WHS Management System Manual or Plan as a minimum

Assessable Criteria

- 1 Cost to Council – 40%
- 2 Appreciation of scope of works and construction methodology – 15%
- 3 Experience and satisfactory performance in undertaking projects of similar size, scope and risk profile including staff qualifications and experience – 15%
- 4 Proposed sub-contractors – 5%
- 5 Project Schedule – 5%
- 6 Demonstrated strengthening of local economic capacity – 10%
- 7 Workplace Health and Safety and Environmental Management Systems – 10%

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Trinity Quality Interiors Pty Ltd	1
Cooper Commercial Constructions Pty Ltd	2
Innovatus Projects Pty Ltd	3
Colin Joss & Co Pty Ltd trading as Joss Facility Management	4
VBuilt Construction Pty Ltd	5
Project Coordination (Australia) Pty Ltd	6
Malsave Pty Ltd	7
Murphy's Remedial Builders Pty Ltd	8

PROPOSAL

Council should authorise the engagement of Trinity Quality Interiors Pty Ltd to carry out the Heininger Hall amenities refurbishment in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 5 “We have a healthy community in a liveable city”. It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
5.5.1 Public facilities in key locations and transport routes are maintained and clean, accessible and inviting to our community and visitors	5.5.1.3 Coordinate an access improvement program through pre-planning and renewal activities	Provide advice on access related matter that reflect the priorities in Council's Disability Inclusion Action Plan 2020-25

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works is considered low based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

SUSTAINABILITY IMPLICATIONS

The following sustainability implications have been considered:

- Durable products incorporated in the design to ensure long-term integrity of the amenities (i.e. compact laminate partitions and vanities)
- Use of lightweight composite partition walls to minimise construction materials
- Installation of sensor based soap dispenser units to minimise waste of soap dispenser liquid
- Installation of sensor based lighting to minimise power usage
- Installation of programmed timed flushing for urinals and auto-sensor taps to save on water usage
- Sustainable procurement by providing open tender to give local companies the opportunity to tender for the work.
- Weighting in tender assessment provided for using local services, labour and materials.

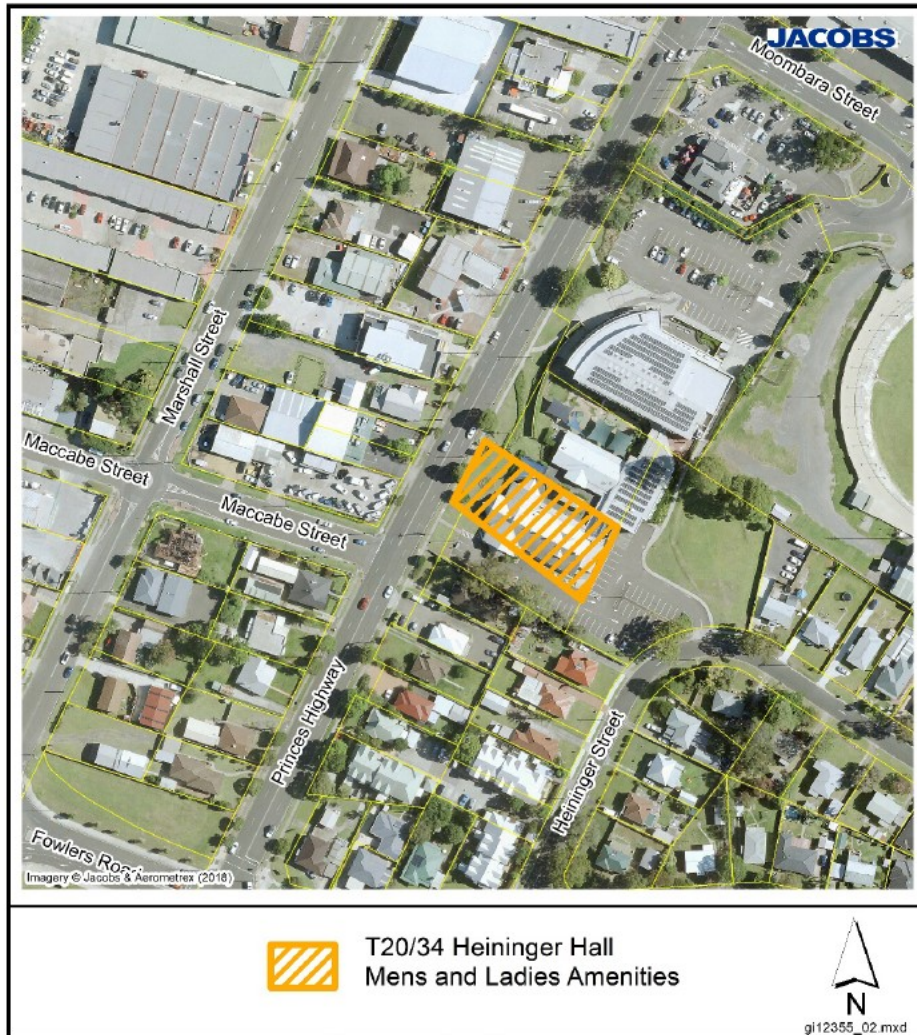
FINANCIAL IMPLICATIONS

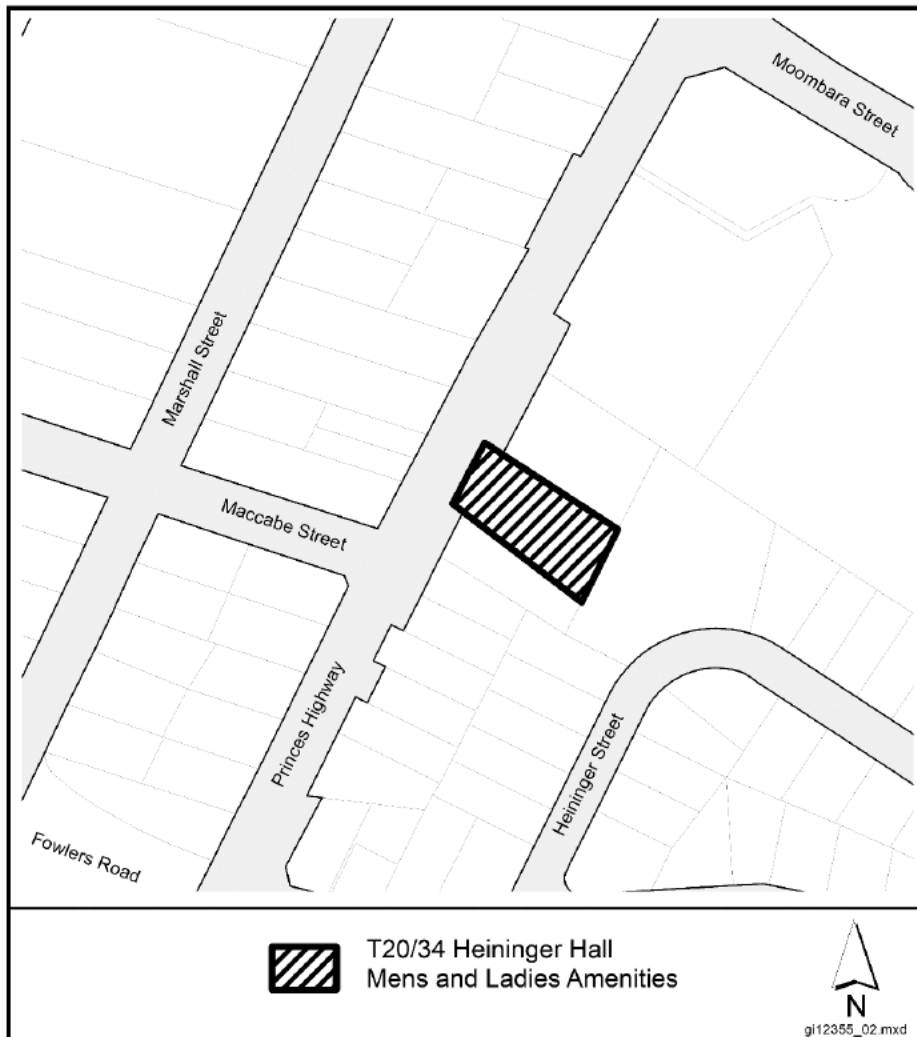
It is proposed that the total project be funded from the following source/s as identified in the Operational Plan –

2020/21 Capital Budget

CONCLUSION

Trinity Quality Interiors Pty Ltd has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





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ITEM 15 TENDER T20/35 - REFURBISHMENT OF AMENITIES AT GUEST PARK, FAIRY MEADOW

This report recommends acceptance of a tender for Refurbishment of Guest Park Amenities in accordance with the requirements of the Local Government Act 1993 and the Local Government (General) Regulation 2005.

The Guest Park Amenities was constructed in 1969, is in poor condition and is not meeting the needs of sporting groups who use this facility. This refurbishment will provide gender neutral change rooms, a referee's room and accessible amenities to ensure the facility meets current community expectations. This work builds on the recent Council resolution to install LED sports field lighting at this ground which will increase utilisation by sporting groups playing and training at this facility.

RECOMMENDATION

- 1 In accordance with clause 178(1)(a) of the Local Government (General) Regulation 2005, Council accept the tender of Batmac Constructions Pty Ltd for refurbishment of Guest Park Amenities, in the sum of \$461,177.73, excluding GST.
- 2 Council delegate to the General Manager the authority to finalise and execute the contract and any other documentation required to give effect to this resolution.
- 3 Council grant authority for the use of the Common Seal of Council on the contract and any other documentation, should it be required, to give effect to this resolution.

REPORT AUTHORISATIONS

Report of: Glenn Whittaker, Manager Project Delivery

Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Location Plan

BACKGROUND

The Guest Park Amenities was built 51-years ago, and a recent asset inspection revealed that the facility had reached the end of service life, was in a poor condition and was not meeting the needs of the user groups. It was built in a time, where gender equity was not taken into consideration and as a result the amenities are not considered functional or suitable for today's modern sporting organisations.

The scope of works to be undertaken includes:

- Reconfiguration of the existing male and female public toilets into one (1) unisex Disability Discrimination Act 1992 (DDA) accessible and three (3) unisex toilets (one (1) configured for ambulant users)
- Provision of a referee's room
- Refurbishment of the kiosk area into a storeroom facility
- Upgrade of the change rooms to include gender neutral amenities including internal toilets, new epoxy flooring, new water efficient plumbing fixtures and fittings, new LED lighting, new hot water system and refurbished bench seating
- Replacement of security grills with aluminium louvers for privacy/security in all areas
- Repainting of internal walls and ceilings
- External works will also include repair, render, seal and painting of all external walls

- Installation of roof guttering and drainage lines
- Replacement of the damaged concrete path with a new concrete path linking the amenities directly to the car park to ensure a compliant DDA path of travel.

The outcome of this project will extend the life of the asset ensuring that with the upgraded gender equity facilities will be available to all patrons. This work builds on the recent Council resolution to install LED sports field lighting at this facility to increase utilisation of this recreational asset.

Tenders were invited for this project by the open tender method with a close of tenders of 10.00 am on Tuesday, 29 September 2020.

Twelve (12) tenders were received by the close of tenders and all tenders have been scrutinised and assessed by a Tender Assessment Panel constituted in accordance with Council's Procurement Policies and Procedures and comprising representatives of the Governance and Customer Service and Project Delivery Divisions.

The Tender Assessment Panel assessed all tenders in accordance with the following assessment criteria and weightings as set out in the formal tender documents:

Mandatory Criteria

- 1 Satisfactory references from referees for previous projects of similar size and scope
- 2 Financial assessment acceptable to Council which demonstrates the tenderer's financial capacity to undertake the works
- 3 Site Inspections for the sole purpose of allowing tenderers to view the site of the proposed works or services at Guest Park Amenities, Fairy Meadow
- 4 WHS Management Systems – with a minimum Safety Management Policy and WHS Management System Manual or Plan.

Assessable Criteria

- 1 Cost to Council – 50%
- 2 Appreciation of scope of works and construction methodology – 15%
- 3 Experience and satisfactory performance in undertaking projects of similar size, scope and risk profile, including staff qualifications and experience – 10%
- 4 Proposed sub-contractors – 5%
- 5 Project Schedule – 5%
- 6 Demonstrated strengthening of local economic capacity – 10%
- 7 Workplace Health and Safety and Environmental Management Systems and Procedures – 5%

The mandatory assessment criteria have been met by the recommended tenderer.

The Tender Assessment Panel utilised a weighted scoring method for the assessment of tenders which allocates a numerical score out of 5 in relation to the level of compliance offered by the tenders to each of the assessment criteria as specified in the tender documentation. The method then takes into account pre-determined weightings for each of the assessment criteria which provides for a total score out of 5 to be calculated for each tender. The tender with the highest total score is considered to be the tender that best meets the requirements of the tender documentation in providing best value to Council. Table 1 below summarises the results of the tender assessment and the ranking of tenders.

TABLE 1 – SUMMARY OF TENDER ASSESSMENT

Name of Tenderer	Ranking
Batmac Constructions Pty Ltd	1
Davone Constructions Pty Ltd	2
Trinity Quality Interiors Pty Ltd	3
Project Coordination (Australia) Pty Ltd	4
Sullivans Constructions	5
Malsave Pty Ltd	6
Joss Facility Management	7
Makki Constructions Pty Ltd	8
Cooper Commercial Constructions Pty Ltd	9
Assett Group Services	10
Kellyville Building Pty Ltd	11
TL Building Pty Ltd	12

PROPOSAL

Council should authorise the engagement of Batmac Constructions Pty Ltd to carry out the refurbishment of the Guest Park Amenities in accordance with the scope of works and technical specifications developed for the project.

The recommended tenderer has satisfied the Tender Assessment Panel that it is capable of undertaking the works to Council's standards and in accordance with the technical specification.

An acceptable financial capability assessment has been received in relation to the recommended tenderer.

Referees nominated by the recommended tenderer have been contacted by the Tender Assessment Panel and expressed satisfaction with the standard of work and methods of operation undertaken on their behalf.

CONSULTATION AND COMMUNICATION

- 1 Members of the Tender Assessment Panel
- 2 Nominated Referees
- 3 External Consultants

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal 5 "We have a healthy community in a liveable city". It specifically delivers on the following:

Community Strategic Plan	Delivery Program 2018-2022	Operational Plan 2020-21
Strategy	4 Year Action	Operational Plan Actions
5.5.1 Public facilities in key locations and transport routes are maintained and clean, accessible and inviting to our community and visitors	5.5.1.2 Manage and maintain community infrastructure portfolio with a focus on asset renewal	Achieve our expenditure targets for capital renewal by programming these works with sufficient flexibility to allow re-phasing, deferral and/or the introduction of other deferred renewal works as required.

RISK ASSESSMENT

The risk in accepting the recommendation of this report is considered low on the basis that the tender process has fully complied with Council's Procurement Policies and Procedures and the Local Government Act 1993.

The risk of the project works is considered low based upon Council's risk assessment matrix and appropriate risk management strategies will be implemented.

SUSTAINABILITY IMPLICATIONS

The following sustainability implications have been considered:

- Specification of fittings and fixtures that have minimum water efficiency and labelling standards (WELS) 'star' ratings
- Energy efficient light fittings
- Sustainable procurement by providing open tender to give local companies the opportunity to tender for the work
- Weighting in tender assessment provided for using local services, labour and materials.

FINANCIAL IMPLICATIONS

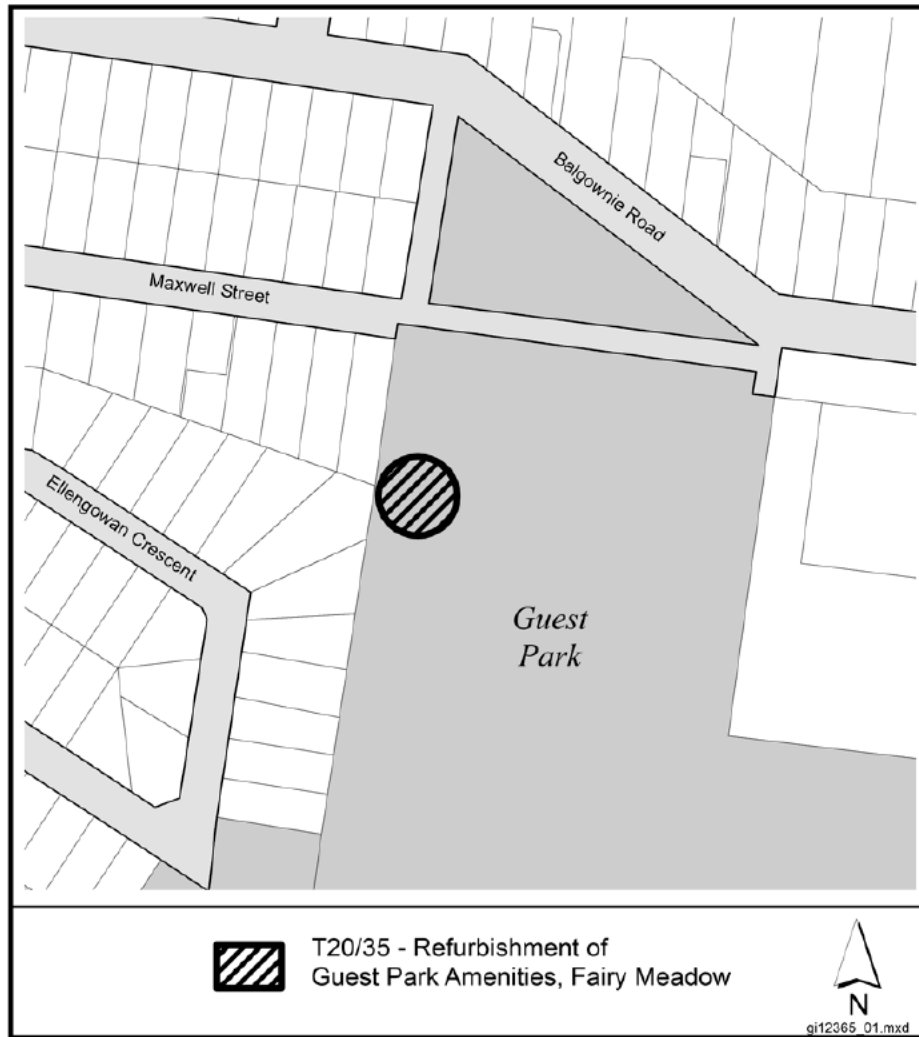
It is proposed that the total project be funded from the following source/s as identified in the Operational Plan –

2020/21 Capital Budget

CONCLUSION

Batmac Constructions Pty Ltd has submitted an acceptable tender for these works and Council should endorse the recommendations of this report.





ITEM 16 SEPTEMBER 2020 FINANCIALS

The September 2020 financial results are not necessarily indicative of full year performance as it represents a small portion of the year with the phased budget patterns continuing to be refined.

Actual results for September compared to phased budget are favourable over the key performance indicators. The Operating Result [pre-capital] and the Funds Available from Operations show favourable variances of \$7.4M and \$5.3M respectively while the Funds Result shows a favourable variance of \$8.1M.

The balance sheet at the end of the period indicates that there is enough cash to support external restrictions.

Council has expended \$17.6M on its capital works program representing 18% of the annual budget. The year to date budget for the same period was \$20.4M.

RECOMMENDATIONS

- 1 The financials be received and noted.
- 2 Council approves the proposed changes to the Capital Budget.

REPORT AUTHORISATIONS

Report of: Brian Jenkins, Chief Financial Officer
Authorised by: Renee Campbell, Director Corporate Services - Connected + Engaged City

ATTACHMENTS

- 1 Income and Funding Statement - September 2020
- 2 Capital Project Report - September 2020
- 3 Balance Sheet - September 2020
- 4 Cash Flow Statement - September 2020

BACKGROUND

This report presents the Income and Expense Statement, Balance Sheet and Cash Flow Statement for September 2020. Council's current budget has a Net Funding (cash) deficit of \$9.1M, an Operating Deficit [pre capital] of \$22.0M and a capital expenditure of \$95.7M.

The following table provides a summary view of the organisation's overall financial results for the year to date.

FORECAST POSITION	Original Budget	Revised Budget	YTD Forecast	YTD Actual	Variation
	\$M	\$M	\$M	\$M	\$M
KEY MOVEMENTS	1-Jul	25-Sep	25-Sep	25-Sep	
Operating Revenue	269.0	269.0	62.0	65.2	3.2
Operating Costs	(291.0)	(291.0)	(69.9)	(65.7)	4.2
Operating Result [Pre Capital]	(22.0)	(22.0)	(7.9)	(0.5)	7.4
Capital Grants & Contributions	35.5	35.5	4.6	3.6	(1.0)
Operating Result	13.4	13.4	(3.3)	3.1	6.4
Funds Available from Operations	56.9	56.9	10.3	15.6	5.3
Capital Works	93.4	95.7	20.4	17.5	2.9
Contributed Assets	11.6	11.6	-	-	-
Transfer to Restricted Cash	1.4	1.4	0.3	0.4	(0.0)
Borrowings Repaid	5.2	5.2	1.2	1.2	-
Funded from:					
- Operational Funds	56.9	56.9	9.0	14.3	5.3
- Other Funding	45.6	48.0	5.9	5.9	(0.0)
Total Funds Surplus/(Deficit)	(9.1)	(9.1)	(5.8)	2.3	8.1

Financial Performance

The September 2020 Operating Result [pre-capital] shows a favourable variance compared to phased budget of \$7.4M that is due to greater operating income (\$3.2M) that is largely associated with COVID-19 and lower expenditure trends across a range of areas (\$4.2M).

The Operating Result shows a positive variance of \$6.4M compared to budget. This includes the net variation above decreased by a lower level of capital income of \$1.0M. This is mainly due to lower West Dapto Developer Contributions.

The Funds Available from Operations indicate a favourable variation of \$5.3M. This result excludes variations in non-cash transactions and transfers to and from Restricted Assets but includes the cash payments for Employee Entitlements to represent the operational budget variations that may impact our funding.

The year to date budget includes a net provision for the impacts of COVID-19 of \$6.4M of which \$2.8M has been utilised. The COVID-19 provisions applied in the budget were focused on income loss and heavily weighted in the phasing of estimates to the first quarter. While the current revenue trends are positive against budget to date, the impacts of COVID-19 currently being experienced will have impacts beyond expectation in expenditure due to increased delivery cost required to meet regulatory requirements. This may become more significant in delivering summer services to larger numbers of people.

Funds Result

The Total Funds result as at 25 September 2020 shows a favourable variance of \$8.1M compared to phased budget.

Capital Budget

At the end of September, the capital program shows an expenditure of \$17.5M compared to a phased budget of \$20.4M.

Through the adoption of the Operational Plan 2020-2021, Council approved a capital budget for 2020-2021 of \$93.4M with an increase of \$2.1M through the July monthly reporting process. The revised projections at August and September 2020 recommend an increase of \$0.3M and a reduction of \$0.1M respectively. These revised projections are offset by corresponding adjustments in funding. The overall increase is primarily due to re-phasing of various projects which are supported by external funding or

restricted cash. Details of these changes are provided in the Capital Project reports and commentary (attachment 2).

Available Cash

CASH, INVESTMENTS & AVAILABLE FUNDS				
		Actual 2019/20	Original Budget 2020/21	Actual Ytd September 2020
		\$M	\$M	\$M
Total Cash and Investments	\$M	157.5	114.2	146.0
Less Restrictions:				
External	\$M	74.7	73.7	77.7
Internal	\$M	62.7	39.0	59.5
Total Restrictions		137.4	112.7	137.2
Available Cash	\$M	20.1	1.6	8.8
Adjusted for :				
Payables		(30.6)	(26.2)	(38.3)
Receivables		21.7	23.7	41.4
Other		14.2	12.5	15.8
Net Payables & Receivables		5.3	10.1	18.9
Available Funds	\$M	25.4	11.6	27.8

Council's cash and investments increased during September 2020 to holdings of \$146.0M compared to \$144.9M at the end of August 2020.

Cash receipts from rates have been impacted in 2020 due to the one-month extension to the first instalment date to 30 September. Council's accounting periods end on the last Friday of month, in this case 25 September. A further \$13.7 million of rates were received between this date and 30 September which will be reflected in the October reporting period. Early analysis indicates that there are a similar number of outstanding payments at 30 September as last year. Further and more reliable data will be available during October to determine any longer-term impacts of COVID-19 on rates debtors.

The Available Funds position excludes restricted cash. External restrictions are funds that must be spent for a specific purpose and cannot be used by Council for general operations. Internal restrictions are funds that Council has determined will be used for a specific future purpose.

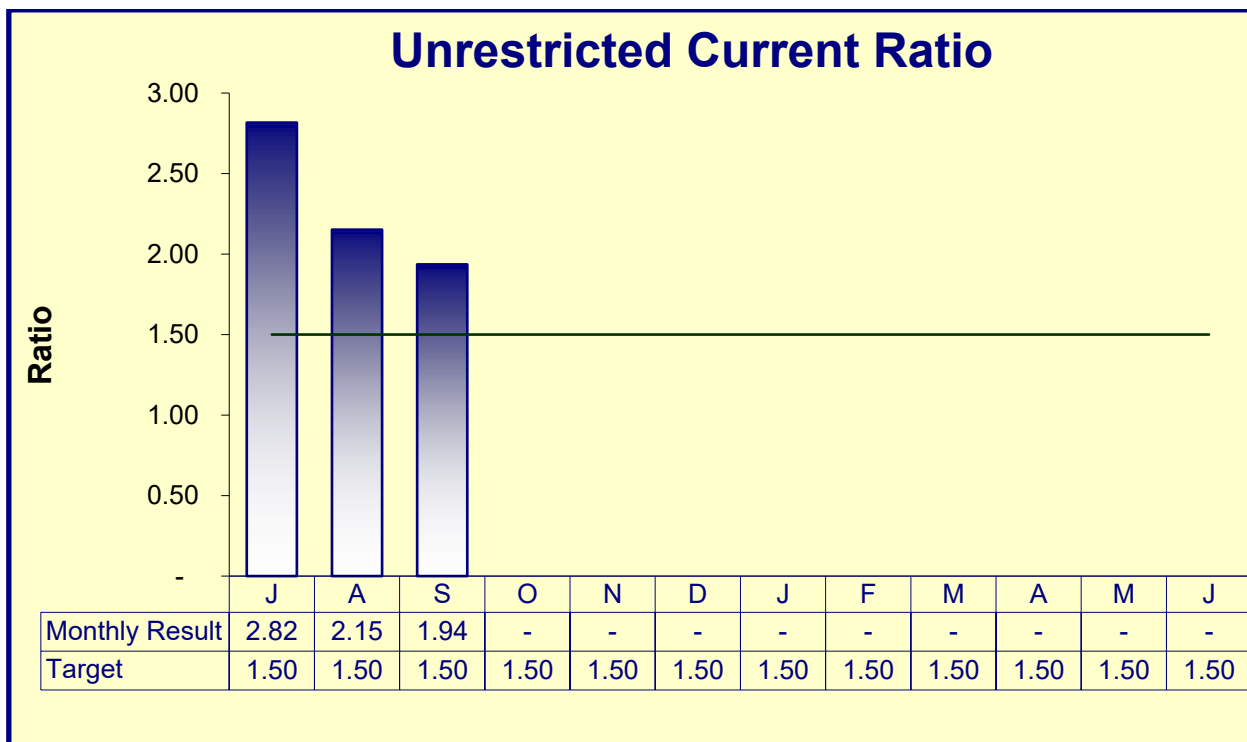
Receivables

Receivables are the amount of money owed to Council or funds that Council has paid in advance. At September 2020, receivables totalled \$41.4M compared to receivables of \$35.9M at August 2020.

Payables

Payables (the amount of money owed to suppliers) of \$38.3M were owed at September 2020 compared to payables of \$32.0M at August 2020. The increase relates to rates paid to council before being due.

The Unrestricted Current Ratio is intended to disclose the ability of an organisation to satisfy payment obligations in the short term from the unrestricted activities of Council. Council's current ratio exceeds the Local Government Benchmark of >1.5:1.



Council continues to have financial strength in its low level of borrowing. The industry measure of debt commitment is the Debt Service Ratio that measures the proportion of revenues that are required to meet annual loan repayments.

Council's Financial Strategy includes provision for additional borrowing in the future and Council will consider borrowing opportunities from time to time to bring forward the completion of capital projects where immediate funding is not available.

Council's Debt Service Ratio forecast for 2020-2021 is a target of <4% and remains low in comparison to the Local Government's benchmark ratio of <20%.

Assets

The Balance Sheet shows that \$2.9B of assets (written down value) are controlled and managed by Council for the community as at 25 September 2020. The 2020-2021 capital works program includes projects such as the West Dapto Infrastructure Expansion, civil asset renewals including roads, car parks and buildings and purchase of library books. Council has expended \$17.6M on its capital works program representing 18% of the Annual Budget. The year to date budget for the same period was \$20.4M.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Wollongong 2022 goal '*We are a connected and engaged community*'. It specifically delivers on the following:

Community Strategic Plan Strategy	Delivery Program 2018-2022 4 Year Action	Operational Plan 2020-2021 Operational Plan Actions
4.3.2 Resources (finance, technology, assets and people) are effectively managed to ensure long term financial sustainability	4.3.2.1 Effective and transparent financial management systems are in place	Monitor and review achievement of Financial Strategy
		Continuous budget management is in place, controlled and reported
		Provide accurate and timely financial reports monthly, quarterly and via the annual statement
		Manage and further develop a compliance program to promote awareness and compliance with Council's procurement policies and procedures and other related policies

CONCLUSION

The financial result at the end of September is positive across the leading indicators. Included in this is recognition of the impact of the COVID-19 pandemic and it is not considered that the September trends are necessarily indicative of full year performance as they represent only a small portion of the year.

WOLLONGONG CITY COUNCIL					
1 July 2020 to 25 September 2020					
	21GLBUD Original Budget \$'000	21GLBUD Current Budget \$'000	21GLPHAS YTD Budget \$'000	21GLACT Actual YTD \$'000	Actual v Phase Variance \$'000
Income Statement					
Income From Continuing Operations					
Revenue:					
Rates and Annual Charges	208,445	208,445	49,684	49,725	41
User Charges and Fees	27,034	27,034	3,204	6,289	3,085
Interest and Investment Revenues	2,343	2,343	578	644	66
Other Revenues	9,498	9,498	1,839	1,157	(682)
Rental Income	0	0	0	1,403	1,403
Grants & Contributions provided for Operating Purposes	21,679	21,679	6,711	6,007	(704)
Grants & Contributions provided for Capital Purposes	35,460	35,460	4,543	3,560	(983)
Other Income:					
	0	0	0	0	0
Profit/Loss on Disposal of Assets	0	0	0	(14)	(15)
Total Income from Continuing Operations	304,459	304,459	66,559	68,772	2,213
Expenses From Continuing Operations					
Employee Costs	140,635	140,635	33,774	33,550	224
Borrowing Costs	2,374	2,374	566	544	22
Materials, Contracts & Other Expenses	104,493	104,493	24,321	20,579	3,742
Depreciation, Amortisation + Impairment	63,702	63,702	16,056	15,246	811
Internal Charges (labour)	(18,432)	(18,432)	(4,393)	(3,721)	(672)
Internal Charges (not labour)	(1,733)	(1,733)	(447)	(497)	50
Total Expenses From Continuing Operations	291,038	291,038	69,876	65,699	4,177
Operating Result	13,421	13,421	(3,318)	3,072	6,390
Operating Result [pre capital]	(22,039)	(22,039)	(7,861)	(488)	7,373
NET SURPLUS (DEFICIT) [Pre capital] %	4.4%	4.4%	(5.0%)	4.5%	288.7%
Funding Statement					
Net Operating Result for the Year	13,421	13,421	(3,318)	3,072	6,390
Add back:					
- Non-cash Operating Transactions	82,363	82,363	20,591	19,666	(924)
- Restricted cash used for operations	29,815	29,815	7,107	4,742	(2,364)
- Income transferred to Restricted Cash	(54,169)	(54,169)	(10,400)	(9,161)	1,239
- Payment of Right of Use Leases	0	0	0	0	0
- Payment of Accrued Leave Entitlements	(14,533)	(14,533)	(3,720)	(2,739)	981
- Payment of Carbon Contributions	0	0	0	0	0
Net Share Joint Venture using Equity Method	0	0	0	0	0
Funds Available from Operations	56,897	56,897	10,259	15,581	5,322
Borrowings repaid	(5,242)	(5,242)	(1,249)	(1,249)	0
Advances (made by) / repaid to Council	0	0	0	0	0
Operational Funds Available for Capital Budget	51,655	51,655	9,009	14,332	5,322
CAPITAL BUDGET					
Assets Acquired	(93,359)	(95,746)	(20,430)	(17,576)	2,854
Contributed Assets	(11,562)	(11,562)	0	0	0
Transfers to Restricted Cash	(1,447)	(1,447)	(345)	(362)	(17)
Funded From :-					
- Operational Funds	51,655	51,655	9,009	14,332	5,322
- Sale of Assets	1,801	1,801	114	345	231
- Internally Restricted Cash	13,371	13,349	1,913	2,020	108
- Borrowings	0	0	0	0	0
- Capital Grants	4,762	4,753	553	314	(239)
- Developer Contributions (Section 94)	11,931	12,789	1,387	1,171	(216)
- Other Externally Restricted Cash	1,170	2,720	1,767	1,672	(95)
- Other Capital Contributions	12,559	12,569	197	388	192
TOTAL FUNDS SURPLUS / (DEFICIT)	(9,119)	(9,119)	(5,838)	2,304	8,140

Manager Project Delivery Division

Commentary on August to September 2020 Capital Budget Report

On 29 June 2020, Council adopted a Capital budget for 2020-2021 of \$93.4M with an increase of \$2.1M approved in July. During August and September 2020, the approved Capital Budget has increased to \$95.7M from the previously adjusted \$95.5M, due primarily to the introduction of additional external funding related to the West Dapto Access Infrastructure Expansion, as well as rephasing of Stormwater Levy, Waste Reserve and Regional Roads funding.

Council achieved expenditure at the end of September 2020 of \$17.6M compared to the phased budget expenditure of \$20.4M.

Program	Major Points of change to Capital Budget
Traffic Facilities	Introduce NSW Govt Regional Roads and Sect 94 funding.
Floodplain Management	Reallocate Budget from Floodplain Management Program to Stormwater Management Program.
Stormwater Management	Reallocate Budget to Stormwater Management Program from Floodplain Management Program
Stormwater Treatment Devices	Rephase Stormwater Levy Funding to a future year.
Community Buildings	Rephase Strategic Projects Reserve funding for an existing project to a future year.
Play facilities	Reallocate Budget (and Strategic Projects Reserve funding) from Play Facilities Program to Recreation Facilities Program.
Recreation Facilities	Reallocate Budget (and Strategic Projects Reserve funding) from Play Facilities Program to Recreation Facilities Program.
Sporting Facilities	Reallocate Budget to Sporting Facilities from Recreation Facilities.
Helensburgh Rehabilitation	Rephase Budget and Waste Reserve Funding to a future year due to delays with approvals
Land Acquisitions	Introduce Sect 94 West Dapto funding from West Dapto Infrastructure Expansion Program to fund Property Acquisition Program for land parcel acquisition on Princes Highway, DAPTO from TAFE NSW as per existing resolution of Council.
Capital Project Contingency	Reallocate Budget from Capital Project Contingency to Capital Project Plan.
Capital Project Plan	Reallocate Budget to Capital Project Plan from Capital Project Contingency.

CAPITAL PROJECT REPORT							
1 August to 25 September 2020							
ASSET CLASS PROGRAMME	\$'000		\$'000		YTD EXPENDITURE	\$'000	
	CURRENT BUDGET		WORKING BUDGET			VARIATION	
	EXPENDITURE	OTHER FUNDING	EXPENDITURE	OTHER FUNDING		EXPENDITURE	OTHER FUNDING
Roads And Related Assets							
Traffic Facilities	2,024	(962)	2,098	(1,036)	385	74	(74)
Public Transport Facilities	335	0	335	0	14	(0)	0
Roadworks	11,433	(1,555)	11,433	(1,555)	2,545	(0)	(0)
Bridges, Boardwalks and Jettyes	1,980	0	1,980	0	325	0	0
TOTAL Roads And Related Assets	15,772	(2,518)	15,846	(2,592)	3,570	74	(74)
West Dapto							
West Dapto Infrastructure Expansion	8,031	(8,031)	8,031	(8,031)	834	(0)	0
TOTAL West Dapto	8,031	(8,031)	8,031	(8,031)	834	(0)	0
Footpaths And Cycleways							
Footpaths	7,330	(2,800)	7,330	(2,800)	1,891	(0)	(0)
Cycle/Shared Paths	4,055	(2,025)	4,055	(2,025)	815	(0)	0
Commercial Centre Upgrades - Footpaths and Cycleways	5,547	(2,082)	5,547	(2,082)	1,223	0	0
TOTAL Footpaths And Cycleways	16,942	(6,907)	16,942	(6,907)	3,630	(0)	(0)
Carparks							
Carpark Construction/Forming	915	(200)	915	(200)	42	0	0
Carpark Reconstruction/Upgrading	1,555	(280)	1,555	(280)	145	0	0
TOTAL Carparks	2,460	(480)	2,460	(480)	188	0	0
Stormwater And Floodplain Management							
Floodplain Management	1,971	(251)	1,771	(251)	125	(200)	(0)
Stormwater Management	2,754	(335)	2,954	(335)	1,117	200	0
Stormwater Treatment Devices	185	(185)	85	(85)	0	(100)	100
TOTAL Stormwater And Floodplain Mar	4,911	(781)	4,811	(681)	1,241	(100)	100
Buildings							
Cultural Centres (IPAC, Gallery, Town Hall)	1,090	0	1,090	0	199	0	0
Administration Buildings	1,935	0	1,935	0	227	0	0
Community Buildings	13,157	(1,814)	13,128	(1,775)	2,912	(39)	39
Public Facilities (Shelters, Toilets etc.)	230	0	230	0	0	(0)	0
TOTAL Buildings	16,422	(1,814)	16,383	(1,775)	3,338	(39)	39
Commercial Operations							
Tourist Park - Upgrades and Renewal	870	0	870	0	580	0	0
Crematorium/Cemetery - Upgrades and Renewal	265	0	265	0	69	(0)	0
Leisure Centres & RV/GC	170	0	170	0	4	0	0
TOTAL Commercial Operations	1,305	0	1,305	0	653	(0)	0
Parks Gardens And Sportfields							
Play Facilities	2,510	(770)	2,510	(670)	9	(100)	100
Recreation Facilities	2,651	(1,751)	2,899	(1,851)	58	(62)	(100)
Sporting Facilities	2,143	(850)	2,305	(850)	219	162	(0)
Lake Illawarra Foreshore	100	0	100	0	0	(0)	0
TOTAL Parks Gardens And Sportfields	7,514	(3,381)	7,514	(3,381)	287	(0)	(0)
Beaches And Pools							
Beach Facilities	827	0	827	0	19	(0)	0
Rocks/Tidal Pools	970	0	970	0	732	0	0
Treated Water Pools	1,480	0	1,480	0	142	0	0
TOTAL Beaches And Pools	3,277	0	3,277	0	893	(0)	0
Waste Facilities							
Wrightes Gully New Cells	2,590	(2,590)	2,590	(2,590)	535	(0)	(0)
Wrightes Gully Renewal Works	520	(520)	520	(520)	14	0	0
Hensborough Renovation	500	(500)	400	(400)	153	(100)	100
TOTAL Waste Facilities	3,610	(3,610)	3,710	(3,710)	611	(100)	100
Fleet							
Motor Vehicles	1,800	(949)	1,800	(949)	0	0	0
TOTAL Fleet	1,800	(949)	1,800	(949)	0	0	0
Plant And Equipment							
Portable Equipment (Mowers etc.)	100	(27)	100	(27)	12	(0)	0
Mobile Plant (trucks, backhoes etc.)	4,280	(825)	4,280	(825)	66	(0)	0
TOTAL Plant And Equipment	4,380	(853)	4,380	(853)	78	(0)	0

1 August to 25 September 2020							
ASSET CLASS PROGRAMME	\$'000		\$'000		YTD EXPENDITURE	\$'000	
	CURRENT BUDGET		WORKING BUDGET			VARIATION	
	EXPENDITURE	OTHER FUNDING	EXPENDITURE	OTHER FUNDING		EXPENDITURE	OTHER FUNDING
Information Technology							
Information Technology	2,134	0	2,134	0	61	(0)	0
TOTAL Information Technology	2,134	0	2,134	0	61	(0)	0
Library Books							
Library Books	1,251	0	1,251	0	429	(0)	0
TOTAL Library Books	1,251	0	1,251	0	429	(0)	0
Public Art							
Art Gallery Acquisitions	100	0	100	0	3	0	0
TOTAL Public Art	100	0	100	0	3	0	0
Emergency Services							
Emergency Services Plant and Equipment	20	0	20	0	3	(0)	0
TOTAL Emergency Services	20	0	20	0	3	(0)	0
Land Acquisitions							
Land Acquisitions	4,386	(4,336)	4,720	(4,670)	1,337	334	(334)
TOTAL Land Acquisitions	4,386	(4,336)	4,720	(4,670)	1,337	334	(334)
Non-Project Allocations							
Capital Project Contingency	954	0	944	0	0	(10)	0
Capital Project Plan	0	0	10	0	0	10	0
TOTAL Non-Project Allocations	954	0	954	0	0	(0)	0
GRAND TOTAL	35,480	(35,660)	35,659	(34,023)	17,576	169	(169)

WOLLONGONG CITY COUNCIL		
as at 25 September 2020		
	Actual 2020/21 \$'000	Actual 2019/20 \$'000
Balance Sheet		
Current Assets		
Cash Assets	56,376	56,050
Investment Securities	81,658	97,422
Receivables	41,352	21,668
Inventories	464	381
Current Contract Assets	5,613	5,669
Other	10,299	10,572
Assets classified as held for sale	0	0
Total Current Assets	195,762	191,761
Non-Current Assets		
Non Current Cash Assets	8,000	4,000
Non-Current Receivables	0	0
Non-Current Inventories	5,972	5,972
Property, Plant and Equipment	2,657,044	2,655,487
Investment Properties	5,000	5,000
Westpool Equity Contribution	3,484	3,484
Intangible Assets	229	254
Right-Of-Use Assets	1,790	1,790
Total Non-Current Assets	2,681,518	2,675,987
TOTAL ASSETS	2,877,280	2,867,748
Current Liabilities		
Current Payables	38,285	30,592
Current Contract Liabilities	1,792	3,571
Current Lease Liabilities	341	341
Current Provisions payable < 12 months	14,568	13,272
Current Provisions payable > 12 months	47,811	47,811
Current Interest Bearing Liabilities	5,260	5,260
Total Current Liabilities	108,057	100,848
Non-Current Liabilities		
Non Current Payables	0	0
Non Current Interest Bearing Liabilities	11,318	12,439
N/C Lease Liabilities	1,519	1,519
Non Current Provisions	40,147	39,775
Total Non-Current Liabilities	52,985	53,734
TOTAL LIABILITIES	161,042	154,582
NET ASSETS	2,716,238	2,713,166
Equity		
Accumulated Surplus	1,364,779	1,360,922
Asset Revaluation Reserve	1,214,858	1,214,858
Restricted Assets	136,601	137,386
TOTAL EQUITY	2,716,238	2,713,165

WOLLONGONG CITY COUNCIL		
CASH FLOW STATEMENT as at 25 September 2020		
	YTD Actual 2020/21 \$ '000	Actual 2019/20 \$ '000
CASH FLOWS FROM OPERATING ACTIVITIES		
<u>Receipts:</u>		
Rates & Annual Charges	27,437	203,414
User Charges & Fees	23,566	29,485
Interest & Interest Received	1,080	4,130
Grants & Contributions	10,326	68,993
Other	1,972	25,886
<u>Payments:</u>		
Employee Benefits & On-costs	(28,598)	(118,396)
Materials & Contracts	(14,984)	(73,390)
Borrowing Costs	(172)	(910)
Other	(8,772)	(46,162)
Net Cash provided (or used in) Operating Activities	11,855	93,050
CASH FLOWS FROM INVESTING ACTIVITIES		
<u>Receipts:</u>		
Sale of Investments	-	-
Sale of Infrastructure, Property, Plant & Equipment	345	958
Deferred Debtors Receipts	-	-
<u>Payments:</u>		
Purchase of Infrastructure, Property, Plant & Equipment	(22,518)	(83,558)
Purchase of Interests in Joint Ventures & Associates	-	-
Net Cash provided (or used in) Investing Activities	(22,174)	(82,600)
CASH FLOWS FROM FINANCING ACTIVITIES		
<u>Receipts:</u>		
Proceeds from Borrowings & Advances	-	-
<u>Payments:</u>		
Repayment of Borrowings & Advances	-	(7,935)
Repayment of Finance Lease Liabilities	-	(312)
Net Cash Flow provided (used in) Financing Activities	-	(8,247)
Net Increase/(Decrease) in Cash & Cash Equivalents	(10,319)	30,865
plus: Cash & Cash Equivalents and Investments - beginning of year	157,475	126,610
Cash & Cash Equivalents and Investments - year to date	147,156	157,475

WOLLONGONG CITY COUNCIL		
CASH FLOW STATEMENT as at 25 September 2020		
	YTD Actual 2020/21 \$ '000	Actual 2019/20 \$ '000
Total Cash & Cash Equivalents and Investments - year to date	147,156	157,475
<u>Attributable to:</u>		
External Restrictions (refer below)	77,651	75,470
Internal Restrictions (refer below)	59,535	62,675
Unrestricted	9,970	19,330
Total External Restrictions	77,651	75,470
External Restrictions		
Developer Contributions	36,799	35,099
RMS Contributions	887	350
Specific Purpose Unexpended Grants	4,351	3,568
Special Rates Levy Wollongong Mail	398	214
Special Rates Levy Wollongong City Centre	149	49
Local Infrastructure Renewal Scheme	276	274
Unexpended Loans	1,146	2,686
Domestic Waste Management	14,432	14,216
Private Subsidies	6,070	6,167
West Dapto Home Deposit Assistance Program	11,037	10,987
Stormwater Management Service Charge	2,106	1,860
Total External Restrictions	77,651	75,470
Internal Restrictions		
Property Investment Fund	7,072	7,889
Strategic Projects	40,306	42,900
Sports Priority Program	1,013	938
Car Parking Strategy	1,671	2,195
MacCabe Park Development	1,478	1,440
Darcy Wentworth Park	171	171
Garbage Disposal Facility	992	561
West Dapto Development Additional Rates	6,318	6,067
Southern Phone Natural Areas	173	173
Lake Illawarra Estuary Management Fund	341	341
Total Internal Restrictions	59,535	62,675

ITEM 17 STATEMENT OF INVESTMENT - SEPTEMBER 2020

This report provides an overview of Council's investment portfolio performance for the month of September 2020.

Council's average weighted return for September 2020 was 1.44% which was above the benchmark return of 0.11%. This result was primarily due to positive marked to market valuation of the aggregated Emerald Mortgage Backed Securities and the NSW TCorp Hourglass facilities. The remainder of Council's portfolio continues to provide a high level of consistency in income and a high degree of credit quality and liquidity.

RECOMMENDATION

Council receive the Statement of Investment for September 2020.

REPORT AUTHORISATIONS

Report of: Brian Jenkins, Chief Financial Officer

Authorised by: Renee Campbell, Director Corporate Services - Connected + Engaged City

ATTACHMENTS

- 1 Statement of Investment - September 2020
- 2 Investment Income Compared to Budget 2020-2021

BACKGROUND

Council is required to invest its surplus funds in accordance with the Ministerial Investment Order and Division of Local Government guidelines. The Order reflects a conservative approach and restricts the investment types available to Council. In compliance with the Order and Division of Local Government guidelines, Council adopted an Investment Policy on 10 December 2018. The Investment Policy provides a framework for the credit quality, institutional diversification and maturity constraints that Council's portfolio can be exposed to. Council's investment portfolio was controlled by Council's Finance Division during the period to ensure compliance with the Investment Policy. Council's Audit, Risk and Improvement Committee's (ARIC) role of overseer provides for the review of Council's Investment Policy and the Management Investment Strategy.

Council's Responsible Accounting Officer is required to sign the complying Statement of Investment contained within the report, certifying that all investments were made in accordance with the Local Government Act 1993 and the Local Government Regulation 2005.

Council's investment holdings as at 25 September 2020 were \$145,776,471 (Statement of Investment attached) [27 September 2019 \$157,796,872].

Council's average weighted return for September 2020 was 1.44% which was above the benchmark return of 0.11%. This result was primarily due to positive marked to market valuation of the aggregated Emerald Mortgage Backed Securities and the NSW TCorp Hourglass facilities. The remainder of Council's portfolio continues to provide a high level of consistency in income and a high degree of credit quality and liquidity. The global markets are still experiencing unprecedented turmoil and volatility in the wake of COVID-19 and may still provide uncertainty for the foreseeable future.

At 25 September 2020, year to date interest and investment revenue of \$644,136 was recognised compared to the year to date budget of \$498,483. This significant variation is the result of a conservative COVID-19 cash flow scenario. The inflow of funds from rates was higher than initially anticipated resulting in additional interest income above the budget.

Council's 21 floating rate notes had a net decrease in value of \$42,703 for September 2020.

Council holds two Mortgaged Backed Securities (MBS) that recorded a net increase in value of \$102,508 for September 2020. The coupon margins on these investments reflect pre-Global Financial Crisis (GFC)

pricing. For example, the Emerald A is paying 45 basis points over the BBSW where a comparative investment is now paying 100 basis points over the BBSW. This is reflected in the coupon rates on both these investments. While the maturity dates are outside Council's control, the investment advisors had previously indicated that capital is not at risk at that stage and recommended a hold strategy due to the illiquid nature of the investment.

Council has two investment holdings under the NSW TCorp Hour Glass Facility: the Long-Term Growth Facility and the NSW TCorpIM Cash Fund. The Long-Term Growth recorded a net decrease in value of \$6,359 and the Cash Fund recorded a net increase in value of \$9,771 in September 2020. The fluctuation in the Long-Term Growth Facility is a reflection of the current share market volatility both domestically and internationally and is diversified across a number of different asset classes that have differing risk and return characteristics. The TCorp Cash Fund generally provides relatively stable returns with low potential for capital loss while maintaining high levels of liquidity, similar to an at call account, and only invests in Australian cash and fixed interests.

At their September 2020 meeting, the Reserve Bank of Australia (RBA) maintained the official cash rate at the record low of 0.25%. The RBA will continue to assess the outlook and adjust policy as needed to foster sustainable growth in demand and inflation outcomes consistent with the inflation target over time.

The current investment portfolio complies with Council's Investment Policy which was endorsed by Council on 10 December 2018. Council's Responsible Accounting Officer has signed the Statement of Investment contained within the report, certifying that all investments were made in accordance with the Local Government Act 1993 and the Local Government Regulation 2005.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Wollongong 2022 goal '*We are a connected and engaged community*'. It specifically delivers on the following:

Community Strategic Plan Strategy	Delivery Program 2018-2022 4 Year Action	Operational Plan 2020-2021 Operational Plan Actions
4.3.2 Resources (finance, technology, assets and people) are effectively managed to ensure long term financial sustainability	4.3.2.1 Effective and transparent financial management systems are in place	Monitor and review achievement of Financial Strategy
		Continuous budget management is in place, controlled and reported
		Provide accurate and timely financial reports monthly, quarterly and via the annual statement
		Manage and further develop a compliance program to promote awareness and compliance with Council's procurement policies and procedures and other related policies

CONCLUSION

The investments for September 2020 recorded an average weighted return above the annualised Bloomberg Bank Bill Index Benchmark and performed favourably when compared to the year to date budget.

WOLLONGONG CITY COUNCIL STATEMENT OF INVESTMENT 25 September 2020								
DIRECT INVESTMENTS								
Investment Body	Rating	Purchase Price \$	Fair Value of Holding \$	Security	Purchase Date	Maturity Date	Interest / Coupon Rate	
NAB Professional Maximiser	A1+	-	26,626,176	Prof Fund A/c	25/09/2020	25/09/2020	0.65%	
NAB General Fund	A1+	-	1,827,363	General A/c	25/09/2020	25/09/2020		
IMB Ltd	S&P ST A2	2,000,000	2,000,000	T/Deposit	25/11/2019	23/10/2020	1.50%	
IMB Ltd	S&P ST A2	1,000,000	1,000,000	T/Deposit	28/11/2019	29/10/2020	1.50%	
National Australia Bank Ltd	S&P ST A1+	1,030,000	1,030,000	T/Deposit	19/11/2019	18/11/2020	1.46%	
IMB Ltd	S&P ST A2	2,000,000	2,000,000	T/Deposit	28/11/2019	26/11/2020	1.50%	
Bank of Queensland Ltd	Moody's A3	3,000,000	3,000,000	T/Deposit	14/03/2019	14/12/2020	2.73%	
IMB Ltd	S&P ST A2	3,000,000	3,000,000	T/Deposit	20/12/2019	18/12/2020	1.50%	
Westpac Banking Corporation Ltd	S&P AA-	5,000,000	5,000,000	T/Deposit	23/12/2019	23/12/2020	1.48%	
Members Equity Bank Ltd	S&P ST A2	3,000,000	3,000,000	T/Deposit	6/03/2020	31/12/2020	1.25%	
Members Equity Bank Ltd	S&P ST A2	2,000,000	2,000,000	T/Deposit	25/09/2020	25/01/2021	0.65%	
Bank of Queensland Ltd	Moody's A3	3,000,000	3,000,000	T/Deposit	21/02/2019	19/02/2021	2.80%	
Bendigo & Adelaide Bank Ltd	Moody's A3	2,000,000	2,000,000	T/Deposit	28/11/2019	24/02/2021	1.55%	
Bank of Queensland Ltd	Moody's ST P-2	2,000,000	2,000,000	T/Deposit	6/03/2020	1/03/2021	1.40%	
Westpac Banking Corporation Ltd	S&P AA-	2,000,000	2,000,000	T/Deposit	6/03/2020	8/03/2021	0.99%	
Bendigo & Adelaide Bank Ltd	Moody's A3	2,000,000	2,000,000	T/Deposit	28/11/2019	25/06/2021	1.55%	
Commonwealth Bank of Australia Ltd	S&P ST A1+	4,000,000	4,000,000	T/Deposit	25/09/2020	22/07/2021	0.66%	
Bank of Queensland Ltd	Moody's A3	2,000,000	2,000,000	T/Deposit	25/09/2020	25/11/2021	0.80%	
Westpac Banking Corporation Ltd	S&P AA-	2,000,000	2,000,000	T/Deposit	2/12/2019	2/12/2021	0.97%	
Bank of Queensland Ltd	Moody's A3	2,000,000	2,000,000	T/Deposit	25/09/2020	25/01/2022	0.80%	
Westpac Banking Corporation Ltd	S&P AA-	2,000,000	2,000,000	T/Deposit	6/03/2019	6/03/2024	2.83%	
Total			73,483,539					

WOLLONGONG CITY COUNCIL STATEMENT OF INVESTMENT 25 September 2020 continued								
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Bond and Floating Rate Note Securities

DIRECT INVESTMENTS								
Investment Body	Rating	Purchase Price \$	Fair Value of Holding \$	Security	Purchase Date	Maturity Date	Interest / Coupon Rate	
Suncorp Bank	S&P A+	1,500,000	1,504,815	FRN	20/10/2015	20/10/2020	1.35%	
National Australia Bank Ltd	S&P AA-	1,000,000	1,002,760	FRN	5/11/2015	5/11/2020	1.18%	
Newcastle Permanent Building Society Ltd	S&P BBB	500,000	502,030	FRN	26/02/2019	26/02/2021	1.20%	
Suncorp Bank	S&P A+	2,000,000	2,018,340	FRN	12/04/2016	12/04/2021	1.49%	
AMP Bank Ltd	S&P BBB+	2,000,000	2,008,060	FRN	24/05/2016	24/05/2021	1.46%	
Westpac Banking Corporation Ltd	S&P AA-	3,000,000	3,023,790	FRN	3/06/2016	3/06/2021	1.26%	
ANZ Banking Group Ltd	S&P AA-	2,000,000	2,020,140	FRN	16/08/2016	16/08/2021	1.23%	
Credit Union Australia Ltd	S&P BBB	1,200,000	1,210,020	FRN	6/09/2018	6/09/2021	1.34%	
AMP Bank Ltd	S&P BBB+	1,500,000	1,501,890	FRN	10/09/2018	10/09/2021	1.17%	
Westpac Banking Corporation Ltd	S&P AA-	1,500,000	1,511,175	FRN	16/11/2018	25/10/2021	0.83%	
Credit Union Australia Ltd	Moody's Baa1	1,000,000	1,011,100	FRN	4/03/2019	4/03/2022	1.32%	
AMP Bank Ltd	S&P BBB+	3,000,000	2,997,870	FRN	30/03/2017	30/03/2022	1.15%	
Suncorp Bank	S&P A+	1,500,000	1,518,405	FRN	30/08/2017	16/08/2022	1.07%	
Bendigo & Adelaide Bank Ltd	Fitch A-	3,000,000	3,024,990	FRN	12/12/2019	12/09/2022	0.94%	
Bank Australia Limited	S&P BBB	4,000,000	3,988,720	FRN	2/12/2019	2/12/2022	0.99%	
ANZ Banking Group Ltd	S&P AA-	1,000,000	1,016,400	FRN	9/05/2018	9/05/2023	1.00%	
National Australia Bank Ltd	S&P AA-	3,000,000	3,056,910	FRN	26/09/2018	26/09/2023	1.03%	
Westpac Banking Corporation Ltd	S&P AA-	1,500,000	1,528,350	FRN	16/11/2018	16/11/2023	1.05%	
ANZ Banking Group Ltd	S&P AA-	2,000,000	2,043,000	FRN	6/12/2018	6/12/2023	1.12%	
National Australia Bank Ltd	S&P AA-	2,000,000	2,033,480	FRN	19/06/2019	19/06/2024	1.01%	
Macquarie Bank	Moody's A2	2,000,000	2,021,200	FRN	12/02/2020	12/02/2025	0.94%	
Emerald Reverse Mortgage Trust	S&P AAA	516,521	413,491	MBS	17/07/2006	21/08/2051	0.55%	
Emerald Reverse Mortgage Trust	Fitch AA	2,000,000	1,301,620	MBS	17/07/2006	21/08/2056	0.85%	
Total			42,258,556					

Managed Funds

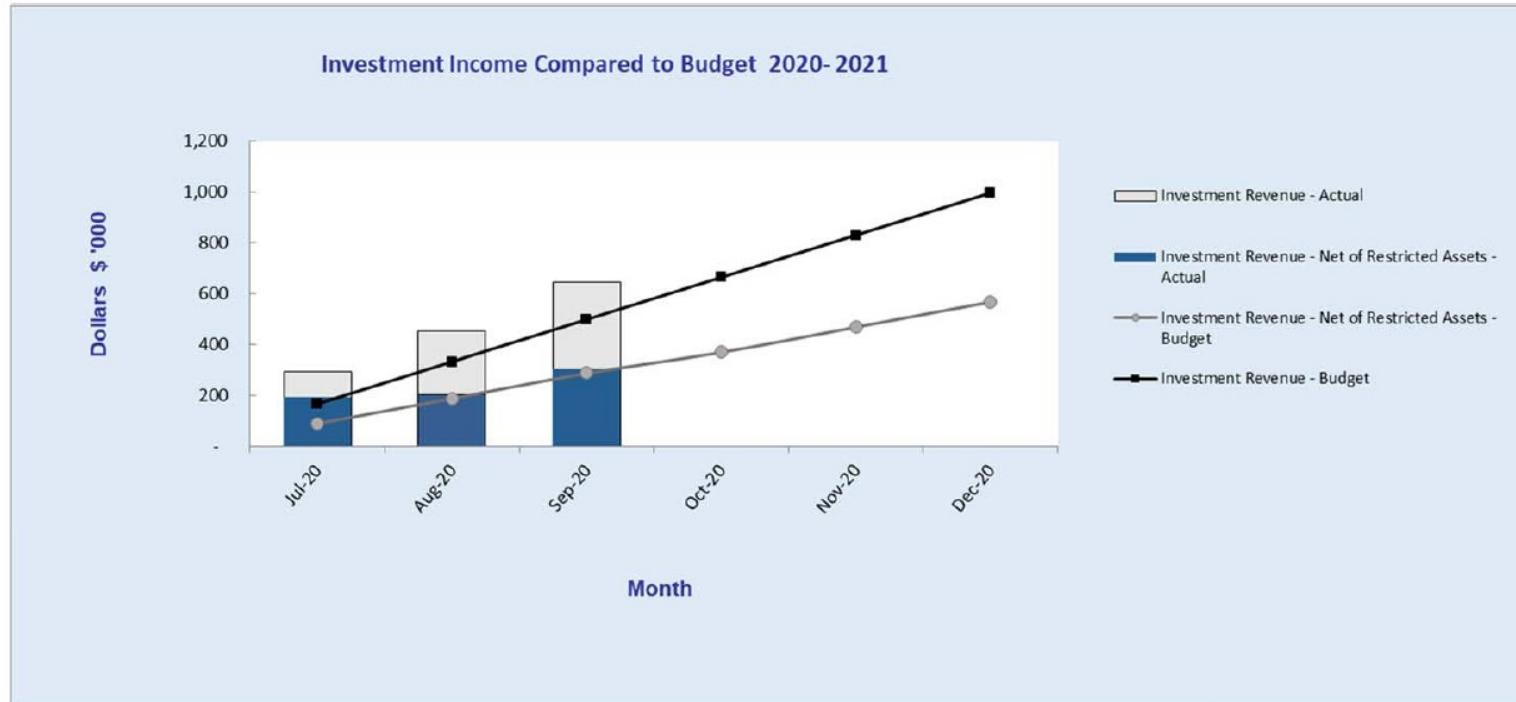
MANAGED FUNDS							
Investment Body	Rating	Purchase Price \$	Fair Value of Holding \$	Purchase Date	Monthly	FYTD (Actual)	
Tcorp M Cash Fund Facility	N/A	27,664,747	27,664,747	28/06/2019	0.04%	0.16%	
Tcorp Long Term Growth Facility Trust	N/A	1,773,197	2,369,630	13/06/2007	-0.27%	3.34%	
Total			30,034,376				

TOTAL INVESTMENTS \$ 145,776,471

* The maturity date provided is the weighted-average life of the security. This is the average amount of time that will elapse from the date of security's issuance until each dollar is repaid based on an actuarial assessment. Assessments are carried out on a regular basis which can potentially extend the life of the investment. Current assessments anticipate an extension of life of the investment.

This is to certify that all of the above investments have been placed in accordance with the Act, the regulations and Council's Investment Policies.

Brian Jenkins
RESPONSIBLE ACCOUNTING OFFICER



ITEM 18

ANNUAL TABLING OF RETURNS OF DISCLOSURE OF PECUNIARY INTERESTS AND OTHER MATTERS - 2019 TO 2020

The *Model Code of Conduct for Local Councils in NSW (the Model Code)* requires the General Manager to table all Annual Returns of Disclosure of Pecuniary Interests and Other Matters (the return) lodged by the prescribed due date of 30 September 2020 at the first Council meeting after that date. This report is submitted in addition to the bi-monthly report which tables Primary and Interim Returns.

RECOMMENDATION

Council note the tabling of the Annual Returns of Disclosure of Pecuniary Interests and Other Matters as required by Clause 4.26 of the Model Code of Conduct for Local Councils in NSW.

REPORT AUTHORISATIONS

Report of: Todd Hopwood, Manager Governance and Customer Service
Authorised by: Renee Campbell, Director Corporate Services - Connected + Engaged City

ATTACHMENTS

- 1 Register of Disclosures of Pecuniary Interests and Other Matters (to be tabled)

BACKGROUND

Council Officers forwarded the Office of Local Government guidelines and designated forms for the completion of returns to all relevant Councillors, staff and designated persons outlining their responsibilities to complete the return for the period 1 July 2019 to 30 June 2020 in order to comply with the provisions of the *Model Code*.

All designated persons must lodge their return with the General Manager by 30 September each year. Council has identified 95 designated persons and as at the date of this report all returns have been submitted. A register of these returns will be tabled during the meeting in the Council Chambers.

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal under the objective 4.3 *our local Council has the trust of the community*.

It specifically delivers on core business activities as detailed in the Governance and Community Services, Service Plan 2020-21.

ITEM 19

CITY OF WOLLONGONG TRAFFIC COMMITTEE MINUTES OF MEETING HELD ON 30 SEPTEMBER 2020

The City of Wollongong Traffic Committee meeting was held on 30 September 2020. Items list in 2 and 4 are to be adopted by Council through delegated authority. The items listed in Section 3 of the minutes are the object of this report and must be determined by Council. They are recommended to Council for approval for temporary Regulation of Traffic on public roads for works or events by independent parties.

RECOMMENDATION

In accordance with powers delegated to Council, the Minutes and Recommendation related to Regulation of Traffic, of the City of Wollongong Traffic Committee held on 30 September 2020, be adopted.

REPORT AUTHORISATIONS

Report of: Andrew Heaven, Manager Infrastructure Strategy and Planning (Acting)
Authorised by: Andrew Carfield, Director Infrastructure + Works - Connectivity Assets + Liveable City

ATTACHMENTS

- 1 Tower Crane Plan - Simpson Place Kembla and Crown Streets Wollongong

BACKGROUND

1. WOLLONGONG Simpson Place, Kembla and Crown Streets – Ward 2 (Item 3.1 of Wollongong Traffic Committee Minutes of Meeting)

Tower Crane – Simpson Place, Kembla and Crown Street - Road Closures
on a suitable night between Sunday 1 November and Wednesday 16 December 2020.

Background

A road closure is required to install a tower crane for the development of a building at the Langs Corner site located at the corner of Kembla and Crown Streets, Wollongong. The builder has proposed that for the assembly of their tower crane Kembla Street and Simpson Place will need to be closed for a 12-hour shift between 6pm and 6am on a night between Sunday and Wednesday in the period 1 November to 16 December 2020.

Traffic in this area can be diverted via Crown, Stewart and Corrimal Streets and any vehicles with a destination in Simpson Place will be escorted in and out via Burelli Street. However, the builder will need approval from Transport for NSW as the closures affect the two nearby sets of traffic signals.

Transport for NSW signals staff have asked that the detour to the right for northbound traffic on Kembla Street be relocated to the intersection of Stewart and Kembla Streets. There is an existing No Right Turn into Burelli Street from Kembla Street, which needs to be maintained during the closure. The detour sign at this point should be "All Traffic Left". An amended plan will be obtained to reflect this requirement.

Consultation

Consultation with affected businesses is a condition of this approval.

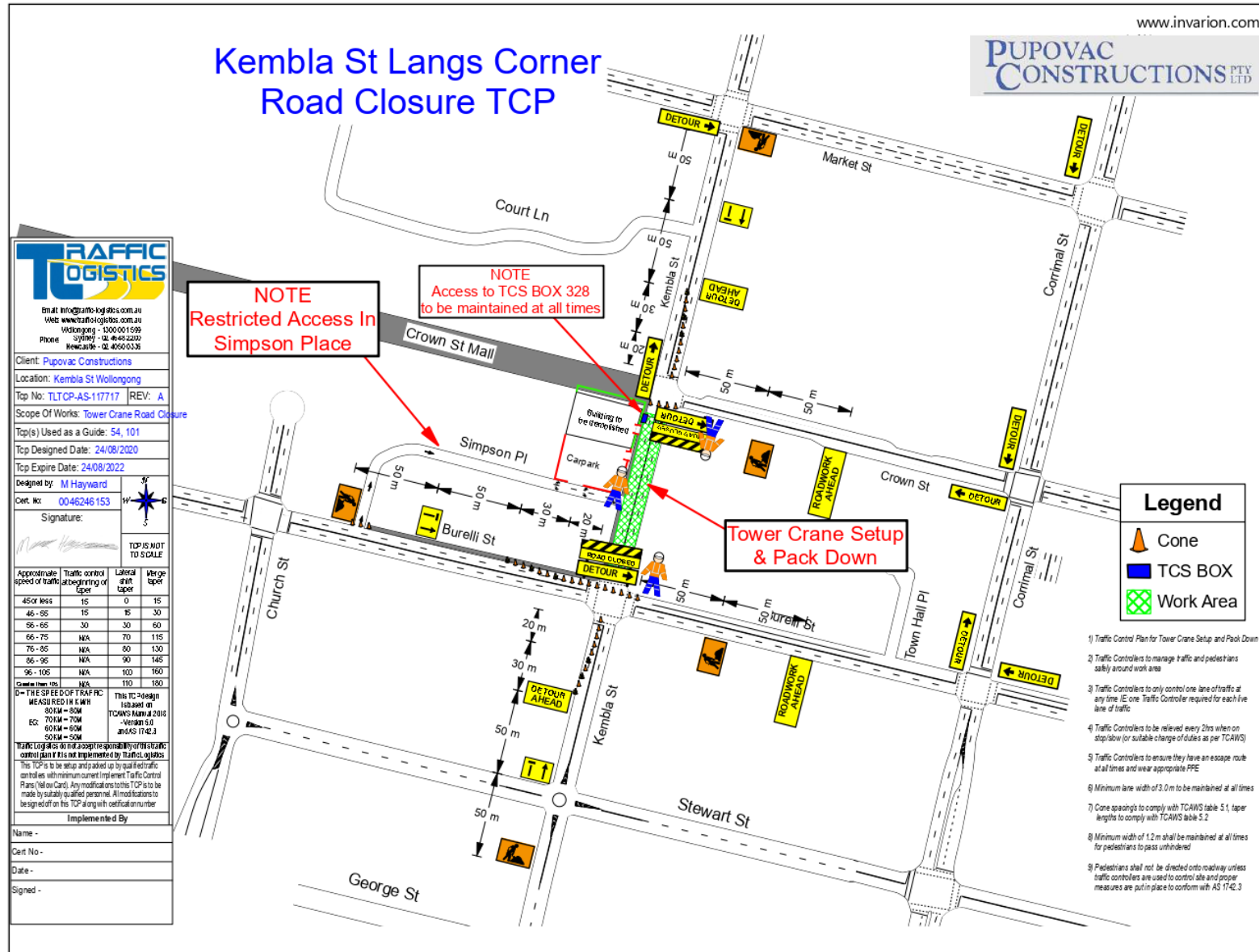
Committee's Recommendation

A road closure of Kembla Street and Simpson Place be approved for a 12 hour shift between 6pm and 6am on a night between Sunday and Wednesday for the period 1 November to 16 December 2020, subject to the amended traffic management plans, which shows the detour relocated to the east (right) on the Stewart and Kembla Street intersections rather than at Kembla and Burelli Street, and approval from Transport for NSW.

Proposal Supported Unanimously

PLANNING AND POLICY IMPACT

This report contributes to the delivery of Our Wollongong 2028 goal “We have affordable and accessible transport”. It specifically delivers on the Service Plan 2020-21.



ITEM 20 NOTICE OF MOTION - COUNCILLOR COLACINO - OVERHEAD ELECTRONIC SIGNAGE

Councillor Colacino has submitted the following Notice of Motion –

“I formally move that -

- 1 Council immediately discuss with TforNSW the possibility, and need for, making use of their overhead electronic signage, along the main roads leading into and throughout the Wollongong area, to announce the current parking and driving situation of the beaches of, but not limited to, the Northern suburbs of Wollongong. The use of these electronic signs should cover the following scenarios -
 - a Alerting visitors to the region to periods when car parks have reached their capacity
 - b Alerting drivers to traffic congestion along Lawrence Hargrave Drive during peak usage events
 - c Providing expected travel times and expected lengths of delay.”

Background provided by Councillor Colacino:

We are all aware of the congestion that occurs along Lawrence Hargrave Drive. The use of these signs is an ideal way to alert visitors and drivers to our region of these delays. The current frustration caused by the COVID-19 restrictions have made the finding of a solution to alleviate this situation more immediate.

Lawrence Hargrave Drive has been under immense pressure over the past years and the increase population growth in South and South Western Sydney is magnifying the situation. This population growth is putting extreme pressure on the amenity of the entire Wollongong region.

Actions need to be put in place right now to create usage habits that will be able to work into the future.

ITEM 21

NOTICE OF MOTION - COUNCILLOR TANIA BROWN - WOLLONGONG FORESHORE TRAFFIC MANAGEMENT

Councillor Tania Brown has submitted the following Notice of Motion –

“Recognising the work already undertaken by Wollongong City Council, in the yet to be released ‘Wollongong Foreshore Parking Strategy’, I formally move that –

- 1 Staff review this impending strategy and advise whether there are any pop-up parking opportunities that could be brought forward to alleviate pressure on the foreshore precincts of the city over Summer 2020/21, noting the particularly high traffic volume areas of Austinmer, Bulli, Thirroul, Stanwell Park, Port Kembla and the city precinct comprising Stuart Park, North Wollongong Beach, City Beach and Wollongong Harbour.
- 2 Given that Council contributes financially to the Gong Shuttle, that we increase promotion of this service as a convenient way to avoid parking congestion and gain easy access to the city beaches.
- 3 Staff investigate the ability to quarantine a small number of parking spaces for Surf Life Saving volunteers, close to the two city surf clubs, to enable our volunteers ready access to the Club facilities.”

Background provided by Councillor Tania Brown:

Following COVID-19 and the restrictions on international travel, we have seen a surge in visitors to our prime foreshore locations, and it is anticipated that this will continue into the peak summer period, despite social distancing requirements. Historically we have seen this has had an impact on parking availability and caused traffic congestion in high volume areas.

Wollongong City Council has been working on a foreshore parking strategy that is due to be completed by December 2020.

The increased parking and traffic congestion has impacted the two central surf clubs with volunteers late for duty due to their inability to secure parking within a reasonable distance.

ITEM 22

NOTICE OF MOTION - COUNCILLOR CATH BLAKEY - MINE EXPANSION IN THE CATARACT DRINKING WATER CATCHMENT

Councillor Cath Blakey has submitted the following Notice of Motion –

“I formally move that Council -

- 1 Write to the Independent Planning Commission to object to the Russell Vale Underground Expansion Project on the basis of community concerns of increased heavy vehicle traffic, dust, noise, threat to the drinking water catchment, Aboriginal heritage sites and greenhouse gas emissions, as well as Wollongong Coal’s poor track record on workplace safety, debt servicing and operating compliance. Council also requests that if the project is approved then the conditions should require:
 - a the completion of infrastructure at Russell Vale Colliery to protect residents from particulate and noise pollution before any coal is extracted
 - b the fulfilment of outstanding consent conditions of the 1989 and 1990 development approvals for Russell Vale mine operators, including the payment of security bond to Wollongong Council, the remediation of the Russell Vale Emplacement Area to an agreed final landform and the dedication of the parcel of golf course land to Council.
- 2 Write to the Planning Minister to seek reform of the Environmental Planning and Assessment Act to enable a Fit and Proper test of a proponent to be encompassed in the assessment process for state significant developments.”

Background provided by Councillor Blakey:

The proposed Russell Vale Colliery Underground Expansion Project is an extension application for bord-and-pillar extraction within the Cataract Reservoir catchment. If approved, the project will also activate permission to longwall mine the remaining 25 metres of longwall 6. The project aims to extract 3.7 million tonnes of run-of-mine coal over a five-year period from the Wongawilli Seam beneath the Bulli and Balgownie seams which have already been extracted. Approximately 50% of the material to be extracted is high quality metallurgical coal, 25% is thermal coal and the remaining 25% is other rock. The metallurgical and thermal coal is destined for over-seas markets, being sold by the proponent, Wollongong Coal, to their parent company, Jindal Steel and Power Ltd, India.

The Russell Vale Expansion Proposal is identified as -

- Generating noise and dust from the new coal processing plant and 2 new coal stockpiles very close to residents
- Adding 34 truck movements per hour between the mine and Port Kembla coal terminal
- Requiring a license to release mine-affected water into Bellambi Gully Creek with exceedance of the ANZG (2018) default guideline values for freshwater aquatic ecosystems (95% species protection level) for copper, zinc and nickel
- Occurring under significant Aboriginal Heritage sites with rock shelter with art, grinding groves along water courses and camp sites
- Generating 304,600 tonnes of CO₂-e greenhouse gas emissions
- Blocking public access to the Lower Escarpment Fire Trail adjacent to Brokers Nose.

The mine operates underneath the catchment of the Cataract Reservoir. This is located within the Metropolitan Special Area, a restricted-access area designated to protect Sydney and Illawarra’s drinking-water catchments.

The project is on the Woronora Plateau which supports groundwater-dependent ecosystems such as Coastal Upland Swamps in the Sydney Basin Bioregion. These swamps are listed as Endangered Ecological Communities under the Environment Protection and Biodiversity Conservation Act 1999 and the New South Wales Threatened Species Conservation Act 1995. Where upland swamps are impacted by subsidence, they dry out and increase the bushfire fuel load in the Illawarra Escarpment.

The NSW Department of Planning, Industry and Environment referral states the project is “approvable, subject to strict conditions of consent”. The Independent Planning Commission is currently considering the matter with the deadline for written submission being 5pm Tuesday 27 October 2020. The proponent, Wollongong Coal, has a history of non-compliance when it comes to the conditions of consent. Wollongong Coal has been fined and issued with orders by multiple agencies, regulators and courts for offences including polluting Bellambi Gully Creek, “poor maintenance and operation” of infrastructure, failing to publicly disclose water monitoring data, failing to hold community consultative meetings and for stockpiling 200,000 tonnes of waste coal on Council land in breach of its development consent at Russell Vale. On 8 November 2017, a conviction was recorded in the Downing Centre Local Court for the failure of Wollongong Coal to pay annual rental fees and administrative levies under section 292C(3) of the Mining Act. In March 2018, the NSW EPA stated that “in recent years” Wollongong Coal “has demonstrated they cannot consistently manage and maintain pollution control equipment and plant on site”.

In March 2018 the NSW Resources Regulator shut down the proponent’s Wongawilli operation due to a serious roof collapse leading to a determination that workplace safety issues were too serious for underground work to continue. In dealings with Wollongong City Council, Wollongong Coal has also a long-standing practice of obfuscation in relation to land dedication of the Russell Vale Golf Course, security bonds, remediation of the emplacement area and creek realignment.

Wollongong Coal currently has no income and debts which exceed its current assets by more than a billion dollars (AUD\$1,089,243,000). Current auditors UHY Haines Norton noted in March 2020 that “a material uncertainty exists that may cast significant doubt on the Group’s ability to continue as a going concern and therefore, the Group may be unable to realise its assets and discharge its liabilities in the normal course of business.” In August 2020 Wollongong Coal delisted from the Australian Stock Exchange. Wollongong Coal was first suspended from trade on the Australian Stock Exchange in 2017 when it failed to lodge its half-yearly report. Wollongong Coal estimates rehabilitation of its Russell Vale mine would cost \$215 million. The NSW Government hold a bond of just \$12.4 million for the mine site. Since 2013 Wollongong Coal has paid \$0 corporate tax.

In India, the courts have determined that there is sufficient evidence for a criminal corruption case to be brought to trial against the majority owner of Wollongong Coal, Jindal Steel and Power (JSPL) and its chairman Naveen Jindal. In July 2019, Naveen Jindal and four other JSPL officials were charged under sections 420 (cheating) and 120-B (criminal conspiracy) of the Indian Penal Code. All have pleaded not guilty. JSPL, via a holding company, JSPL Mauritius, is majority shareholder in Wollongong Coal Ltd. Then Minister for Resources, the Honourable Don Harwin, described JSPL’s problems in India in response to a question in the NSW Parliament on 1 June 2017: “On 29 April it was announced that Jindal Steel and Power Limited, and one of its directors had been investigated by India’s Central Bureau of Investigation on potential criminal corruption charges. The courts in India decided that there was sufficient evidence for a case to be brought to trial. I am advised that no conviction of criminal corruption has been made at this stage.”

In the NSW Department of Planning, Industry and Environment final assessment report on the Russell Vale Expansion Project it states that a “fit and proper test is not a requirement under the EP&A Act and is an irrelevant consideration for a consent authority when making a determination on a development application”.

The “Fit And Proper” test is within the Protection of the Environment Operations (POEO) Act which governs the Environmental Protection Agency (EPA) and awards pollution licenses, and the Mining Act through which the NSW Resource Regulator awards mining leases. However, these agencies have

rarely denied an operating license or lease on the Fit and Proper test. To do so would likely be contested through the courts.

The NSW Resources Regulator regulates the mining leases on the basis that the right to mine coal that belongs to the people of NSW is a “special privilege” which our community expects the government to regulate to “prevent harm to the community, other industries and the environment”. The regulator’s ‘Fit And Proper Person Policy’ requires that the right of exploration and mining is “only conducted by companies that can and will respect that privilege”. The NSW Resources Regulator commenced investigation in mid-2016 over whether Wollongong Coal is a “fit and proper person” to hold a mining licence. In July 2020, the Illawarra Mercury reported that the state’s mining regulator had “quietly shelved” this investigation, saying there was “insufficient evidence to support a finding that the company is not a fit and proper person”. It appears that the NSW Resources Regulator - under the NSW Mining Act - may choose not to escalate action against Wollongong Coal unless convictions are recorded in India against significant individuals within JSPL.

A review of the evidence already in the public domain causes great community concern over whether Wollongong Coal should be granted further rights to mine in NSW.